



CITY OF BEAVERTON  
 DEVELOPMENT SERVICES DIVISION  
 4755 S.W. GRIFFITH DRIVE  
 P.O. BOX 4755  
 BEAVERTON, OR 97076  
 PHONE: (503) 526-2420

# APPLICATION FOR DEVELOPMENT REVIEW

EXHIBIT 3.C

FILE NAME: Beaverton High School  
Historic Trees

TYPE: TPP

FILE NUMBERS: TPP2001-0008

SUBMIT: 12/21/01 REVIEW: JK

RESUBMIT: \_\_\_\_\_ REVIEW: \_\_\_\_\_

APPLICATION MATERIAL RECEIVED: \_\_\_\_\_

FEE RECEIVED: \_\_\_\_\_ FEE PAID: \$ \_\_\_\_\_

CHECK #: \_\_\_\_\_ RECEIPT #: \_\_\_\_\_

COMPLETENESS DATE: \_\_\_\_\_

PRINT OR TYPE ALL INFORMATION BELOW

APPLICANT  USE MAILING ADDRESS FOR MEETING NOTIFICATION

YOUR NAME: Frank Angelo PHONE #: 503-227-3664

BUSINESS NAME: Angelo Eaton & Assoc. FAX #: 503-227-3679

ADDRESS: 620 SW Main Suite 201 SUITE: \_\_\_\_\_

CITY, STATE: Portland, Oregon ZIP: 97205

SIGNATURE (ORIGINAL REQUIRED): [Signature] DATE: 12/18/01

PROPERTY/ZONING DATA

13000 SW 2nd  
Address

Beaverton High School  
Location Description

R-10

CONSTRUCTION EXPECTED TO BEGIN: \_\_\_\_\_

MAP & TAX LOT (list one per line)	DO NOT USE LOT & BLOCK	SITE ACRES	ZONING	
			EXISTING	PROPOSED
<u>15116AD - 11100</u>		<u>6.15</u>	<u>R10</u>	<u>R10</u>
<u>15116AD - 11000</u>		<u>.13</u>	<u>R10</u>	<u>R10</u>
<u>15116AD - 10900</u>		<u>11.41</u>	<u>R10</u>	<u>R10</u>
-				
-				
TOTAL GROSS SITE:		ACRES <u>17.69</u>	SQ. FT. <u>770,576.40</u>	
TOTAL NET SITE (GROSS SITE MINUS DRIVEWAYS & R.O.W.)				

PROPERTY OWNER  Attach separate sheet if more than one owner

YOUR NAME: Mike Maloney PHONE #: 503-591-4303

BUSINESS NAME: Beaverton School Dist. FAX #: 503-591-4484

ADDRESS: 16550 SW Merlo Rd SUITE: \_\_\_\_\_

CITY, STATE: Beaverton OR ZIP: 97006-5152

SIGNATURE (ORIGINAL REQUIRED): [Signature] DATE: 12/19/01

COMMERCIAL/INDUSTRIAL DEVELOPMENT

Public High School  
Use or Development Description

BUILDING USE PROPOSED USES (LIST ONE PER LINE)	BLDG. AREA		PARKING # SPACES REQ/PROP.
	GROSS SQ. FT.	% OF BLDG.	
<u>Cafeteria</u>			<u>/</u>
<u>Parking Lot</u>			<u>/</u>
			<u>/</u>
			<u>/</u>
TOTAL BUILDING AREA:		<u>100%</u>	<u>/</u>

ARCHITECT/DESIGNER

YOUR NAME: Russ Pitkin/Eric Nielsen PHONE #: 503-274-5432

BUSINESS NAME: LSW Architecture FAX #: 503-274-0085

ADDRESS: 1953 NW Kearny SUITE: \_\_\_\_\_

CITY, STATE: Portland OR ZIP: 97209

SUBDIVISION/LAND PARTITION

# LOTS PROPOSED: \_\_\_\_\_

PARTITION LOT SIZES IN SQUARE FEET

A	B	C
---	---	---

LANDSCAPE ARCHITECT

YOUR NAME: Ron Matela PHONE #: 360-695-0776

BUSINESS NAME: Matela Associates FAX #: 360-695-0776

ADDRESS: 601 Main St #201 SUITE: \_\_\_\_\_

CITY, STATE: Vancouver WA ZIP: 98660

MULTI-FAMILY/P.U.D. DEVELOPMENT

TOTAL BUILDING SPACE	GR. SQ. FT. / % SITE	# UNITS	
SITE AREA PER UNIT	/	PARKING SPACES	
TOTAL OPEN SPACE	/	# COVERED	
OPEN SPACE PER UNIT	/	# UNCOVERED	
BUILDING COVERAGE	/	TOTAL SPACES	
DRIVEWAY AREA	/	RV STORAGE	YES <input type="checkbox"/> NO <input type="checkbox"/>
PARKING AREA	/	# SPACES	

MAXIMUM BLDG. HEIGHT: \_\_\_\_\_ FEET/STORIES

UNITS PER BEDROOM NO: \_\_\_\_\_ S \_\_\_\_\_ 1 \_\_\_\_\_ 2 \_\_\_\_\_ 3 \_\_\_\_\_

ATTACHED UNITS: RENTAL  CONDO  LOT OWNERSHIP

ENGINEER/SURVEYOR

YOUR NAME: Gary Alfson PHONE #: 503-221-1131

BUSINESS NAME: Harper Houf Righellis FAX #: 503-221-1171

ADDRESS: 5200 SW Macadam Suite 580 SUITE: \_\_\_\_\_

CITY, STATE: Portland OR ZIP: 97201

NOTE: ACCEPTABLE SUBMITTAL  
 MAXIMUM SHEET SIZE 24 X 36  
 FOLDED TO FIT LEGAL SIZE  
 ASSEMBLED INTO SETS



CITY OF BEAVERTON  
DEVELOPMENT SERVICES DIVISION  
4755 S.W. GRIFFITH DRIVE  
P.O. BOX 4755  
BEAVERTON, OR 97076  
PHONE: (503) 526-2420

# APPLICATION FOR DEVELOPMENT REVIEW

FILE NAME: <u>Beaverton High School Cafeteria &amp; Pkg lots</u>	SUBMIT: <u>12/11/01</u>	REVIEW: <u>JK</u>
TYPE: <u>BDR-3</u>	RESUBMIT: _____	REVIEW: _____
FILE NUMBERS: <u>BDR2001-0213</u>	APPLICATION MATERIAL RECEIVED: _____	
	FEE RECEIVED: _____	FEE PAID: \$ _____
	CHECK #: _____	RECEIPT #: _____
	COMPLETENESS DATE: _____	

PRINT OR TYPE ALL INFORMATION BELOW

APPLICANT  USE MAILING ADDRESS FOR MEETING NOTIFICATION

YOUR NAME	PHONE #
Frank Angelo	503-227-3664
BUSINESS NAME	FAX #
Angelo Eaton & Assoc.	503-227-3679
ADDRESS	SUITE
620 SW Main Suite 201	
CITY, STATE	ZIP
Portland, Oregon	97205
SIGNATURE (ORIGINAL REQUIRED)	DATE
<i>Frank Angelo</i>	12/10/01

### PROPERTY/ZONING DATA

13000 SW 2nd  
Address

Beaverton High School  
Location Description

R-10

CONSTRUCTION EXPECTED TO BEGIN: \_\_\_\_\_

MAP & TAX LOT (list one per line)	DO NOT USE LOT & BLOCK	SITE ACRES	ZONING	
			EXISTING	PROPOSED
15116AD - 11100		6.15	R10	R10
15116AD - 11000		.13	R10	R10
15116AD - 10900		11.41	R10	R10
-				
-				
TOTAL GROSS SITE:		ACRES	SQ. FT.	
		17.69	770,576.40	
TOTAL NET SITE: (GROSS SITE MINUS DRIVEWAYS & ROW)				

PROPERTY OWNER  Attach separate sheet if more than one owner

YOUR NAME	PHONE #
Mike Maloney	503-591-4303
BUSINESS NAME	FAX #
Beaverton School Dist.	503-591-4484
ADDRESS	SUITE
16550 SW Merlo Rd	
CITY, STATE	ZIP
Beaverton OR	97006-5152
SIGNATURE (ORIGINAL REQUIRED)	DATE
<i>Mike Maloney</i>	12/19/01

### COMMERCIAL/INDUSTRIAL DEVELOPMENT

Public High School  
Use or Development Description

ARCHITECT/DESIGNER

YOUR NAME	PHONE #
Russ Pitkin/Eric Nielsen	503-274-5432
BUSINESS NAME	FAX #
LSW Architecture	503-274-0085
ADDRESS	SUITE
1953 NW Kearny	
CITY, STATE	ZIP
Portland OR	97209

BUILDING USE PROPOSED USES (LIST ONE PER LINE)	BLDG. AREA		PARKING # SPACES REOPROP.
	GROSS SQ. FT.	% OF BLDG.	
Cafeteria			/
Parking Lot			/
			/
			/
TOTAL BUILDING AREA:		100%	/

LANDSCAPE ARCHITECT

YOUR NAME	PHONE #
Ron Matela	360-695-0776
BUSINESS NAME	FAX #
Matela Associates	360-695-0776
ADDRESS	SUITE
601 Main St #201	
CITY, STATE	ZIP
Vancouver WA	98660

### SUBDIVISION/LAND PARTITION

# LOTS PROPOSED	PARTITION LOT SIZES IN SQUARE FEET:		
	A	B	C

ENGINEER/SURVEYOR

YOUR NAME	PHONE #
Gary Alfson	503-221-1131
BUSINESS NAME	FAX #
Harper Houf Righellis	503-221-1171
ADDRESS	SUITE
5200 SW Macadam Suite 580	
CITY, STATE	ZIP
Portland OR	97201

### MULTI-FAMILY/P.U.D. DEVELOPMENT

TOTAL BUILDING SPACE	GR. SQ. FT./% SITE	# UNITS	
SITE AREA PER UNIT	/	PARKING SPACES	
TOTAL OPEN SPACE	/	# COVERED	
OPEN SPACE PER UNIT	/	# UNCOVERED	
BUILDING COVERAGE	/	TOTAL SPACES	
DRIVEWAY AREA	/	RV STORAGE	YES <input type="checkbox"/> NO <input type="checkbox"/>
PARKING AREA	/	# SPACES	
MAXIMUM BLDG. HEIGHT	FEET/STORIES		
	/		
UNITS PER BEDROOM NO.		S	1
ATTACHED UNITS		2	3
		RENTAL <input type="checkbox"/>	CONDO <input type="checkbox"/>
		LOT OWNERSHIP <input type="checkbox"/>	

NOTE: ACCEPTABLE SUBMITTAL  
MAXIMUM SHEET SIZE 24 X 36  
FOLDED TO FIT LEGAL SIZE  
ASSEMBLED INTO SETS



# FILE COPY

City of Beaverton, OR

4755 SW GRIFFITH DR  
BEAVERTON, OR 97076

02/15/2002  
3:11:45PM

Permit Number

**TPP2001-0008**

Receipt Number

20020735

Paid By BEAVERTON SCHOOL DISTRICT

Contractor

Site Address 13200 SW 2ND

Description BEAVERTON HIGH SCHOOL HISTORIC TREES

Paid Date 02/15/2002

Pay Method Check

Check Number 268113

<b>FEES</b>	<b>Amount</b>	<b>Account</b>	<b>FEES</b>	<b>Amount</b>	<b>Account</b>
Development Services	600.00	001-03-20-331			

**Total Fees** 600.00



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## DECISION FINAL

May 28, 2002

To whom it may concern:

**RE: TPP 2001-0008 – BEAVERTON HIGH SCHOOL CAFETERIA AND  
PARKING LOT EXPANSION TREE PRESERVATION PLAN**

Please be advised that the Historic Resource Review Committee decision April 30, 2002, on the above referenced application was not appealed, therefore, the decision is final.

If you have any questions, please contact me at 526-2427.

Sincerely,

Steven A. Sparks, AICP  
Development Services Manager

C: Frank Angelo  
Russ Pitkin/Eric Nielsen  
Gary Alfon  
David Cory  
Jim Duggan  
Sue Nelson  
Jan Youngquist  
Project File

Mike Maloney  
Ron Matela  
Casey Schleich  
Vlad Voytilla  
Brad Roast  
Phil Healy  
Bill Avery



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

SK

## NOTICE OF DECISION

May 14, 2002

To Whom It May Concern:

Attached, please find a copy of the approved Land Use Order finalizing the Historic Resource Review Committee's action on **TPP 2001-0008 – BEAVERTON HIGH SCHOOL CAFETERIA AND PARKING LOT EXPANSION TREE PRESERVATION PLAN.**

An applicant or aggrieved person may appeal the order of the Historic Resource Review Committee by delivering written notice to the City Recorder by **5:00 p.m. Friday, May 24, 2002.** In order to file an appeal, the notice of appeal must contain the following pursuant to Section 50.40.2.B of the Development Code:

1. A reference to the matter sought to be reviewed and the date of the Historic Resource Review Committee order.
2. If the appellant is not the applicant, a statement of why the appellant is an "aggrieved person".
3. The specific grounds including ordinance and section numbers relied on for the appeal. To be effective the appeal must relate to the specific criteria on which the decision was based. If the notice of appeal is filed prior to the receipt by the appellant of the Committee's written order, the notice shall state this fact and indicate the notice of appeal will be supplemented with a statement of the specific grounds relied upon within 15 days of the date of the Historic Resource Review Committee order. The Director or City Recorder, at the direction of the City Attorney, may reject the request if it does not meet the requirements of this ordinance. In such instances, the City Attorney shall provide written notice that the request for review has been rejected within 10 days of receipt of the request for review.

A filing fee of \$580.00 is due at the time the appeal is filed. The fee amount depends upon the action being appealed and the number of appeals being filed. Furthermore, if the appeal is requested to be on the record, a fee to cover the cost of preparing a transcript of the Historic Resource Review Committee meeting(s) shall be paid within five (5) working days after the Planning Director estimates the cost of the transcript. Within ten (10) days of the notice of completion of the transcript, the appellant shall remit the balance due on the cost of the transcript. If the estimate exceeds the cost, the balance shall be refunded.

For further information about your rights to appeal, please contact the City Recorder at 526-2495.

Sincerely,

A handwritten signature in black ink, appearing to read "Steven A. Sparks". The signature is fluid and cursive, with the first name "Steven" and last name "Sparks" clearly legible.

Steven A. Sparks, AICP  
Development Services Manager

C: Frank Angelo	Mike Maloney
Russ Pitkin/Eric Nielsen	Ron Matela
Gary Alfon	Casey Schleich
David Cory	Vlad Voytilla
Jim Duggan	Brad Roast
Sue Nelson	Phil Healy
Jan Youngquist	Bill Avery
Project File	

**BEFORE THE HISTORIC RESOURCE REVIEW COMMITTEE**  
**FOR THE CITY OF BEAVERTON, OREGON**

IN THE MATTER OF A REQUEST FOR	)	ORDER NO. 1505
	)	
APPROVAL OF A TREE PRESERVATION	)	TPP 2001-0008
	)	
PLAN FOR THE REMOVAL OF 20 TREES	)	ORDER APPROVING
	)	
DESIGNATED AS HISTORIC BY THE	)	REQUEST WITH
	)	
CITY OF BEAVERTON FOR THE	)	CONDITIONS
	)	
CONSTRUCTION OF A NEW CAFETERIA	)	
	)	
BUILDING AT BEAVERTON HIGH SCHOOL	)	
	)	
(BEAVERTON HIGH SCHOOL CAFETERIA	)	
	)	
AND PARKING LOT EXPANSION TREE	)	
	)	
PRESERVATION PLAN), FRANK ANGELO,	)	
	)	
BEAVERTON SCHOOL DISTRICT, APPLICANT)	)	
	)	

This matter came before the Historic Resource Review Committee on March 21, 2002, and postponed until April 30, 2002, on a proposal by the Beaverton School District for approval to remove historic trees, as identified on the City of Beaverton’s Historic Tree Inventory, in conjunction with the proposed expansion of the Beaverton High School campus. The proposal requests approval to remove 20 trees to accommodate a new drive aisle, parking areas, pedestrian ways, and a new two-level, 30,000 square foot cafeteria building located directly north of and connecting to the existing high school building at 13000 SW Second Street. The Beaverton High School campus is specifically described on Washington County Assessor’s Map 1S1-16AD, Tax Lots 11100, 11000,

02900, 07100 and 10900, and Map 1S1-16AC, Tax Lots 02100 and 02500. The site is zoned Urban Low Density (R-10) and is approximately 27 acres in size.

Pursuant to Ordinance 2050 (Development Code), Section 40.75.15.1, the Historic Resource Review Committee conducted a Public Hearing and considered testimony and exhibits.

After holding the public hearing and considering testimony, the Historic Resource Review Committee adopts the findings of the Staff Report dated March 14, 2002, as well as all exhibits thereto, including materials presented to the Historic Resource Review Committee at the public hearing, as to applicable criteria contained in Section 40.75.15.1.C.2 of the Development Code. The Historic Resource Review Committee also adopts the following supplemental findings, contained herein, in response to the primary issues of concern that were presented to the Historic Resource Review Committee on April 30, 2002, during the period of oral testimony. Supplemental findings in response to key issues of concern, as identified herein, are as follows:

*1. The Removal of Tree #47.* The applicant stated that this tree is located within the needed temporary bus loading and unloading area. The applicant identified the tree as a volunteer tree with no historical value and likely to be less than 20 years old. The Historic Resource Review Committee agreed that tree 47 was not closely associated with the historic trees on site. In consideration of Tree #47's age and location, it had little historical value and would be appropriate to remove.

*2. Landscaping Around the Merle Davies Building.* The applicant requested that the Historical Resource Review Committee clarify if the Historic Tree Inventory intends to include only the trees on the site. The applicant and staff noted that there are no



established criteria in evaluating the removal of historic shrubs. Staff stated that the City's Historic Tree Inventory appears to indicate that landscaping around the Merle Davies School building is historic. The Historic Resource Review Committee clarified this matter by stating that the large evergreen shrubs, primarily rhododendrons, are historic, but there are no requirements or code standards by which to guide in their potential removal or maintenance. While the Historic Resource Review Committee agreed the shrubs are of an established size, the Development Code addresses criteria only for historic trees, not shrubs which may be historic.

3. *Design of the Cafeteria.* The Historic Resource Review Committee was concerned with the removal of the trees to allow for the addition of a few additional parking spaces. The applicant clarified that the proposed project is requesting under a separate application, a 10% reduction in the required minimum parking space requirements for the entire campus. Any additional reduction in parking would result in the school not meeting the minimum parking requirements, therefore requiring the need for a Variance application. The applicant's landscape architect stated that the condition of the trees along SW 2<sup>nd</sup> Street has been identified as being in poor condition. It appears that symptoms of a soil-borne fungus may be found on one of the trees on SW 2<sup>nd</sup> Street, similar to the fungus that in the past has resulted in the removal of trees in this area. If the other trees are infected, it is likely one tree would be lost each year over the next five or six years. The Historic Resource Review Committee agreed that if the condition of the trees were such that eventual removal is likely to occur, then the applicant's proposal to remove the existing trees and to provide new trees would prevent a "hodgepodge" of

replacement trees. The proposed landscape design of the Cafeteria building would provide continuity to the historic character of the Beaverton High School campus.

**IT IS HEREBY ORDERED** that TPP 2001-0008 is approved, with the following conditions:

1. A certified arborist or other qualified professional shall have responsibility for establishing the limits of disturbance near protected trees which, at a minimum, are subject to the requirements of this report. The arborist or other professional shall review plans, notify the City, and be on site during construction within the established limits of disturbance.
2. Except for enhancement measures to ensure the health of protected trees, no development or disturbance shall occur within the root zone of such trees.
3. The applicant shall provide temporary six-foot chain link fencing with bright colored flagging or other city approved protection devices shall be placed outside the root zone of all trees proposed to remain. The root zone is defined by Ordinance 2050 as the area of ground around the base of the tree measured from the trunk to five feet beyond the outer base of the branching system.
4. No equipment or other material shall be stored or placed within the root zone of the protected trees.
5. The proposed pedestrian transit plaza at the northeast corner of the Beaverton High School campus, along SW Farmington Road, shall be located outside of the root zone of the historic cedar tree identified in the applicant's Tree Preservation Plan map as Tree No. 3.

6. The following trees, as identified on the Tree Preservation Plan map, are hereby approved for removal: Tree Nos. 28 through 46.

**CARRIED** by the following roll call vote:

<b>AYES:</b>	Bliss Nardozza White	<b>NAYS:</b>	Mawson Vojta
--------------	----------------------------	--------------	-----------------

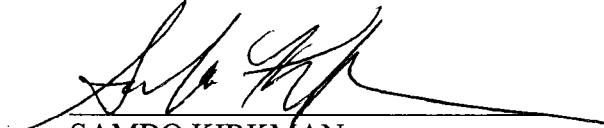
Dated this 14th day of May, 2002.

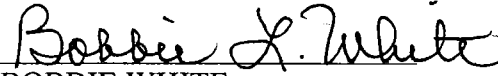
To appeal the decision of the Historic Resource Review Committee, as articulated in Land Use Order No. 1505, an appeal must be filed with the City of Beaverton Recorder's Office by no later than 5:00 p.m. on Friday, May 24, 2002.

HISTORIC RESOURCE REVIEW  
COMMITTEE  
FOR BEAVERTON, OREGON

ATTEST:

APPROVED:

  
 SAMBO KIRKMAN  
 Associate Planner

  
 BOBBIE WHITE  
 Chairman

  
 STEVEN A. SPARKS, AICP  
 Development Services Manager




# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## STAFF REPORT

TO: Historic Resource Review Committee

STAFF REPORT DATE: Thursday, March 14, 2002

STAFF: Sambo Kirkman, Associate Planner 

APPLICATION: **TPP2001-0008 (Beaverton High School Historic Trees Preservation Plan)**

LOCATION: 13000 SW 2<sup>nd</sup> Street  
Map 1S1-16AD; Tax Lots 11100, 11000, 02900, 07100 and 10900  
Map 1S1-16AC, on Tax Lots 02100 and 02500

ZONING: Urban Low Density (R-10)

REQUEST: The applicant requests Tree Preservation approval for removal and preservation of historic trees as identified on the City of Beaverton's Historic Tree Inventory in conjunction with the proposed building and parking area of the Beaverton High School Campus.

PROPERTY OWNER Beaverton School District  
16550 SW Merlo Road, Beaverton, OR 97006

APPLICANT: Frank Angelo, Angelo Eaton & Associates  
620 SW Main, Suite 201, Portland, OR 97205

AUTHORIZATION: Ordinance 2050, The Development Code

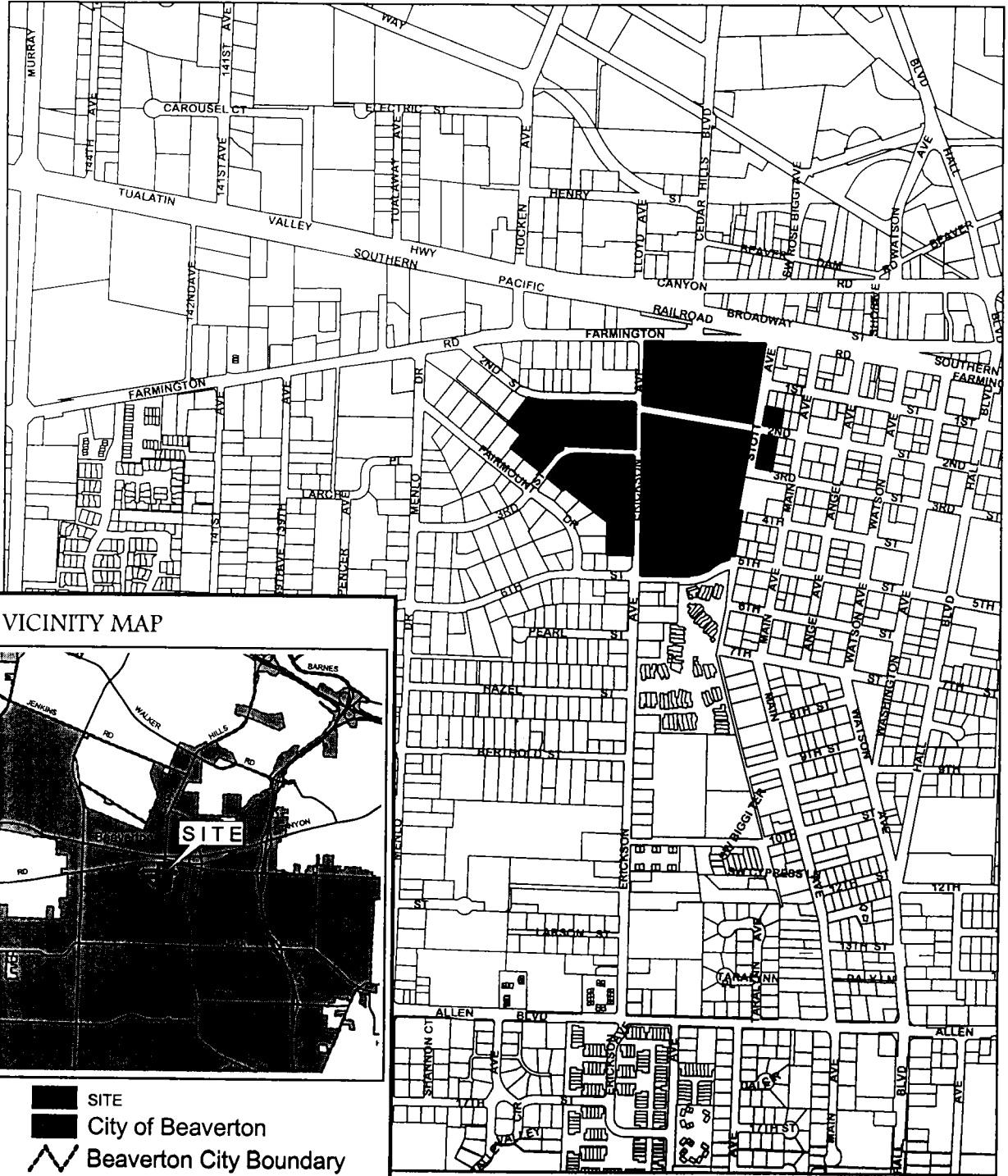
APPROVAL CRITERIA: Development Code, Section 40.75.15.1.C.3.a

HEARING DATE: Thursday March 21, 2002

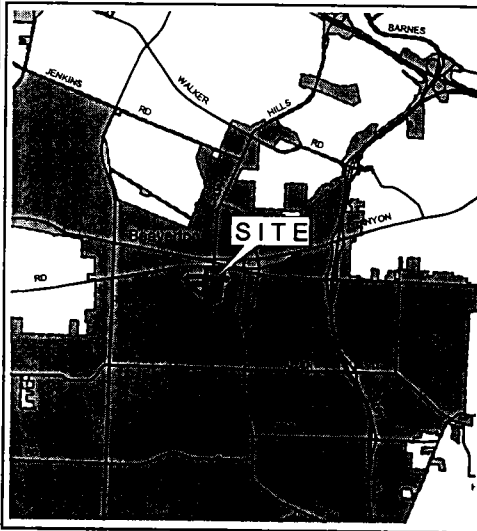
RECOMMENDATION: **APPROVAL of TPP2001-0008 (Beaverton High School Historic Trees Preservation Plan) subject to conditions identified at the end of this report.**




# DETAIL MAP

EXHIBIT 1



VICINITY MAP



-  SITE
-  City of Beaverton
-  Beaverton City Boundary



City of Beaverton

BEAVERTON HIGH SCHOOL CAFETERIA & PARKING LOTS  
 COMMUNITY DEVELOPMENT DEPARTMENT  
 Development Services Division

01/02/02  
 Map Number

1S116AD

Application #  
 BDR 2001-0213/CUP 2001-0031  
 VAR 2002-0002/TPP 2001-0008







provide written notice that the request for review has been rejected within 10 days of receipt of the request for review. (ORD 3739).

**120-Day Requirement:**

In accordance with State statute, all land use applications must have a final decision within 120 days from the date the application was made complete. Completeness of the application was perfected on January 30, 2002. Therefore, the City must take final action on the application on or before May 30, 2002. The Commission's hearing date of March 21, 2002 is day 50 of the review process.

**Public Notice:**

1. Property was posted in two locations on February 7, 2002.
2. Mailed notice to property owners within 500 feet on February 5, 2002 and March 11, 2002.
3. Legal notice was published in the Valley Times on February 7, 2002.
4. In addition, notice was posted at the Library, City Hall, and Post Office.

**Previous Actions:**

BDR2001-0085 Beaverton High School Parking Lot Improvement  
CUP2001-0018 Beaverton High School Parking Lot Improvement  
BDR99-00083 Beaverton High School Repaint  
BDR91-00091 Beaverton High School Two Portable Classrooms  
CUP91-00027 Beaverton High School Two Portable Classrooms  
BDR3-84 Beaverton High School Parking Lot  
BDR31-84 Beaverton High School Batting Cage  
BDR39-79 Beaverton High School Addition  
CUP4-70 Beaverton High School Expansion

**Other Pending Applications:**

**CPA2001-0022 Beaverton High School Comprehensive Plan Amendment**

Application submitted November 15, 2001

**SV2001-0003 Beaverton High School Street Vacation**

Application submitted November 15, 2001

**BDR2001-0213 Beaverton High School Cafeteria and Parking Lot**

Application submitted December 21, 2001

**TPP2001-0008 Beaverton High School Historic Tree Removal**

Application submitted December 21, 2001

**VAR2002-0002 Beaverton High School Bicycle Parking Variance**

Application submitted January 30, 2002. On March 4, 2002 the application was withdrawn.



## **BACKGROUND INFORMATION**

### **Summary:**

The applicant is requesting a Tree Preservation Plan approval for historic trees to be removed and preserved within the limits of the BHS campus, north of the main building. The applicant has requested approval to vacate SW 2<sup>nd</sup> Street between SW Stott and SW Erickson. If the vacation is approved, the street will be removed and the applicant proposes to replace the street with a 30,000 square foot building with parking lots and driveways located east and west of the facility. The main school driveway from Erickson Avenue is proposed to be relocated north of current SW 2<sup>nd</sup> Street to align with SW 2<sup>nd</sup> Street on the west side of SW Erickson. The proposed building and the main driveway will require the removal of 20 trees currently identified as part of the historic trees around the Merle Davies school.

### **Major Issues:**

The applicant is proposing to expand the existing Beaverton High School (BHS) campus to include an additional parking lot and a new two-story cafeteria facility. The location of the cafeteria is within the current public right-of-way of SW 2<sup>nd</sup> Street. The design of the proposed expansion, which includes the removal of the historic trees and the reason for the Tree Preservation application are contingent upon the approval to vacate SW 2<sup>nd</sup> Street. Prior to approval of the Street Vacation, the applicant will need to have approved a Comprehensive Plan Amendment to remove SW 2<sup>nd</sup> Street from the Street Functional Classification Plan as a neighborhood route.

### **Public Comment:**

As of the date of this staff report no written comments have been received.

### **Comprehensive Plan Designation:**

#### **Land Use:**

The Comprehensive Plan identifies the parcels as Public Facilities – Schools and Parks, except for the parcel, located south of the existing tennis courts along SW Erickson Street. This parcel is designated as Residential – Urban Low.

#### **Street Functional Classification Plan:**

SW 2<sup>nd</sup> Avenue is designated on the Functional Classification Plan Map as a Neighborhood Route. The applicant has requested a Comprehensive Plan Amendment to remove this designation on SW 2<sup>nd</sup> Street between SW Stott and SW Erickson Avenues.

SW Farmington Road (State Highway 10) is designated as an Arterial.

SW Erickson Street and SW 5<sup>th</sup> Street are designated as “Collector” streets.

SW Stott Avenue is designated as a local street.

Street Improvement Master Plan:

The Street Improvement Master Plan Map illustrates the future street widening of SW Farmington Road west of SW Hocken.

Traffic Signals Master Plan:

There are no future traffic signals identified on the Traffic Signals Master Plan. There are two traffic signals located on SW Farmington within close proximity to the project area located at SW Hocken and at SW Cedar Hills Boulevard.

Bicycle Master Plan and Action Plan:

SW Erickson is designated as a street where bike lanes are proposed. However, the applicant, in their Design Review application includes a Design Modification requesting not to add bicycle lanes along SW Erickson Avenue.

Zoning:

Subject Property: Residential – Urban Low Density (R-10)

Surrounding Area:	<u>North:</u>	Regional Center Old- Town (RC-OT) and General Commercial (GC)
	<u>South:</u>	Urban Standard Density (R-5) and Urban Low Density (R-10)
	<u>East:</u>	Regional Center – Old Town (RC-OT)
	<u>West:</u>	Urban Low Density (R-10)

Existing Land Uses:

Subject Property: Public High School

Surrounding Area:	<u>North:</u>	Commercial and SW Farmington Road
	<u>South:</u>	Residential
	<u>East:</u>	Residential & Commercial
	<u>West:</u>	Residential

Existing Site Conditions:

The Beaverton High School (BHS) campus consists of seven parcels totaling approximately 27 acres in size. There are three existing buildings on the site, the main building, the Merle Davies Building, and a building located west of the main building across SW Erickson Avenue. The Merle Davies building along with the trees surrounding this building is listed in the City’s Historic Inventory. There are athletic fields located north of the main building and west of the Merle Davies building, south of the main building, and on parcels located along the west side of SW Erickson Avenue. The applicant has identified 311 private and 77 public parking spaces on the BHS campus and 57% open space on the existing campus. The site is relatively flat with an overall grade change of less than 10 feet. SW Erickson Avenue, SW Stott Avenue, and SW 2<sup>nd</sup> Street are located

within the BHS campus, providing access to the buildings. SW 2<sup>nd</sup> Street and SW Stott Avenue have gates closing the roadway to vehicular traffic during school hours.

**TREE PRESERVATION PLAN**  
**CRITERIA, FACTS and FINDINGS**

**Tree Preservation Plan Standards for Approval:**

Section 40.75.10 of the Development Code provides the authority for the review of impacts to Historic Trees due to development or other reasons. Section 40.75.15.1.A.2 allows for a Tree Preservation Plan (TPP) to be heard by the appropriate hearing body. Section 40.75.15.1.C.2 contains the standards for decisions by the HRRC on Tree Preservation Plan applications.

At the public hearing on this matter, the HRRC will determine whether the TPP application as presented, or with conditions of approval, meets the TPP criteria. The HRRC may choose to modify the Conditions of Approval in the staff report, or may simply adopt the staff report's recommended Conditions. In this report, staff evaluates the application in accordance with the TPP criteria.

**Background Information: City of Beaverton Historic Tree Inventory**

The project site contains Historic Trees as identified on the City of Beaverton's Historic Tree Inventory. On May 20, 1991, the City Council approved a list of sites containing historic trees on Consent Agenda (Agenda Bill 91-118). This list was approved by the HRRC, which included landscaping around the Merle Davies Elementary School.

**Applicant's Tree Preservation Submittal**

The applicant has submitted an arborist report dated May 17, 1999, by David R. Cory, Consulting Arborist for Pruett Tree and Landscaping. The report includes a tree inventory depicting some trees within the project limits. In conjunction with the Tree Inventory Report, a Tree Preservation Plan map is provided depicting which trees are being retained or removed.

Of the 20 trees proposed to be removed, none of these trees are specifically recommended to be removed in the arborist's report; however many are identified as being in poor condition. It is the applicant who proposes removal of 20 trees. The 20 trees are located in the proposed development area for the new cafeteria building and the two parking lot areas on either side of the new building.

## **Findings and Analysis of Tree Preservation Plans**

**40.75.15.1.C.2.a** *“The HRRC and Council shall approve the application if it finds that the request meets at least one of the following factors:*

- 1) *That removal is necessary to accommodate a new improvement, structure or remodeled structure, and no alternative exists for relocation of the improvement on the site, or that variances to setback provisions of the Development Code will not allow the tree to be saved or will cause other undesirable circumstances on the site or adjacent properties.*

### **Facts and Findings:**

The applicant’s proposal requests the removal of 20 trees to locate the proposed cafeteria building between the existing main building and the Merle Davies building. The applicant has stated, “...*the existing campus site is constrained in terms of where facility expansion can occur. There are no expansion opportunities to the east or west of the existing BHS. Expansion of facilities to the south would eliminate athletic fields that could not be replaced on campus and would disrupt current physical plant operations.*” Although the applicant has provided this statement, additional information has not been provided to staff. For example, an analysis of other alternatives or identifying reasons as to why certain other location would not work for the proposed cafeteria have not been described by the applicant. Expansion within a limited space may require the loss of certain uses, such as athletic fields. Additional information as to the detriment in placing the site in other areas has not been fully discussed. Therefore staff find that additional information is needed in order to make a finding that no other alternative exists in locating the proposed cafeteria and driveway area.

Therefore, staff find that the information provided does not clearly show how this factor is met.

- 2) *That the tree has become a nuisance by virtue of damage to personal property or improvements, either public or private, on the subject site or adjacent sites, or that extraordinary maintenance is required to prevent damage to such improvements or property.*

### **Facts and Findings:**

The applicant has stated that the proposed development is not applicable to this application. There is no indication that the trees identified for removal have not become a nuisance as identified with this factor or is shown to require extraordinary maintenance.

Therefore, staff find that this factor is not applicable to this development proposal.

- 3) *That the tree has lost its significance in terms of its original designation on the list due to damage from natural or accidental causes, or for some other reason it can be established that it is no longer of historic significance.*

Facts and Findings:

The applicant has stated that this criterion is not applicable to this proposed development. There is no indication that the trees have lost their significance in terms of its original designation. Therefore, staff find that this factor, which would potentially allow removal of trees, does not support the applicant's request to remove trees

Therefore, staff find that this factor is not applicable to the proposal to remove trees.

- 4) *That removal of the tree is necessary to accomplish other public purposes such as installation of public utilities, street widening and similar needs and that no alternative exists to the removal of the tree without significantly increasing public costs or inconvenience or reducing safety.*

Facts and Findings:

The applicant stated, "The removal of the identified tree is necessary to widen the main entrance street to BHS and to provide sufficient space for pedestrian and bicycle connections through the campus." The proposal is the expansion of the BHS campus to accommodate the school's need for a more modern facility and comply with certain ADA standards. Beaverton High School is a public high school, which provides a public purpose. The location of the proposed building is within the public right-of-way, but if the proposed street vacation for SW 2<sup>nd</sup> Street were approved, the property would revert to the owners of the abutting parcels, the Beaverton School District. The expansion of the proposed campus to this area allows the school district the opportunity to locate in an area of the campus that would have minimal affects on services such as the athletic fields that surround the campus. Theses areas of the campus are used by the students of the school and by the community. Additionally, the proposed location of the main driveway would align with the portion of SW 2<sup>nd</sup> Street located on the west side of SW Erickson improving safety access.

Therefore, staff find that the proposal meets this factor.

- 5) *That the tree is unsafe to the occupants of the property, and adjacent property, or the general public.*

Facts and Findings:

The applicant has stated that the proposed development is not applicable to this factor. The arborist report identifies some of the trees as being in poor condition, but there are no indication that the trees are unsafe to the users of the BHS campus.

Therefore, staff find that this factor is not applicable to the proposal to remove trees.

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**SUMMARY OF FINDINGS for Section 40.75.15.1.C.2.a Historic Tree Removal Approval Criterion:**

**FINDING:** Staff find that although the proposed development does not meet factor #1 and that factors #2, #3, #5 are not applicable, by meeting factor #4, the proposal meets this approval criterion. The Code allows the removal of historic trees even if only one of the factors (criteria for removal) has been met.

---

***40.75.15.1.C.2.b. The historic tree shall be preserved unless the applicant proves to the satisfaction of the reviewing body that removal is necessary as a result of:***

- 1) *The necessity to remove trees which pose a safety hazard to pedestrians, property or vehicular traffic or threaten to cause disruption of public service; or which pose a safety hazard to persons or buildings.*
- 2) *The necessity to remove diseased trees or trees weakened by age, storm, fire or other injury.*

Facts and Findings:

The applicant states these criteria are not applicable. Neither the applicant nor the project arborist identifies any of the trees on the site as a safety hazard or diseased which could necessitate their removal.

Therefore, staff find that the criteria are not applicable to the proposal.

- 3) *The necessity to observe good forestry practices according to adopted City standards on the subject.*

Facts and Findings:

Removal of the trees associated with this project does not appear to be related to the need to observe good forestry practices. For instance, no need for thinning or selective removal for grove health has been indicated. The applicant has provided an arborist report identifying the condition of some of the historic tree; however, there are no recommendations of measures to minimize impacts to the remaining historic trees. In order to ensure that no adverse affects to these trees occur, staff will recommend some conditions of approval. Staff will require construction fencing to mitigate disturbance to the remaining trees. The fencing should be temporary 6-foot chain-link fencing around the critical root zone to minimize impacts to the remaining trees on site. A certified arborist will be on hand during construction to identify the specific trees to be removed and to ensure that the impacts to the trees being preserved are minimal. Additionally no equipment or materials will be placed within the root zone of the trees to be preserved to minimize impacts to these trees during construction. These measures are necessary to reduce the possibility of damage to the established trees proposed for preservation.

Therefore, staff find that, by satisfying Conditions #1 through 4, the criterion is met.

- 4) *Need for access immediately around the proposed structure for construction equipment.*
- 5) *Need for access to the building site for construction equipment.*

Facts and Findings:

The applicant stated the 20 trees illustrated for removal are those necessary for construction of their proposal and that access to the site by construction equipment have been incorporated in the design of the proposal limiting the number of trees to be removed. All other trees are to remain on the site. The applicant states “Measures will be taken to ensure the existing trees’ safety during construction, such as fencing and marking of trees that are near to the construction area.” Staff recommend that a certified arborist is on site during construction and temporary cyclone fencing be provided to clearly mark off the trees to be saved and to ensure that the preserved trees around the construction area, are not disturbed or accidentally removed.

Therefore staff find that by meeting Conditions #1 and #3, the proposal will meet these criteria.

- 6) *Essential grade changes. Essential grade changes are those grade changes needed to implement standards common to standard engineering or architectural practices.*
- 7) *Surface water drainage and utility installations.*

Facts and Findings:

The removal of the proposed 20 trees are for the location of the proposed building, parking area, and driveway area. There are minor grade changes proposed with this development associated with the construction of the building, parking area, and driveway. However, no trees are identified for removal as a result of a surface water drainage facility, utility construction or grading.

Therefore, staff find that these criteria are not applicable to this proposal.

- 8) *Location of driveways, buildings, or other permanent improvements.*

Facts and Findings:

The proposal is for the construction of a two-story building, with parking areas and driveways. The location of the main driveway off of SW Erickson Street is to align with SW 2<sup>nd</sup> Street west of SW Erickson. In the current plan, the location of the driveway area is such that seven trees identified as trees #30 through #36 are to be removed. Four trees, identified as trees #37, #38, #42, and #43, in the area between the two existing building are proposed to be removed for the construction of the cafeteria building, and an additional five trees, trees #39, #40, #41, #44, and #45 are to be removed for the parking area located to the east of the proposed building. Four additional trees are proposed to be removed. The trees marked #28, #29, #46 and #47. Trees #28, #29, and #46 are located along SW Stott Street in the landscape area between the parking lot and the sidewalk. With construction of the parking area and the improved sidewalks, damage to a majority of the root zone of these three trees are expected. However, tree #47 is near the eastern entrance of the Merle Davies building. It appears that the applicant is proposing to remove existing planter areas along the eastern elevation of Merle Davies School and providing additional impervious area to the site. The area is not identified in their plans as a pedestrian plaza or required parking area. With the construction of a 24 foot driveway access from SW Stott Avenue approximately 220 feet south of SW Farmington Road, it appears to be used as excess parking. The removal of tree #47 would be to expand the existing impervious surface area with no specific use of the area with the proposed development. Staff find that tree #47 need not be removed as part of the proposed development and that a condition of approval is necessary to require the retention of tree #47 while approving other elements of the proposed tree removal.



Additionally, the applicant proposes a transit facility along SW Farmington Road on the northeast corner of the campus. The pedestrian plaza is to be located on the existing lawn area east of Tree #3. Tree #3 is a large cedar tree approximately 40 inches in diameter and is not proposed for removal. The pedestrian plaza appears to be located in a small portion of the Tree #3's critical root zone. The location of the pedestrian plaza is such that minimal impacts should occur to this tree. However, to ensure damage does not occur to a historic tree of such a large size, staff recommend the pedestrian plaza be located outside of the critical root zone of Tree #3.

Therefore staff find that by meeting Condition #5 and #6, the criterion is met.

9) *Compliance with other ordinances or codes.*

Facts and Findings:

As stated by the applicant, "This criterion is not applicable to this application." The removal of the proposed 20 trees are for the location of the proposed building, parking area, and driveway area and are not required to show compliance with other ordinances and codes other than the Development Code, Municipal Code, and Engineering Design Manual.

Therefore, staff find that this criterion is not applicable to this proposal.

10) *Necessity to install solar energy equipment.*

Facts and Findings:

As stated by the applicant, "This criterion is not applicable to this application." The removal of the proposed 20 trees are for the location of the proposed building, parking area, and driveway area and are not due to the installation of solar energy equipment.

Therefore, staff find that this criterion is not applicable to this proposal.

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**SUMMARY OF FINDINGS for Section 40.75.15.1.C.2.b Historic Tree Removal Approval Criterion:**

**FINDING:** Staff find that Criteria #3, #4, #5, and #8 are applicable in the review of the request for TPP approval. Criteria #1, #2, #6, #7, #9, and #10 are not applicable. Because the application for tree removal does not require a finding for conformance to every one of the factors or criteria to approve tree removal, staff find that with the exception of tree #47, the proposed Tree Preservation Plan meets the standards for approval. Therefore, staff find that by satisfying the conditions identified at the end of this report, the proposal meets this approval criterion.

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**TREE PRESERVATION PLAN CONCLUSION**

Based on the facts and findings presented, staff conclude that by satisfying the conditions of approval the proposal, **TPP2001-0008 (Beaverton High School Historic Trees Preservation Plan)**, meets the criteria.

**RECOMMENDATION**

Based on the facts and findings presented, staff recommend **APPROVAL** of **TPP2001-0008 (Beaverton High School Historic Trees Preservation Plan)**, subject to the following conditions:

**CONDITIONS OF APPROVAL**

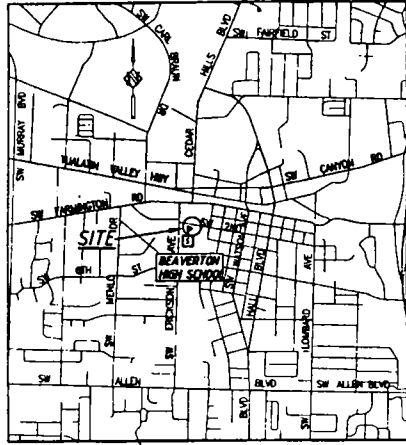
1. A certified arborist or other qualified professional shall have responsibility for establishing the limits of disturbance near protected trees which at a minimum are subject to the requirements of this report. The arborist or other professional shall review plans, notify the City, and be on site during construction within the established limits of disturbance.
2. Except for enhancement measures to ensure the health of protected trees, no development or disturbance shall occur within the root zone of such trees.

3. The applicant shall provide temporary six-foot chain link fencing with bright colored flagging or other city approved protection devices shall be placed outside the root zone of all trees proposed to remain. The root zone is defined by Ordinance 2050 as the area of ground around the base of the tree measured from the trunk to 5 feet beyond the outer base of the branching system.
4. No equipment or other material shall be stored or placed within the root zone of the protected trees.
5. The Red Maple tree identified on the Tree Preservation Plan as Tree #47 shall not be removed.
6. The proposed pedestrian transit plaza at the northeast corner of the Beaverton High School campus, along SW Farmington Road, shall be located outside of the root zone of the historic cedar tree identified in the applicant's Tree Preservation Plan map as Tree #3.
7. The following trees, as identified on the Tree Preservation Plan map, are hereby approved for removal: Trees #28 through #46
8. The following trees, as identified on the Tree Preservation Plan map, shall be preserved: Trees #1 through 27 and #47. Additionally all historic landscaping around the Merle Davies School shall remain, with exception to the trees identified in Condition #7 for removal.

### EXHIBITS

- Exhibit 1. Vicinity Map (located on page 2 of the Staff Report)
- Exhibit 2 Reduced Plans
- Exhibit 3 Applicant's Material Packet
- Exhibit 4 Neighborhood Meeting Materials
- Exhibit 5 Agenda Bill 91-118

# BEAVERTON HIGH SCHOOL CAFETERIA ADDITION

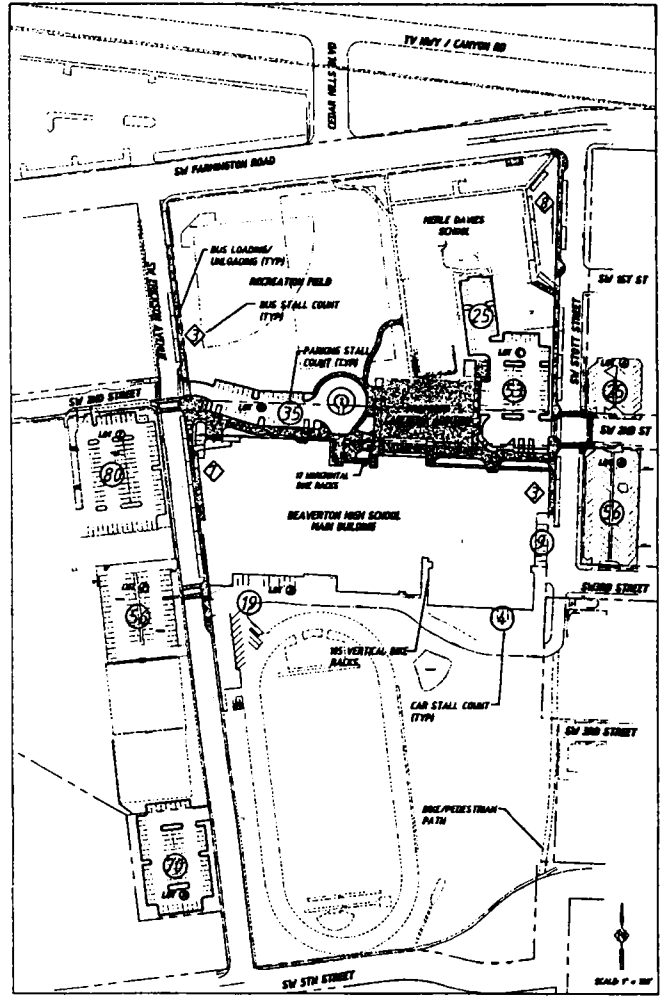


**VICINITY MAP**  
S12

**INDEX**

- 1 COVER SHEET
- 2 EXISTING CONDITIONS PLAN
- 3 FUTURE PARKING LOT EXISTING CONDITIONS
- 4 DIMENSION PLAN PUBLIC IMPROVEMENTS
- 5 DIMENSION PLAN PRIVATE IMPROVEMENTS (WEST)
- 6 DIMENSION PLAN PRIVATE IMPROVEMENTS (EAST)
- 7 FUTURE PARKING LOT DIMENSION GRADING AND UTILITY PLAN
- 8 GRADING, STORM AND EROSION CONTROL PUBLIC IMPROVEMENTS
- 9 GRADING, STORM AND EROSION CONTROL PRIVATE IMPROVEMENTS (WEST)
- 10 GRADING, STORM AND EROSION CONTROL PRIVATE IMPROVEMENTS (EAST)
- 11 LANDSCAPE PLAN PUBLIC IMPROVEMENTS
- 12 LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (WEST)
- 13 LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (EAST)
- 14 FUTURE PARKING LOT LANDSCAPE AND LIGHTING PLAN
- 15 DETAIL SHEET
- 16 DETAIL SHEET
- 17 DETAIL SHEET

EXISTING PARKING / SIDEWALK AREA	90,000 SF (PUBLIC) 110,000 SF (PRIVATE)
EXISTING NUMBER OF PARKING SPACES	311 (PRIVATE) 66 (PUBLIC)
PROPOSED NUMBER OF PARKING SPACES	362 + 70 LOT H
PROPOSED PARKING/ SIDEWALK AREA	225,000 SF (PRIVATE)
EXISTING OPEN SPACE AREA	640,000 SF (PERVIOUS) 120,000 SF (IMPERVIOUS)
PROPOSED OPEN SPACE AREA	570,000 SF (PERVIOUS) 140,000 SF (IMPERVIOUS)
EXISTING BUILDING AREA	195,000 SF
PROPOSED BUILDING AREA	212,500 SF
EXISTING LANDSCAPED AREA	27,000 SF
PROPOSED LANDSCAPED AREA	44,000 SF
TOTAL SITE AREA	27 ACRES
EX % OPEN SPACE (PERVIOUS)/LANDSCAPE	56%
PRO% OPEN SPACE (PERVIOUS)/LANDSCAPE	48%



**PARKING PLAN & SITE MAP**

**GENERAL NOTES:**

WORK SHALL CONFORM WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF BEAVERTON (STANDARD, CLEAN HAZARD SERVICES, THE STREET MARKING CODE BOOK, AND SURVEY PLANNING CODE BOOK).  
 LATEST TOPOGRAPHIC INFORMATION AND UTILITIES SHOWN BASED ON AS-BUILT DRAWINGS AND TOPOGRAPHIC SURVEY COMPLETED MAY 2005 BY CONSULTING ENGINEERS, PUELLER, ANDERSON, YONKIN & ASSOCIATES.  
 EXISTING UTILITIES, PROPERTY CORNERS, AND SURVEY MARKERS SHALL BE PROTECTED. REPLACEMENT SHALL BE AT THE CONTRACTOR'S EXPENSE.  
 CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.  
 BEFORE ANY WORK IS TO BE PERFORMED THE RULES ADOPTED BY THE BEAVERTON UTILITY INSPECTION CENTER. THESE RULES ARE SET FORTH IN EACH YEAR-END UTILITY SURVEY AND REVISIONS. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER AT 503-340-3000.  
 CONTRACTOR TO PROVIDE FINISHED GROUND COPY OF FIELD PLANS SHOWING ANY AND ALL RETAINMENT FROM DESIGN FOR AS-BUILT SUBMITTAL TO OWNER AND CITY.

**STORM DRAINAGE**

STORM DRAIN PIPE, MANHOLE, AND PIPES SHALL BE PVC 1500 RIGID PIPE. STORM DRAINAGE SHALL BE DESIGNED FOR 100 YEAR RETURN PERIOD AS PROVIDED AND SPECIFIED BY AEC, PROJECT NAME, DATE, OR DRAWING.

**EROSION CONTROL**

**TEMPORARY EROSION CONTROL:**  
 THE CONSTRUCTION NOTES AND DETAILS REFLECT RECOMMENDED PRACTICES AS ADAPTED TO THE TERRAIN, VEGETATION AND EXISTING CHANNELS. PLANNING AND DESIGN MANUAL, PUELLER ANDERSON YONKIN & ASSOCIATES BY THE CITY OF BEAVERTON.  
 IT IS THE INTENT OF THESE PLANS TO SHOW POSSIBLE APPLICATIONS OF RECOMMENDED PRACTICES TO THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN AND IMPLEMENTATION OF EROSION CONTROL MEASURES. ALL RECOMMENDED PRACTICES ARE APPROVED AND CONSTRUCTION METHODS, STAGING, SITE CONDITION, WEATHER AND SCHEDULE. THE PRACTICES SHOWN ON THE PLANS ARE NOT INTENDED TO BE EXCLUSIVE OF ALL THE PRACTICES REQUIRED IN AN AREA AS A PARTICULAR REQUIREMENT. THEY SHOULD BE USED AS A GUIDE ONLY.  
 THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING NECESSARY PERMITS PRIOR TO STARTING CONSTRUCTION. IF AN INSTALLED EROSION CONTROL SYSTEM DOES NOT ADEQUATELY CONTROL EROSION ON SITE, THEN THE EROSION CONTROL MEASURES MUST BE FIELD ADAPTED BY THE CONTRACTOR AS NECESSARY AND CONSTRUCTION STOPS UNTIL THE DESIGNER HAS BEEN NOTIFIED AND APPROVED. THESE MEASURES SHALL BE MONITORED BY THE CONTRACTOR AND BEADJUSTED AS NECESSARY TO CONTROL EROSION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN AND IMPLEMENTATION OF EROSION CONTROL MEASURES. ALL RECOMMENDED PRACTICES ARE APPROVED AND CONSTRUCTION METHODS, STAGING, SITE CONDITION, WEATHER AND SCHEDULE. THE PRACTICES SHOWN ON THE PLANS ARE NOT INTENDED TO BE EXCLUSIVE OF ALL THE PRACTICES REQUIRED IN AN AREA AS A PARTICULAR REQUIREMENT. THEY SHOULD BE USED AS A GUIDE ONLY.  
 EROSION CONTROL MEASURES SHALL BE MAINTAINED AS LONG AS NEEDED TO PREVENT EROSION. MEASURES ARE TO BE IN PLACE AND OPERATIONAL. THESE MEASURES SHALL BE REMOVED ONCE THE STABILIZATION HAS BEEN ESTABLISHED.

PERMANENT EROSION CONTROL - VEGETATION ON ALL CHANGES AND EXCLUDED AREAS SHALL BE RE-ESTABLISHED AS SOON AS CONSTRUCTION IS COMPLETE.  
 THE EROSION CONTROL MEASURES SHALL BE MONITORED DAILY BY CONTRACTOR AND ADJUSTED AS NECESSARY TO PREVENT EXCESSIVE SOIL EROSION.  
 ALL EXISTING AND NEWLY CONSTRUCTED STORM DRAINS AND DRAINAGE SHALL BE PROTECTED WITH PAVING OR FACED WITH CONCRETE AND BE TYPED TO MATCH THE EXISTING. ANY DAMAGE TO EXISTING STORM DRAINS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.  
 STORM IS TO BE PLACED ON ALL EXCLUDED AREAS IMMEDIATELY AFTER COMPLETION OF EACH SECTION OF CONSTRUCTION ACTIVITY. AFTER THE TEMPORARY EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

**PARKING SUMMARY**

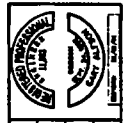
	EXISTING	PROPOSED	REMOVED
SPACES	311	437	0
SPACES	66	0	0
SPACES	0	0	0

\* SEE THE DRAWINGS

RECEIVED  
 11/17/05  
 11/17/05

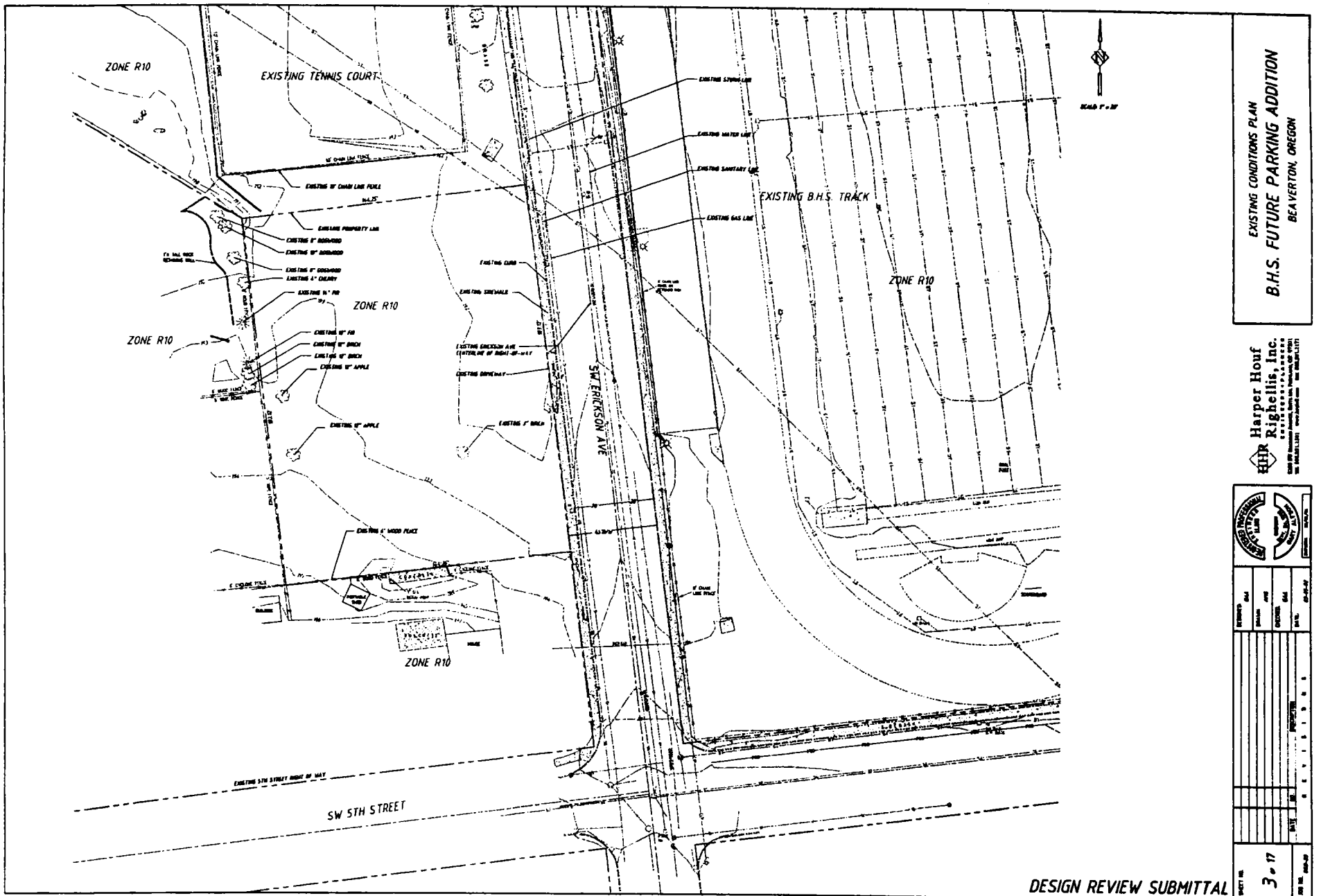
COVER SHEET  
 B.H.S. CAFETERIA ADDITION  
 BEAVERTON, OREGON

Harper Houf  
 Rightell, Inc.  
 ENGINEERS, ARCHITECTS, PLANNERS  
 1100 N. BROADWAY, SUITE 100  
 BEAVERTON, OREGON 97008



NO.	DATE	BY	FOR





EXISTING CONDITIONS PLAN  
**B.H.S. FUTURE PARKING ADDITION**  
 BEAVERTON, OREGON

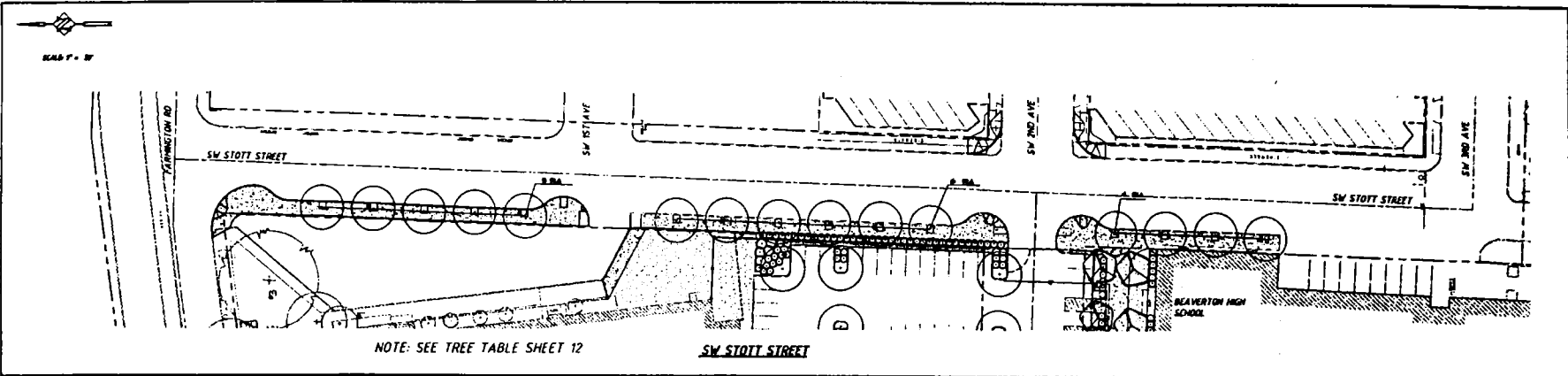
Harper Houff  
**HR Righellis, Inc.**  
 ENGINEERS, ARCHITECTS, PLANNERS  
 10000 SW 28th Street, Beaverton, Oregon 97005  
 TEL: 503-640-1111 FAX: 503-640-1112



REVISION	DATE	BY	CHKD.

DESIGN REVIEW SUBMITTAL

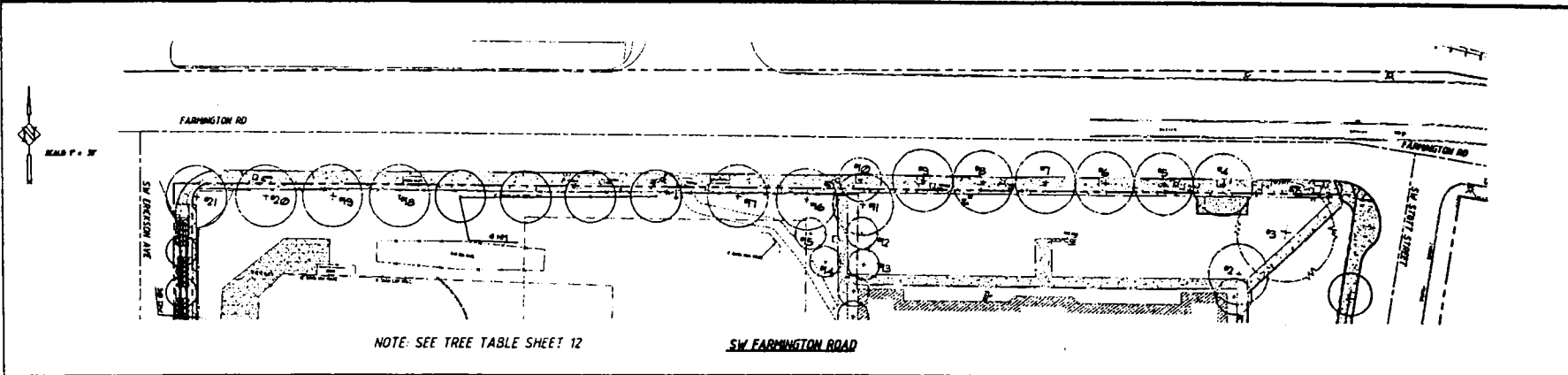
SHEET NO. **3 of 17**  
 DATE: 02-27-20



NOTE: SEE TREE TABLE SHEET 12

SW STOTT STREET

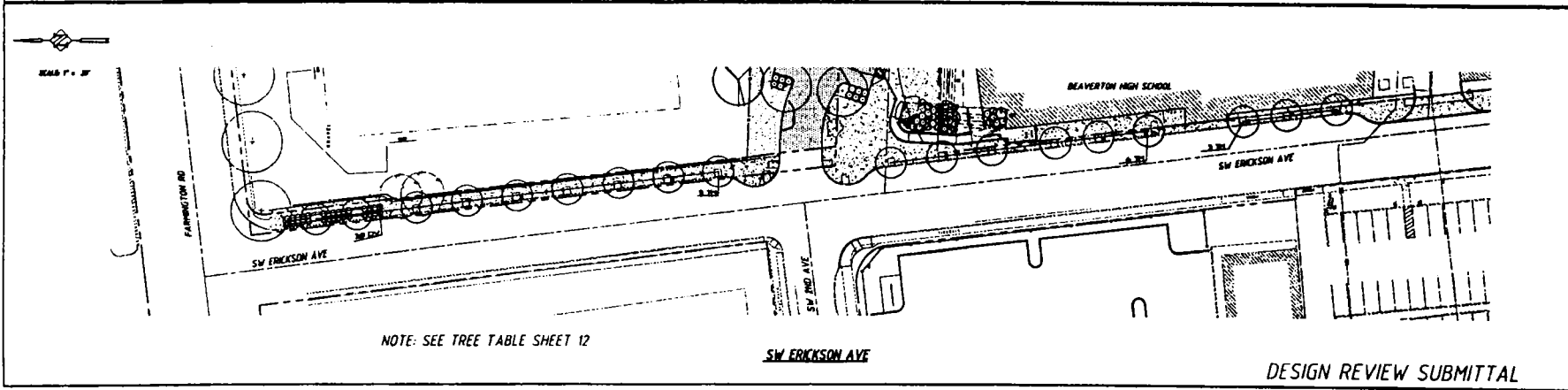
PUBLIC LANDSCAPING PLAN  
 B.H.S. CAFETERIA ADDITION  
 BEAVERTON, OREGON



NOTE: SEE TREE TABLE SHEET 12

SW FARMINGTON ROAD

EHR Harper Houf  
 Rigbellis, Inc.  
 LANDSCAPE ARCHITECTS  
 1000 NE 10TH ST. BEAVERTON, OREGON 97004  
 PHONE 503/764-8888 FAX 503/764-8899



NOTE: SEE TREE TABLE SHEET 12

SW ERICKSON AVE

DESIGN REVIEW SUBMITTAL

DATE	BY	APP'D.
SHEET NO.	11 - 17	TOTAL SHEETS 17







**LANDSCAPE NOTES**

1. STREET TREES AND UTILITY LINES SHOWN AND SCHEDULED FOR APPROVED PLANS.
2. ALL LANDSCAPE AREAS WILL BE FULLY IRRIGATED BY AN APPROVED SYSTEM TO BE SUBMITTED WITH EXISTING REGULATION.

**IRRIGATION NOTES**

1. ALL LANDSCAPE AREAS WILL BE FULLY IRRIGATED BY AN APPROVED SYSTEM TO BE SUBMITTED WITH EXISTING REGULATION.

**LEGEND**

- EXISTING TREES TO BE REMOVED
- DESIRED LAWN
- STREET LAMP

**PLANT SCHEDULE**

SYM	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
RA	FRAXINUS OXYCARPA 'TAYWOOD'	WATWOOD ASH	3	7' CAL.
BA	ACER TATARICUM	TATARICAN MAPLE	6	7' CAL.
<b>SHRUBS</b>				
7B	SCYTHIUS KALININOVICIA	MANHATTAN SCOTTONE	62	1 GAL.
BCH	SPY CORN 'WILFORD COPPINACK'	WILFORD COPPINACK HOLLY	64	1 GAL.
CLL	PRUNUS LAURO 'OTTO LYNCH'	OTTO LYNCH LAUREL	38	1 GAL.
BSY	'VIBURNUM TRISTE SPICATA' TREES	SPICATA VIBURNUM TREES	2	1 GAL.
<b>GROUND COVERS</b>				
8F	AJUDA REPENS 'ATROPURPUREA'	BRONZE BURNING BURNING	150	4" POT
8G	ARCHONIA LANTANA 'WAXWING'	WAXWING	250	4" POT
	DESIRED LAWN	DESIRED LAWN		

**EXISTING TREE DATA**

**TREES TO BE REMOVED**

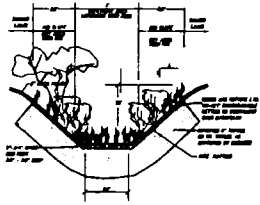
SPACES KEY	SPACES	SIZE (DBH)
1	APPLE	8"

**BIOQUALITY PLANT SCHEDULE**

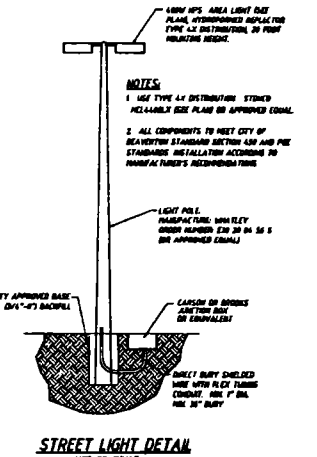
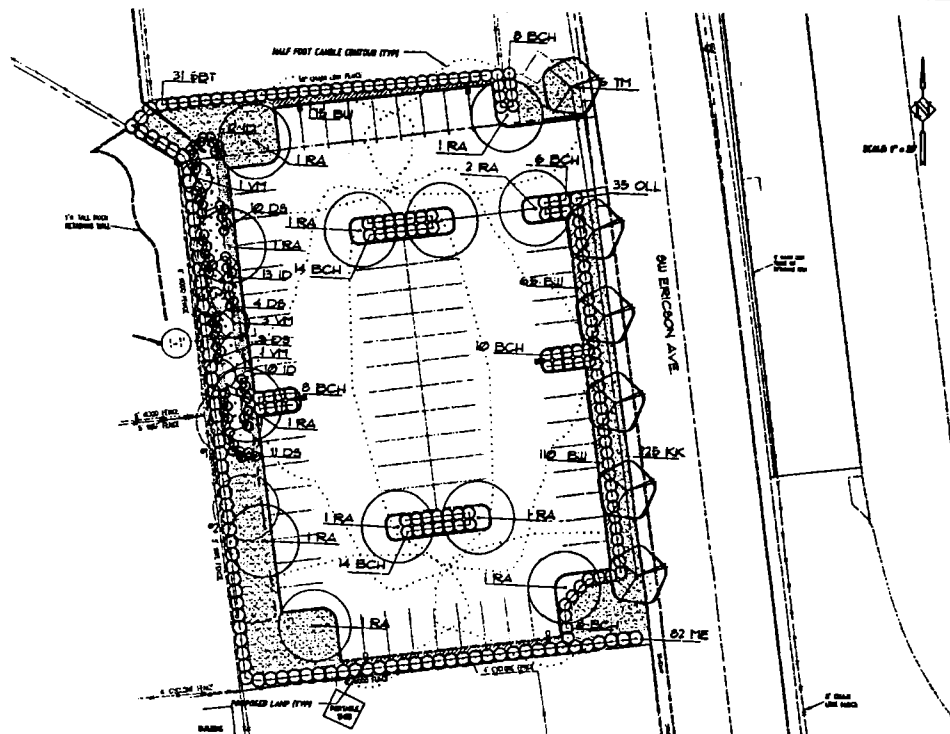
SYM	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
7A	FRAXINUS OXYCARPA	WATWOOD ASH	3	7' CAL.
7B	ACER TATARICUM	TATARICAN MAPLE	6	7' CAL.
<b>SHRUBS</b>				
8A	SPY CORN 'WILFORD COPPINACK'	WILFORD COPPINACK HOLLY	64	1 GAL.
8B	PRUNUS LAURO 'OTTO LYNCH'	OTTO LYNCH LAUREL	38	1 GAL.
<b>GROUND COVERS</b>				
8C	AJUDA REPENS 'ATROPURPUREA'	BRONZE BURNING BURNING	150	4" POT
8D	ARCHONIA LANTANA 'WAXWING'	WAXWING	250	4" POT

**BIOQUALITY PLANTING NOTES**

1. ALL TREES TO BE REMOVED MUST BE REMOVED WITHIN 30 DAYS OF APPROVAL.
2. ALL TREES TO BE REMOVED MUST BE REMOVED WITHIN 30 DAYS OF APPROVAL.
3. ALL TREES TO BE REMOVED MUST BE REMOVED WITHIN 30 DAYS OF APPROVAL.



1. WATER QUALITY BIOQUAL-TYPICAL SECTION



**STREET LIGHT DETAIL**  
NOT TO SCALE

MATEA ASSOCIATES INC.  
LANDSCAPE ARCHITECTURE  
1000 N. W. 11th St., Suite 100  
Beaverton, OR 97005  
503.638.1111

Harper Houf  
Righelisi, Inc.  
ARCHITECTS  
1000 N. W. 11th St., Suite 100  
Beaverton, OR 97005  
503.638.1111

PRIVATE LANDSCAPING AND LIGHTING PLAN  
B.H.S. FUTURE PARKING LOT  
BEAVERTON, OREGON

NO.	DATE	DESCRIPTION	BY	CHKD.
1	10-10-20	DESIGN REVIEW SUBMITTAL		

DESIGN REVIEW SUBMITTAL

14 of 17



**Tree Preservation Plan/Historical Tree Review  
Beaverton High School Improvements**

**Beaverton High School  
13000 SW 2<sup>nd</sup> Street  
Beaverton, Oregon 97005**

**RECEIVED**  
**MAR 11 2002**  
City of Beaverton  
Development Services

Prepared for:

**Beaverton School District #48  
16550 SW Merlo Road  
Beaverton, Oregon 97006-5152**

Submitted to:

**City of Beaverton  
Community Development Department  
4755 SW Griffith Drive  
P.O. Box 4755  
Beaverton, Oregon 97076**

Submitted by:

**ANGELO  EATON**  
& Associates

**620 SW Main, Suite 201  
Portland, Oregon 97205  
503-224-6974**

**Revised February 2002**

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I. Summary.....	ii
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III. Historic Tree Removal Review Criteria .....	2
IV. Conclusion.....	8

### FIGURES

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--	----

### REVISED ATTACHMENTS

- |   |  |
|---|--|
| 1. REVISED COVER SHEET  | 2. REVISED EXISTING CONDITIONS PLAN  |
| 3. REVISED EXISTING CONDITIONS PLAN   | 4. REVISED DIMENSION PLAN PUBLIC IMPROVEMENTS                                  |
| 5. REVISED DIMENSION PLAN PRIVATE IMPROVEMENTS (WEST)                         | 6. REVISED DIMENSION PLAN PRIVATE IMPROVEMENTS (EAST)                          |
| 7. REVISED DIMENSION, GRADING UTILITY LOT H                                   | 8. REVISED GRADING, STORM AND EROSION CONTROL PUBLIC IMPROVEMENTS              |
| 9. REVISED GRADING, UTILITIES AND EROSION CONTROL PRIVATE IMPROVEMENTS (WEST) | 10. REVISED GRADING, UTILITIES AND EROSION CONTROL PRIVATE IMPROVEMENTS (EAST) |
| 11. REVISED LANDSCAPE AND LIGHTING PLAN PUBLIC IMPROVEMENTS                   | 12. REVISED LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (WEST)            |
| 13. REVISED LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (EAST)           | 14. REVISED LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (LOT H)           |
| 15. REVISED DETAIL SHEET FOR WATER QUALITY CONSTRUCTION                       | 16. REVISED DETAIL SHEET   |
| 17. REVISED DETAIL SHEET FOR BIKE RACK DETAIL                                 | 18. REVISED TREE PRESERVATION PLAN   |
| 19. REVISED ARCHITECTURAL ELEVATIONS  |  |

### APPENDICES

INCLUDED IN ORIGINAL APPLICATION

## I. Summary

**Applicant:** Beaverton School District #48  
16550 SW Merlo Road  
Beaverton, Oregon 97006-5152  
Mike Maloney  
Phone: (503) 591-4303  
Fax: (503) 591-4484  
Mike\_Maloney@beavton.k12.or.us

**Applicant's Representative:** Frank Angelo  
Angelo Eaton & Associates, Inc.  
620 SW Main Street, Suite 201  
Portland, Oregon 97205  
Phone: (503) 227-3664  
Fax: (503) 227-3679  
fangelo@angelo Eaton.com

**Applicant's Architect:** Russ Pitkin  
LSW Architects, PC  
1953 NW Kearney Street  
Portland, Oregon 97209  
Phone: (503) 274-5432  
Fax: (503) 274-0085  
rpitkin@lsw-architects.com

**Applicant's Landscape Architect:** Ron Matela  
Matela Associates Inc.  
601 Main Street #201  
Vancouver, WA 98660  
Phone: (360) 695-0776  
Fax: (360) 695-0776  
rmatela@pacifier.com

**Applicant's Engineer:** Gary Alfson  
Harper Houf Righellis, Inc.  
5200 SW Macadam, Suite 580  
Portland, Oregon 97201  
Phone: (503) 221-1131  
Fax: (503) 221-1171  
gary@hrcivil.com

**Site Address/Location:** Beaverton High School  
13000 SW 2<sup>nd</sup> Street  
Beaverton, Oregon 97005

**Map and Tax Lot #:** 1S116AD tax lots 11100, 11000, 02900, 07100 and  
10900  
1S116AC tax lots 02100 and 02500

## II. Introduction

The Beaverton School District (BSD) is seeking revisions to the existing Beaverton High School (BHS) campus (Site Improvement Plan, Figure 1) in order to provide a new cafeteria and to create a more urban high school campus that is integrated with the surrounding Beaverton urban area. As part of the funded improvement program, the improvements will include:

- A new cafeteria building located north of the existing BHS main building;
- Renovation to the interior of the existing high school to enhance educational facilities;
- Additional parking; and
- Improvements to the front and entrance to BHS.

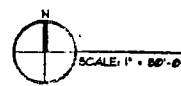
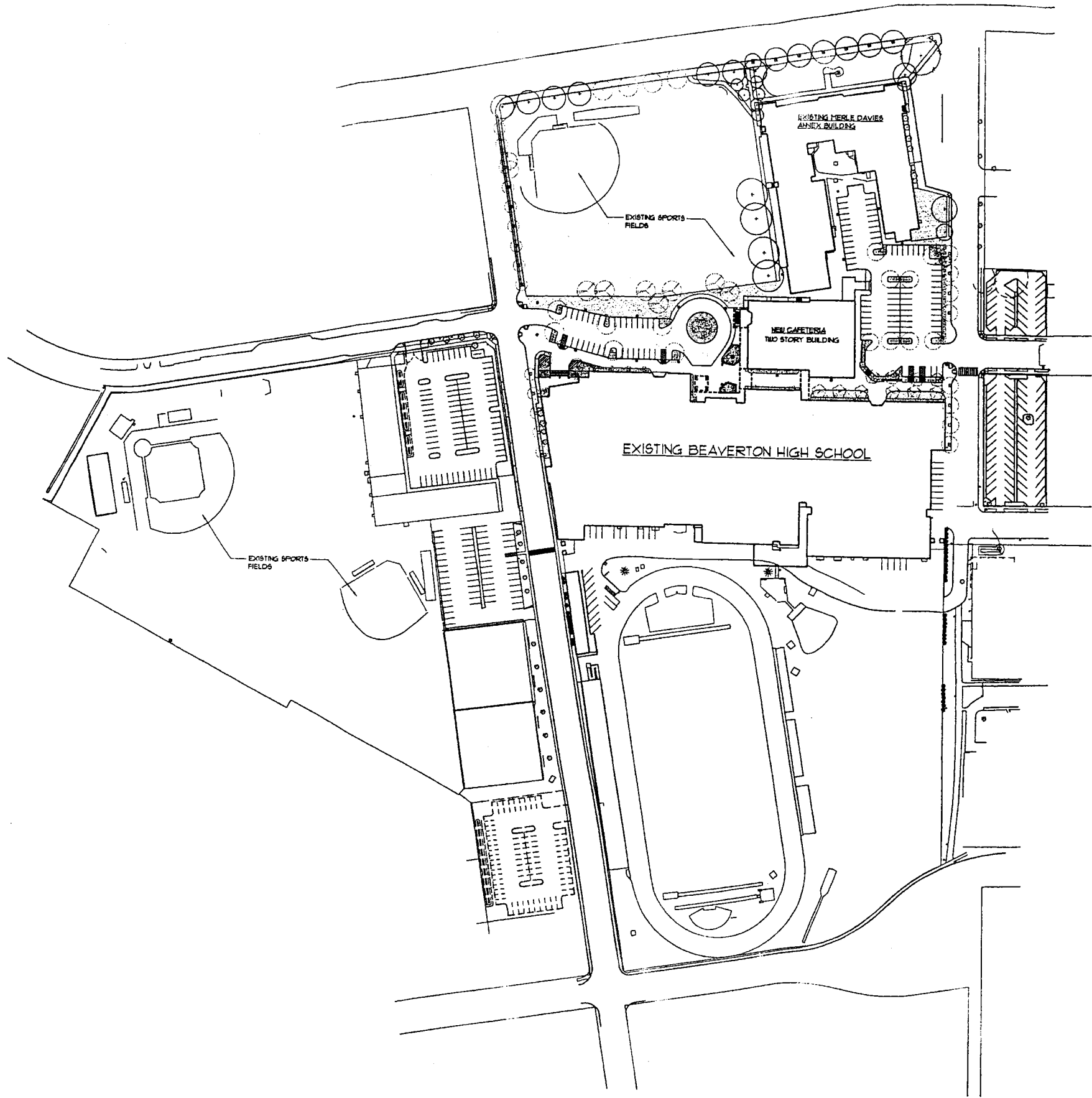
The improvements at BHS are being designed not only to provide a modern educational facility but also to provide the community accessible space through pedestrian plazas and open space. The improvements will include measures to improve compliance with the Americans with Disabilities Act (ADA), such as easily accessible parking near the school entrance and auditorium and pedestrian system enhancements. The development will include the addition of one new building to be used for expanded cafeteria facilities. The new building will be located immediately north of the existing high school structure. In recent years Beaverton High School student population has ranged from 1,800 to 2,200 students (prior to the opening of Southridge High School). The Beaverton School District expects that enrollment will not exceed 2,200 in the future. The proposed improvements will enable the Beaverton School District to effectively accommodate curriculum for this range in student population. The campus improvements are intended to enable the District to enhance the educational facilities at BHS and improve the campus environment for students and the overall community.

The cafeteria addition, renovations to the existing high school, and the new parking lots are funded improvements through a school bond measure approved by district voters. There are no other funded improvements at BHS that will require land use review at this time.

This application seeks approval of a Tree Preservation Plan. This Plan includes a proposal to remove trees designated on the City's Tree Inventory. This removal of trees is part of a larger development/expansion project for BHS as described above. The Revised Landscape and Lighting Plans, Attachments 11-14, provide mitigation for this removal by installing a variety of trees, including Japanese Maples, Ash and White Oak trees.

The following written application is in response to the Tree Preservation Plan (TPP) submittal requirements for a Narrative Statement. As a consequence of the proposed improvements at BHS, a row of existing trees adjacent to the current alignment of SW 2<sup>nd</sup> Street (Tree Preservation Plan, Attachment 18) will be removed to enable new access to the school and pedestrian streetscape improvements to be constructed. Improvements will be made to the front of BHS to enhance the pedestrian environment and to clearly identify the main entrance. A more urban streetscape will be developed along the northern face of BHS. These improvements will emphasize pedestrian and bicycle access to and through the site. A strong pedestrian and bicycle connection between SW Stott and SW Erickson will be promoted through the streetscape improvements.

File: 01151-205P.dwg  
 Plot Scale: 1" = 60'-0"  
 Plot Date: 07-23-02  
 Plot Time: 5:38 PM



ARCHITECTURE  
 PLANNING/INTERIORS

TEL (503) 274-5432  
 FAX (503) 274-0085

1953 NW Kearney St.  
 Portland, OR. 97209

PRELIMINARY

**B.H.S. CAFETERIA ADDITION**  
**BEAVERTON SCHOOL DISTRICT #48**  
 13000 S.W. SECOND ST.  
 BEAVERTON, OREGON 97005

Issued	Revisions

Date : 1-9-01  
 Job No. : 0151  
 Drawn by : EGN  
 Checked by : JDW

Sheet No.  
**2.0**

**Figure 1. Revised Site Improvement Plan**



The City of Beaverton Tree Inventory provided by the City identifies trees as Historic or Significant, using a listing of criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above (indicating a historic tree by either age or condition), see City of Beaverton, Historic Tree Inventory Sheets in original application, Appendix C. The proposed improvements at BHS will require removal of 20 trees which are designated on the City's inventory as historic. No trees designated as Significant Trees in the City's Inventory will be impacted by the proposed development.

The proposed improvements to Beaverton High School also requires submittal of the following applications:

- Street Vacation Permit Application for the vacation of SW 2<sup>nd</sup> between SW Stott and SW Erickson.
- Comprehensive Plan Amendment to remove the Neighborhood Route Designation of SW 2<sup>nd</sup> between SW Stott and SW Erickson from the City's Functional Classification Plan Map.
- Design Review Type 3 Application.
- Conditional Use Application.
- Bicycle Parking Variance-Public Hearing Application

The Street Vacation Permit Application and the Comprehensive Plan Amendment applications were submitted to the City of Beaverton on November 15, 2001. The Design Review and Conditional Use applications have been submitted concurrently with this application.

### **III. Historic Tree Removal Review Criteria**

The City of Beaverton Tree Inventory provided by the City identifies trees as Historic or Significant, using a listing of criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above (indicating a historic tree by either age or condition), see City of Beaverton, Historic Tree Inventory Sheets in original application, Appendix C. The proposed improvements at BHS will require removal of 20 trees which are designated on the City's inventory as historic. No trees designated as Significant Trees in the City's Inventory will be impacted by the proposed development.

The following table illustrates the condition and status of the trees to be retained and the trees to be removed on the site with a cross-reference to the report completed by Pruett Tree and Landscape, a licensed arborist. Most of the trees to be removed are in poor or fair condition as noted by both the City's Inventory and the arborist report.

As noted in the far right column, City Tree Inventory Rating, Table 1 and 2, the majority of the trees to be removed are rated as a "3" for "some historic value" for age and a 2 for "fair condition." None of the trees to be removed were rated as having historical value or significant historic value.

## Proposed Trees to be Preserved

### Table 1

Tree Number on Tree Plan *	Arborist Report #	Species	Tree Diameter (DBH) (in inches)	Conditions/Comments	City Tree Inventory (approximate location)	City Tree Inventory Rating***	
1	n/r**	Beech	6.0	Not inspected	C	3	3
2	n/r**	Cherry	14.0	Not inspected	C	3	3
3	n/r**	Deodar cedar	40.0	Not inspected	C	3	3
4	n/r**	Norway Maple	10.0	Not inspected	C	3	3
5	n/r**	Norway Maple	8.0	Not inspected	C	3	3
6	n/r**	Norway Maple	10.0	Not inspected	C	3	3
7	n/r**	Norway Maple	10.0	Not inspected	C	3	3
8	n/r**	Norway Maple	10.0	Not inspected	C	3	3
9	n/r**	Norway Maple	10.0	Not inspected	C	3	3
10	n/r**	Norway Maple	4.0	Not inspected	C	3	3
11	24	Cherry	18.0	Not inspected	C	3	3
12	n/r**	Pine	12.0	Not inspected	C	3	3
13	n/r**	Pine	8.0	Not inspected	C	3	3
14	n/r**	Cedar	6.0	Not inspected	C	3	3
15	n/r**	Cedar	10.0	Not inspected	C	3	3
16	n/r**	Norway Maple	26.0	Not inspected	C	3	3
17	n/r**	Norway Maple	26.0	Not inspected	C	3	3
18	28	Norway Maple	14.0	Very poor	F	3	2
19	29	Norway Maple	18.8	Poor	F	3	2
20	30	Norway Maple	16.7	Poor	F	3	2
21	31	Norway Maple	17.0	Poor	F	3	2
22	n/r**	Pine	26.0	Not inspected	E	4	2
23	n/r**	Pine	26.0	Not inspected	E	4	2
24	n/r**	Sycamore	29.0	Not inspected	G	2	3
25	n/r**	Sycamore	29.0	Not inspected	G	2	3
26	n/r**	Sycamore	29.0	Not inspected	G	2	3
27	n/r**	Sycamore	29.0	Not inspected	G	2	3

\* Tree Protection Plan, Attachment 15

\*\* n/r = Not Referenced in Arborist Report

\*\*\* City's Tree/Grove Historical Rating Sheet

Section 40.75.15.1.C.2.a (1-5) of the Beaverton Development Code contains the review criteria to be considered in an application for removal of trees designated as Historic Trees by the City. The following provides findings that demonstrate how the proposed development at BHS will meet the applicable criteria for removal.

## Section 40.75.15.1.C.2

### 2. *Historic Trees*

a. *The HRRC and Council shall approve the application if it finds the request meets at least one of the following factors:*

(1) *That removal is necessary to accommodate a new improvement, structure or remodeled structure, and no alternative exists for relocation of the improvement on the site, or that variances to setback provisions of the Development Code will not allow the tree to be saved or will cause other undesirable circumstances on the site or adjacent properties.*

**RESPONSE:** The request meets this factor. Removal of the 20 trees is the result of construction of the new cafeteria and driveway entrance at BHS. The new cafeteria building will be located immediately north of and connected to the existing BHS building on a vacated portion of SW 2<sup>nd</sup> Street. It is necessary to place the cafeteria at this location to take advantage of student circulation patterns and because the existing campus site is constrained in terms of where facility expansion can occur. There are no expansion opportunities to the east or west of the existing BHS. Expansion of facilities to the south would eliminate athletic facilities that could not be replaced on campus and would disrupt current physical plant operations.

(2) *That the tree has become a nuisance by virtue of damage to personal property or improvements, either public or private, on the subject site or adjacent sites, or that extraordinary maintenance is required to prevent damage to such improvements or property.*

**RESPONSE:** This criterion is not applicable to this application.

(3) *That the tree has lost its significance in terms of its original designation on the list due to damage from natural or accidental causes, or for some other reason it can be established that it is no longer of historic significance.*

**RESPONSE:** This criterion is not applicable to this application.

(4) *The removal of the tree is necessary to accomplish other public purposes such as installation of public utilities, street widening and similar needs and that no alternative exists to the removal of the tree without significantly increasing public costs or inconvenience or reducing safety.*

**RESPONSE:** Removal of the identified trees is necessary to widen the main entrance street to BHS and to provide sufficient space for pedestrian and bicycle connections through the campus.

As is indicated on Revised Tree Protection Plan, Attachment 18, seven trees are located in what will be the alignment of the revised main entrance to BHS.

- (5) *That the tree is unsafe to the occupants of the property, and adjacent property, or the general public.*

**RESPONSE:** This criterion is not applicable to this application.

- b. *The historic tree shall be preserved unless the applicant proves to the satisfaction of the reviewing body that removal is necessary as a result of:*

- (1) *The necessity to remove trees which pose a safety hazard to pedestrians, property or vehicular traffic or threaten to cause disruption of public service; or which pose a safety hazard to persons or buildings*

**RESPONSE:** This criterion is not applicable to this application.

- (2) *The necessity to remove diseased trees or trees weakened by age, storm, fire or other injury.*

**RESPONSE:** This criterion is not applicable to this application.

- (3) *The necessity to observe good forestry practices according to adopted City standards on the subject.*

**RESPONSE:** This criterion is not applicable to this application.

- (4) *Need for access immediately around the proposed structure for construction equipment.*

**RESPONSE:** The proposed Revised Tree Protection Plan, Attachment 18, has taken the need for access into consideration and has only illustrated the removal of trees necessary for construction. All other trees will remain on-site. Measures will be taken to ensure the existing trees' safety during construction, such as fencing and marking of trees that are near to the construction area. The Revised Tree Preservation Plan, Attachment 18 identifies the limits of disturbance. The line type has been emboldened and identified in the legend for further clarity. The approximate location of material storage during construction of the cafeteria will be the area between the new cafeteria and SW Stott Street. There are no historic or significant trees in this location.

*(5) Need for access to the building site for construction equipment.*

**RESPONSE:** The Revised Tree Preservation Plan, Attachment 18 has taken the need for access into consideration and has only illustrated the removal of trees necessary for construction. All other trees will remain on-site. Measures will be taken to ensure the existing trees safety during construction, such as fencing and marking of trees, which are near to the construction area. The Revised Tree Preservation Plan, Attachment 18 identifies the limits of disturbance. The line type has been emboldened and identified in the legend for further clarity. The approximate location of material storage during construction of the cafeteria will be the area between the new cafeteria and SW Stott Street. There are no historic or significant trees in this location.

*(6) Essential grade changes. Essential grade changes are those grade changes needed to implement standards common to standard engineering or architectural practices.*

**RESPONSE:** As shown on the Revised Grading, Storm, and Erosion Control Plan and the Grading, Utilities and Erosion Control Plans (Attachments 8-10), there are minor grade changes associated with the development of the cafeteria and associated parking. The proposed Revised Tree Preservation Plan, Attachment 18 has taken this need into consideration and has only illustrated the removal of trees necessary for construction. All other trees will remain on-site.

*(7) Surface water drainage and utility installations.*

**RESPONSE:** As shown on the Revised Tree Preservation Plan, Attachment 18, no trees will need to be removed for installation of utilities.

*(8) Location of driveways, buildings, or other permanent improvements.*

**RESPONSE:** The proposed Revised Tree Preservation Plan, Attachment 18 illustrates which trees will need to be removed for access and buildings on the site. The Plan only illustrates the removal of trees necessary for construction. All other trees will remain on-site. Measures will be taken to ensure the existing trees safety during construction, such as fencing and marking of trees that are near to the construction area.

*(9) Compliance with other ordinances or codes.*

**RESPONSE:** This criterion is not applicable to this application.

*(10) Necessity to install solar energy equipment.*

**RESPONSE:** This criterion is not applicable to this application.

#### IV. Conclusion

In conclusion, the proposed Revised Tree Preservation Plan, Attachment 18, indicates trees on the Beaverton School District site to be either preserved or removed. The Revised Landscape and Lighting Plan Public Improvements and Landscape and Lighting Plan Private Improvements, Attachments 11-14, indicate that the applicant has proposed mitigation for the removal of trees by planting seven White Oak trees. White Oak trees were designated by the City of Beaverton as a desired species for creating a landmark tree according to the Pre-application notes, dated October 9, 2001.

Revised  
ATTACHMENTS

1. COVER SHEET
2. EXISTING CONDITIONS PLAN
3. DIMENSION PLAN PUBLIC IMPROVEMENTS
4. DIMENSION PLAN PRIVATE IMPROVEMENTS (WEST)
5. DIMENSION PLAN PRIVATE IMPROVEMENTS (EAST)
6. GRADING, STORM AND EROSION CONTROL PUBLIC IMPROVEMENTS
7. GRADING, UTILITIES AND EROSION CONTROL PRIVATE IMPROVEMENTS (WEST)
8. GRADING, UTILITIES AND EROSION CONTROL PRIVATE IMPROVEMENTS (EAST)
9. LANDSCAPE AND LIGHTING PLAN PUBLIC IMPROVEMENTS
10. LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (WEST)
11. LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (EAST)
12. DETAIL SHEET FOR WATER QUALITY CONSTRUCTION
13. DETAIL SHEET FOR WATER QUALITY CONSTRUCTION
14. DETAIL SHEET FOR WATER QUALITY CONSTRUCTION AND BIKE RACK DETAIL
15. TREE PROTECTION PLAN

## APPENDICES

APPENDIX A  
APPLICATION FOR DEVELOPMENT REVIEW

APPENDIX C  
CITY OF BEAVERTON TREE INVENTORY

APPENDIX B  
TREE PRESERVATION APPLICATION

APPENDIX D  
ARBORIST REPORT, PRUETT TREE AND  
LANDSCAPE



**APPENDIX A**



CITY OF BEAVERTON  
DEVELOPMENT SERVICES DIVISION  
4755 S.W. GRIFFITH DRIVE  
P.O. BOX 4755  
BEAVERTON, OR 97076  
PHONE: (503) 526-2420

# APPLICATION FOR DEVELOPMENT REVIEW

FILE NAME: _____	SUBMIT: _____ REVIEW: _____
TYPE: _____	RESUBMIT: _____ REVIEW: _____
FILE NUMBERS: _____	APPLICATION MATERIAL RECEIVED: _____
	FEE RECEIVED: _____ FEE PAID: \$ _____
	CHECK #: _____ RECEIPT #: _____
	COMPLETENESS DATE: _____

**PRINT OR TYPE ALL INFORMATION BELOW**

APPLICANT  USE MAILING ADDRESS FOR MEETING NOTIFICATION

YOUR NAME	PHONE #
Frank Angelo	503-227-3664
BUSINESS NAME	FAX #
Angelo Eaton & Assoc.	503-227-3679
ADDRESS	SUITE
620 SW Main Suite 201	
CITY, STATE	ZIP
Portland, Oregon	97205
SIGNATURE (ORIGINAL REQUIRED)	DATE
<i>Frank Angelo</i>	12/18/01

**PROPERTY/ZONING DATA**

13000 SW 2nd  
Address

Beaverton High School  
Location Description

R-10

CONSTRUCTION EXPECTED TO BEGIN: \_\_\_\_\_

MAP & TAX LOT (list one per line)	DO NOT USE LOT & BLOCK	SITE ACRES	ZONING	
			EXISTING	PROPOSED
15116AD - 11100		6.15	R10	R10
15116AD - 11000		.13	R10	R10
15116AD - 10900		11.41	R10	R10
TOTAL GROSS SITE:		ACRES	SQ. FT.	
		17.69	770,576.40	
TOTAL NET SITE: (GROSS SITE MINUS DRIVEWAYS & P.O.W.)				

PROPERTY OWNER  Attach separate sheet if more than one owner

YOUR NAME	PHONE #
Mike Maloney	503-591-4303
BUSINESS NAME	FAX #
Beaverton School Dist.	503-591-4484
ADDRESS	SUITE
16550 SW Merlo Rd	
CITY, STATE	ZIP
Beaverton OR	97006-5152
SIGNATURE (ORIGINAL REQUIRED)	DATE
<i>Mike Maloney</i>	2/15/01

**COMMERCIAL/INDUSTRIAL DEVELOPMENT**

Public High School  
Use or Development Description

ARCHITECT/DESIGNER

YOUR NAME	PHONE #
Russ Pitkin/Eric Nielsen	503-274-5432
BUSINESS NAME	FAX #
LSW Architecture	503-274-0085
ADDRESS	SUITE
1953 NW Kearny	
CITY, STATE	ZIP
Portland OR	97209

BUILDING USE PROPOSED USES (LIST ONE PER LINE)	BLDG. AREA		PARKING # SPACES REQ/PROP
	GROSS SQ. FT.	% OF BLDG.	
Cafeteria			/
Parking Lot			/
			/
			/
TOTAL BUILDING AREA:		100%	/

LANDSCAPE ARCHITECT

YOUR NAME	PHONE #
Ron Matela	360-695-0776
BUSINESS NAME	FAX #
Matela Associates	360-695-0776
ADDRESS	SUITE
601 Main St #201	
CITY, STATE	ZIP
Vancouver WA	98660

**SUBDIVISION/LAND PARTITION**

# LOTS PROPOSED	PARTITION LOT SIZES IN SQUARE FEET.		
	A	B	C

**MULTI-FAMILY/P.U.D. DEVELOPMENT**

ENGINEER/SURVEYOR

YOUR NAME	PHONE #
Gary Alfson	503-221-1131
BUSINESS NAME	FAX #
Harper Houf Righellis	503-221-1171
ADDRESS	SUITE
5200 SW Macadam Suite 580	
CITY, STATE	ZIP
Portland OR	97201

TOTAL BUILDING SPACE	GR. SQ. FT. / % SITE	# UNITS	
SITE AREA PER UNIT	/	PARKING SPACES	
TOTAL OPEN SPACE	/	# COVERED	
OPEN SPACE PER UNIT	/	# UNCOVERED	
BUILDING COVERAGE	/	TOTAL SPACES	
DRIVEWAY AREA	/	RV STORAGE YES <input type="checkbox"/> NO <input type="checkbox"/>	
PARKING AREA	/	# SPACES	
	FEET/STORIES		
	/		
MAXIMUM BLDG HEIGHT			
UNITS PER BEDROOM NO	S	1	2
ATTACHED UNITS	RENTAL <input type="checkbox"/>	CONDO <input type="checkbox"/>	LOT OWNERSHIP <input type="checkbox"/>

NOTE: ACCEPTABLE SUBMITTAL  
MAXIMUM SHEET SIZE 24 X 36  
FOLDED TO FIT LEGAL SIZE  
ASSEMBLED INTO SETS

**APPENDIX B**



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97078 General Information (503) 526-2222 V/TDD

## TREE PRESERVATION PLAN (TPP) Submittal Requirements

The Tree Preservation Ordinance applies to individual trees or groups of trees that are important because of their age, size, beauty, or habitat value. If your property contains trees that have been designated as *significant* or *historic*, a plan for their protection must be prepared prior to any development or construction which might affect the health of the trees. This plan will be reviewed at a public hearing by the Planning Commission or the Board of Design Review to ensure that significant or historic trees are adequately protected. The Tree Preservation Ordinance is part of the *City of Beaverton Development Code*. Section 40.75 contains the specific standards for tree preservation. Below is additional information that may help you prepare a successful Tree Preservation Plan. Staff recommends that an arborist, biologist, or other qualified professional be responsible for submitting applications for Tree Preservation Plans.

The plan should be drawn to the same scale as the site plan submitted for review. The required sheet size is 24" x 36" at a scale of 1" = 20', and should contain the following:

### Items to be included on the 24" x 36" PLAN:

1. Specific trees species.
2. Diameter measured at 54" above natural grade for all trees 6" diameter and larger.
3. Drip line canopy of each tree or entire grove.
4. Existing topography and proposed grading in intervals of 2 feet of elevation.
5. Existing and proposed utility locations and construction easements.
6. Location of existing and proposed streets, proposed grading, and easements necessary for street construction.
7. Clear delineation of construction disturbance limits.
8. (If applicable): Boundaries of delineated jurisdictional wetlands and surface water quality sensitive areas as defined by Unified Sewerage Agency standards.

### Items to be included in the NARRATIVE STATEMENT:

#### For all trees and groves:

1. List of the various species and the number of trees included in each species.
2. Range of sizes over 6" in diameter.
3. Analysis of the health of the trees, specifically addressing each tree or grove of trees.

#### For trees to be preserved:

5. Methods that will ensure the survival during and after construction.

#### For trees to be removed:

6. A written evaluation of each of the removal criteria in Section 40.75.15.1.C.3 of the *City of Beaverton Development Code*. Trees shall be preserved unless the applicant proves to the satisfaction of the reviewing body that removal of the trees is necessary for one or more of the reasons noted in Section 40.75.15.1.C.3.

**APPENDIX C**

BEAVERTON TREE INVENTORY

ADDRESS: STOTT C 2nd  
\_\_\_\_\_  
\_\_\_\_\_

CROSS STREETS: SW STOTT C 2nd NE Corner  
\_\_\_\_\_

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
(DBH: Diameter at breast height)  
Formula for DBH

Diameter = Circumference divided by pi (3.1416)

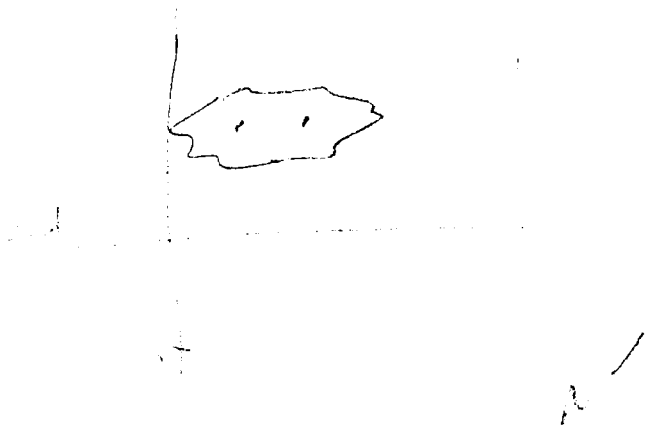
Species:

- 1. 2 OAKS
- 2.
- 3.
- 4.
- 5.
- 6.
- 7.
- 8.
- 9.
- 10.
- 11.
- 12.
- 13.
- 14.

Groves - number of trees 2 Average DBH 1-25" - 30"

Number of Street Trees  
Define street trees

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
TREE RATING SHEET

RATING SCALE:

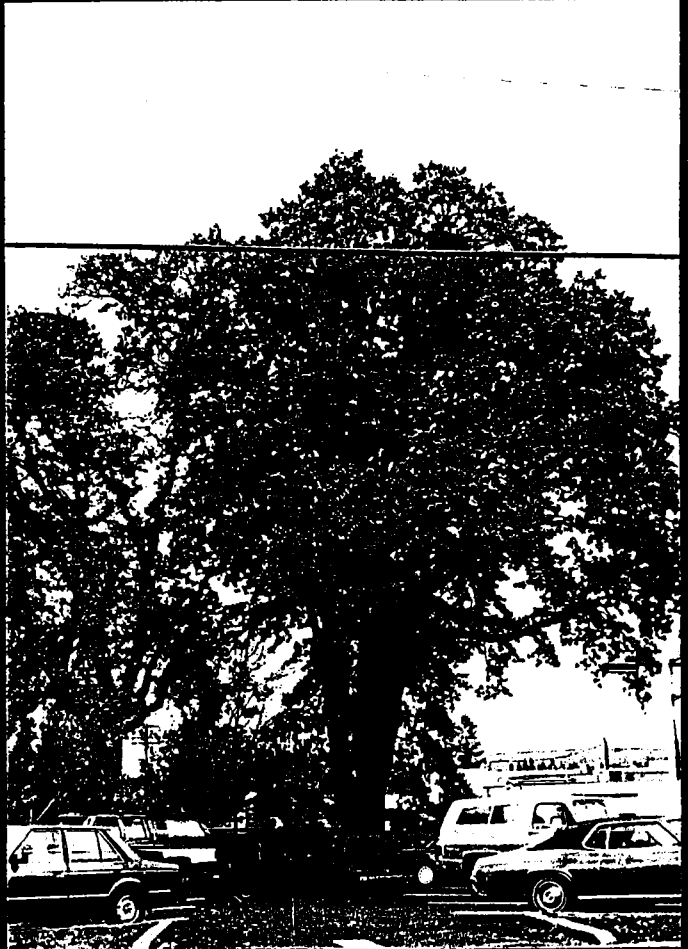
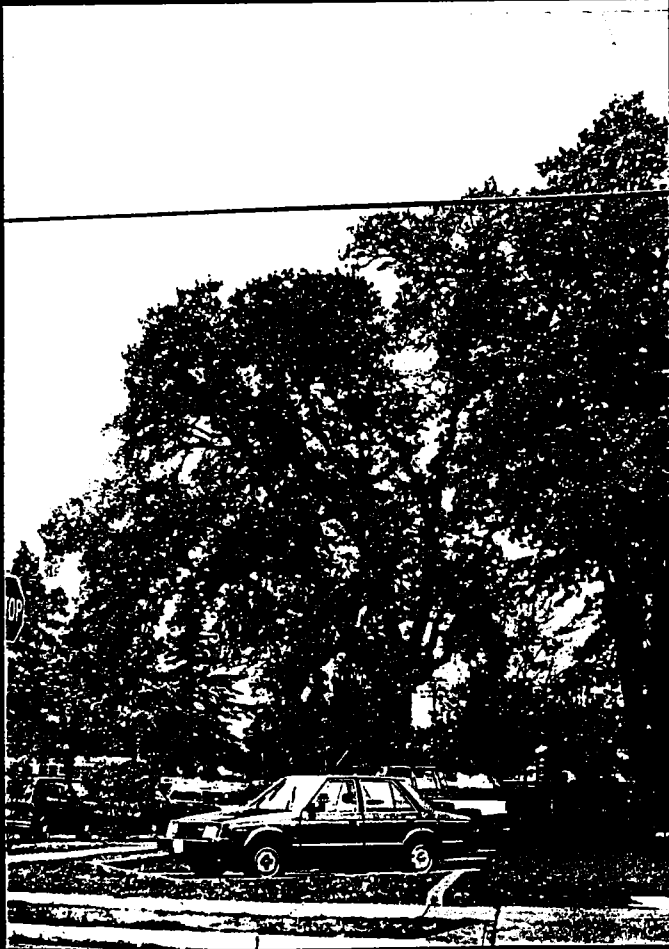
- 1. - No significant value
- 2. - Little significant value
- 3. - Some significant value
- 4. - Has significant value
- 5. - Has a very significant value

CONDITION ASSESSMENT:

- 1. - Poor condition  
Unhealthy with parts dead or dying
- 2. - Fair condition - OK health-  
but needing care
- 3. - Good condition - Healthy -  
(May still need care)

LOCATION 142nd West St. V

TREE ANALYSIS		RATING	RATING	RATING
1) Tree has a distinctive	Size	_____	_____	_____
Tree Species <sup>2</sup> <u>DOCK</u>	Shape	_____	_____	_____
<u>1 Small white flower</u>	Location	<u>3</u>	_____	_____
Comments: _____				
_____				
2) Tree has exceptional beauty		<u>3</u>	_____	_____
Comments: <u>Very nice</u>				
_____				
3) Tree has a functional or aesthetic relationship to a natural resource		<u>1</u>	_____	_____
Comments: _____				
_____				
4) Tree's Condition	Poor	_____	_____	_____
	Fair	<u>2/3</u>	_____	_____
	Good	_____	_____	_____
Comments: _____				
_____				
_____				
TOTAL		<u>9/10</u>	_____	_____





BEAVERTON TREE INVENTORY

ADDRESS: 13000 Farmington  
 \_\_\_\_\_  
 \_\_\_\_\_

CROSS STREETS: West side of 57th  
 \_\_\_\_\_

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
 (DBH: Diameter at breast height)  
 Formula for DBH

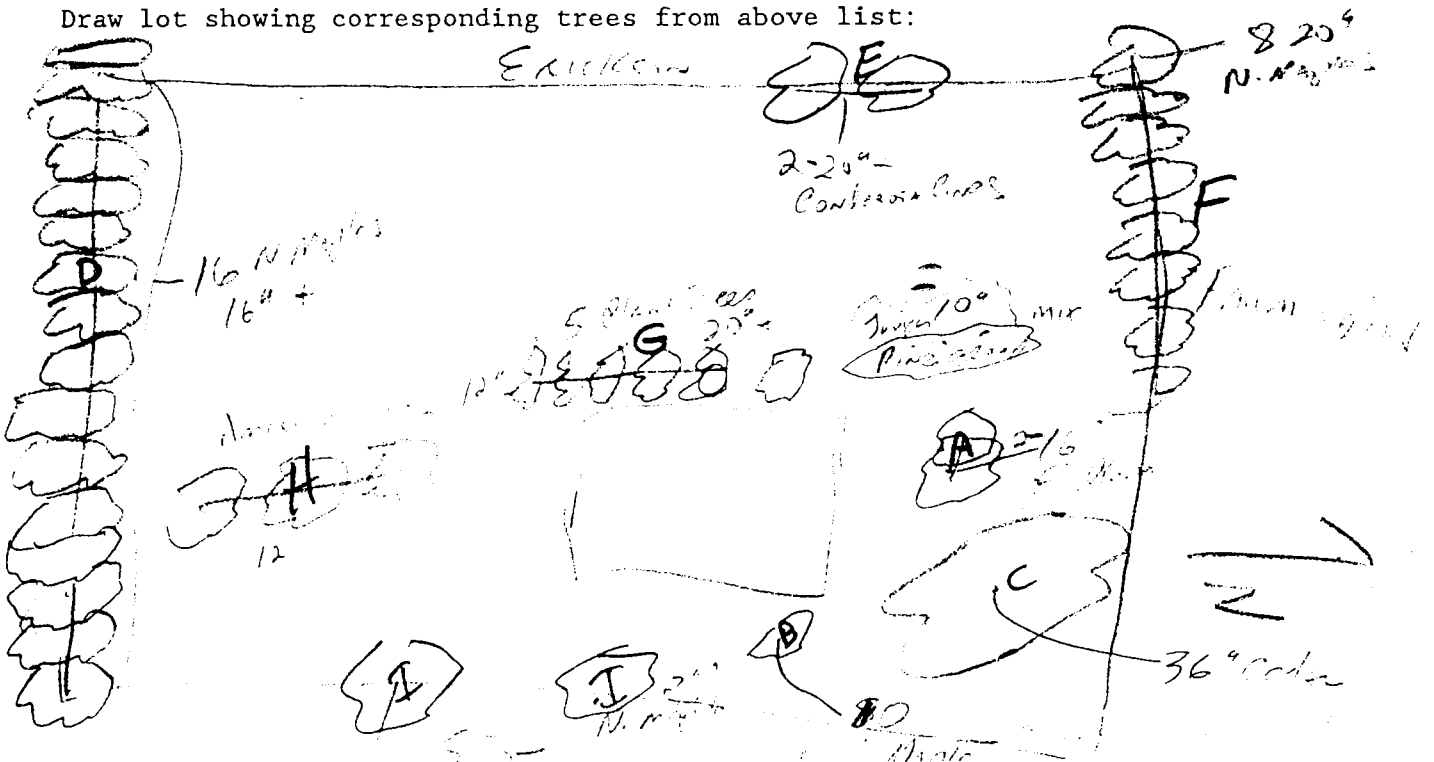
Diameter = Circumference divided by pi (3.1416)

Species:

- 1. All Potential
- 2. \_\_\_\_\_
- 3. \_\_\_\_\_
- 4. \_\_\_\_\_
- 5. \_\_\_\_\_
- 6. \_\_\_\_\_
- 7. \_\_\_\_\_
- 8. \_\_\_\_\_
- 9. \_\_\_\_\_
- 10. \_\_\_\_\_
- 11. \_\_\_\_\_
- 12. \_\_\_\_\_
- 13. \_\_\_\_\_
- 14. \_\_\_\_\_

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_  
 Number of Street Trees \_\_\_\_\_  
 Define street trees \_\_\_\_\_

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

1. - No historical value.  
Age 20 years or under.
2. - Little historical value.  
Age 20 years or over.
3. - Some historical value.  
Age 50 years or over...
4. - Has historical value  
Age 80 years or over.
5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

1. - Poor condition  
Unhealthy with parts dead  
or dying.
2. - Fair condition - OK health  
but needing care.
3. - Good condition - Healthy.  
(May still need care)

	<u>AGE</u>	<u>CONDITION</u>
I	3	2
H	3	2
A	2	3
B	1	3
C	3	3
D	3	2
E	4	2
F	3	2
G	2 (30yrs+)	3
	<u>RATING</u>	<u>RATING</u>

LOCATION 13000 Farmington

TREE/GROVE HISTORICAL ANALYSIS:

- 1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

Comments: Arbor day / Earth day circa 1986

Member Davis early suggested teacher  
in Beaverton landscaping of an  
age that it was probably planted  
with original structure

- 2) Tree/Grove association with general growth and development of the City.

Comments: Trees are associated with  
structure of historic importance  
in Beaverton

TOTAL

<u>RATING</u>	<u>RATING</u>	<u>RATING</u>
<u>3</u>	---	---
<u>3</u>	---	---
<u>6</u>	---	---

**APPENDIX D**

May 16, 1999

Mr. Justin Miller  
LSW Architects, P. C.  
825 NW Glisan Street  
Portland, OR 97209

Dear Mr. Miller:

From the ground, I visually inspected 31 trees on May 12 at the Beaverton High School at 13000 SW Second Avenue, Beaverton. These trees are numbered on the accompanying site drawing which is a reduced copy of a portion of the drawing you gave me. The evaluation of specific trees is structured by these numbers in ascending order.

This is only a preliminary report for individual trees which require further in-depth investigation. Some of these trees offer indications requiring aerial inspection, increment boring or root crown examination to rule out potential unacceptable risks. They are discussed later in this section of the report, after the trees clearly posing unacceptable risks.

#### ELECTRICAL HAZARD

There is evidence that nonprofessionals have been climbing ten (10) of the trees located below primary electrical conductors. Notify the owner/operator (PGE?) of this, and require a safety reassessment of the site.

It appears that the primary conductors in the first span from SW Erickson Street have been spliced in twelve places. These splices

Cory to Miller, 11/18/01

suggest a history of downed service. Contact with the chain link fence below could energize the entire length of fence.

Ingredients for electrical shock, even electrocution, exist at this site. It is required that the possibilities, however minimal, be eliminated.

Trees, #1- #16, appear from the ground to be approximately ten (10) feet from the primaries, but they are in direct contact with secondaries and telephone equipment. Oregon OSHA requires that only a qualified electrical line clearance trimmer with a second person qualified in line-clearance tree-trimming within normal voice communication can perform work on these trees.

#### CLEAR RISK OF TRUNK OR LARGE BRANCH FAILURE

Both sycamores (#17 and #18) should be pruned for crown restoration as soon as possible. They have been "topped" in the past and partially restored, however, there remain crowded, poorly attached, crossed and grafted branches above the asphalt concrete sidewalk. Both these trees are near aerial conductors (probably telephone equipment), which also are at risk.

The elm, #19, should be cabled with through bolts in its major branches connected by extra high strength cable in a "box" pattern at about thirty-five (35) feet above grade. This tree is going to split apart. Cabling will not prevent that, but it will help to control the failure.

Tree #21, a Norway maple, recently lost a buttress root so large on its east side, that it must have a thorough root crown examination as part of an overall risk assessment. If the risk assessment cannot be performed immediately, this tree should be removed.

Another Norway maple, #12, also requires a root crown examination – the prolific growth of fungal fruiting bodies at the base of this tree suggests that it could pose an unacceptable risk of failure of the entire tree. This examination requires hand excavation. If the root crown examination proves unremarkable, then an aerial inspection of another site of probable decay in the tree must be made.

Norway maples, #25 and #30, both on SW Farmington Street, have dead large branches posing an unacceptable risk to pedestrian and/or vehicular traffic. This risk can be reduced to an acceptable level by pruning.

### POSSIBLE UNACCEPTABLE RISKS

Elm tree, #20, has a major leader with apparent decay which should be aurally inspected. It is positioned over the parking lot.

Norway maples, #13, #26, #28, and #29, all have indications of decay and should be further examined by increment boring and/or aerial inspection. Norway maple, #23, should receive a root crown examination because of recent sidewalk construction.

### GENERAL OBSERVATIONS

The trees at this site designated for evaluation are one (1) cherry, one (1) sugar maple, two (2) elms, two (2) sycamores, and twenty-five (25) Norway maples. They range in size from 6.8-inch DBH<sup>1</sup> (sugar maple) to 29.0-inch DBH (a sycamore).

The average health of the trees in this report would be rated as "Poor." Thirteen (13) trees were rated "Fair," fifteen (15) trees were rated "Poor," and three (3) were rated as "Very poor."

All of the trees rated "Very poor" should be removed, and two (2) rated as "Poor," should also be removed. These trees are all Norway maples and their numbers are #8, #9, #10, #11, and #27. All of the trees selected to remain should be pruned. As they will all suffer to some degree from proposed construction activity, it is recommended that pruning be limited to crown cleaning of dead wood only, except in the cases where unacceptable risks must be mitigated.

The trees, #1-#16, Norway maples, are positioned under primary electrical conductors, as mentioned above, and will never be allowed to conform to their natural growth habit. It is a situation of the wrong

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<sup>1</sup> Diameter at breast height, or DBH, is a commonly used, standard measure of the diameter of a tree at 54 inches above grade.

tree in the wrong place. They will be pruned at one to two year intervals to keep them away from the power lines. Four (4) of them are dying or in such serious decline that they should be removed. Although there other contributing factors to their sad condition, these other conditions exist elsewhere on the property where trees of the same species are in relatively better condition.

The Norway maple ranks very high as a "street" tree. Its presence as the predominant species on the property and in the neighborhood, however, should not be augmented by future replacement plantings. There is a strong suggestion of the presence of verticillium wilt, tree #9 possibly being the epidemiologic "index" tree, and its immediate neighbors the second generation to succumb. Verticillium is soilborne, and vectored by roots. Replacement species should be resistant to verticillium.

Herbicide contact with trees, or any part of them, including surface roots, can have cumulative negative effects on their health. It can even kill them, regardless of some manufacturer's claims. It is recommended that another method of removing grass from the root zones be implemented. Bark mulch applications, or weeding by hand are two possibilities.

An effective plant health program should be implemented to control harmful insects, especially aphids.

### SPECIFIC OBSERVATIONS & CONDITION RATINGS

The condition ratings for the trees were determined by the following rating factors.

Rate each of the five factors below from 1 to 5. High numbers indicate a high rating.

EXAMPLE:	No problem <sup>2</sup>	5
	No apparent problem	4
	Minor problem	3
	Major problem	2
	Extreme problem	0

Factors:

1. Roots
2. Trunk
3. Scaffold Branches
4. Smaller Branches & Twigs
5. Foliage

Total points 23 to 25	Condition Excellent
Total points 19 to 22	Condition Good
Total points 15 to 18	Condition Fair
Total points 11 to 14	Condition Poor
Total points 5 to 10	Condition Very Poor

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<sup>2</sup> This rating was not used in this evaluation was by visual inspection only. There was no excavation or taking of samples or specimens for laboratory pathology examination.



1. Norway maple (*Acer platanoides*), DBH 16.1 inches. Damage to sidewalk. Heavy pedestrian traffic in root zone – fence gate, trash can and portable toilet within drip line. Concrete fence-post anchor next to buttress. Surface roots. No grass or other vegetation within 3-4 feet of base – just bare soil and gravel. Dead branches to 3 inches in diameter. Aphids. Energized primary electrical conductors above tree. Condition rating: 15 (Fair).
2. Norway maple, DBH 16.6 inches. Damage to sidewalk. Surface roots with old wounds. Concrete fence-post anchor next to buttress. No grass or other vegetation within 3-4 feet of base – just bare soil and gravel. Significant amount of dead branches – to 3 inches in diameter. Sparse canopy. Aphids. Energized primary electrical conductors above tree. Condition rating: 12 (Poor).
3. Norway maple, DBH 19.0 inches. Damage to sidewalk. Some pedestrian traffic – tree is a “climber.” Surface roots – “scalped” surface root in lawn area. Concrete fence-post anchor next to buttress. No grass or other vegetation within 4-5 feet of base – just bare soil and gravel. Nail in trunk. Dead branches to 3 inches in diameter. Aphids and, perhaps, leaf miners. Energized primary electrical conductors above tree. Condition rating: 12 (Poor).
4. Norway maple, DBH 20.4 inches. Damage to sidewalk. Some pedestrian traffic – tree is a “climber.” Surface roots. No grass or other vegetation within 3-4 feet of base – just bare soil and gravel. Dead branches to 5 inches in diameter. Sparse canopy. Energized primary electrical conductors above tree. Condition rating: 14 (Poor).

5. Norway maple, DBH 18.3 inches. Damage to sidewalk. Some pedestrian traffic – tree is a “climber.” Surface roots. No grass or other vegetation within 4-5 feet of base – just bare soil and gravel. Two longitudinal trunk wounds (6 and 7 feet long) which have compartmentalized (old verticillium cankers?). Dead branches to one inch in diameter. Sparse canopy. Energized primary electrical conductors above tree. Condition rating: 16 (Fair).
6. Norway maple, DBH 21.7 inches. Damage to sidewalk. Some pedestrian traffic – tree is a “climber.” Surface roots with old wounds. No grass or other vegetation within 6-8 feet of base – just bare soil and gravel. Possible fungal fruiting bodies on snag. Major crossed laterals. Much decay. Dead branches to 6 inches in diameter. Energized primary electrical conductors above tree. Condition rating: 14 (Poor).
7. Norway maple, DBH 15.8 inches. Damage to sidewalk. Some pedestrian traffic – tree is a “climber.” Surface roots. No grass or other vegetation within 3-5 feet of base – just bare soil and gravel. Possible canker (high). Dead branches to one inch in diameter. Many aphids. Energized primary electrical conductors above tree. Condition rating: 16 (Fair).
8. Norway maple, DBH 16.9 inches. Damage to sidewalk. Some pedestrian traffic – tree is a “climber.” Surface roots. No grass or other vegetation within 2-4 feet of base – just bare soil and gravel. Evidence of herbicide use on nearby grass. Old buttress wound. Decay in main crotch. Large (approximately 10 inches in diameter) decayed branch appears unlikely to continue to support its subsidiary branches. Dead branches to 5 inches in diameter. Only about 40 percent canopy remains. Foliage small and chewed by insects, and is locally chlorotic. Energized primary electrical conductors above tree. Tree is in decline. Condition rating: 11 (Poor).

9. Norway maple, DBH 15.7 inches. Surface roots. No grass or other vegetation within 2-3 feet of base – just bare soil and gravel. Evidence of herbicide use on nearby grass. Large plates of exfoliated bark, revealing stains typical of verticillium wilt. Dead branches to 10 inches in diameter. Less than 5 percent canopy remains. Energized primary electrical conductors above tree. Tree is dying. Condition rating: 6 (Very poor).
10. Norway maple, DBH 18.3 inches. Surface roots. No grass or other vegetation within 2-3 feet of base – just bare soil and gravel. Evidence of herbicide use on nearby grass. Old Amtrol ant poison bottle imbedded in soil near base (removed). Dead branches to 6 inches in diameter. About 10 percent of canopy remains. Energized primary electrical conductors above tree. Tree is dying. Condition rating: 8 (Very poor).
11. Norway maple, DBH 19.1 inches. Damage to sidewalks (concrete and asphalt sidewalks join in root zone). Some pedestrian traffic – tree is a “climber.” Surface roots. Concrete post anchor next to buttress. Dead branches to 4 inches in diameter. About 60 percent of canopy remains. Energized primary electrical conductors above tree. Condition rating: 12 (Poor).
12. Norway maple, DBH 18.1 inches. Surface roots. No grass or other vegetation within 3-4 feet of base – just bare soil and gravel. Evidence of herbicide use on nearby grass. Fungal fruiting bodies (mushrooms) at base. Probable decay at 10 feet at site of major pruning cut. Sparse canopy. Energized primary electrical conductors above tree. Condition rating: 14 (Poor).
13. Norway maple, DBH 15.5 inches. Some pedestrian traffic – tree is a “climber.” Surface roots. Irregularly shaped area of bare soil at base includes anchor spot for light-pole guy. Evidence of herbicide usage on grass. Trunk cavity with decay at site of old pruning cut. Dead branches to 4 inches. Localized sparse canopy. Energized primary electrical conductors above tree. Condition rating: 15 (Fair).

14. Norway maple, DBH 20.0 inches. Some pedestrian traffic – tree is a “climber.” Surface roots – one is a large girdling root with mechanical damage. Concrete fence post anchor at buttress. No grass or other vegetation within 3 feet of base – just bare soil and gravel. Evidence of herbicide use on nearby grass. Some pedestrian traffic – tree is a “climber.” Dead branches to 4 inches in diameter. Epicormic shoots. Energized primary electrical conductors above tree. Tree is in distress. Condition rating: 13 (Poor).
15. Norway maple, DBH 19.3 inches. Some pedestrian traffic – tree is a “climber.” Damage to asphalt concrete parking lot. Energized primary electrical conductors above tree. Condition rating: 17 (Fair)
16. Norway maple, DBH 18.2 inches. Surface roots. Pruning event other than electrical line clearing, left “stubs.” The size of adventitious shoots at pruning cuts suggest pruning was more than a year ago (before the last growth season). Dead branches to 5 inches in diameter. Energized primary electrical conductors above tree. Condition rating: 15 (Fair).
17. Sycamore (*Platanus X acerifolia*), DBH 28.6 inches. Surface roots – scalped. Damage to asphalt concrete sidewalk. No grass or other vegetation within 4 feet of base – just bare soil and small quantity of bark chips. Evidence of herbicide use on nearby grass. Tree has been “topped,” and had a first phase crown restoration in the past. Branches forming at the sites of old heading cuts, however, are still too crowded and poorly attached in some cases. Some of these branches are above the sidewalk and pose an unacceptable risk. Energized aerial secondary electrical conductor near tree. Condition rating: 17 (Fair).

Cory to Miller, 11/16/01

18. Sycamore (*Platanus X acerifolia*), DBH 29.0 inches. Very large surface roots – scalped. Damage to asphalt concrete sidewalk. No grass or other vegetation within 4 feet of base – just bare soil. Evidence of herbicide use on nearby grass. Crossed and grafted lateral branches over sidewalk pose an unacceptable risk. This tree also has been topped and partially restored, but still has crowded and, in some cases, poorly attached branches. Some of these branches are over the sidewalk and pose an unacceptable risk. Canopy has limited growth space because it is crowded by neighboring trees. Energized aerial secondary electrical conductors near tree. Condition rating: 15 (Fair).
19. Elm (*Ulmus pumila*), DBH 20.0 inches. Adequate root space, however, cars are parking in the root zone, compacting the soil and killing roots. Trunk formed by two codominants which join at too narrow an angle. There is included wood at the crotch of the codominants. There is a bacterial flux from a large pruning cut. There are dead branches to 3 inches in diameter. Condition rating: 13 (Poor).
20. Elm (*Ulmus pumila*), DBH 17.9 inches. Adequate root space, however, cars are parking in the root zone, compacting the soil and killing roots. Damage to asphalt concrete parking lot. There is one major leader with decay over the parking lot which may pose an unacceptable risk. Many snags. Dead branches to 3 inches in diameter. Condition rating: 17 (Fair).
21. Norway maple, DBH 25.4 inches. There has been recent sidewalk construction activity in the root zone of this tree, and bark mulch has recently been spread (both sidewalk and bark mulch appear “new” – with no observable oxidation). In an inset of the newly constructed section of sidewalk, there is visible the severed end of a buttress root at least 10 inches in diameter. This cut was made since the last growing season. It is highly likely that other roots were crushed, broken or severed at the same time. Tree very probably represents unacceptable risk potential.

There is a compartmentalized wound (stress crack? old verticillium canker?) in a very large leader (10-inch diameter) over the sidewalk. Dead wood to 7 inches in diameter. Sparse canopy – about 75 percent remains. There is one localized area of very dense foliage, but it is not a witches' broom. Because of recent punishing events in this tree's life, its present condition rating is likely to change negatively over the next year. Condition rating: 13 (Poor).

22. Sugar maple (*Acer saccharum*), DBH 6.8 inches. Root zone appears to have been hoed, actually reducing the grade in some areas. Evidence of herbicide use on nearby grass. Mechanical damage to the trunk at 18 and 50 inches above grade (Damage occurred prior to last growing season). Tree did not fruit this year. Heavy aphid infestation. Condition rating: 18 (Fair).
23. Norway maple, DBH 17.7 inches. Root zone appears recently disturbed – broken roots sticking up out of the soil. New sidewalk construction in root zone. Evidence of herbicide use on nearby grass. There is some decay in a major leader at 13 feet above grade. Dead branches to 2 inches. There are “cupped” leaves localized between 10 and 12 feet immediately above the sidewalk. It is probable that these leaves were exposed to herbicide. Condition rating: 16 (Fair).
24. Cherry (*Prunus avium*), DBH 14.0 inches. Surface roots with suckers. Tree has poor form – two 3-inch laterals attached very low to 14-inch trunk – which may take repeated pruning to correct. There have been heading cuts made to the tree, leaving stubs, some of which are developing adventitious shoots. There is insect damage (chewing) to the foliage. Condition rating: 14 (Poor).

25. Norway maple, DBH 22.9 inches. Damage to sidewalk. Bark borers. Six-inch diameter snag. Dead branches to 6 inches in diameter. Some smaller branches separated and hanging in canopy. One large dead branch, 6 inches at the trunk, crossing the sidewalk, and tapering to 3 inches over SW Farmington Street clearly poses an unacceptable risk to vehicular and pedestrian traffic. Canopy space is limited by a nearby cedar. Condition rating: 17 (Fair).
26. Norway maple, DBH 23.6 inches. Surface roots – some scalped. Approximately 40 nails in the trunk. Evidence of decay in trunk of unknown extent. Snags. Truck damage to branches over SW Farmington Street. Dead branches to one inch diameter. There is some die-back in the canopy top. Foliage is chlorotic. Condition rating: 14 (Poor).
27. Norway maple, DBH 15.3 inches. This tree is a total loss and should be removed. The main stem (trunk) is broken off. There is serious decay in what remains of the trunk. Condition rating: 9 (Very poor).
28. Norway maple, DBH 13.1 inches. Thick and long surface roots. Damage to sidewalk. Cracked bark in a major leader with wet wood in the cavity. Very recent mechanical injury to buttress bark. Condition rating: 15 (Minimally fair).
29. Norway maple, DBH 18.6 inches. Very large population of surface roots – scalped and decayed. Evidence of herbicide use on nearby grass. Damage to sidewalk. Possible main stem decay (requires aerial inspection). Snags. Dead branches to one inch in diameter. Epicormic shoots. Sparse canopy. Tree is in distress. Condition rating: 13 (Poor).
30. Norway maple, DBH 16.7 inches. Very large surface roots – scalped. Evidence of herbicide use on nearby grass. "Pea" gravel scattered in root zone. Dead branches to 4 inches over sidewalk pose an unacceptable risk. There are branches of 3 inches diameter losing bark. Condition rating: 12 (Poor).

31. Norway maple, DBH 17.0 inches. Surface roots – scalped. Damage to sidewalk. Evidence of herbicide use on nearby grass. There is an extraordinarily large number of nails in the trunk of this tree. It is located on the corner of Erickson and Farmington, outside the fence – convenient for the posting of notices. There are crossed major leaders. Dead branches to 5 inches in diameter. Foliage is small, chlorotic, and infested with aphids. Condition rating: 11 (Poor).

Please call me if you have any questions.

Yours for a greener northwest,  
PRUETT TREE AND LANDSCAPE

David R. Cory,  
Consulting Arborist, WC1403  
American Society of Consulting Arborists



620 SW Main, Suite 201  
Portland, Oregon 97205Tel: 503 . 224 . 6974  
Fax: 503 . 227 . 3679

August 28, 2001

**RE: NEIGHBORHOOD REVIEW MEETING  
PROPOSED DEVELOPMENT AT BEAVERTON HIGH SCHOOL**

Dear Resident:

Angelo Eaton & Associates is representing the Beaverton School District (BSD), the owner of the Beaverton High School (BHS) property located at 13000 SW Second Street. The BSD is considering the expansion of the BHS campus via the proposed construction of a new school building north of the existing BHS building. The new building would include 34 classrooms and a cafeteria. The property involved with this proposed development is located both north and south of SW Second Street between SW Erickson Avenue and SW Stott Street, shown in greater detail on the attached site plan. The property located north of SW Second Street (tax lot identification 1S116AD11100 and 1S116AD11000) is 6.28 acres, and the property south of SW Second Street (tax lot identification 1S116AD10900) is 11.41 acres. The property is zoned R-10 (single family residential, one dwelling per 10,000 square foot lot).

The BSD is seeking several land use reviews and approvals: a street vacation of SW Second Street, a Master Plan/Conditional Use approval (schools are conditional uses on R-10 land), and a Design Review for development of the cafeteria. This proposal would also require the removal of the Merle Davies school, and therefore, will require review by the City of Beaverton's Historic Resource Review Committee. Prior to applying to the Community Development Department for the City of Beaverton, we would like to discuss the proposal in more detail with you.

The purpose of this meeting is to provide a forum for the applicant and surrounding property owners/residents to review the proposal and identify any issues for consideration before the formal applications are submitted to the City. We will attempt to answer questions that may be relevant to meeting development standards consistent with the City Development Code.

Pursuant to the City of Beaverton Development Code Section 50.10.1, you are invited to attend a meeting as part of the Central Neighborhood Association Committee (NAC) on:

**September 20, 2001****8:00 – 8:30 p.m.****Beaverton Community Center, Community Room****12350 SW 5<sup>th</sup> Street, Suite 100****(across from the Beaverton City Library)****Beaverton, OR 97005****Contact: Susan Kozak, Chairperson Central Beaverton NAC, (503) 643-3818**

Please note this will be an informational meeting on preliminary development plans, which may be altered prior to submittal of the application. In addition to this public meeting notice, you may receive official notice from the City of Beaverton after the application is submitted for you to either participate with written comments and/or an opportunity to attend a public hearing.

We look forward to discussing the proposal with you in greater detail. Please feel free to call me at (503) 227-3664 or fax me at (503) 227-3679 if you have questions.

Sincerely,



Frank Angelo, Principal, Angelo Eaton &amp; Associates

Frank Angelo

Chris Eaton, AICP

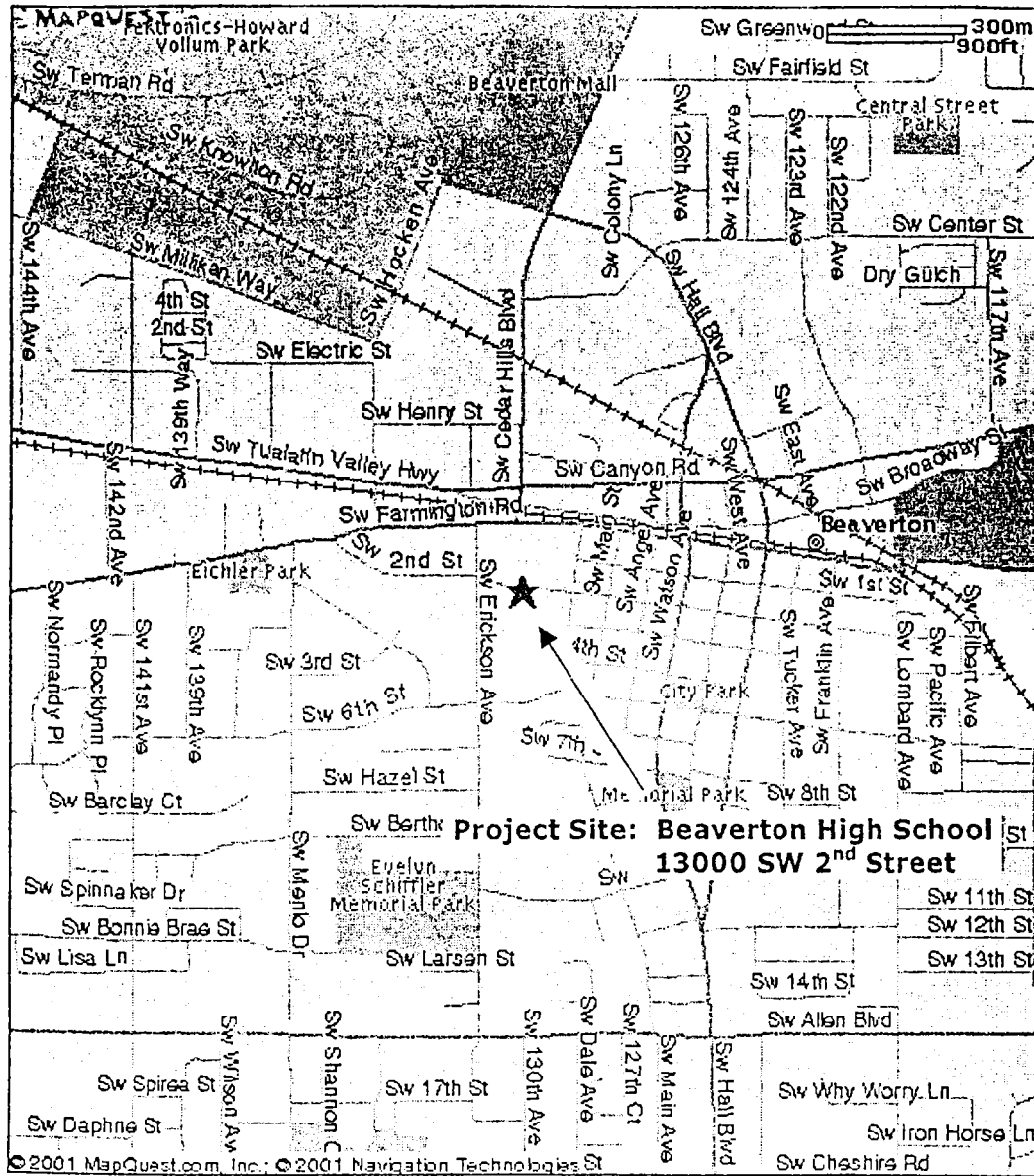
Catherine Corliss, AICP

Jean D Agostino, AICP

Karen Siegel

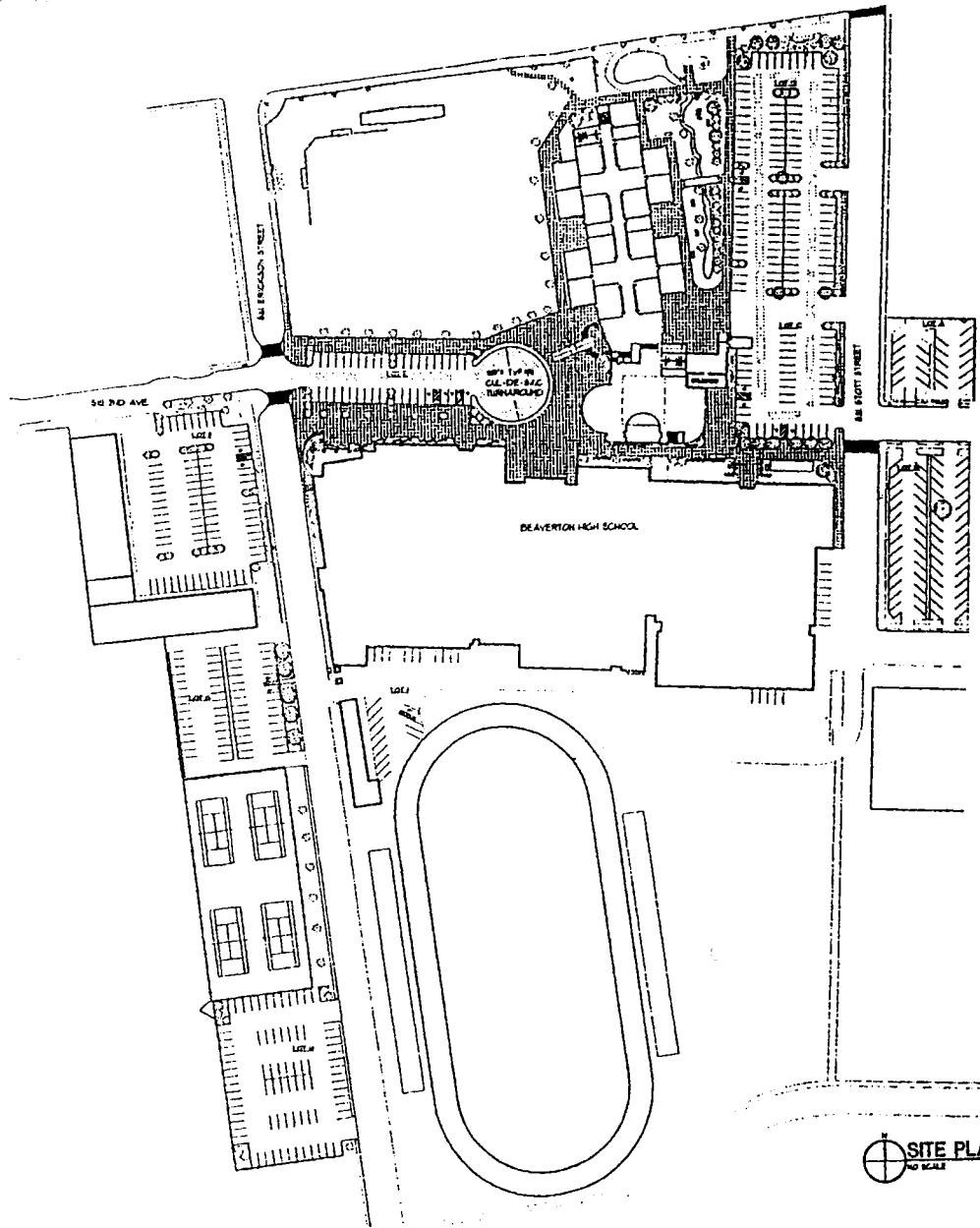


# Vicinity Map



**\*PLEASE NOTE\***

Nighborhood Meeting to be held:  
September 20, 2001  
8:00 - 8:30 p.m.  
Beaverton Community Center, Community Room  
12350 SW 5<sup>th</sup> Street, Suite 100  
(across from the Beaverton City Library)  
Beaverton, OR 97005



**PARKING SPACES**

Lot	ADA	TOTAL
1	1	10
2	1	10
3	1	10
4	1	10
5	1	10
6	1	10
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9	1	10
10	1	10
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30	1	10
31	1	10
32	1	10
33	1	10
34	1	10
35	1	10
36	1	10
37	1	10
38	1	10
39	1	10
40	1	10
41	1	10
42	1	10
43	1	10
44	1	10
45	1	10
46	1	10
47	1	10
48	1	10
<b>TOTAL</b>	<b>48</b>	<b>480</b>

**LSW**  
 ARCHITECTURAL  
 PLANNING/INTERIOR  
 TEL (503) 374-54  
 FAX (503) 374-04  
 1973 N.W. Gateway  
 Portland, OR 97217

PRELIMINAR

**BEAVERTON HIGH SCHOOL**  
 BEAVERTON SCHOOL DISTRICT # 48  
 CAFETERIA  
 48000 N.W. 28th ST. BEAVERTON, OREGON 97006

DATE: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 APPROVED BY: \_\_\_\_\_  
 TITLE: \_\_\_\_\_

5.0

Site Plan

1S116AA04700  
ADAMS-HAGLUND, JENNIFER  
2605 NE ROBERTS PL  
GRESHAM OR 97030

1S116AD02500  
ADOLPHSON LOVING TRUST  
BY RICHARD & SHIRLEY ADOLPHSON  
25505 SW LONE FIR LANE  
WEST LINN OR 97068

1S116AD02501  
ADOLPHSON LOVING TRUST  
25505 SW LONE FIR LN  
WEST LINN OR 97068

1S116AD02300  
ADOLPHSON, R G  
ADOLPHSON, R G JR  
BY A-2 ENTERPRISES  
4555 SW MAIN ST  
BEAVERTON OR 97005

1S116DB00400  
AFOURKEEFF, ANJA T  
5075 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD07000  
AHN, DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

1S116AD07001  
AHN, DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

1S116AC04700  
ANDERSON, ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC05500  
ANDERSON, ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DB03400  
ARLT, LINDA L  
13275 SW PEAR ST  
BEAVERTON OR 97005

1S116AC01800  
ARMSTRONG, JAMES R  
LINDA L  
13325 SW 2ND  
BEAVERTON OR 97005

1S116DA90027  
BABB, DORIS LEE  
5136 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90023  
BAKER, VIRGIL A & MARIE  
5166 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AC04500  
BARTLETT, CLIFFORD L  
13500 SW 2ND  
BEAVERTON OR 97005

1S116AA05200  
BASEEL, PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

1S116AA05301  
BASEEL, PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

BEA  
Beaverton  
PO BOX 4755  
BEAVERTON OR 97076

Beaverton  
PERSEY, JIM  
12345 SW DAVIES ROAD  
BEAVERTON OR 97008

1S116AB03100  
BEAVERTON FOODS INC  
PO BOX 687  
BEAVERTON OR 97005

1S116AC04400  
BEAVERTON PROPERTIES  
13470 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD11100  
BEAVERTON SCHOOL DISTRICT #48  
MERLE DAVIS ELEMENTARY  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02100  
BEAVERTON SCHOOL DISTRICT #48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD11000  
BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD02900  
BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD07100  
BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02100  
BEAVERTON SCHOOL DISTRICT #48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02500  
BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD10900  
BEAVERTON SCHOOL DISTRICT #48  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD11200  
BEAVERTON, CITY OF  
000000

1S116AC04600  
BEAVERTON, CITY OF  
4850 SW HALL BLVD  
BEAVERTON OR 97005

IS116AD01300  
BEAVERTON, CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

IS116DB11100  
BEAVERTON, CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

IS116AA02800  
BEAVERTON, CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

IS116AA06000  
BENNETT, CLEVE E & LOIS W TRUS  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

IS116AA05900  
BENNETT, CLEVE E AND  
LOIS W TRUSTEES  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

IS116AD10400  
BENNETT, TERRI LOUISE  
12820 SW 4TH ST  
BEAVERTON OR 97005

IS116AC01500  
BERRY, HARRY L AND  
DANISH, CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005

IS116BD00801  
BERRY, HARRY L AND  
DANISH, CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005

IS116AA02900  
BIGGI, STEVE AND LAURA  
3843 SW HALL BLVD  
BEAVERTON OR 97005

IS116DB00100  
BLOOMER, DANIEL D MARY L  
4925 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DB02700  
BOYER, ROBERT M &  
ARRUDA, JANA R  
13370 SW 6TH ST  
BEAVERTON OR 97005

IS116DA90055  
BYRNES, DONALD B & B JERI  
12900 SW 5TH ST  
BEAVERTON OR 97005

IS116AA05400  
CAFFEE CENTER JOINT VENTURE  
BY ANDERSON & ASSOCIATES  
PO BOX 3821  
BELLEVUE WA 98009

IS116DB03000  
CARSON, JERRY/PAULA  
13270 SW 6TH  
BEAVERTON OR 97005

IS116AC05700  
CASSIDY, MARI  
4820 SW MENLO  
BEAVERTON OR 97005

IS116DA02500  
CASSON, KATHLEEN ANN  
3818 SE 9TH AVE  
PORTLAND OR 97202

IS116DB03700  
CASTANETTE, JOHN P AND  
KRISTI M  
13355 SW PEARL STREET  
BEAVERTON OR 97005

IS116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

IS116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

IS116AC01300  
CHIA, CHIH CHIANG & YEA HUEY  
13200 SW FARMINGTON RD  
BEAVERTON OR 97006

IS116AC01400  
CHIA, FUO FUH &  
WU, CHUN ER  
13150 SW FARMINGTON RD  
BEAVERTON OR 97005

IS116AD10601  
COLEMAN, ANNE S  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116AC03000  
COLEMAN, ANNE SCOTT  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116AC01900  
COOLEY, CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

IS116DA07003  
COOLEY, CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

IS116AD08000  
CORP OF PRESIDING BISHOP OF  
CHURCH OF JESUS CHRIST LDS  
TAX ADMIN FILE: 536-249C  
50E NORTH TEMPLE  
SALT LAKE CIT UT 84150-2201

IS116AD07400  
CRITELLI FAMILY TRUST  
c/o CRITELLI, TINA  
BY GARY A & SHARON L CRITELL  
PO BOX 87058  
VANCOUVER WA 98687

IS116AD10300  
CRITELLI, GARY A & SHARON L TR  
PO BOX 87058  
VANCOUVER WA 98687

IS116DA01600  
CRITELLI, GARY A & SHARON L TR  
PO BOX 87058  
VANCOUVER WA 98687

IS116DB00200  
D'ALFONSO, PAULA L  
4975 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116AD10500  
DAMIANA, RONALD G  
GEORGIA M  
12870 SW 4TH  
BEAVERTON OR 97005

IS116AD03100  
D'SILVA, FELIX & LOURDES  
8765 SW JAMIESON RD  
PORTLAND OR 97225

IS116AA05700  
ETON LANE LTD PARTNERSHIP  
WALKER, HUBERE E TRUSTEE &  
ETON LANE LLC  
PO BOX 14746  
PORTLAND OR 97293

IS116AC01200  
FISHBACK, MERL E &  
GLORIA M TRUST  
FISHBACK, MERL E/GLORIA M TR  
9700 NW GROVELAND RD  
HILLSBORO OR 97124

IS116AC04900  
FRIEZE, REX D SHIRLEY L  
4720 SW FAIRMONT DR  
BEAVERTON OR 97005

IS116DA90025  
GARTLAND, SHARIN  
5162 SW ERICKSON  
BEAVERTON OR 97005

IS116AD02100  
GENEST, VIRGINIA E  
BY VALLEY DAIRY QUEEN  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

IS116AD02600  
GHIM, MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

IS116DA01300  
GORMAN, JOHN & PATRICIA A  
16555 SW HIGH HILL LN  
BEAVERTON OR 97007

IS116AA02100  
GREER, DAVID J & LISA D  
19370 SW SOUTHVIEW ST  
ALOHA OR 97007

IS116DB00800  
DANEIL, HOLLIS MARION  
13425 SW 6TH  
BEAVERTON OR 97005

IS116DA01700  
EIMERS, SANDRA P &  
EIMERS, ANGELA  
6045 SW 167TH AVE  
ALOHA OR 97007

IS116AA05800  
ETON LANE LTD PARTNERSHIP  
WALKER, HUBERT E TRUSTEE  
PO BOX 14746  
PORTLAND OR 97214

IS116DA02200  
FLOREN, JANET M AND GREG E  
12870 SW 6TH AVE  
BEAVERTON OR 97005

IS116DB01100  
GABRIEL, KATHLEEN  
13470 SW 3RD ST  
BEAVERTON OR 97005

IS116AD03000  
GARVER, JON & REBECCA  
12855 SW 2ND ST  
BEAVERTON OR 97005

IS116AC05000  
GERETY, JEAN  
4750 SW FAIRMONT DR  
BEAVERTON OR 97005

IS116DB05000  
GLASNER, GENE R  
c/o DREY, BRIAN S & PAMELA R  
5225 SW ERICKSON AVE  
BEAVERTON OR 97005

IS116AC01000  
GRABHORN, CHAD  
350 NW FREEMAN  
HILLSBORO OR 97124

IS116AC05300  
GREMAUX, DEANE B AND  
PATRICIA A  
4755 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

IS116DB00900  
DANIEL, AVERILL H/HOLLIS M  
13425 SW 6TH  
BEAVERTON OR 97005

IS116DB03101  
ENGEL, MICHAEL J &  
REBECCA C  
13210 SW 6TH ST  
BEAVERTON OR 97005

IS116AC04200  
EVANS, CATHERINE B  
11775 SW 11TH STREET  
BEAVERTON OR 97005

IS116DB00300  
FRANKLIN, JOHN A  
5025 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116AC05600  
GARCIA, WAGNER  
4800 SW MENLO DR  
BEAVERTON OR 97005

IS116AD02200  
GENEST, VIRGINIA E  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

IS116AD02700  
GHIM, MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

IS116AC06000  
GLICKMAN, TERRY L  
13555 SW THIRD ST  
BEAVERTON OR 97005

IS116AC03700  
GREEN, ERNESTINE RUTH  
4820 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

IS116AB02100  
GRL INC  
13095 SW CANYON RD  
BEAVERTON OR 97005

IS116DB10800  
GUSTAFSON, CONRAD W & ANNA M  
13245 SW PEARL ST  
BEAVERTON OR 97007

IS116AA02500  
GUTZLER, CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

IS116AA02600  
GUTZLER, CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

IS116AA02300  
GUTZLER, THOMAS M & CAROLYN E  
12925 SW CANYON RD  
BEAVERTON OR 97005

IS116AA02400  
GUTZLER, THOMAS M AND  
CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

IS116DA90058  
HALL, LAURA I  
12888 SW 5TH  
BEAVERTON OR 97005

IS116DA01900  
HALSTEN, ELSIE M TRUSTEE  
12855 SW 6TH  
BEAVERTON OR 97005

IS116DA02000  
HAMREUS, CALVIN E & JOYCE K  
5075 SW MAIN AVE  
BEAVERTON OR 97005

IS116DA07004  
HATTENHAUER, CHRISTINE  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

IS116DA07002  
HATTENHAUER, CHRISTINE  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

IS116DA01800  
HEDGES, SALLY C  
12850 SW 5TH AVE  
BEAVERTON OR 97005

IS116DB04600  
HEISLER, MARYANNE &  
NEUFELL, PATRICIA J  
13250 SW PEARL  
BEAVERTON OR 97005

IS116BD00400  
HESS, EVE  
SINGER, SHARON  
2150 NW 139TH PL  
PORTLAND OR 97229

IS116DA01100  
HETU, TED G  
5075 SW ANGEL AVE  
BEAVERTON OR 97005

IS116AD10100  
HOUDROJ, ALI  
4970 SW MAIN ST  
BEAVERTON OR 97005

IS116DA90028  
HUESMAN, NICOLE K  
5134 SW ERICKSON AVE  
BEAVERTON OR 97005

IS116AA03100  
HUNKAPILLAR, PATRICIA W  
SMITH, HARRIET L AND GEORGE  
BY TAGGART TRUST, THE  
21185 NW EVERGREEN PKW STE 101  
HILLSBORO OR 97124

IS116AA03000  
HUNKAPILLAR, PATRICIA W  
BY THE TAGGART TRUST  
21185 NW EVERGREEN PKWY  
SUITE 101  
HILLSBORO OR 97124

IS116AC00800  
HUNT, SHERYLL R/WILLIAM J &  
HUNT, JONATHAN J  
13380 SW FARMINGTON RD  
BEAVERTON OR 97005

IS116DA02100  
HUTCHENS, DANA C & LORI E  
12820 SW SIXTH ST  
BEAVERTON OR 97005

IS116DA06700  
HUYGENS, JOSEPH D &  
JULIE ANN HUTCHINSON  
7830 SW LAUREL ST  
PORTLAND OR 97225

IS116AC05801  
JEFFERS, STEPHEN R & REAGAN N  
4850 SW MENLO DR  
BEAVERTON OR 97005

IS116AD09901  
JOHNSON, CAROL L R  
PO BOX 524  
SCAPPOOSE OR 97056

IS116AC04000  
JONES, E CHANNING & PAULA KAY  
13420 SW 2ND AVE  
BEAVERTON OR 97005

IS116DA90052  
JONES, JERRY C  
12912 SW 5TH #52  
BEAVERTON OR 97005

IS116AC03600  
JONES, KRISTIN L  
4870 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DA07000  
JUNELL, BONNIE M &  
BABCOCK, DOUGLAS C  
4820 SW 18TH PL  
PORTLAND OR 97201

IS116AD01600  
KABDEBO, JOSEPH & CAMILLE  
725 SW VIEWMONT DR  
PORTLAND OR 97225

IS116AD01700  
KABDEBO, JOSEPH & CAMILLE  
725 SW VIEWMONT DR  
PORTLAND OR 97225

IS116AD01800  
KABDEBO, JOSEPH AND CAMILLA  
725 SW VIEWMONT DR  
PORTLAND OR 97225

IS116BD00500  
KADEL, RICHARD A & CAROL L  
9350 SW TIGARD ST  
TIGARD OR 97223

IS116BD00600  
KADEL, RICHARD A & CAROL L  
9350 SW TIGARD ST  
TIGARD OR 97223

IS116BD00700  
KADEL, RICHARD A & CAROL L  
9350 SW TIGARD ST  
TIGARD OR 97223

IS116DB03202  
KAUTH, JANE F  
5115 SW ERICKSON AVE  
BEAVERTON OR 97007

IS116DA07300  
KIKO, JOHN R &  
ORTIZ, INGRID  
5155 SW MAIN AVE  
BEAVERTON OR 97005

IS116AA02700  
KIM, YOUNG M & LINDA  
12855 SW CANYON RD  
BEAVERTON OR 97005

IS116DA06800  
KING, CHARLES C & ROM N  
5150 SW ERICKSON AVE  
BEAVERTON OR 97005

IS116DA90024  
KLANECKY, ARDITH ANN  
5164 SW ERICKSON  
BEAVERTON OR 97005

IS116DB10900  
KO, JOON B  
13215 SW PEARL ST  
BEAVERTON OR 97005

IS116AD06800  
KRAEMER, JOHN L & CLAUDIA R  
33713 NE WILLIAMS APT F-6  
SCAPPOOSE OR 97056

IS116BD02101  
KRASAUSK, PAUL &  
MAHMOOD, WALLY  
8061 SW 168TH ST  
BEAVERTON OR 97007

IS116DA90049  
KRIEG, MARGIE J  
12924 SW 5TH ST  
BEAVERTON OR 97005

IS116BD00800  
L L & F INVESTMENT CO  
13549 SW ASCENSION DR  
TIGARD OR 97223

IS116DA90057  
LAMB, KEVIN M &  
YANT, SUSAN A  
12892 SW 5TH ST  
BEAVERTON OR 97005

IS116AA04690  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116AC00500  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

IS116BD00100  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116BD00200  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116BD00300  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116AC00600  
LANPHERE, ROBERT  
ATTN: DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

IS116BD00902  
LEE, EUN SANG/MIN JA &  
LEE, MICHAEL S &  
LEE, THOMAS S  
13773 SW FARMINGTON RD  
BEAVERTON OR 97005

IS116DB03100  
LEE, SANDY  
344 NE 156TH AVE  
PORTLAND OR 97230

IS116DB02900  
LEIVA, LUIS ALONSO & SANDRA J  
13320 SW 6TH ST  
BEAVERTON OR 97005

IS116AC04300  
LEONARD, CLIFFORD A & SHARON B  
7345 SW 84TH AVE  
PORTLAND OR 97223

IS116AC06200  
LEWIS, SUNNY M  
13425 SW 3RD  
BEAVERTON OR 97005

IS116DA90047  
LIDBERG, MARY ALICE C  
12932 SW 5TH AVE  
BEAVERTON OR 97005

IS116AD08100  
LIEDTKE, CATHERINE A  
17561 SW ALBERT CT  
BEAVERTON OR 97007

IS116DA01400  
LISICKI, THOMAS D & RUTH S  
13332 SW BENCHVIEW TER  
TIGARD OR 97223

IS116DA07400  
LOCKARD, DOROTHY J  
LIVING TRUST, THE  
5125 SW SHERWOOD PL  
BEAVERTON OR 97005

IS116AC02800  
LONG, PATRICIA C & JOE PAT  
5070 SW FAIRMONT DR  
BEAVERTON OR 97005



IS116AD06600  
PHAM, HIEN & NGUYEN, THAO &  
NGUYEN, GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006

IS116AD08400  
PIES, SANDRA J & EVERETT M TRS  
29395 SW HILLECKE RD  
HILLSBORO OR 97123

IS116DA90046  
PODEBSKI, ADAM P & GUTA H  
PODEBSKI, BENNY  
12936 SW 5TH  
BEAVERTON OR 97005

IS116BD02200  
PRESERVE SPENCER HOUSE INC  
BY TUALATIN VALLEY HOUSING PAR  
14355 SW ALLEN #130  
BEAVERTON OR 97005

IS116AC02001  
QUINN, CAROLYN F  
13395 SW 2ND ST  
BEAVERTON OR 97005

IS116AD07500  
QUTUB, WAHBI M &  
MATAR, RIAD A  
7883 SW BARNARD DR  
BEAVERTON OR 97007

IS116AD02890  
RANDOLPH, GLORIA  
2725 SW 185TH  
ALOHA OR 97006

IS116AD02800  
RANDOLPH, GLORIA C  
2725 SW 185TH AVE  
ALOHA OR 97005

IS116DB00600  
REMCHEK, GEORGE & FLORENCE  
4240 SW 102ND AVE  
BEAVERTON OR 97005

IS116AC05400  
ROBERTS, LEON &  
CARRIE  
4725 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DA01500  
ROBINSON, JACK A & PATRICIA A  
3830 SE BENTLEY AVE  
HILLSBORO OR 97123

IS116DB04700  
ROSENHOLM, DAVID  
ROSENHOLM, ANDREA  
13220 SW PEARL ST  
BEAVERTON OR 97005

IS116DB04800  
SANABRIA-BARILLAS, JOSE ALFRED  
ALVARADO, JUANA &  
MAX, MARTHA L PAR  
13170 SW PEARL ST  
BEAVERTON OR 97005

IS116AC02700  
SCALONE, DAVID  
13255 SW 6TH ST  
BEAVERTON OR 97005

IS116AD01500  
SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

IS116AD01400  
SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

SDBEA  
BEAV SCH DIST-JAN YOUNGQUIST,  
16550 SW MERLO RD  
BEAVERTON OR 97006

IS116DA90059  
SESSIONS, KEITH W & LAVERNA M  
TRUSTEES  
12884 SW 5TH  
BEAVERTON OR 97005

IS116AB01901  
SKORO, MARION & MIRELA  
19300 NW GILLIHAN LOOP RD  
PORTLAND OR 97231

IS116DB03500  
SMITH, DONALD J PHYLLIS J  
13295 SW PEARL ST  
BEAVERTON OR 97005

IS116AD09900  
SMITH, IAN C & NADINE T  
12750 SW 4TH ST  
BEAVERTON OR 97005

IS116DA90053  
SMITH, RICHARD BRENDAN &  
HILARY ANTOINETTE MARY  
12908 SW 5TH ST  
BEAVERTON OR 97005

IS116DB00700  
SNODGRASS, PHILIP R & PATRICIA  
13420 SW THIRD ST  
BEAVERTON OR 97005

IS116AC05800  
SOTH, COLLEEN V TRUSTEE  
SOTH, FORREST C TRUSTEE  
4890 SW MENLO DR  
BEAVERTON OR 97005

IS116AC00190  
SOUTHERN PACIFIC TRANSPORTATIO  
BY UNION PACIFIC RAILROAD CO  
1700 FARNAM ST, 10TH FLOOR SOU  
OMAHA NE 68102

IS116DA90030  
SPURGEON, JOAN P  
5130 SW ERICKSON  
BEAVERTON OR 97005

IS116DB00500  
ST CLAIR, JERALD LEE  
SHIRLEY ANN  
13355 SW 6TH ST  
BEAVERTON OR 97005

IS116AC02600  
STARKE, DONALD B EDNA A  
13155 SW 6TH  
BEAVERTON OR 97005

IS116AA02101  
STEICHEN, NANCY & MCCOURT, JOH  
c/o HOVERSLAND, GORDON & MYRNA  
4350 SW CEDAR HILLS BLVD  
BEAVERTON OR 97005

IS116AD10600  
STEIGER, JOHN M & LOU-WAYNE  
301 NW MURRAY BLVD  
PORTLAND OR 97229

IS116AC01100  
STOCKWELL, KATHERINE R &  
BROWN, GARY E  
13240 SW FARMINGTON RD  
BEAVERTON OR 97005

IS116DA01000  
STOLTENBURG, BARBARA  
c/o RAINEY, WALLACE L & LESLIE  
5025 SW ANGEL AVE  
BEAVERTON OR 97005

IS116AC02000  
SURBY, PAUL W L FRANCES  
13375 SW 2ND  
BEAVERTON OR 97005

IS116AC03500  
THAO, PANCHONG & MAO HER  
4920 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DA90029  
THOMAS, BARBARA A  
5132 SW ERICKSON  
BEAVERTON OR 97005

IS116AD06500  
THOMAS, KELLY R &  
BOTHNE, MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005

IS116AD06900  
THOMAS, KELLY R &  
BOTHNE, MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005

IS116AA05001  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

IS116AA05101  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

IS116AA05100  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

IS116AA05102  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

IS116DA90056  
THOMPSON, DANIEL HARRISON &  
THOMPSON, TOM G &  
THOMPSON, MARGIE J  
12896 SW 5TH AVE  
BEAVERTON OR 97005

IS116AC06100  
THOMPSON, MAURICE J & KIMBERLY  
13475 SW 3RD ST  
BEAVERTON OR 97005

IS116AC01700  
THOMPSON, TOMMY G  
JEANNETTE L  
13305 SW 2ND ST  
BEAVERTON OR 97005

IS116DB04900  
TISCHLER, STEPHEN & KATHLEEN  
5195 SW ERICKSON AVE  
BEAVERTON OR 97005

IS116AA02200  
TOSCO CORP  
PROPERTY TAX DEPT-DC17  
PO BOX 52085  
PHOENIZ AZ 85072

IS116AD07600  
TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

IS116AD07700  
TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

IS116BD02100  
TUALATIN HILLS PARK &  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97006

TVFR  
T-V FIRE & RESCUE  
7401 SW WASHO CT  
TUALATIN OR 97062

IS116AD06700  
UNDERHILL, MICHAEL  
12795 SW 3RD ST  
BEAVERTON OR 97005

IS116AA04900  
VADEN, MICHAEL & JANE  
12740 SW CANYON RD  
BEAVERTON OR 97005

IS116AA05000  
VADEN, MICHAEL & JANE  
12740 SW CANYON RD  
BEAVERTON OR 97005

IS116AA04800  
VADEN, MICHAEL & JANE  
BY LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116DA02501  
VEAZEY, THOMAS C AND  
ROXANNE T  
12770 SW 6TH ST  
BEAVERTON OR 97005

IS116DA90045  
VINK, NANCY E  
12940 SW 5TH ST  
BEAVERTON OR 97005

IS116AC05100  
VOGEL, MICHAEL J & D'ANNE T  
4770 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116AC05200  
VON W METHENEY FAMILY TRUST  
4775 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DA90026  
VOSE, BENJAMIN A  
PO BOX 73  
TOLOVANA PAF OR 97145

IS116AC03100  
WABNITZ, STEVEN G  
2648 SW 153RD AVE  
BEAVERTON OR 97006

**ANGELO EATON**

& Associates

620 SW Main, Suite 201  
Portland, Oregon 97205

NOT DELIVERABLE  
AS ADDRESS  
UNABLE TO FORWARD  
BEAVERTON OR 97005

1S116AC05600  
GARCIA, WAGNER  
4800 SW MENLO DR  
BEAVERTON OR 97005



**ANGELO EATON**

& Associates

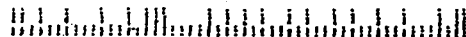
620 SW Main, Suite 201  
Portland, Oregon 97205

1S116AC00800  
HUNT, SHERYLL R/WILLIAM J &  
HUNT, JONATHAN J  
13380 SW FARMINGTON RD  
BEAVERTON OR 97005

BEAVERTON OR 97005 1000 10 03/31/01  
FORWARD TIME EXP RUN TO SEND  
HUNT  
14230 SW 117TH AVE  
TIGARD OR 97224-2807

RETURN TO SENDER

97005-2807



**ANGELO** ♦ **EATON**

& Associates

620 SW Main, Suite 201  
Portland, Oregon 97205

  
RETURN  
TO  
SENDER  
Not Deliverable as Addressed  
UNABLE TO FORWARD  
BEAVERTON OR 97005-9998

1S116AC04600  
BEAVERTON, CITY OF  
4950 SW HALL BLVD  
BEAVERTON OR 97005


*Handwritten scribbles*



**ANGELO** ♦ **EATON**

& Associates

620 SW Main, Suite 201  
Portland, Oregon 97205

  
RETURN  
TO  
SENDER  
NOT DELIVERABLE  
AS ADDRESS  
UNABLE TO FORWARD  
BEAVERTON

1S116DA01800  
HEDGES, SALLY C  
12850 SW 5TH AVE  
BEAVERTON OR 97005



## NOTICE SIGN MOCK-UP

## PUBLIC MEETING

On A  
Preliminary Development Proposal  
Affecting

13000 SW 2nd Street /  
13000 SW Farmington Rd.

PROPOSED

Street Vacation / Conditional Use

A meeting to discuss the preliminary  
development proposal is scheduled for

September 20, 2001 8-8:30 PM  
Beaverton Community Ctr - 12350 SW 5th St.

ALL INTERESTED PERSONS MAY ATTEND

FOR MORE INFORMATION  
CONTACT:

Frank Angelo (503) 227-3664  
Kirsten Pennington (503) 227-3669

[Use this form and fill in the blank spaces with the information you provided on the notice sign posted at the site if you used the signs available from the City]

DEVELOPER OR AGENT: Angelo Eaton Associates

PROJECT LOCATION: 13000 SW 2nd St.

**AFFIDAVIT OF POSTING NOTICE**

\*\*\*\*\*

I, Kirsten Pennington, being first duly sworn; say that I am (represent) the party intended to submit an application to the City of Beaverton for a proposed Street Vacation/Conditional Use affecting land located at 13000 SW 2nd St., and that pursuant to Ordinance 2050, Section 50, and the guidelines set out by the Planning Director, did on the 30 day of August, 2001, personally post public notice on the proposed development site.

*Sign and Date in the presence of a Notary Public. Certain City staff are Notary Publics and are available for witnessing.*

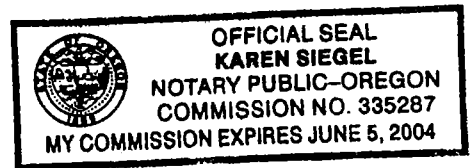
Signature: Kirsten Pennington

Dated this 31 day of August, 2001.

Subscribed and sworn to before me this 31st day of August, 2001.

Karen Siegel  
Notary Public for the State of Oregon

My Commission expires: June 5, 2004



TURN PAGE OVER FOR POSTING INSTRUCTIONS

DEVELOPER OR AGENT: Angelo Eaton & Associates

PROJECT LOCATION: 13000 SW 2nd Street

### AFFIDAVIT OF MAILING NOTICE

\*\*\*\*\*

I, Robin Scholetzky, being first duly sworn; say that I am (represent) the party intended to submit an application to the City of Beaverton for a proposed street vacation/Conditional Use affecting land located at 13000 SW 2nd Street, and that pursuant to Ordinance 2050, Section 50, and the guidelines set out by the Planning Director, did on the 28th day of August, 2001, personally mail notice to affected property owners and NAC's within 500 feet of the proposed development site.

*Sign and Date in the presence of a Notary Public. Certain City staff are Notary Publics and are available for witnessing.*

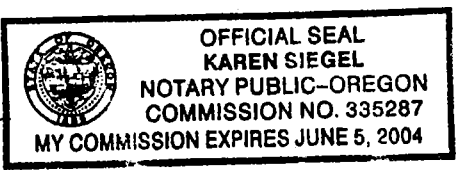
Signature: [Handwritten Signature]

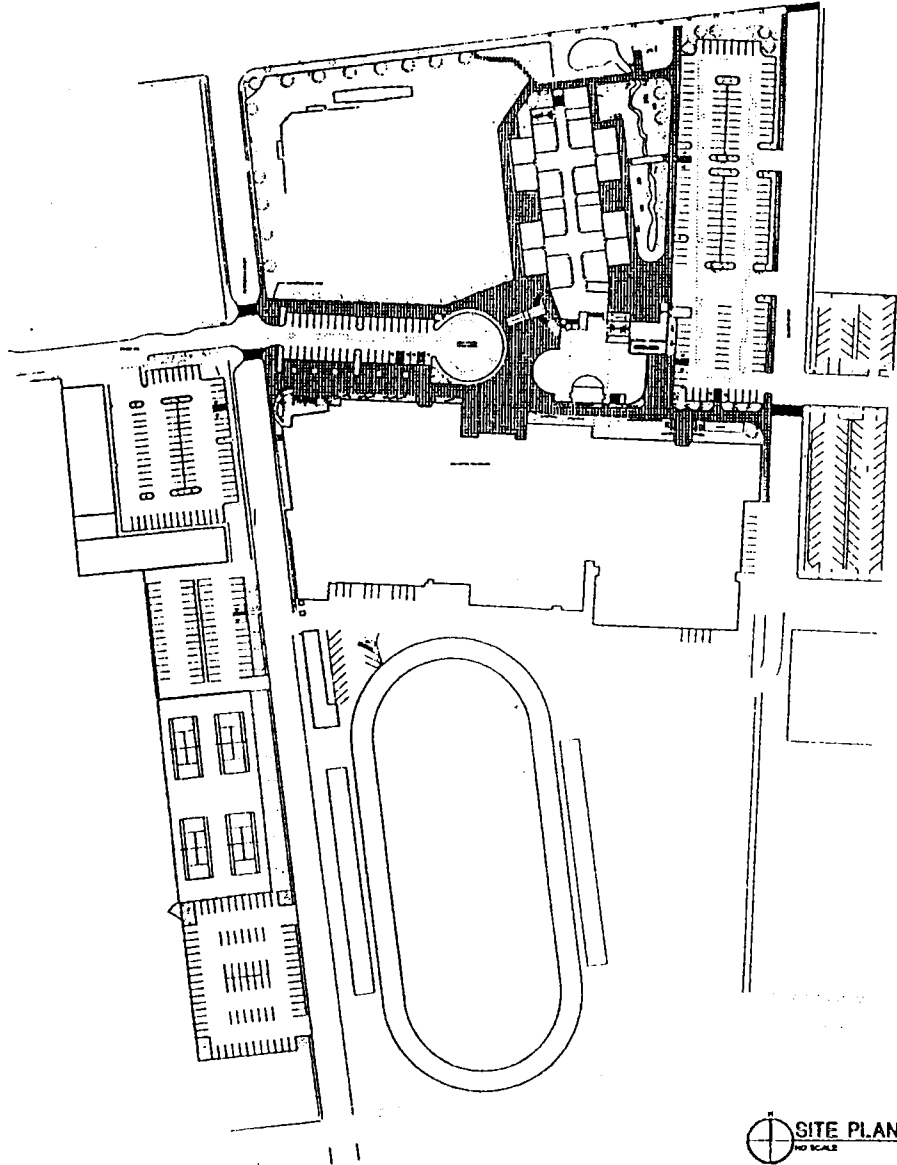
Dated this 29th day of August, 2001.

Subscribed and sworn to before me this 29th day of August, 2001.

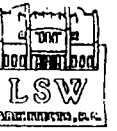
Karen Siegel  
Notary Public for the State of Oregon

My Commission expires: June 5, 2004






**SITE PLAN**  
 NO SCALE



**ARCHITECTURE**  
 PLANNING/INTERIORS  
 TEL (503) 376-6522  
 FAX (503) 376-0065  
 1751 NW Kearney St.  
 Beaverton, OR 97109

**PRELIMINARY**

**BEAVERTON HIGH SCHOOL**  
 CAFETERIA  
 BEAVERTON SCHOOL DISTRICT # 48  
 13000 S.W. 2nd ST. BEAVERTON, OREGON 97005

Date: 08-09-01  
 Job No.: 01070  
 Drawn by: KAT  
 Checked by: JCU  
 Drawing Title:  
 SITE PLAN

Sheet No.  
**5.0**

11/11/01 10:57 AM



## MEETING MINUTES

Beaverton School District, Beaverton High School Street Vacation and Master Plan  
Neighborhood Meeting, Central Beaverton Neighborhood Association Committee  
September 20, 2001

8:00 – 8:30

Beaverton Community Center, Community Room  
12350 SW 5<sup>th</sup> Street, Suite 100  
Beaverton, OR 97005

Presented by: Frank Angelo, Angelo Eaton & Associates  
Mike Maloney, Beaverton School District  
Russ Pitkin, LSW Architects  
Vlad Voytilla, Beaverton School District

### I. Overview of Project (Frank Angelo)

- Described the street vacation, SW 2<sup>nd</sup> Street, between Stott Street and Erickson Avenue
- Description of proposed overall site master plan (new building to house cafeteria and classroom facilities, to be located north of existing high school building)
- New parking lot at SW 5<sup>th</sup> Street and Erickson Avenue and other locations for a total of 500 new parking spaces
- Proposed for development in two Phases

#### *Phase 1*

- Vacation of SW 2<sup>nd</sup> Street
- Temporary bus loading on SW 2<sup>nd</sup> Street
- Pedestrian connections to the surrounding area
- New cafeteria addition
- Modification of Merle Davies Building
- Funded by school board levy

#### *Phase 2*

- 60,000 square feet of class space
  - Expansion of parking facilities
  - Removal of Merle Davies building
  - Additional parking on Erickson
  - Enhanced pedestrian connection
  - Creation of an urban campus
- The application submitted will contain an application for a street vacation and a Master Plan
  - In terms of timing, the Phase 1 funding has been secured and will be completed as soon as possible with Phase 2 to follow when funding is available

### II. Questions (Project Team)

(note: Questions and Answers are not verbatim).

1. What type of community space will be offered?

- A. Cafeteria space will be available for residents to use free of charge.
2. Removal of the Merle Davies building—can it be avoided, what about the existing food court and when will that be removed as part of the demolition?
  - A. The Merle Davies building would be very expensive to rehabilitate as a high school building. Removal of the building is necessary to provide the improvements.
3. Where will busses load and unload after the second phase is completed?
  - A. There are two bus loading options, loading and unloading on either Erickson and Stott or they will locate the bus loading area in the Erickson parking area—adjacent to the parking area.
4. Historic Building demolition—I'm very disappointed.
  - A. Comment noted.
5. (Regarding the closure of 2<sup>nd</sup> Street)...I'm concerned about the closure of another street in Beaverton; it is already tough to get around.
  - A. The street vacation is seen as a way to connect the campus and also to connect the surrounding area with the school. The street vacation is the best way to use the existing land on the campus. Other options were considered, but this was the best way to utilize the space available to the School District. Pedestrian and bicycle access will continue to be provided through the campus.
6. Erickson is a busy street---What about safety concerns?
  - A. The City Engineer and the Beaverton School District Traffic Engineer are reviewing options to address traffic circulation and safety concerns. It is likely that they City Engineer will ask for some street improvements and safety precautions.
7. How will bus loading and bus unloading operate?
  - A. The BSD won't know until the decision has been made on where to locate the facilities. We know that there are safety concerns and that the intersections are difficult due to the bus traffic. We will be working with the City to develop the best solution for traffic. We know that traffic concerns are important for the community.
8. What is the number of busses currently running? How many students take the bus?
  - A. At least two thirds of the students take the bus and there are currently about 11 busses being used each day. BSD will confirm this number.
9. As a resident, I hate to see 1700 students being crammed into one school—I like the idea of having smaller magnet schools.
  - A. BSD has school within a school programs and the extra classroom space that we are asking for helps to facilitate this. The School District has some magnet schools, but a lot of small campuses are difficult to administrate and cost much more for janitors, cafeteria service etc.

10. There is a need in Beaverton for historic buildings -- I hate to see one being torn down and replaced with a parking lot.
  - A. The building is listed on the City's Goal 5 Historic inventory but not on any national lists. It is a concern to remove the building, however, economically, it's just not feasible to retrofit the building as a high school structure. It would be very expensive. The architect will be incorporating elements of the older building within the new construction, such as windows.
11. Why is the new cafeteria on two floors?
  - A. The cafeteria is on two floors to match the Phase 2 building and to match the existing construction, which has two floors. It also provides an opportunity for using the High School space as meeting space for the community and a place for the school to have dances, etc. The school doesn't have any place like this right now. It is important to centralize the space instead of having three separate cafeterias.
12. What is the number of total parking spaces?
  - A. There are a total of 508 total parking spaces. This will bring the site into compliance with City code. We expect the spaces to be completely utilized on event nights.
13. Why does Phase 2 add a total of 34 classrooms?
  - A. This is because of the anticipated increase in student population to the year 2010. The School District is using 2,200 students as the anticipated high school student population for the District's high schools.
14. What about the overall size of the school as being dehumanizing?
  - A. The new design will create small spaces within the school in order to be more comfortable and to provide a better learning environment.

**Beaverton Central Neighborhood Association Meeting**  
 Neighborhood Review Meeting for Beaverton School District Application  
 September 20, 2001 8:00 to 8:30 PM

PLEASE PRINT YOUR INFORMATION BELOW

Name	Address	Phone
ROSE GALANTE	4925 SW 141st Ave Beav. OR 97005	503 647-1111
William J. Dea	13570 SW Chelsea Dr. 97005-2451	644
Carol Franklin	5025 SW Fairmount Beaverton 97005	
Jack	" "	" "
R. A. Taberson	13755 S.W. Bonne Bee. Ct Beav. 97005	
Jacob D Godden	11815 SW 5th (503) 762-1026	
AL HAMRELS	5075 S.W. MAIN	644
Joe Knight	4925 SW 141st Ave 97005	
Deborah J. ...	17660 SW ... Beaverton 97005	
Randy Kaison	4570 SW COMRADE	503 350 0868

Neighborhood Meeting



Beaverton Central Neighborhood Association Meeting  
Neighborhood Review Meeting for Beaverton School District Application  
September 20, 2001 8:00 to 8:30 PM

PLEASE PRINT YOUR INFORMATION BELOW

Name	Address	Phone
Jim & Linda Christensen	13325 SW 2nd	503-645-2433
Vicki Van Buren	9908 NW Abbey Rd Pld	503-246-9380
Mike Howser	3536 NE 25th Ave Pld	503-254-3022
Jenna Fishback	9700 NW Greenwood Rd Hillsboro 97124	503-447-7931
JENNA FISHBACK	1951 SW 10th Ave, Beaverton 97005	503-246-1011
RUSSEL L. PITKIN	1953 NW KEARNEY PORTLAND 97209	503 274 5432
Lana Eunderson	The Oregonian	
Louis HAGG	12636 NW Barnes Rd	503 469-8520

**Central Beaverton Neighborhood Association Committee**  
**Regular Board Meeting - Sept. 20, 2001**

Call to order and greeting 6:30 PM

Board Members Present: Sue Kozak, Chair, Carol Franklin, Vice Chair, Jack Franklin, Rose Galante, Jacob Godden, Freddie McBride, D.A. 'Tobie' Tobiason, Randy Kaiser

Board Members Absent: Dorothy Fisher(excused), Rita McCormick (excused)

Others Present: Steve Enyart, Beaverton Police Dept., Louis Haga, Larry Eisenberg, Frank Angelo, Ginny Kingsley, Kathy Aulwes, Cal Hamrens, Louise Penion, Lyle Stevenson, Jim Armstrong, Linda Armstrong, Vickie Van Buren, Mike Howser, Gloria Fishback, Loren Fishback, Russel Pitkin, Laura Gunderson, Robin Scholetzky, Alisa Brodhay Pyszka

Presentation: Robin Scholetzky, Angelo Eaton & Associates-Proposed construction of a loading dock at the Washington County Elections Office. Handouts of the 9/20/01 meeting agenda and the drawings from DiLoreto Architects were given out. The loading dock is needed for handling the mail during election times. Kathy Aulwes of DiLoreto Architects presented the model and discussed screening and landscaping materials. Completion is due Spring 2002; approx. cost \$75-100K at completion.

Presentation: Alisa Brodhay Pyszka, WRG Design Inc. Proposed Comprehensive Plan Amendment on the Round. Drawings and discussion of changes in plans for the Round. Addition of green spaces, landscaping, removal of Esplanade Road and adding a parking structure. Discussion and questions keyed in on changing from proposed condo units to proposed rental units. Parking will have 256 parking stalls in the first phase. Removal of Esplanade Road. General discussion revealed concerns for safety crossing Canyon Road to get to the Round.

Presentation: Frank Angelo, Angelo Eaton and Associates. Proposed Development at Beaverton High School. Overview and discussion centered on added classroom and cafeteria space. Merle Davies School is to be demolished to accommodate those changes plus added parking spaces. Phase 2 is not funded yet. Jack Franklin received two calls from citizens concerned about the removal of Merle Davies School, which is on the City's register of Historical Buildings. Concern was raised as to the buses unloading/loading on Erickson. Questions about traffic flow and the safe unloading of school busses could not be responded to until the City Engineer made the decision on the best approach.

1. Beaverton Police Report: Officer Enyert provided crime statistics for August 2001. Again we are in the busiest district for calls taken. A list of details was provided for the Board to review.
2. TVF&R Report: Not present / no report this month.
3. Minutes: Approved as submitted
4. Treasurer's report: CBNAC received a check for \$340 for City Clean-Up volunteers. Check will be given to the Treasurer for deposit.
5. CCI Report:-Last meeting was a general discussion; Washington County is asking for applicants for 2 vacancies on Land Use Ordinance Advisory Commission; County Planning Division is planning 3 open houses in September. One coming up 9/24; Beaverton Resource Center open house is 9/29;Wa. Co. Historical Museum fundraiser is on 10/20; study group started on the Farmington Road improvement project.
6. Announcements:
  - All CBNAC mail received is on the table for viewing by all.
  - Discussion on upcoming election of CBNAC officers. Nominations for offices will be taken at the Oct meeting and elections will be held at the General Meeting in Nov.
7. New Business/Discussion and concerns:
  - Schiffler Park Report- Dorothy Fisher sent minutes of last meeting. Discussion regarding CBNAC's plan to help the park with available City matching funds. Pricing on suggested projects was not available. We will need more details to make a firm decision.
  - Merle Davies School-There should be a public hearing regarding taking the school off the Historic Sites list. Cal asked that the CBNAC support keeping it on the list. Motion to support was made and seconded. Passed unanimously. CBNAC will write a letter supporting keeping Merle Davies School on the list of Historic Building in Beaverton and modifying the expansion of Beaverton H.S. so as not to demolish it.
  - Randy made a motion that some CBNAC funds be contributed to the NYC Disaster relief efforts. Discussion led to the decision that NW Medical Teams would be the best recipients of those funds for that purpose. Motion was amended to have \$500 contributed to NW Medical Teams to be used for the NY Relief effort as they see fit. Vote was taken and passed unanimously.

Meeting adjourned 9PM

Submitted by Rose Galante (Temporary Recorder)



& Associates

620 SW Main Suite 201  
Portland OR 97205  
503-224-6974  
503-227-3679 – FAX

## TRANSMITTAL

**To:** Susan Kozak

**From:** Robin Scholetzky  
Kirsten Pennington

**Firm:** Chair, Central Beaverton NAC

**Date:** 10/15/01

**Address:** 4925 SW 141<sup>st</sup> Ave  
Beaverton, OR 97005

**RE:** September 20, 2001  
Neighborhood Meeting Notes  
1) Washington Co. Elections Division  
2) BHS Street Vacation/General Plan

**Phone:** 503-643-3818

**Cc:** Frank Angelo, Angelo Eaton & Associates  
Vlad Voytilla, Beaverton School District  
Larry Eisenberg, Washington County

**Urgent**     **For Review**     **Please Comment**     **Please Reply**     **Please Recycle**

---

Hi Susan,

We have enclosed copies of the meeting notes from the September 20, 2001 Central Beaverton Neighborhood Association Committee (NAC), specifically for (1) the Washington County Elections Division loading dock project and (2) the Beaverton School District Beaverton High School street vacation and general plan project. As per Beaverton Development Code Section 50.10.1.B.5, we will be submitting these notes as part of the applications to the City of Beaverton.

Per Section 50.10.1.B.5, the meeting notes include the meeting date, time, and location in addition to meeting content. We have also attached copies of the meeting sign-in sheets.

Please do not hesitate to contact us if you have additional questions or comments. Thank you for the opportunity to share our planning process with you and your neighbors.

Angelo Eaton & Associates

5

A G E N D A B I L L

Beaverton City Council  
Beaverton, Oregon

SUBJECT: INVENTORY OF  
HISTORIC TREES

FOR AGENDA OF: 5/20/91 BILL NO. 91-118

APPROVED FOR AGENDA BY: *[Signature]*

DEPT. OF ORIGIN: PLANNING DIVISION *[Signature]*

DATE SUBMITTED: APRIL 30, 1991

PROCEEDING: CONSENT AGENDA

CLEARANCES:

EXHIBITS: REPORT DATED MARCH 7, 1991  
HRRC MINUTES DATED APRIL 18, 1991

EXPENDITURE	AMOUNT	APPROPRIATION
REQUIRED: \$ 0	BUDGETED \$ 0	REQUIRED \$ 0

BACKGROUND

On April 18, 1991, the Historic Resource Review Committee approved the attached list of sites as containing historic trees.

RECOMMENDED ACTION

Approve on Consent Agenda.



TO: Historic Resource Review Committee

FROM: Nadine M. Smith  
Senior Planner

RE: Historic Tree Inventory

DATE: March 7, 1991

A. Background

The City Council recently adopted an amendment to Ordinance 2050 addressing tree and vegetation preservation. One of the requirements of that ordinance amendment was as follows:

"An inventory shall be conducted of trees or groves which may qualify as historic. After drawing up a proposed list of historic trees and groves of trees, the Historic Resource Review Committee (HRRRC) shall hold at least one public hearing of which the affected property owners have been notified, pursuant to the procedures of Section 130. Each property owner of the tree or trees under consideration shall be notified by certified mail 30 days prior to any public hearing.

"Following public hearing, the HRRRC shall develop a recommended list of historic trees and groves. The Council may adopt the recommended list on its consent agenda or may hold a public hearing in the same manner and form as the hearing held by the HRRRC. Any affected party may request a hearing before the City Council prior to adoption."  
(Section 265(A and B), Ordinance 2050)

Using the adopted list of historic resources in Beaverton and following research to determine the existence of historic trees in Beaverton, approximately 50 sites were evaluated for potential historic designation.

Historic trees are defined in the ordinance as "those designated by the City to be of historic significance based upon their association with historic figures, properties or general growth and development of the City.

Prior to completion of the inventory, Neighborhood Association were contacted for their input and staff appeared before most associations to provide information on the ordinance and inventory process.

B. Historic Tree Inventory

The final proposed historic tree inventory consists of a total of six sites. Attached to this report is a summary sheet on each of the sites. Historic trees will be evaluated by the City's Historic Resource Review Committee as a separate public hearing process.

C. Ordinance Regulation of Trees on the Inventory

Those trees finally adopted as historic will require Historic Resource Review Committee approval for removal. If development is proposed that will impact an individual historic tree, a public hearing will be required before the Historic Resource Review Committee to evaluate the impact on the tree or trees. Trees may be removed if the HRRC finds that the circumstances listed under Section 263 of the Ordinance have been met or the tree has lost its historic significance for some reason.

The ordinance will also regulate pruning of historic trees. A permit from the Planning Department is required for pruning. The permit will require that pruning take place in such a way that will not damage or kill the protected tree.

D. HRRC Action

Staff held a public information meeting on February 7th to meet with individuals who are proposed for the inventory and address individual concerns. As of the preparation of this report, the Planning Department has received one letter of opposition to listing on the inventory which is attached.

It is our intent to give a presentation at the public hearing on March 7th on the individual sites that are proposed to be listed as containing historic trees. Unless further information is required, the Historic Resource Review Committee can adopt the historic inventory at that hearing.

The inventory that is adopted by the Committee will appear on the City Council's consent agenda for final adoption. An optional public hearing date has also been provided for a hearing before Council.

Historic Tree Inventory

March 7, 1991

Pag 3 of 3

E. Recommendation

Staff recommends that the Historic Resource Review Committee adopt following trees as historic trees:

1. Ponderosa pine located at 4875 SW Angel
2. Sequoias located at 4440 SW 148th (St. Mary's Academy)
3. Birches located at 16270 SW Hart Road
4. Oak located at 10010 SW 125th
5. Landscaping at Merle Davies Elementary school on SW Farmington
6. "Century trees" at Fanno Farm house in the Creekside industrial park.

Attachments

1. Section 260 - 272 of Ordinance 2050 (Preservation of Trees and Vegetation)
2. Inventory Forms and public responses



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## APPLICATION COMPLETENESS OPTION

TO: City of Beaverton

FROM: CITY OF BEAVERTON, DEVELOPMENT SERVICES DIVISION

RE: CASE FILE #: BDR #20010213; TPP 2001-0008; CUP 2001-0031

The Facilities Review Committee finished its completeness review of the referenced application(s) on \_\_\_\_\_, and determined that the application is incomplete, as identified in the letter from \_\_\_\_\_ dated \_\_\_\_\_. Per Oregon Revised Statutes (ORS) 227.178, the following two options are available to proceed with this application.

**Please check the appropriate box, sign and date this form and return this completed form to the City of Beaverton, Development Services Division, no later than \_\_\_\_\_. The form may be hand delivered, mailed or faxed.**

### CITY OF BEAVERTON

**By Mail:**  
Development Services Division  
Information Counter  
PO Box 4755  
Beaverton, OR 97076

**By Hand Delivery:**  
Development Services Division  
Information Counter  
4755 SW Griffith Drive  
Beaverton, OR 97076

**By Fax:** 503-526-3720

**TEL:** 503-526-2420

**Option 1:**

I, as the applicant or with authority to act on behalf of the applicant(s), hereby request to have the above application processed "as is"; I refuse to submit the remaining required information.

**Option 2:**

I, as the applicant or with authority to act on behalf of the applicant(s), hereby state that some or all of the remaining information required to make the above application complete will be submitted. I understand that the required information must be submitted within 180 days of the date the application was first submitted or the application will be retired without a decision.

Dated January 30, 2002

Signature [Handwritten Signature]

Name (Please Print) Frank Angelo



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

January 18, 2002

Frank Angelo  
Angelo, Eaton and Associates  
620 SW Main  
Suite 201  
Portland, OR 97205

RE: TPP2001-0008 (Beaverton High School Historic Trees)

Dear Mr. Angelo:

Thank you for submitting the Beaverton High School Tree Preservation Plan, on behalf of the Beaverton School District. The Facilities Review Committee finished its completeness review on January 9, 2002 and determined your application deemed incomplete for the following reasons:

**COMMUNITY DEVELOPMENT DEPT. (CDD):  
Tree Transportation Plan Review Application**

The Tree Preservation Plan Checklist requires submission of elements that were not included with your application submittal. The following items must be addressed and submitted in order for this application to be deemed complete:

1. **Specific Tree Species:** Please label all trees with their common names.
2. **Drip Line of Each Tree or Entire Grove:** Please show the drip line of each tree to be removed and to remain on site.
3. **Clear Delineation of Construction Disturbance Limits:** Please indicate the area in which construction of the proposed cafeteria will be located; include the location(s) where material will be stored.
4. **Methods of Survival:** Please describe the methods or techniques to be used to ensure the survival of the existing trees both during and after construction.

When you are ready to re-submit your application, please provide three (3) copies each of the written narrative, reports, and folded complete plan sets bound. The materials provided should be collated into three submittal packages. Additional copies will be required at a later time when your project has been scheduled for final review and

TPP2001-0008 (Beaverton High School Historic Trees)

processing. Additionally, please provide one (1) letter/memorandum addressing the specific items requested in this letter so that we might better understand the materials that you submit.

One set of the original application materials is kept on file at the Development Services Division. At the time of a future application, we can provide the information on file to assist you in preparing your materials. For information about application requirements, forms, fees and schedules, please contact the Development Services Counter at 503-526-2420.

If you have any questions regarding this letter or any other aspect of our process, please don't hesitate to call. I am including a list of the primary members of the Facilities Review Committee who were involved in the completeness review.

LAND USE & DESIGN: Jeff Caines – 503-526-2419

TRANSPORTATION PLANNING: Don Gustafson – 503-350-4057

SITE ENGINEERING: Jim Duggan – 503-526-2442

Thank you and we look forward to working with you to complete your application.

Sincerely,



Jeff Caines  
Assistant Planner

cc: (5): Lampa; Caines (2); Counter; Dept. file

RECEIVED

JAN 09 2002

City of Beaverton  
Development Services



ARCHITECTURE  
PLANNING/INTERIORS

TEL (503) 274-5432  
FAX (503) 274-0085

1953 NW Kearney St.  
Portland, OR. 97209

PRELIMINARY

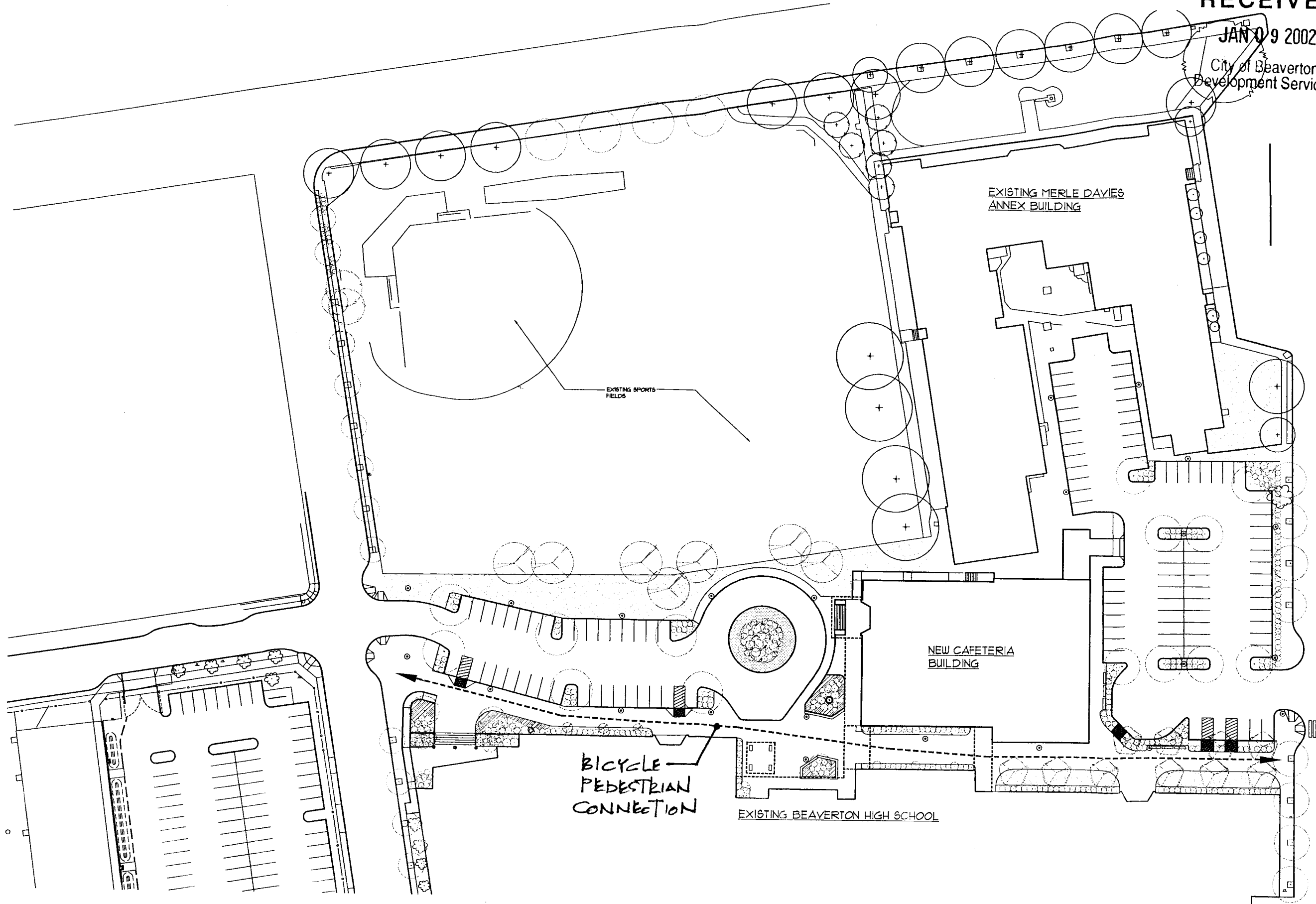
**B.H.S. CAFETERIA ADDITION**  
**BEAVERTON SCHOOL DISTRICT #48**  
13000 S.W. SECOND ST.  
BEAVERTON, OREGON 97005

Issued : \_\_\_\_\_  
Revisions : \_\_\_\_\_  
\_\_\_\_\_

Date : 12-21-01  
Job No. : 01151  
Drawn by : EGN  
Checked by : JDW

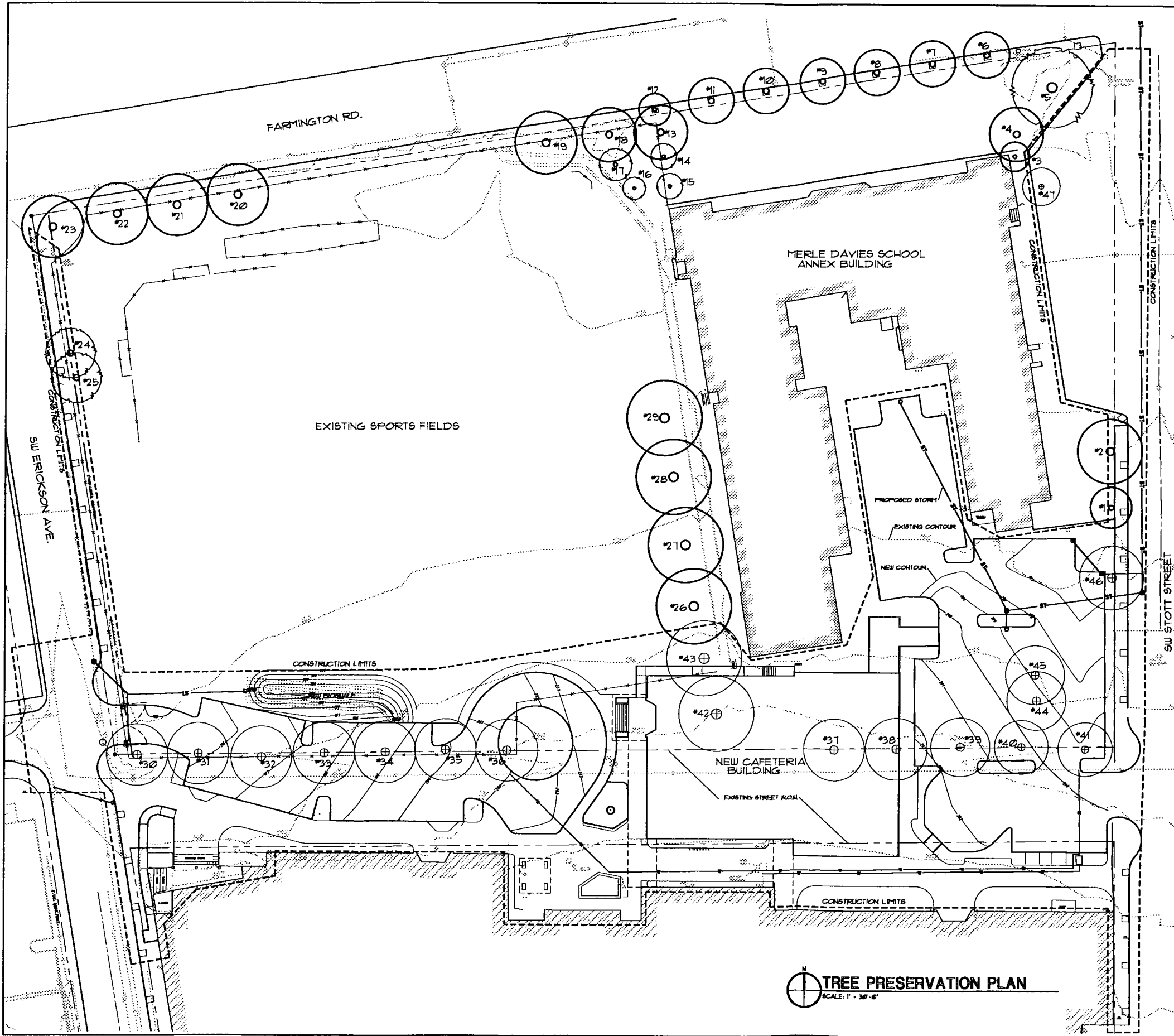
Drawing Title :  
ENLARGED  
SITE PLAN

Sheet No.  
**2.1**



File: .....01151-21SP.dwg  
Plot Scale: 1  
Plot Date: 01-08-02  
Plot Time: 1:39 PM

**ENLARGED SITE PLAN**  
SCALE: 1" = 30'-0"



- GENERAL NOTES**
- SEE TREE PRESERVATION NARRATIVE FOR ARBORIST REPORT INFORMATION ON EXISTING TREES.
  - TREE SYMBOL APPROXIMATES ACTUAL CANOPY DIMENSION.
  - PRESERVED TREES WITHIN THE LIMITS OF CONSTRUCTION SHALL BE PROTECTED BY APPROVED FENCING PLACED FIVE FEET OUTSIDE OF DRIP LINE OF TREE.
  - WHERE CONSTRUCTION OCCURS WITHIN THE DRIP LINE OF ANY TREE TO BE PRESERVED EXISTING GRADES SHALL NOT BE CHANGED WITHOUT APPROVAL OF LANDSCAPE ARCHITECT.
  - SEE CIVIL DRAWINGS FOR EXISTING AND PROPOSED UTILITY LOCATIONS.

- LEGEND**
- (Circle with dot) EXISTING TREE TO REMAIN
  - (Circle with cross) EXISTING TREE TO BE REMOVED

**EXISTING TREE DATA**

NUMBER	SPECIES	SIZE (DBH)
1	SUGAR MAPLE (ACER SACCHARINUM)	26.6"
2	NORWAY MAPLE	17.1"
3	DECIDUOUS TREE	17.1"
4	CHERRY (PRUNUS AVIUM)	16.8"
5	DECIDUOUS CEDAR (CEDRUS DECURVA)	16.8"
6	NORWAY MAPLE	15.1"
7	NORWAY MAPLE	15.1"
8	NORWAY MAPLE	15.1"
9	NORWAY MAPLE	15.1"
10	NORWAY MAPLE	15.1"
11	NORWAY MAPLE	15.1"
12	NORWAY MAPLE	15.1"
13	CHERRY (PRUNUS AVIUM)	15.1"
14	PINE (PINUS)	15.1"
15	PINE	15.1"
16	CEDAR	15.1"
17	NORWAY MAPLE	15.1"
18	NORWAY MAPLE	15.1"
19	NORWAY MAPLE	15.1"
20	NORWAY MAPLE	15.1"
21	NORWAY MAPLE	15.1"
22	NORWAY MAPLE	15.1"
23	NORWAY MAPLE	15.1"
24	PINE (PINUS)	15.1"
25	PINE	15.1"
26	SYCAMORE	15.1"
27	SYCAMORE	15.1"
28	SYCAMORE	15.1"
29	SYCAMORE	15.1"

**TREES TO BE REMOVED**

NUMBER KEY	SPECIES	SIZE (DBH)
30	NORWAY MAPLE (ACER PLATANOIDES)	16.7"
31	NORWAY MAPLE	16.6"
32	NORWAY MAPLE	15.8"
33	NORWAY MAPLE	15.4"
34	NORWAY MAPLE	15.3"
35	NORWAY MAPLE	15.1"
36	NORWAY MAPLE	15.1"
37	NORWAY MAPLE	15.1"
38	NORWAY MAPLE	15.1"
39	NORWAY MAPLE	15.1"
40	NORWAY MAPLE	15.1"
41	NORWAY MAPLE	15.1"
42	SYCAMORE (PLATANUS ACERFOLIA)	15.6"
43	SYCAMORE (PLATANUS ACERFOLIA)	15.6"
44	ELM (ULMUS PURPURA)	15.5"
45	ELM (ULMUS PURPURA)	15.5"
46	NORWAY MAPLE	15.4"
47	RED MAPLE (ACER RUBRUM)	15.2"

**TREE PROTECTION PLAN**  
**B.H.S. CAFETERIA ADDITION**  
 BEAVERTON, OREGON

**MATELA ASSOCIATES INC.**  
**LANDSCAPE ARCHITECTURE**  
 RONALD M. MATELA, A.S.L.A. 338 990-0775 PHONE/FAX  
 401 MAIN STREET 8001 VANCOUVER, WA 98660

**FIGURE 5**

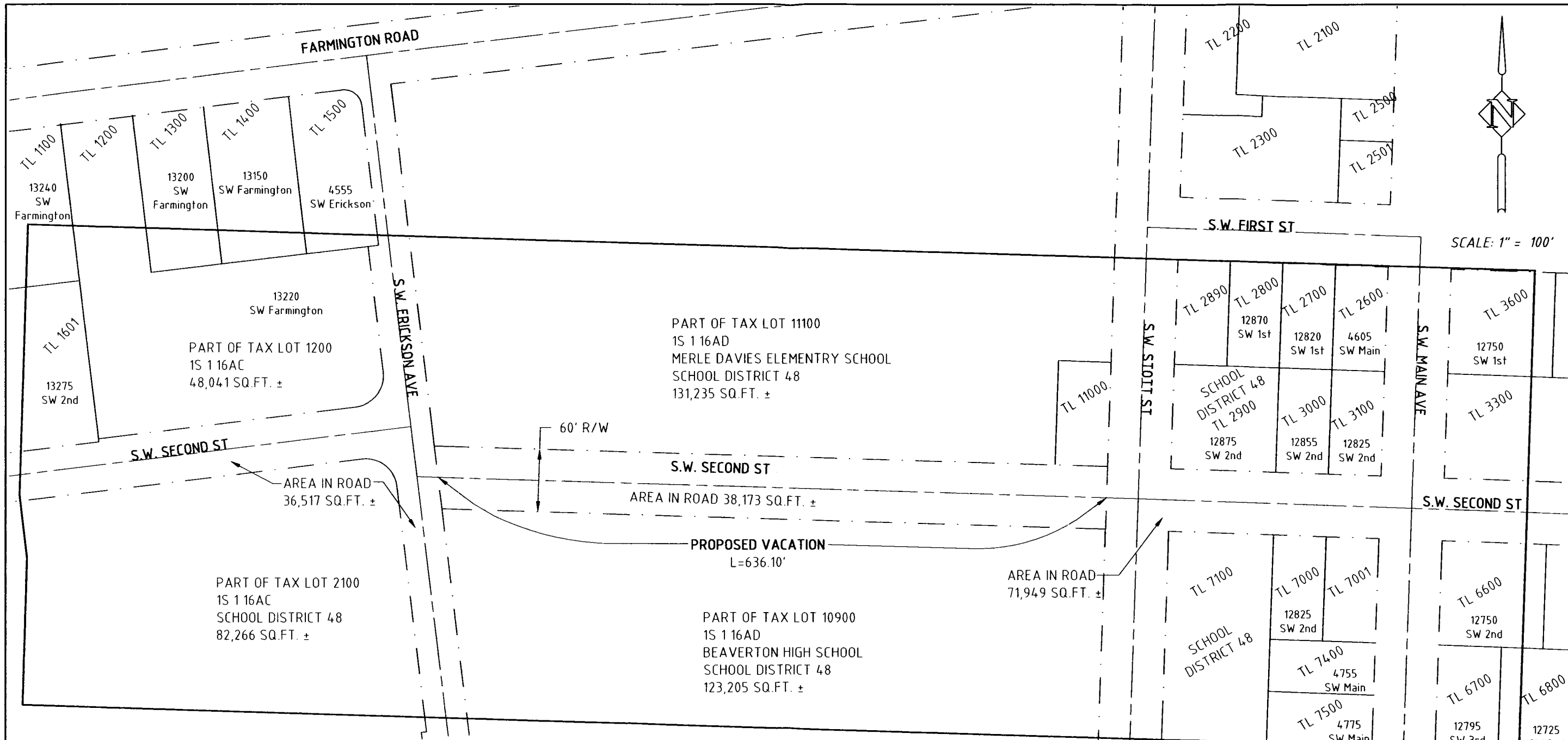
**TREE PRESERVATION PLAN**  
 SCALE: 1" = 30'-0"

**DESIGN REVIEW SUBMITTAL**

DESIGNED	DRAWN	CHECKED	DATE
			02/19/01
REVISIONS			
NO.	DESCRIPTION	DATE	

**RECEIVED**  
 1 of 1 **JAN 09 2002**  
 City of Beaverton  
 Development Services





13220 SW Farmington  
 PART OF TAX LOT 1200  
 1S 1 16AC  
 48,041 SQ.FT. ±

PART OF TAX LOT 11100  
 1S 1 16AD  
 MERLE DAVIES ELEMENTARY SCHOOL  
 SCHOOL DISTRICT 48  
 131,235 SQ.FT. ±

PART OF TAX LOT 2100  
 1S 1 16AC  
 SCHOOL DISTRICT 48  
 82,266 SQ.FT. ±

PART OF TAX LOT 10900  
 1S 1 16AD  
 BEAVERTON HIGH SCHOOL  
 SCHOOL DISTRICT 48  
 123,205 SQ.FT. ±

S.W. SECOND ST

S.W. SECOND ST

S.W. FIRST ST

S.W. SECOND ST

S.W. THIRD ST

AREA IN ROAD  
 36,517 SQ.FT. ±

AREA IN ROAD 38,173 SQ.FT. ±

AREA IN ROAD  
 71,949 SQ.FT. ±

PROPOSED VACATION  
 L=636.10'

60' R/W

AFFECTED AREA LESS ROAD RIGHT-OF-WAY: 512,406 SQ.FT.

APPLICANT'S PROPERTY AREA LESS ROAD RIGHT-OF-WAY:

T.L. 2100 1S 1 16AC:	82,266 SQ.FT. ±
T.L. 2900 1S 1 16AD:	10,000 SQ.FT. ±
T.L. 7100 1S 1 16AD:	20,000 SQ.FT. ±
T.L. 10900 1S 1 16AD:	123,205 SQ.FT. ±
T.L. 11100 1S 1 16 AD:	126,235 SQ.FT. ±
	<u>361,706 SQ.FT. ±</u>

TAX LOT 1100 1S 1 16AC (PART OF):	2,652 SQ.FT. ±
TAX LOT 1200 1S 1 16AC (PART OF):	45,453 SQ.FT. ±
TAX LOT 1300 1S 1 16AC (PART OF):	2,588 SQ.FT. ±
TAX LOT 1400 1S 1 16AC (PART OF):	2,189 SQ.FT. ±
TAX LOT 1500 1S 1 16AC (PART OF):	1,029 SQ.FT. ±
TAX LOT 1601 1S 1 16AC (PART OF):	9,789 SQ.FT. ±
TAX LOT 2600 1S 1 16AD:	5,000 SQ.FT. ±
TAX LOT 2700 1S 1 16AD:	5,000 SQ.FT. ±
TAX LOT 3000 1S 1 16AD:	5,000 SQ.FT. ±
TAX LOT 3100 1S 1 16AD:	5,000 SQ.FT. ±

TAX LOT 3300 1S 1 16AD (PART OF):	8,000 SQ.FT. ±
TAX LOT 3600 1S 1 16AD (PART OF):	8,000 SQ.FT. ±
TAX LOT 6600 1S 1 16AD (PART OF):	8,000 SQ.FT. ±
TAX LOT 6700 1S 1 16AD:	6,000 SQ.FT. ±
TAX LOT 6800 1S 1 16AD (PART OF):	2,000 SQ.FT. ±
TAX LOT 7000 1S 1 16AD:	5,000 SQ.FT. ±
TAX LOT 7001 1S 1 16AD:	5,000 SQ.FT. ±
TAX LOT 7400 1S 1 16AD:	5,000 SQ.FT. ±
TAX LOT 7500 1S 1 16AD:	5,000 SQ.FT. ±
TAX LOT 11,000 1S 1 16AD:	5,000 SQ.FT. ±

DATE	NO.	DESCRIPTION

DESIGNED: J.B.  
 DRAWN: J.B.  
 CHECKED: GAA  
 DATE: DEC 2001

**Harper Houf Righellis, Inc.**  
 ENGINEERS - PLANNERS  
 5200 SW MACADAM AVENUE, SUITE 500, PORTLAND, OR 97201  
 TEL. 503.221.1131 www.hrcivil.com FAX 503.221.1171

**FIGURE 4**  
**AFFECTED PROPERTIES MAP**



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

February 1, 2002

Ms. Claire Doolittle  
Beaverton School District  
Facilities Department  
16550 SW Merlo Road  
Beaverton, OR 97006

RE: REQUEST FOR PAYMENT

Dear Ms. Doolittle:

On 1/31/2002 we received a **Purchase Order** for the Beaverton School District for **Beaverton High School Cafeteria Phase II, Tree Preservation, Conditional Use and Variance for Bicycle Rack Storage** fees as listed below. The request was accompanied by Purchase Order(s) # F 05997 dated 1/31/2002 for processing payment of the following fees:

\$ 1,662.00	Type 3 Design Review Fee – Cafeteria Phase II – BDR 2001-0213
\$ 1,199.00	Conditional Use Permit Fee – CUP 2001-0031
\$ 883.00	Variance Fee – Bicycle Rack Storage – VAR 2002-0002
\$ 600.00	Tree Preservation Fee – TPP 2001-0008

Please remit a check payable to the City of Beaverton in the amount of **\$4,344.00** representing payment for **PO # F 05997** per above-referenced fees, and refer to the following permit number(s) on the check: **BDR 2001-0213, CUP 2001-0031, VAR 2002-0002, TPP 2001-0008**

If you have any questions, please call me at (503) 526-2429.

Sincerely,

Steven A. Sparks, AICP  
Development Services Manager

Enclosure(s)

cc: file

G:Bonnie/Beaverton School Dist. PO Letter

**BEAVERTON SCHOOL DISTRICT #48**  
 16550 SW Merlo Rd. Beaverton, Oregon 97006-5152  
 503-591-8000 FAX: 503-591-4139

Contact: Vlad Voytilla 503-591-4457 1-31-02  
 Name Phone FAX Date

PURCHASE ORDER NUMBER  
 VALID ONLY WHEN NUMBERED, DATED AND SIGNED

**F05997 JAN 31 8**

**DELIVER TO:**

SCHOOL:  
Facilities Department

STREET ADDRESS:  
16550 SW Merlo Rd.

CITY:  
Beaverton, OR 97006-5152

**BILL TO:**

ACCOUNTS PAYABLE  
 Beaverton School District #48  
 16550 SW Merlo Rd.  
 Beaverton, Oregon 97006-5152  
 Telephone: 503/591-4324

**VENDOR INSTRUCTIONS**

1. Send original invoice and duplicate to the District. A separate invoice for each purchase order is required.
2. Enclose a packing list with all deliveries.
3. Ship prepaid.
4. Include purchase order number on all packages, invoices, shipping notices, and all other correspondence relating to this order.
5. Additional terms and conditions on the reverse are a part of this purchase order.

VENDOR ADDRESS

CITY OF BEAVERTON  
 PO BOX 4755  
 BEAVERTON, OR 97076-4755

RECEIVED  
 JAN 31 2002

**PAYMENT TERMS: NET 30**  
**THIS IS A TAX EXEMPT DISTRICT.**

ATTENTION: PROJECT # 1045

QTY.	UNIT	CATALOG NUMBER	DESCRIPTION	UNIT PRICE	AMOUNT
			BEAVERTON HIGH SCHOOL, PHASE II	\$	\$
		<u>BDR 2001-0213</u>	CAFETERIA BDR APPLICATION FEE		1662.00
		<u>TPP 2001-0008</u>	TREE PRESERVATION APPL. FEE		600.00
		<u>CUP 2001-0031</u>	CONDITIONAL USE PERMIT FEE		1199.00
		<u>VAR 2002-0002</u>	VARIANCE FOR BICYCLE RACK STORAGE FEE		883.00
SHIP VIA:				FOB POINT:	SHIPPING/HANDLING:
FOR EDUCATIONAL PURPOSES ONLY. TAX EXEMPTION NO. A-93-730104K Federal Tax I.D. No. 93-6001065. Acceptance of and filling this order certifies compliance with all Federal and State of Oregon safety and health laws. FAILURE TO INCLUDE OUR PURCHASE ORDER NUMBER ON ALL CORRESPONDENCE COULD RESULT IN DELAY IN PAYMENT OF YOUR INVOICE.				<b>TOTAL</b>	\$ 4,344.00

*C. Wick*  
 Beaverton School District #48

**BELOW INFORMATION IS FOR DISTRICT USE ONLY.**

FISCAL YEAR 01-02

RETURN VENDOR COPY TO ORIGINATOR.

✓ 0452	1045	62	3003	\$
410	640	4152	0670	\$
				\$
				\$
				\$
				\$
FUND OR GRANT COST CENTER PROGRAM			OBJECT	TOTAL

Total must match total above.

**PARTIAL PAYMENT RECORD**

Date Partial Order Rec'd.	Received By (Name)	Amount Approved
		\$
		\$
		\$
		\$
		\$
	Completed Order Total	\$

*M. Money*  
**COST CENTER AUTHORIZATION**  
 Retain GOLDENROD copy in building. Forward all other copies to Business Office.



& A s s o c i a t e s

M E M O R A N D U M

**TO:** City of Beaverton  
Historic Review Committee  
**FROM:** Frank Angelo *FA*  
**DATE:** April 30, 2002  
**CC:** Project Team  
**FILE #:** 007-008  
**RE:** Beaverton High School Tree Preservation Plan

---

In reviewing the Staff Report for TPP 2001-008 (Beaverton High School Tree Preservation Plan) it appears that page 4 of the Applicant's submittal was not printed in the agenda packet. This page provides a listing of the Proposed Trees to be Removed (Table 2). I have attached this page to this memorandum for the Historic Review Committee's consideration.

Also the Tree Preservation Plan map (Figure 1 of 1) inadvertently did not show Trees #28, #29, and #46 as being removed. The legend on this figure does indicate that these trees are proposed for removal, as does Table 2 in the application. The application also refers to 20 trees proposed for removal, which is the correct number including Trees #28, #29, and #46.

I have attached a revised Tree Preservation Plan map that shows Trees #28, #29, and #46 as proposed to be removed.

---

620 SW Main, Suite 201  
Ph: 503.224.6974 Fax: 503.227-3679

Land Use Planning Transportation Planning Project Management

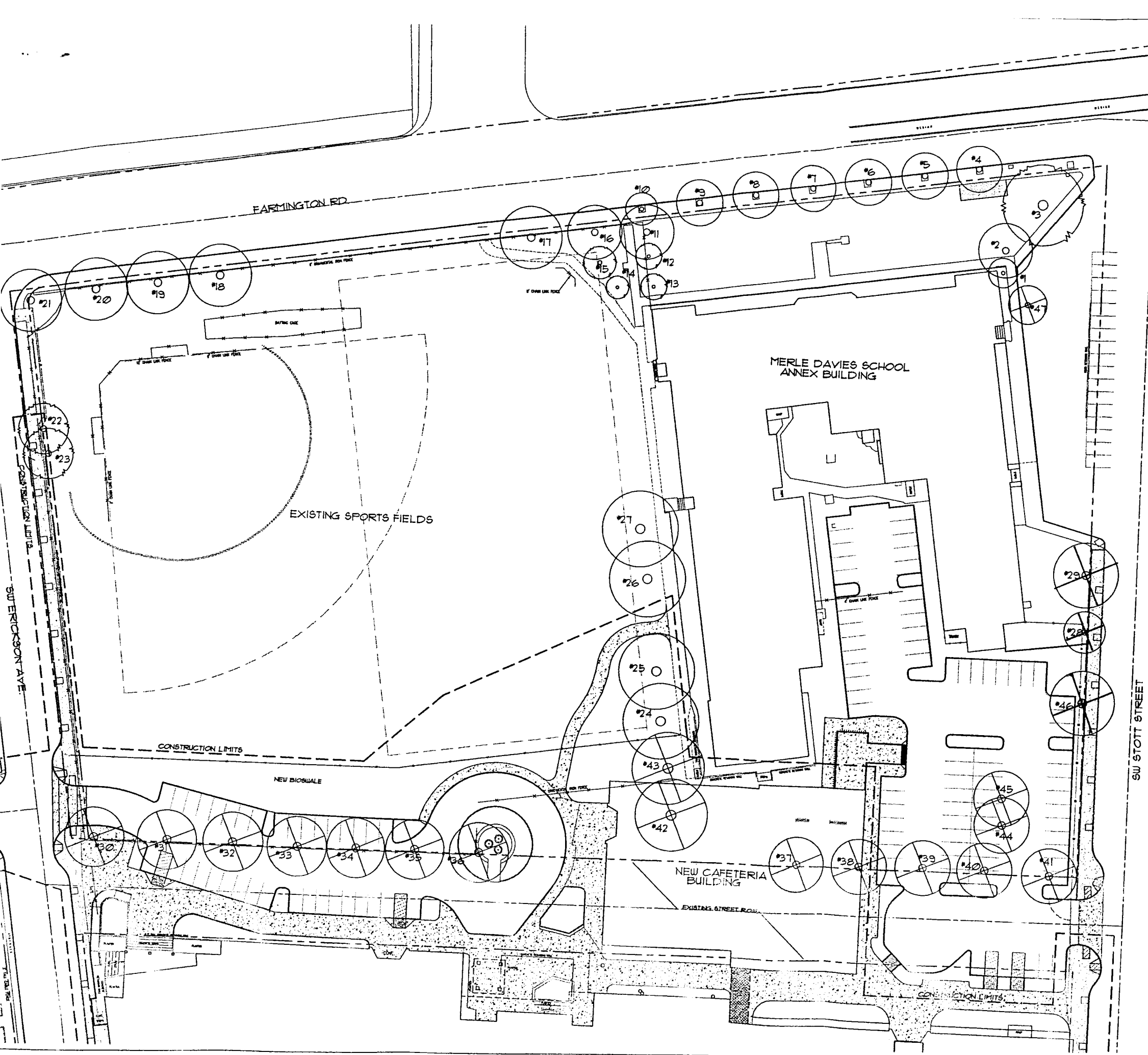
**Proposed Trees to be Removed**  
**Table 2**

Tree Number on Tree Plan *	Arborist Report #	Species	Tree Diameter (DBH) (in inches)	Conditions/Comments	City Tree Inventory (approximate location)	City Tree Inventory Rating***	
28	22	Sugar Maple	6.8	Fair	I	3	2
29	23	Norway Maple	17.7	Fair	I	3	2
30	1	Norway Maple	16.1	Fair	D	3	2
31	2	Norway Maple	16.6	Poor	D	3	2
32	3	Norway Maple	19.0	Poor	D	3	2
33	4	Norway Maple	20.4	Poor	D	3	2
34	5	Norway Maple	18.3	Fair	D	3	2
35	6	Norway Maple	21.7	Poor	D	3	2
36	7	Norway Maple	15.8	Fair	D	3	2
37	12	Norway Maple	18.1	Poor	D	3	2
38	13	Norway Maple	15.5	Fair	D	3	2
39	14	Norway Maple	20.0	Poor	D	3	2
40	15	Norway Maple	19.3	Fair	D	3	2
41	16	Norway Maple	18.2	Fair	D	3	2
42	17	Sycamore	28.6	Fair	H	3	2
43	18	Sycamore	29.0	Fair	H	3	2
44	19	Elm	20.0	Poor	D	3	2
45	20	Elm	17.9	Fair	D	3	2
46	21	Norway Maple	25.4	Poor	I	3	2
47	n/r**	Red Maple	10.0	Not inspected	C	1	3

\* Tree Protection Plan, Attachment 15

\*\* n/r = Not Referenced in Arborist Report

\*\*\* City of Beaverton Tree Inventory. Tree/Grove Historical Rating Sheet



**GENERAL NOTES**

- SEE TREE PRESERVATION NARRATIVE FOR ARBORIST REPORT INFORMATION ON EXISTING TREES.
- TREE SYMBOL APPROXIMATES ACTUAL CANOPY DIMENSION.
- PRESERVED TREES WITHIN THE LIMITS OF CONSTRUCTION SHALL BE PROTECTED BY APPROVED FENCING PLACED FIVE FEET OUTSIDE OF DRIP LINE OF TREE.
- WHERE CONSTRUCTION OCCURS WITHIN THE DRIP LINE OF ANY TREE TO BE PRESERVED EXISTING GRADES SHALL NOT BE CHANGED WITHOUT APPROVAL OF LANDSCAPE ARCHITECT.
- SEE CIVIL DRAWINGS FOR EXISTING AND PROPOSED UTILITY LOCATIONS.

**LEGEND**

- EXISTING DECIDUOUS TREE TO REMAIN
- EXISTING EVERGREEN TREE TO REMAIN
- EXISTING DECIDUOUS TREE TO BE REMOVED
- LIMITS OF CONSTRUCTION LINE

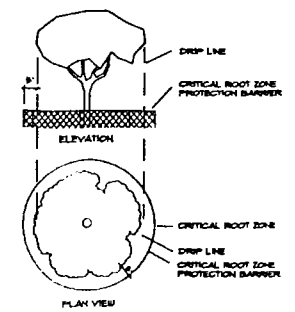
**EXISTING TREE DATA**

**TREES TO BE PRESERVED**

#	SPECIES	SIZE (DBH)
1	BEECH	16"
2	CHERRY (PRUNUS AVIUM)	16.0"
3	DECIDUOUS CEDAR (CEDRUS DECCADUR)	16.0"
4	NORWAY MAPLE	16.0"
5	NORWAY MAPLE	15"
6	NORWAY MAPLE	15"
7	NORWAY MAPLE	15"
8	NORWAY MAPLE	15"
9	NORWAY MAPLE	15"
10	NORWAY MAPLE	15"
11	CHERRY (PRUNUS AVIUM)	15"
12	PINE (PINUS)	15"
13	PINE (PINUS)	15"
14	CEDAR	15"
15	CEDAR	15"
16	NORWAY MAPLE	15"
17	NORWAY MAPLE	24"
18	NORWAY MAPLE	24"
19	NORWAY MAPLE	24"
20	NORWAY MAPLE	14"
21	NORWAY MAPLE	14"
22	PINE (PINUS)	26"
23	PINE	26"
24	SYCAMORE	29"
25	SYCAMORE	29"
26	SYCAMORE	29"
27	SYCAMORE	29"

**TREES TO BE REMOVED**

#	SPECIES	SIZE (DBH)
28	SUGAR MAPLE (ACER SACCHARINUM)	8.6"
29	NORWAY MAPLE	17.7"
30	NORWAY MAPLE (ACER PLATANOIDES)	16.6"
31	NORWAY MAPLE	16.6"
32	NORWAY MAPLE	15.0"
33	NORWAY MAPLE	20.4"
34	NORWAY MAPLE	18.3"
35	NORWAY MAPLE	21.1"
36	NORWAY MAPLE	15.0"
37	NORWAY MAPLE	18.1"
38	NORWAY MAPLE	25.5"
39	NORWAY MAPLE	20.0"
40	NORWAY MAPLE	18.3"
41	NORWAY MAPLE	18.2"
42	SYCAMORE (PLATANUS ACERIFOLIA)	28.6"
43	SYCAMORE (PLATANUS ACERIFOLIA)	28.6"
44	ELM (ULMUS PURILLAI)	20.0"
45	ELM (ULMUS PURILLAI)	17.8"
46	NORWAY MAPLE	25.4"
47	RED MAPLE (ACER RUBRUM)	10.0"



1 TREE PROTECTION DETAIL

**TREE PRESERVATION PLAN**  
**B.H.S. CAFETERIA ADDITION**  
 BEAVERTON, OREGON

**Harper Houff**  
**Righellis, Inc.**  
 ENGINEERS • PLANNERS  
 5200 SW MACDONALD AVENUE, SUITE 500, PORTLAND, OR 97201  
 TEL. 503.221.1131 www.hhrif.com FAX 503.221.1171

DESIGNED:	DRAWN:	CHECKED:	DATE:
			02-19-02
REVISIONS			
NO.	DATE	DESCRIPTION	
SHEET NO.	1 of 1		JOB NO. B52-23



& Associates

Fax

620 SW Main Suite 201  
Portland OR 97205  
503-224-6974  
503-227-3679 - FAX

To: Sambo Kirkman From: Frank Angelo  
 Firm: \_\_\_\_\_ Date: March 21, 2002  
 Fax: 503-526-3720 RE: \_\_\_\_\_  
 Phone: \_\_\_\_\_ Pages 2

- Urgent     For Review     Please Comment     Please Reply     Please Recycle

Please call and confirm receipt.  
A.



620 SW Main, Suite 201  
Portland, Oregon 97205

Tel: 503 . 224 . 6974  
Fax: 503 . 227 . 3679

March 21, 2002

Sambo Kirkman  
City of Beaverton  
Planning Department  
4755 SW Griffith Drive  
P.O. Box 4755  
Beaverton, Oregon 97076

RE: Beaverton High School Tree Preservation Plan (TPP 2001-0008)  
Beaverton High School Board of Design Review (BDR 2001-0213)  
Beaverton High School Conditional Use Permit (CUP 2001-0031)

As the representative for the Beaverton School District, I request that the public hearings on the above applications be continued as noted below. We will waive the 120-day limit on all three applications to a date certain – June 18, 2002. According to staff, this will provide sufficient time for the respective hearing bodies to conduct the hearings, render a decision and cover any potential appeals.

Beaverton High School Conditional Use Permit (CUP 2001-0031)

Based on our request at the March 20, 2002 Planning Commission meeting, this hearing has been continued to April 24, 2002.

Beaverton High School Tree Preservation Plan (TPP 2001-0008)

We are requesting a continuance of the March 21, 2002 hearing to April 30, 2002. You indicated that the next meeting of the Historic Review Committee is scheduled for April 30, 2002. It is my understanding that TPP 2001-0008 will be scheduled on that date.

Beaverton High School Board of Design Review (BDR 2001-0213)

We are requesting a continuance of the March 28, 2002 hearing to May 9, 2002. You indicated that there is room on the May 9<sup>th</sup> agenda for this application.

Thank you,

  
Frank Angelo

cc. Mike Maloney, Beaverton School District



- Frank Angelo
- Chris Eaton, AICP
- Catherine Corliss, AICP
- Jean D'Agostino, AICP
- Karen Siegel







## M E M O R A N D U M

**TO:** Jeff Caines, City of Beaverton  
**FROM:** Frank Angelo, Principal   
Robin Scholetzky, AICP, Planner   
**DATE:** January 30, 2002  
**RE:** Design Review Type 3, Beaverton High School Cafeteria Development  
Conditional Use, Beaverton High School  
Tree Preservation Plan  
Variance—Public Hearing, Bicycle Parking  
**CC:** Vlad Voytilla, Project Manager, Beaverton School District

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The attached plans are in response to your request dated January 18, 2002 for additional material for the following land use applications:

- Design Review Type 3, Beaverton High School Improvements
- Conditional Use, Beaverton High School Improvements
- Tree Preservation Plan, Beaverton High School Improvements

Please note that in addition to these three applications, we are submitting a Variance – Public Hearing request relating to bicycle parking requirements.

We have included revised narratives for each of the three applications. Any related attachments or exhibits that have not been amended are not included in this resubmittal package and should refer to the original application.

---

620 SW Main, Suite 201  
Phone: 503.224.6974 Fax: 503.227-3679

Land Use Planning Transportation Planning Project Management

- Application Resubmittal Materials - continued
- Page 2

## Plans

The following revised plans replace plans submitted with the original application:

ORIGINAL PLANS	ARE NOW	REVISED PLANS
1. Cover Sheet	→	1. Revised Cover Sheet
2. Existing Conditions Plan	→	2. Revised Existing Conditions Plan
3. Dimension Plan Public Improvements	→	3. Revised Existing Conditions Plan
4. Dimension Plan Private Improvements (west)	→	4. Revised Dimension Plan Public Improvements
5. Dimension Plan Private Improvements (east)	→	5. Revised Dimension Plan Private Improvements (west)
		6. Revised Dimension Plan Private Improvements (east)
		7. Revised Dimension, Grading Utility Lot H
6. Grading, Storm and Erosion Control Public Improvements	→	8. Revised Grading, Storm and Erosion Control Public Improvements
7. Grading, Utilities and Erosion Control Private Improvements (west)	→	9. Revised Grading, Utilities and Erosion Control Private Improvements (west)
8. Grading, Utilities and Erosion Control Private Improvements (East)	→	10. Revised Grading, Utilities and Erosion Control Private Improvements (east)
9. Landscape and Lighting Plan Public Improvements	→	11. Revised Landscape and Lighting Plan Public Improvements
10. Landscape and Lighting Plan Private Improvements (west)	→	12. Revised Landscape and Lighting Plan Private Improvements (west)
11. Landscape and Lighting Plan Private Improvements (East)	→	13. Revised Landscape and Lighting Plan Private Improvements (East)
		14. Revised Landscape and Lighting Plan Private Improvements (Lot H)
12. Detail Sheet for Water Quality Construction	→	15. Revised Detail Sheet for water quality construction
13. Detail Sheet for Water Quality Construction	→	16. Revised Detail Sheet
14. Detail Sheet for Water Quality Construction and Bike Rack Detail	→	17. Revised Detail Sheet for Bike Rack Detail
15. Tree Protection Plan	→	18. Revised Tree Preservation Plan
16. Architectural Elevations	→	19. Revised Architectural Elevations
17. Materials Board		NOT INLCUED IN REVISED SET

We have provided six full size set of plans for this application submittal.

### **Design Review Type 3, Beaverton High School Improvements**

In response to the letter dated January 18, 2002 for the Design Review Type 3, Beaverton High School Improvements, please find a revised narrative that addresses the following:

1. The request to indicate trees listed on the Existing Conditions Plan; Dimensioned Site Plan; Grading Plan; Utility Plan and Landscape Plan has been met by the following:

The City of Beaverton Tree Inventory provided by the City does not identify trees as Historic or Significant, it only lists criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above (indicating a potentially historic tree). For more information on this analysis, please find attached the Tree Preservation Plan.

2. Grading Plan. The Revised Grading Plan indicates the dimension from centerline to edge of proposed right-of-way. All existing trees over 6" dbh have been labeled.
3. Utility Plan. The Utility Plan illustrates in greater detail how the proposed utility relocations will work in the narrow corridor. Existing water line and overhead utilities (electrical service, telephone, and cable television) along the section of 2<sup>nd</sup> Street to be vacated, will be relocated underground between the new cafeteria building and the existing High School (See Revised Grading, Utilities and Erosion Control Private Improvements (west) and Revised Grading, Utilities and Erosion Control Private Improvements (east). The corridor between the building is a minimum of 20 feet-wide. Due to the proposed sky bridge between the buildings, the water line will be installed within a pipe casing to eliminate the need for trench excavation under the sky bridge.
4. Landscape Plan. All plants have been identified on the landscape plan. The identification of the plants includes the genus and species. Two symbols have been used to identify trees to be saved and trees to be removed. A notation in each of these two symbols indicates either deciduous trees or evergreen/coniferous trees. The areas for stormwater quality have been revised to meet Clean Water Services standards. The use of seed mix has been minimized.
5. Lighting Plan. The Lighting Plans (Revised Landscape And Lighting Plan Public Improvements; Revised Landscape And Lighting Plan Private Improvements (West); Revised Landscape And Lighting Plan Private Improvements (East) and Revised Landscape And Lighting Plan Private Improvements (Lot H)) better indicate the location and type of lighting proposed. All exterior lighting indicates the area and pattern of illumination measured at ½ foot candlepower. Each isobar has been labeled with its corresponding candlepower.

NOTE: Revised cut sheets (Revised Appendix D) are included with the Design Review Application.

6. Hours of Operation. Beaverton High School is open from 6:30am to 2:30pm for classroom activities. After-school activities can run until 10:00pm, depending on the activity. It is important to note that the school facilities are available for a number of users and are not limited to Beaverton School District programs. Tualatin Hills Parks and Recreation District and community groups conducting classes or events are able to use the facilities at Beaverton High School. These events are scheduled through the school as with all Beaverton School

District schools, Beaverton High School is a community facility and is made available to groups when scheduling permits.

7. Architectural Elevations. A revised set of Architectural Elevations indicates the location and type of materials and colors according to the Materials Board submitted with the initial application. NOTE: One additional Pittsburgh Paint color, #314-4 is shown on the revised Architectural Elevations to be used as an exterior siding color. All other colors to remain the same.
8. Bicycle Parking. An application for a Variance-Public Hearing for a reduction in the amount of required long-term parking for bicycles is included with this submittal package. The locations of the proposed bicycle parking is illustrated on the following plans, Cover Sheet and Revised Grading, Utilities and Erosion Control Private Improvements (East).
9. Parking Standards. The improvements to the Beaverton High School site include development of Lot H. The addition of Lots C, D and H provide 436 spaces which is 46 spaces more than is required based on current enrollment of 1,800 students and 150 staff. Based on a potential enrollment of 2,200 students and increase of staff to 200; the parking minimum required would increase to 480 spaces. This application includes a 10% parking reduction credit for construction of a pedestrian plaza and transit improvements as described in Section 60.20.10.10.A.2 of the Beaverton Development Code. The development of Lots C, D and H along with the pedestrian plaza parking reduction credit will meet the parking requirements for the High School. We have made preliminary contact with Tri-Met representatives regarding the location, size and materials for the plaza. The location of the plaza is shown on the Revised Dimension Plan Public Improvements Plan and the Revised Grading, Storm and Erosion Control Public Improvements Plan.

10. Traffic Impact Analysis. During the Facilities Review meeting with the City of Beaverton on January 23, 2002, the City stated that the Traffic Impact Analysis provided with the Comprehensive Plan Amendment and Street Vacation application was adequate.

11. Four-way Intersections. All sidewalk ramps at four-way intersections have been designed with double ramp configuration where applicable to meet the City of Beaverton standards.

A new appendix, Appendix H has been included to provide information regarding the construction of two proposed raised crosswalks on SW Erickson. These crosswalks, shown on Revised Dimension Plan Public Improvements, Attachment 4 are part of the proposed street improvements around the High School to improve pedestrian safety and connectivity. The proposed raised crosswalks have been designed to be in compliance with the Neighborhood Traffic Calming Program sponsored by the City of Beaverton's Engineering Department, Transportation Division. Compliance with the Traffic Calming Eligibility Criteria are found in Appendix H.

### Conditional Use Beaverton High School

1. The request to indicate which trees listed on the Existing Conditions Plan; Dimensioned Site Plan; Grading Plan; Utility Plan and Landscape Plan has been met by the following:

The City of Beaverton Tree Inventory provided by the City does not identify trees as Historic or Significant, it only lists criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above

for  
CMA  
where  
it  
located

no  
info  
need  
details

- *Application Resubmittal Materials - continued*
  - *Page 5*
- 

(indicating a potentially historic tree). For more information on this analysis, please find attached the revised narrative for the Tree Preservation Plan.

2. **Dimensioned Site Plan.** The Revised Grading Plan indicates the dimension from centerline to edge of proposed right-of-way. All existing trees over 6" dbh have been labeled.
3. **Architectural Elevations.** A revised set of Architectural Elevations indicates the general character of the proposed project with dimensions. Materials and colors have been provided on the Materials and Finishes form and the Materials Board submitted with the initial application.
4. **Hours of Operation.** Beaverton High School is open from 6:30am to 2:30pm for classroom activities. After-school activities can run until 10:00pm, depending on the activity. It is important to note that the school facilities are available for a number of users and are not limited to Beaverton School District programs. Tualatin Hills Parks and Recreation District and community groups conducting classes or events are able to use the facilities at Beaverton High School. These events are scheduled through the school. As are all Beaverton School District schools, Beaverton High School is a community facility and is made available to groups when scheduling permits.
5. **Approval Criteria Findings.** The revised narrative for the Conditional Use application includes a review of how the development meets the Development Code Section 20.05. This includes standards for setbacks, landscaping, vehicle and bicycle parking.
6. **Bicycle Parking.** An application for a Variance-Public Hearing for a reduction in the amount of required long-term parking for bicycles is included with this submittal package. The locations of the proposed bicycle parking is illustrated on the following plans: Cover Sheet and Revised Grading, Utilities and Erosion Control Private Improvements (east).
7. **Parking Standards.** The improvements to the Beaverton High School site include development of Lot H. The addition of Lots C, D and H provide 436 spaces which is 46 spaces more than is required based on current enrollment of 1,800 students and 150 staff. Based on a potential enrollment of 2,200 students and increase of staff to 200; the parking minimum required would increase to 480 spaces. This application includes a 10% parking reduction credit for construction of a pedestrian plaza and transit improvements as described in Section 60.20.10.10.A.2 of the Beaverton Development Code. The development of Lots C, D and H along with the pedestrian plaza parking reduction credit will meet the parking requirements for the High School.
8. **Traffic Impact Analysis.** During the Facilities Review meeting with the City of Beaverton on January 23, 2002, the City stated that the Traffic Impact Analysis provided with the Comprehensive Plan Amendment and Street Vacation application was adequate.

### **Tree Preservation Plan**

In response to the letter dated January 18, 2002 for the Tree Preservation Plan, Beaverton High School Improvements, please find the following:

1. **Specific Tree Species.** The Tree Preservation Plan identifies all existing trees with a numeric key and corresponding list noting common names, and size. The inventory is divided into two lists: trees to be preserved and trees to be removed.
2. **Drip Line of Each Tree or Entire Grove.** The Revised Tree Preservation Plan, Attachment 18 indicates in note 2 that the tree symbols approximate the actual canopy of existing trees (i.e. the drip line). The existing tree symbols on the landscape plan are also approximate to true scale.
3. **Clear Delineation of Construction Disturbance Limits.** The Revised Tree Preservation Plan, Attachment 18 identifies the limits of disturbance. The line type has been emboldened and identified in the legend for further clarity. The approximate location of material storage during construction of the cafeteria will be the area between the new cafeteria and SW Stott Street. There are no historic or significant trees in this location.
4. **Methods of Survival.** The Revised Tree Preservation Plan, Attachment 18 indicates in notes 3 and 4 the following:

NOTE: 3) Preserved trees within the limits of construction shall be protected by approved fencing placed five feet outside of drip line of tree.

?? ( NOTE: 4) Where construction occurs within the drip line of any tree to be preserved, existing grades shall not be changed without approval of landscape architect.

### **Bicycle Parking Variance Public Hearing**

An application for a Variance-Public Hearing is being submitted with this resubmittal package to request a reduction in the amount of bicycle parking required at Beaverton High School. According to Beaverton Development Code Section 60.20.10.5, the minimum parking ratio requirements for bicycle parking for an Educational Institution – High School require zero short-term spaces and one long-term space per 18 students (a 1:18 ratio). Based on a potential enrollment of 2,200 students, the long-term bicycle parking requirement for Beaverton High School would be 122 spaces. This Variance asks that the long-term bicycle parking requirement for Beaverton High School be reduced to 56 spaces (or 66 fewer than the requirement). The narrative describes that the demand for long-term bicycle parking can be met with the proposed number of spaces, more importantly, demand at area schools indicates that high school students do not choose to bicycle to school even when facilities are available. There are a number of transportation alternatives for students including frequent bus service provided by the School District and Tri-Met.

- *Application Resubmittal Materials - continued*
  - *Page 7*
- 

## **Conclusion**

In summary, we believe the following resubmittal package completes the following applications:

- Design Review Type 3, Beaverton High School Improvements
- Conditional Use, Beaverton High School Improvements
- Tree Preservation Plan, Beaverton High School Improvements

We are also submitting a Bicycle Parking Variance Application to modify the required number of long-term bicycle spaces for the High School.

To provide for a timely review of our applications, we are submitting an “Application Completeness Option” form. This Option would apply to the Design Review Type 3; Conditional Use; and Tree Preservation Plan applications.

Please do not hesitate to contact me at 503-227-3664.

September 6, 2002

Mr. Eric Nielssen  
LSW Architecture.  
1953 NW Kearney St.  
Portland, OR 97209

*Via Electronic Mail*

Dear Eric:

This letter is to report my observations regarding the grading and fill between the London plane (sycamore) (*Platanus X acerifolia*) trees on the west side of the Merle Davies building at Beaverton High School.

At last inspection, August 15, 2002, grading and fill were complete, according to Richard Kaur, Excel Excavating Superintendent. I found no damage to visible roots and noted very minimal fill within the root zones.

It is my professional opinion that this grading and fill will have very slight negative impact, if any, on the health of the subject trees.

Please call if you have any questions.

Yours for a greener Northwest,

**David R. Cory**  
Consulting Arborist

LSWBHS0902



December 28, 2001

Gary Alfson  
Harper Houf Righellis, Inc.  
5200 SW Macadam Ave, Suite 580  
Portland, OR 97201

**Re: New cafeteria, addition to Beaverton High School, located on 2<sup>nd</sup> St.  
between Scott and Erickson  
CWS file 1684 (Tax map 1S116AD, Tax lots 10900 and 11100)**

Clean Water Services (formerly USA) has reviewed your proposal for the above referenced activity on your site. Staff has conducted a pre-screen review and requested completion of a Sensitive Areas Certification Form. Following the review it is apparent that sensitive areas are greater than 200' from your project. In light of this result, the above referenced project does not need a Service Provider letter as required by Resolution and Order 00-7, Section 3.02.1. Prior to construction, a Stormwater Connection Permit from Clean Water Services or its designee is required pursuant to Ordinance 27, Section 4.B. All required permits and approvals must be obtained and completed under applicable local, state, and federal law.

This concurrence letter does NOT eliminate the need to protect sensitive areas if they are subsequently identified on your site.

If you have any questions, please feel free to call me at 503-846-3613.

Sincerely,



Heidi Berg  
Site Assessment Coordinator

**U.S. Postal Service**  
**CERTIFIED MAIL RECEIPT**  
*(Domestic Mail Only; No Insurance Coverage Provided)*

7000 0600 0028 0224 2308

TPP 2001-0008 SK

Postage	\$
Certified Fee	
Return Receipt Fee (Endorsement Required)	
Restricted Delivery Fee (Endorsement Required)	

Postmark  
here  
05/14/02

Total **FRANK ANGELO**

Recipient **APPLICANT**  
 Street **ANGELO EATON & ASSOCIATES**  
**620 SW MAIN SUITE 201**  
 City, ST **PORTLAND OR 97205**

PS Form 3800, February 2000 See Reverse for Instructions

**SENDER: COMPLETE THIS SECTION**

- Complete items 1, 2, and 3. Also complete item 4 if Restricted Delivery is desired.
- Print your name and address on the reverse so that we can return the card to you.
- Attach this card to the back of the mailpiece, or on the front if space permits.

1. Article Addressed to:

**FRANK ANGELO**  
**APPLICANT**  
**ANGELO EATON & ASSOCIATES**  
**620 SW MAIN SUITE 201**  
**PORTLAND OR 97205**

**COMPLETE THIS SECTION ON DELIVERY**

A. Received by (Please Print Clearly) **K. Siegel** B. Date of Delivery **5/15/02**

C. Signature **X K. Siegel**  Agent  Addressee

D. Is delivery address different from item 1?  Yes  No  
 If YES, enter delivery address below:

3. Service Type  
 Certified Mail  Express Mail  
 Registered  Return Receipt for Merchandise  
 Insured Mail  C.O.D.

4. Restricted Delivery? (Extra Fee)  Yes

2. Article Number (Copy from service label) **7000 0600 0028 0224 2308**

**U.S. Postal Service**  
**CERTIFIED MAIL RECEIPT**  
*(Domestic Mail Only; No Insurance Coverage Provided)*

7000 0600 0028 0224 2292

TPP 2001-0008 SK

Postage	\$
Certified Fee	
Return Receipt Fee (Endorsement Required)	
Restricted Delivery Fee (Endorsement Required)	

Postage  
 HK  
 05114102

Total F: **MIKE MALONEY**

Recipient: **OWNER**  
 Street: **BEAVERTNO SCHOOL DISTRICT**  
**16550 SW MERLO ROAD**  
 City, St: **BEAVERTO OR 97006-5152**

PS Form 3800, February 2000

Instructions

**SENDER: COMPLETE THIS SECTION**

- Complete items 1, 2, and 3. Also complete item 4 if Restricted Delivery is desired.
- Print your name and address on the reverse so that we can return the card to you.
- Attach this card to the back of the mailpiece, or on the front if space permits.

1. Article Addressed to:

**MIKE MALONEY**  
**OWNER**  
**BEAVERTNO SCHOOL DISTRICT**  
**16550 SW MERLO ROAD**  
**BEAVERTO OR 97006-5152**

**COMPLETE THIS SECTION ON DELIVERY**

A. Received by (Please Print Clearly) B. Date of Delivery  
 5/5/02

C. Signature  
 X *Sharon Brady*  Agent  
 Addressee

D. Is delivery address different from item 1?  Yes  
 If YES, enter delivery address below:  No

3. Service Type  
 Certified Mail  Express Mail  
 Registered  Return Receipt for Merchandise  
 Insured Mail  C.O.D.

4. Restricted Delivery? (Extra Fee)  Yes

2. Article Number (Copy from service label) **7000 0600 0028 0224 2292**



SK

# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## See BDR Packet

Project Number: BDR 2001-0213 / cup 2001-0031 / VAR 2002-0002  
TPP 2001-0008

Project Name: Beaverton High School

Re-notice: Cafeteria & Parking Lot Expansion  
MAP Revision

## AFFIDAVIT OF NOTICE

I, Robyn Lampa, being first duly sworn/affirmed, say that I gave notice of a proposed zone change affecting land described as Lot(s) 07100, 02500, Map 15116AD, 15116AC; and that, pursuant to the City of Beaverton Ordinance 2050, Sections 50.30.1, and 50.30.2, I did on the 11<sup>th</sup> day of March, 2002, give public notice to those listed on the attached Exhibit A. The notices were mailed on 3/11/02, which was on or before the deadline date determined by City Staff for this application(s).

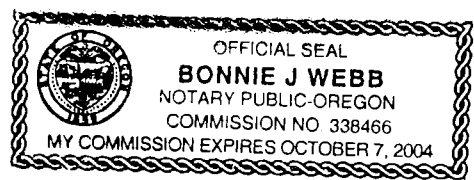
Dated this 11 day of March, 2002.

Robyn Lampa  
Signature

State of OREGON )  
County of Washington )

Signed and sworn/affirmed before me this 11 day of March, 2002 by Robyn Lampa.

Bonnie J Webb  
Notary Public for the State of Oregon  
My Commission Expires: Oct 7 2004





# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

Project Number: BDR 2001-0213  
Project Name: Beaverton High School Cafeteria & Parking Lots Expansion

## AFFIDAVIT OF NOTICE

I, Carolyn W. Hubbard, being first duly sworn/affirmed, say that I gave notice of a proposed cafeteria/parking lot expansion affecting land described as Lot(s) 1100; 11000; 2900; 700; 10900; 2100; 2500, Map PSI-16AD; PSI-16AC; and that, pursuant to the City of Beaverton Ordinance 2050, Sections 50.30.1, and 50.30.2, I did on the 5 day of February, 2002, give public notice to those listed on the attached Exhibit A. The notices were mailed on February 5, 2002, which was on or before the deadline date determined by City Staff for this application(s).

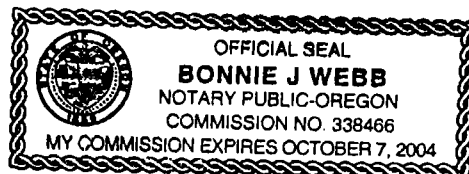
Dated this 5 day of February, 2002.

Carolyn W. Hubbard  
Signature

State of OREGON )  
County of Washington )

Signed and sworn/affirmed before me this 5<sup>th</sup> day of Feb, 2002 by Carolyn Hubbard.

Bonnie J Webb  
Notary Public for the State of Oregon  
My Commission Expires: Oct 7 2004



1S116AA04700  
ADAMS-HAGLUND JENNIFER  
2605 NE ROBERTS PL  
GRESHAM OR 97030

1S116AD02500  
ADOLPHSON LOVING TRUST  
BY RICHARD & SHIRLEY ADOLPHSON  
25505 SW LONE FIR LANE  
WEST LINN OR 97068

1S116AD02501  
ADOLPHSON LOVING TRUST  
25505 SW LONE FIR LN  
WEST LINN OR 97068

1S116AD02300  
ADOLPHSON R G  
ADOLPHSON R G JR  
BY A-2 ENTERPRISES  
4555 SW MAIN ST  
BEAVERTON OR 97005

1S116DB00400  
AFOURKEEFF ANJA T  
5075 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD07000  
AHN DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

1S116AD07001  
~~AHN DANIEL H  
12820 SW 2ND ST~~  
BEAVERTON OR 97005

1S116AD06100  
ALPROP CO  
6149 SW SHATTUCK RD  
PORTLAND OR 97221

1S116AC04700  
ANDERSON ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC05500  
~~ANDERSON ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR~~  
BEAVERTON OR 97005

1S116DB03400  
ARLT LINDA L  
13275 SW PEAR ST  
BEAVERTON OR 97005

1S116AC01800  
ARMSTRONG JAMES R  
LINDA L  
13325 SW 2ND  
BEAVERTON OR 97005

1S116DA90027  
BABB DORIS LEE  
5136 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90023  
BAKER VIRGIL A & MARIE  
5166 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AC04500  
BARTLETT CLIFFORD L  
13500 SW 2ND  
BEAVERTON OR 97005

1S116AA05200  
BASEEL PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

1S116AA05301  
~~BASEEL PRESTON  
6977 SW TIERRA DEL MAR~~  
BEAVERTON OR 97007

1S116AB03100  
BEAVERTON FOODS INC  
PO BOX 687  
BEAVERTON OR 97005

1S116AC04400  
BEAVERTON PROPERTIES  
13470 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD11100  
BEAVERTON SCHOOL DISTRICT 48  
MERLE DAVIS ELEMENTARY  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02100  
BEAVERTON SCHOOL DISTRICT 48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD11000  
BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD02900  
~~BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD~~  
BEAVERTON OR 97006

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~~BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD~~  
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16550 SW MERLO RD~~  
BEAVERTON OR 97006

1S116AD10900  
BEAVERTON SCHOOL DISTRICT 48J  
ATTN FACILITIES MNGMT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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~~BEAVERTON CITY OF  
PO BOX 4755~~  
BEAVERTON OR 97076

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~~BEAVERTON CITY OF  
00000~~

1S116AC04600  
~~BEAVERTON CITY OF  
4950 SW HALL BLVD~~  
BEAVERTON OR 97005

~~1S116AD01300  
BEAVERTON CITY OF  
PO BOX 4755  
BEAVERTON OR 97076~~

~~1S116DB11100  
BEAVERTON CITY OF  
PO BOX 4755  
BEAVERTON OR 97076~~

1S116AA06000  
BENNETT CLEVE E & LOIS W TRUSTE  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

~~1S116AA05900  
BENNETT CLEVE E AND  
LOIS W TRUSTEES  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225~~

1S116AD10400  
BENNETT TERRI LOUISE  
12820 SW 4TH ST  
BEAVERTON OR 97005

1S116AA02900  
BIGGI STEVE AND LAURA  
3843 SW HALL BLVD  
BEAVERTON OR 97005

1S116DB00100  
BLOOMER DANIEL D MARY L  
4925 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA90055  
BYRNES DONALD B & B JERI  
12900 SW 5TH ST  
BEAVERTON OR 97005

1S116AA05400  
CAFFEE CENTER JOINT VENTURE  
BY ANDERSON & ASSOCIATES  
PO BOX 3821  
BELLEVUE WA 98009

1S116DB03000  
CARSON JERRY/PAULA  
13270 SW 6TH  
BEAVERTON OR 97005

1S116AC05700  
CASSIDY MARI  
4820 SW MENLO  
BEAVERTON OR 97005

1S116DA02500  
CASSON KATHLEEN ANN  
3818 SE 9TH AVE  
PORTLAND OR 97202

1S116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

~~1S116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005~~

1S116AC01300  
CHIA CHIH CHIANG & YEA HUEY  
13200 SW FARMINGTON RD  
BEAVERTON OR 97006

1S116AC01400  
CHIA FUO FUH &  
WU CHUN ER  
13150 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD10601  
COLEMAN ANNE S  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC03000  
COLEMAN ANNE SCOTT  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC01900  
COOLEY CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

1S116DA07003  
COOLEY CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

1S116AD08000  
CORP OF PRESIDING BISHOP OF  
CHURCH OF JESUS CHRIST LDS  
TAX ADMIN FILE 536-2490  
50E NORTH TEMPLE  
SALT LAKE CITY UT 84150-2201

1S116AC05801  
CORSON JOHN D  
4850 SW MENLO DR  
BEAVERTON OR 97005

1S116AD07400  
CRITELLI FAMILY TRUST  
c/o CRITELLI TINA  
BY GARY A & SHARON L CRITELLI  
PO BOX 87058  
VANCOUVER WA 98687

1S116AD10300  
CRITELLI GARY A & SHARON L TRS  
PO BOX 87058  
VANCOUVER WA 98687

~~1S116AA01600  
CRITELLI GARY A & SHARON L TRS  
PO BOX 87058  
VANCOUVER WA 98687~~

1S116DB00200  
D'ALFONSO PAULA L  
4975 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD10500  
DAMIANA RONALD G  
GEORGIA M  
12870 SW 4TH  
BEAVERTON OR 97005

1S116DB00800  
DANEIL HOLLIS MARION  
13425 SW 6TH  
BEAVERTON OR 97005

1S116DB00900  
DANIEL AVERILL H/HOLLIS M  
13425 SW 6TH  
BEAVERTON OR 97005

1S116AC01500  
DANISH SEYMOUR R REV TRUST &  
BERRY HARRY L ET AL  
BY DANISH CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005

1S116AD03100  
 D'SILVA FELIX & LOURDES  
 8765 SW JAMIESON RD  
 PORTLAND OR 97225

1S116DA01700  
 EIMERS SANDRA P &  
 EIMERS ANGELA  
 6045 SW 167TH AVE  
 ALOHA OR 97007

1S116DB03101  
 ENGEL MICHAEL J &  
 REBECCA C  
 13210 SW 6TH ST  
 BEAVERTON OR 97005

1S116AA05700  
 ETON LANE LTD PARTNERSHIP  
 WALKER HUBERT E TRUSTEE &  
 ETON LANE LLC  
 PO BOX 14746  
 PORTLAND OR 97293

1S116AA05800  
~~ETON LANE LTD PARTNERSHIP  
 WALKER HUBERT E TRUSTEE  
 PO BOX 14746  
 PORTLAND OR 97214~~

1S116AC04200  
 EVANS CATHERINE B  
 11775 SW 11TH STREET  
 BEAVERTON OR 97005

1S116AC01200  
 FISHBACK MERL E &  
 GLORIA M TRUST  
 FISHBACK MERL E/GLORIA M TR  
 9700 NW GROVELAND RD  
 HILLSBORO OR 97124

1S116DA02200  
 FLOREN JANET M AND GREG E  
 12870 SW 6TH AVE  
 BEAVERTON OR 97005

1S116DB00300  
 FRANKLIN JOHN A  
 5025 SW FAIRMOUNT DR  
 BEAVERTON OR 97005

1S116AC04900  
 FRIEZE REX D SHIRLEY L  
 4720 SW FAIRMONT DR  
 BEAVERTON OR 97005

1S116DB01100  
 GABRIEL KATHLEEN  
 13470 SW 3RD ST  
 BEAVERTON OR 97005

1S116AC05600  
 GARCIA WAGNER  
 4800 SW MENLO DR  
 BEAVERTON OR 97005

1S116DA90025  
 GARTLAND SHARIN  
 5162 SW ERICKSON  
 BEAVERTON OR 97005

1S116AD03000  
 GARVER JON & REBECCA  
 12855 SW 2ND ST  
 BEAVERTON OR 97005

1S116AD02200  
 GENEST VIRGINIA E  
 12870 SW FARMINGTON RD  
 BEAVERTON OR 97005

1S116AD02100  
 GENEST VIRGINIA E  
 BY VALLEY DAIRY QUEEN  
 12870 SW FARMINGTON RD  
 BEAVERTON OR 97005

1S116AD02700  
 GHIM MINJE P  
 495 SW 169TH PL  
 BEAVERTON OR 97006

1S116AD02600  
 GHIM MINJE P  
 495 SW 169TH PL  
 BEAVERTON OR 97006

1S116DB05000  
 GLASNER GENE R  
 c/o DREY BRIAN S & PAMELA R  
 5225 SW ERICKSON AVE  
 BEAVERTON OR 97005

1S116AC06000  
 GLICKMAN TERRY L  
 13555 SW THIRD ST  
 BEAVERTON OR 97005

1S116DA01300  
 GORMAN JOHN & PATRICIA A  
 16555 SW HIGH HILL LN  
 BEAVERTON OR 97007

1S116AC01000  
 GRABHORN CHAD  
 350 NW FREEMAN  
 HILLSBORO OR 97124

1S116AC03700  
 GREEN ERNESTINE RUTH  
 4820 SW FAIRMOUNT DRIVE  
 BEAVERTON OR 97005

1S116AA02100  
 GREER DAVID J & LISA D  
 19370 SW SOUTHVIEW ST  
 ALOHA OR 97007

1S116AC05300  
 GREMAUX DEANE B AND  
 PATRICIA A  
 4755 SW FAIRMOUNT DRIVE  
 BEAVERTON OR 97005

1S116AB02100  
 GRL INC  
 13095 SW CANYON RD  
 BEAVERTON OR 97005

1S116DB10800  
 GUSTAFSON CONRAD W & ANNA M  
 13245 SW PEARL ST  
 BEAVERTON OR 97007

1S116AA02500  
 GUTZLER CAROLYN E  
 BY TOM'S PANCAKE HOUSE  
 12925 SW CANYON RD  
 BEAVERTON OR 97005

1S116AA02600  
~~GUTZLER CAROLYN E  
 BY TOM'S PANCAKE HOUSE  
 12925 SW CANYON RD  
 BEAVERTON OR 97005~~

1S116AA02300  
~~GUTZLER THOMAS M & CAROLYN E  
 12925 SW CANYON RD  
 BEAVERTON OR 97005~~



IS116AA02400  
~~GUTZLER THOMAS M AND  
 CAROLYN E  
 BY TOM'S PANCAKE HOUSE  
 12925 SW CANYON RD  
 BEAVERTON OR 97005~~

IS116DA90058  
 HALL LAURA I  
 12888 SW 5TH  
 BEAVERTON OR 97005

IS116DA01900  
 HALSTEN ELSIE M TRUSTEE  
 12855 SW 6TH  
 BEAVERTON OR 97005

IS116DA02000  
 HAMREUS CALVIN E & JOYCE K  
 5075 SW MAIN AVE  
 BEAVERTON OR 97005

IS116DA07004  
 HATTENHAUER CHRISTINE  
 261 SW ASHDOWN CIR  
 WEST LINN OR 97068

IS116DA07002  
 HATTENHAUER CHRISTINE  
 261 SW ASHDOWN CIR  
 WEST LINN OR 97068

IS116DA01800  
 HEDGES SALLY C  
 12850 SW 5TH AVE  
 BEAVERTON OR 97005

IS116DB04600  
 HEISLER MARYANNE &  
 NEUFELL PATRICIA J  
 13250 SW PEARL  
 BEAVERTON OR 97005

IS116DA01100  
 HETU TED G  
 5075 SW ANGEL AVE  
 BEAVERTON OR 97005

IS116AD06000  
 HILL RICHARD E AND FLORENCE E  
 8405 SW MAVERICK PL  
 BEAVERTON OR 97008

IS116AD10100  
 HOUDROJ ALI  
 4970 SW MAIN ST  
 BEAVERTON OR 97005

IS116DA90028  
 HUESMAN NICOLE K  
 5134 SW ERICKSON AVE  
 BEAVERTON OR 97005

IS116AA03000  
 HUNKAPILLAR PATRICIA W  
 BY THE TAGGART TRUST  
 21185 NW EVERGREEN PKWY  
 SUITE 101  
 HILLSBORO OR 97124

IS116AA03100  
 HUNKAPILLAR PATRICIA W  
 SMITH HARRIET L AND GEORGE  
 BY TAGGART TRUST THE  
 21185 NW EVERGREEN PKW STE 101  
 HILLSBORO OR 97124

IS116AC00800  
 HUNT SHERYLL R/WILLIAM J &  
 HUNT JONATHAN J  
 13380 SW FARMINGTON RD  
 BEAVERTON OR 97005

IS116DA02100  
 HUTCHENS DANA C & LORI E  
 12820 SW SIXTH ST  
 BEAVERTON OR 97005

IS116DA06700  
 HUYGENS JOSEPH D &  
 JULIE ANN HUTCHINSON  
 7830 SW LAUREL ST  
 PORTLAND OR 97225

IS116AD09901  
 JOHNSON CAROL L R  
 PO BOX 524  
 SCAPPOOSE OR 97056

IS116AC04000  
 JONES E CHANNING & PAULA KAY  
 13420 SW 2ND AVE  
 BEAVERTON OR 97005

IS116DA90052  
 JONES JERRY C  
 12912 SW 5TH 52  
 BEAVERTON OR 97005

IS116AC03600  
 JONES KRISTIN L  
 4870 SW FAIRMOUNT DR  
 BEAVERTON OR 97005

IS116DA07000  
 JUNELL BONNIE M &  
 BABCOCK DOUGLAS C  
 4820 SW 18TH PL  
 PORTLAND OR 97201

IS116AD01600  
 KABDEBO JOSEPH & CAMILLE  
 725 SW VIEWMONT DR  
 PORTLAND OR 97225

IS116AD01700  
~~KABDEBO JOSEPH & CAMILLE  
 725 SW VIEWMONT DR  
 PORTLAND OR 97225~~

~~IS116AD01800  
 KABDEBO JOSEPH AND CAMILLA  
 725 SW VIEWMONT DR  
 PORTLAND OR 97225~~

IS116DB03202  
 KAUTH JANE F  
 5115 SW ERICKSON AVE  
 BEAVERTON OR 97007

IS116DA07300  
 KIKO JOHN R &  
 ORTIZ INGRID  
 5155 SW MAIN AVE  
 BEAVERTON OR 97005

IS116AD04300  
 KIM UN & SUE J  
 14756 SW KILCHIS  
 BEAVERTON OR 97007

IS116AA02700  
 KIM YOUNG M & LINDA  
 12855 SW CANYON RD  
 BEAVERTON OR 97005

IS116DA06800  
 KING CHARLES C & ROM N  
 5150 SW ERICKSON AVE  
 BEAVERTON OR 97005

1S116DA90024  
 KLANECKY ARDITH ANN  
 5164 SW ERICKSON  
 BEAVERTON OR 97005

1S116DB10900  
 KO JOON B  
 13215 SW PEARL ST  
 BEAVERTON OR 97005

1S116AD06800  
 KRAEMER JOHN L & CLAUDIA R  
 33713 NE WILLIAMS APT F-6  
 SCAPPOOSE OR 97056

1S116DA90049  
 KRIEG MARGIE J  
 12924 SW 5TH ST  
 BEAVERTON OR 97005

1S116DA90057  
 LAMB KEVIN M &  
 YANT SUSAN A  
 12892 SW 5TH ST  
 BEAVERTON OR 97005

1S116BD00100  
 LANPHERE ENTERPRISES INC  
 ATTN DOUG MEATH  
 12520 SW CANYON RD  
 BEAVERTON OR 97005

1S116AA04690  
 LANPHERE ENTERPRISES INC  
 ATTN DOUG MEATH  
 12520 SW CANYON RD  
 BEAVERTON OR 97005

1S116AC00500  
 LANPHERE ENTERPRISES INC  
 ATTN DOUG MEATH  
 12505 SW BROADWAY ST  
 BEAVERTON OR 97005

1S116BD00200  
 LANPHERE ENTERPRISES INC  
 ATTN DOUG MEATH  
 12520 SW CANYON RD  
 BEAVERTON OR 97005

1S116AC00600  
 LANPHERE ROBERT  
 ATTN DOUG MEATH  
 12505 SW BROADWAY ST  
 BEAVERTON OR 97005

1S116DB03100  
 LEE SANDY  
 344 NE 156TH AVE  
 PORTLAND OR 97230

1S116DB02900  
 LEIVA LUIS ALONSO & SANDRA J  
 13320 SW 6TH ST  
 BEAVERTON OR 97005

1S116AC04300  
 LEONARD CLIFFORD A & SHARON B  
 7345 SW 84TH AVE  
 PORTLAND OR 97223

1S116AD06200  
 LESHAR SAM E/HELENA R &  
 LESHAR CARL H  
 7725 SE 32ND AVE  
 PORTLAND OR 97202

1S116AC06200  
 LEWIS SUNNY M  
 13425 SW 3RD  
 BEAVERTON OR 97005

1S116DA90047  
 LIDBERG MARY ALICE C  
 12932 SW 5TH AVE  
 BEAVERTON OR 97005

1S116AD08100  
 LIEDTKE CATHERINE A  
 17561 SW ALBERT CT  
 BEAVERTON OR 97007

1S116DA01400  
 LISICKI THOMAS D & RUTH S  
 13332 SW BENCHVIEW TER  
 TIGARD OR 97223

1S116DA07400  
 LOCKARD DOROTHY J  
 LIVING TRUST THE  
 5125 SW SHERWOOD PL  
 BEAVERTON OR 97005

1S116AC02800  
 LONG PATRICIA C & JOE PAT  
 5070 SW FAIRMONT DR  
 BEAVERTON OR 97005

1S116AC04800  
 LOPEZ JORGE & CARMEN  
 4700 SW FAIRMOUNT DR  
 BEAVERTON OR 97005

1S116DB03201  
 LOY JOHN G  
 5105 SW ERICKSON AVE  
 BEAVERTON OR 97005

1S116DA90060  
 LUCAS LAURIE G  
 12880 SW 5TH AVE  
 BEAVERTON OR 97005

1S116DA06200  
 LUCKE WILLIAM T TRUSTEE  
 5205 SW MAIN AVE  
 BEAVERTON OR 97005

1S116AC06400  
 LUND BRADLEY W  
 4855 SW FAIRMOUNT DR  
 BEAVERTON OR 97005

1S116AC01601  
 LUNDGREN TIMOTHY  
 18213 PORTLAND AVE  
 GLADSTONE OR 97027

1S116DA90041  
 MACK TIMOTHY F  
 12956 SW 5TH  
 BEAVERTON OR 97005

1S116DA90048  
 MATELICH RAE MARIE  
 12928 SW 5TH AVE  
 BEAVERTON OR 97005

1S116DA90054  
 MAYES PATSY L  
 12904 SW 5TH ST  
 BEAVERTON OR 97005

1S116AD08200  
 MERCADO NEVILLE & RENEE  
 PO BOX 487  
 BEAVERTON OR 97075

1S116DB02800  
MEYER JANE E & RICHARD J &  
JOHN M  
5450 SW 150TH CT  
BEAVERTON OR 97007

1S116AC03800  
MILLARD KATHLEEN  
4790 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

1S116AC03200  
MOHR PATRICIA ALICE  
WILLIAM RICHARD  
7495 SW 136TH AVE  
BEAVERTON OR 97008

1S116AD08300  
MORGAN WILLIAM L & EVE E  
20855 NW CHILOQUIN  
PORTLAND OR 97229

1S116DA01200  
MORRIS GLENN R AND ANITA S  
12755 SW 6TH  
BEAVERTON OR 97005

1S116AB02200  
NELSEN MARY G  
9965 SW 170TH  
BEAVERTON OR 97007

1S116AC00700  
NEUSCHWANDER FRED ANDREW &  
INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064

1S116AC00900  
~~NEUSCHWANDER FRED ANDREW &  
INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064~~

1S116AC02900  
NILSON GAIL ANN  
5020 SW FAIRMOUNT  
BEAVERTON OR 97005

1S116DA90050  
NORMAN CONSTANCE R  
12920 SW 5TH ST  
BEAVERTON OR 97005

1S116DA90051  
OKAMOTO BRETT K  
12916 SW 5TH  
BEAVERTON OR 97005

1S116AC06300  
OVERBY ARDON L  
PO BOX 704  
BEAVERTON OR 97075

~~1S116DB11000  
OVERBY ARDON L  
PO BOX 704  
BEAVERTON OR 97075~~

1S116AD00900  
PARKS LOREN E  
PO BOX 5669  
ALOHA OR 97006

1S116AD10000  
PEARCE JOSEPH J  
4920 SW MAIN  
BEAVERTON OR 97005

1S116AC06500  
PETERSEN DONALD E  
10240 SW HAWTHORNE LN  
PORTLAND OR 97225

1S116AD03500  
PHAM HIEN &  
NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006

~~1S116AD03400  
PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

~~1S116AD03200  
PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

~~1S116AD03300  
PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

~~1S116AD06600  
PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

1S116AD08400  
PIES SANDRA J & EVERETT M TRS  
29395 SW HILLECKE RD  
HILLSBORO OR 97123

1S116DA90046  
PODEBSKI ADAM P & GUTA H  
PODEBSKI BENNY  
12936 SW 5TH  
BEAVERTON OR 97005

1S116AC02001  
QUINN CAROLYN F  
13395 SW 2ND ST  
BEAVERTON OR 97005

1S116AD07500  
QUTUB WAHBI M &  
MATAR RIAD A  
7883 SW BARNARD DR  
BEAVERTON OR 97007

1S116AD02890  
RANDOLPH GLORIA  
2725 SW 185TH  
ALOHA OR 97006

~~1S116AD02800  
RANDOLPH GLORIA C  
2725 SW 185TH AVE  
ALOHA OR 97005~~

1S116DB00600  
REMCHER GEORGE & FLORENCE  
4240 SW 102ND AVE  
BEAVERTON OR 97005

1S116AC05400  
ROBERTS LEON &  
CARRIE  
4725 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA01500  
ROBINSON JACK A & PATRICIA A  
3830 SE BENTLEY AVE  
HILLSBORO OR 97123

1S116DB04700  
ROSENHOLM DAVID  
ROSENHOLM ANDREA  
13220 SW PEARL ST  
BEAVERTON OR 97005

1S116DB04800  
SANABRIA-BARILLAS JOSE ALFREDO  
ALVARADO JUANA  
13170 SW PEARL ST  
BEAVERTON OR 97005

1S116AC02700  
SCALONE DAVID  
13255 SW 6TH ST  
BEAVERTON OR 97005

1S116AD01500  
SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

~~1S116AD01400  
SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661~~

1S116DA90059  
SESSIONS KEITH W & LAVERNA M  
TRUSTEES  
12884 SW 5TH  
BEAVERTON OR 97005

1S116AB01901  
SKORO MARION & MIRELA  
19300 NW GILLIHAN LOOP RD  
PORTLAND OR 97231

1S116DB03500  
SMITH DONALD J PHYLLIS J  
13295 SW PEARL ST  
BEAVERTON OR 97005

1S116AD09900  
SMITH IAN C & NADINE T  
12750 SW 4TH ST  
BEAVERTON OR 97005

1S116DA90053  
SMITH RICHARD BRENDAN &  
HILARY ANTOINETTE MARY  
12908 SW 5TH ST  
BEAVERTON OR 97005

1S116AC05000  
SMOLENSKY G MARIKA  
PO BOX 1114  
BEAVERTON OR 97075

1S116DB00700  
SNODGRASS PHILIP R & PATRICIA  
13420 SW THIRD ST  
BEAVERTON OR 97005

1S116AC05800  
SOTH COLLEEN V TRUSTEE  
SOTH FORREST C TRUSTEE  
4890 SW MENLO DR  
BEAVERTON OR 97005

1S116AD04400  
SPRINGER JAMES F & SUE  
c/o BEAVERTON FUNERAL HOME INC  
4126 NORLAND AVE  
BURNABY BC CANADA  
V5G 358

~~1S116AD04501  
SPRINGER JAMES F & SUE  
c/o BEAVERTON FUNERAL HOME INC  
4126 NORLAND AVE  
BURNABY BC CANADA  
V5G 358~~

1S116DA90030  
SPURGEON JOAN P  
5130 SW ERICKSON  
BEAVERTON OR 97005

1S116DB00500  
ST CLAIR JERALD LEE  
SHIRLEY ANN  
13355 SW 6TH ST  
BEAVERTON OR 97005

1S116AC02600  
STARKE DONALD B EDNA A  
13155 SW 6TH  
BEAVERTON OR 97005

1S116AA02101  
STEICHEN NANCY & MCCOURT JOHN  
c/o HOVERSLAND GORDON & MYRNA L  
4350 SW CEDAR HILLS BLVD  
BEAVERTON OR 97005

1S116AD10600  
STEIGER JOHN M & LOU-WAYNE  
301 NW MURRAY BLVD  
PORTLAND OR 97229

1S116AC01100  
STOCKWELL KATHERINE R &  
BROWN GARY E  
13240 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116DA01000  
STOLTENBURG BARBARA  
c/o RAINEY WALLACE L & LESLIE C  
5025 SW ANGEL AVE  
BEAVERTON OR 97005

1S116AC02000  
SURBY PAUL W L FRANCES  
13375 SW 2ND  
BEAVERTON OR 97005

1S116AC03500  
THAO PANCHONG & MAO HER  
4920 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA90029  
THOMAS BARBARA A  
5132 SW ERICKSON  
BEAVERTON OR 97005

1S116AD04200  
THOMAS KAREN JO  
12650 SW 1ST  
BEAVERTON OR 97005

1S116AD06500  
THOMAS KELLY R &  
BOTHNE MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005

~~1S116AD06900  
THOMAS KELLY R &  
BOTHNE MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005~~

~~1S116AA05001  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005~~

1S116AA05101  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

~~1S116AA05100  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005~~

~~1S116AA05102  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005~~

1S116DA90056  
THOMPSON DANIEL HARRISON &  
THOMPSON TOM G &  
THOMPSON MARGIE J  
12896 SW 5TH AVE  
BEAVERTON OR 97005

1S116AC06100  
THOMPSON MAURICE J & KIMBERLY  
13475 SW 3RD ST  
BEAVERTON OR 97005

1S116AC01700  
THOMPSON TOMMY G  
JEANNETTE L  
13305 SW 2ND ST  
BEAVERTON OR 97005

1S116DB04900  
TISCHLER STEPHEN & KATHLEEN  
5195 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AA02200  
TOSCO CORP  
PROPERTY TAX DEPT-DC17  
PO BOX 52085  
PHOENIX AZ 85072

1S116AD07600  
TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

~~1S116AD07700  
TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005~~

1S116AD06700  
UNDERHILL MICHAEL  
12795 SW 3RD ST  
BEAVERTON OR 97005

1S116AA04900  
VADEN MICHAEL & JANE  
12740 SW CANYON RD  
BEAVERTON OR 97005

~~1S116AA05000  
VADEN MICHAEL & JANE  
12740 SW CANYON RD  
BEAVERTON OR 97005~~

1S116AA04800  
VADEN MICHAEL & JANE  
BY LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

1S116DA02501  
VEAZEY THOMAS C AND  
ROXANNE T  
12770 SW 6TH ST  
BEAVERTON OR 97005

1S116DA90045  
VINK NANCY E  
12940 SW 5TH ST  
BEAVERTON OR 97005

1S116AC05100  
VOGEL MICHAEL J & D'ANNE T  
4770 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116AC05200  
VON W METHENEY FAMILY TRUST  
4775 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA90026  
VOSE BENJAMIN A  
PO BOX 73  
TOLOVANA PARK OR 97145

1S116AC03100  
WABNITZ STEVEN G  
2648 SW 153RD AVE  
BEAVERTON OR 97006

1S116AC00400  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

~~1S116AC00200  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075~~

~~1S116AC00100  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075~~

1S116AC00190  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

1S116AD10800  
WELDON GEORGE E AND  
FLORENCE GRACE  
8783 SW 175TH AVE  
BEAVERTON OR 97007

1S116DA06801  
WENDT WILLIAM J CAROL  
5120 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90042  
WHITACRE SEAN M  
12952 SW 5TH ST  
BEAVERTON OR 97005

1S116AC04100  
WIESMANN LARRY J  
13450 SW 2ND ST  
BEAVERTON OR 97005

1S116AD10700  
WILD GENE D AND BETTY LOU  
4975 SW MAIN  
BEAVERTON OR 97005

1S116AD10200  
YI HA YON &  
YI CHO SIM  
4975 SW ANGEL AVE  
BEAVERTON OR 97005

1S116AD03600  
YOSHIDA JUNKI  
8440 NE ALDERWOOD RD STE A  
PORTLAND OR 97220

**BDR 2001-0213  
BEAVERTON HIGH SCHOOL  
CAFETERIA & PARKING LOTS**

**RUSS PITKIN/ERIC NIELSEN  
ARCHITECT  
LSW ARCHITECTURE  
1953 NW KEARNY  
PORTLAND OR 97209**

**CAROL FRANKLIN  
CENTRAL BEAVERTON NAC  
5025 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005**

**FRANK ANGELO  
APPLICANT  
ANGELO EATON & ASSOCIATES  
620 SW MAIN SUITE 201  
PORTLAND OR 97205**

**RON MATELA  
LANDSCAPE ARCHITECT  
MATELA ASSOCIATES  
601 MAIN STREET SUITE 201  
VANCOUVER WA 98660**

**MARV DOTY  
CENTRAL BEAVERTON CCI  
7350 SW WILSON AVENUE  
BEAVERTON OR 97008**

**MIKE MALONEY  
OWNER  
BEAVERTON SCHOOL DISTRICT  
16550 SW MERLO ROAD  
BEAVERTON OR 97006-5152**

**GARY ALFSON  
ENGINEER  
HARPER HOUF RIGHELLIS  
5200 SW MACADAM SUITE 580  
PORTLAND OR 97201**

BEVERLY FROUDE  
CPO 4 BULL MOUNTAIN  
12200 SW BULL MTN ROAD  
TIGARD OR 97224

DAVID WILSON  
CPO 3  
4170 SW PARKVIEW AVENUE  
PORTLAND OR 97225

[REDACTED]

[REDACTED]

JOHN BREILING  
CPO 7 SUNSET WEST  
4690 NW COLUMBIA  
PORTLAND OR 97229

[REDACTED]

BILL AVERY  
WASHINGTON COUNTY - DLUT  
PLANNING DIVISION # 350-13  
155 NORTH FIRST AVENUE  
HILLSBORO OR 97124-3072

PLANNING DIRECTOR  
CITY OF TIGARD  
13125 SW HALL BOULEVARD  
TIGARD OR 97223

[REDACTED]

BEAVERTON POST OFFICE  
4550 SW BETTS  
BEAVERTON OR 97005

PHIL HEALY  
WASHINGTON COUNTY - DLUT  
LAND DEVELOPMENT # 350-13  
155 NORTH FIRST  
HILLSBORO OR 97124

[REDACTED]

MIKE BURTON  
METRO GROWTH MANAGEMENT  
MANAGER COMMUNITY DEV  
600 NE GRAND AVENUE  
PORTLAND OR 97232

JAN YOUNGQUIST  
BEAVERTON SCHOOL DIST  
16550 SW MERLO ROAD  
BEAVERTON OR 97006

[REDACTED]

SONYA KAZEN  
DEVELOPMENT REVIEW  
ODOT REGION 1  
123 NW FLANDERS  
PORTLAND OR 97209-4037

[REDACTED]

SAM HUNAI  
ODOT DISTRICT 2A  
5440 SW WESTGATE DR STE 350  
PORTLAND OR 97221-2414

[REDACTED]

[REDACTED]

[REDACTED]

1S116AA04700  
ADAMS-HAGLUND JENNIFER  
2605 NE ROBERTS PL  
GRESHAM OR 97030

1S116AD02500  
ADOLPHSON LOVING TRUST  
BY RICHARD & SHIRLEY ADOLPHSON  
25505 SW LONE FIR LANE  
WEST LINN OR 97068

1S116AD02501  
ADOLPHSON LOVING TRUST  
25505 SW LONE FIR LN  
WEST LINN OR 97068

1S116AD02300  
ADOLPHSON R G  
ADOLPHSON R G JR  
BY A-2 ENTERPRISES  
4555 SW MAIN ST  
BEAVERTON OR 97005

1S116DB00400  
AFOURKEEFF ANJA T  
5075 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD07000  
AHN DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

1S116AD07001  
~~AHN DANIEL H~~  
~~12820 SW 2ND ST~~  
BEAVERTON OR 97005

1S116AD06100  
ALPROP CO  
6149 SW SHATTUCK RD  
PORTLAND OR 97221

1S116AC04700  
ANDERSON ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC05500  
~~ANDERSON ANSEL E & MARIAN L~~  
~~4715 SW FAIRMOUNT DR~~  
BEAVERTON OR 97005

1S116DB03400  
ARLT LINDA L  
13275 SW PEAR ST  
BEAVERTON OR 97005

1S116AC01800  
ARMSTRONG JAMES R  
LINDA L  
13325 SW 2ND  
BEAVERTON OR 97005

1S116DA90027  
BABB DORIS LEE  
5136 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90023  
BAKER VIRGIL A & MARIE  
5166 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AC04500  
BARTLETT CLIFFORD L  
13500 SW 2ND  
BEAVERTON OR 97005

1S116AA05200  
BASEEL PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

1S116AA05301  
~~BASEEL PRESTON~~  
~~6977 SW TIERRA DEL MAR~~  
BEAVERTON OR 97007

1S116AB03100  
BEAVERTON FOODS INC  
PO BOX 687  
BEAVERTON OR 97005

1S116AC04400  
BEAVERTON PROPERTIES  
13470 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD11100  
BEAVERTON SCHOOL DISTRICT 48  
MERLE DAVIS ELEMENTARY  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02100  
BEAVERTON SCHOOL DISTRICT 48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD11000  
BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD02900  
~~BEAVERTON SCHOOL DISTRICT 48~~  
~~BEAVERTON HIGH SCHOOL~~  
~~ATTN FACILITIES DEPT~~  
~~16550 SW MERLO RD~~  
BEAVERTON OR 97006

1S116AD07100  
~~BEAVERTON SCHOOL DISTRICT 48~~  
~~BEAVERTON HIGH SCHOOL~~  
~~ATTN FACILITIES DEPT~~  
~~16550 SW MERLO RD~~  
BEAVERTON OR 97006

1S116AC02100  
BEAVERTON SCHOOL DISTRICT 48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02500  
BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD10900  
BEAVERTON SCHOOL DISTRICT 48J  
ATTN FACILITIES MNGMT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AA02800  
~~BEAVERTON CITY OF~~  
~~PO BOX 4755~~  
BEAVERTON OR 97076

1S116AD11200  
~~BEAVERTON CITY OF~~  
00000

1S116AC04600  
~~BEAVERTON CITY OF~~  
~~4950 SW HALL BLVD~~  
BEAVERTON OR 97005



1S116AD01300  
BEAVERTON CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

1S116DB11100  
BEAVERTON CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

1S116AA06000  
BENNETT CLEVE E & LOIS W TRUSTE  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

1S116AA05900  
BENNETT CLEVE E AND  
LOIS W TRUSTEES  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

1S116AD10400  
BENNETT TERRI LOUISE  
12820 SW 4TH ST  
BEAVERTON OR 97005

1S116AA02900  
BIGGI STEVE AND LAURA  
3843 SW HALL BLVD  
BEAVERTON OR 97005

1S116DB00100  
BLOOMER DANIEL D MARY L  
4925 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA90055  
BYRNES DONALD B & B JERI  
12900 SW 5TH ST  
BEAVERTON OR 97005

1S116AA05400  
CAFFEE CENTER JOINT VENTURE  
BY ANDERSON & ASSOCIATES  
PO BOX 3821  
BELLEVUE WA 98009

1S116DB03000  
CARSON JERRY/PAULA  
13270 SW 6TH  
BEAVERTON OR 97005

1S116AC05700  
CASSIDY MARI  
4820 SW MENLO  
BEAVERTON OR 97005

1S116DA02500  
CASSON KATHLEEN ANN  
3818 SE 9TH AVE  
PORTLAND OR 97202

1S116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

1S116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

1S116AC01300  
CHIA CHIH CHIANG & YEA HUEY  
13200 SW FARMINGTON RD  
BEAVERTON OR 97006

1S116AC01400  
CHIA FUO FUH &  
WU CHUN ER  
13150 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD10601  
COLEMAN ANNE S  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC03000  
COLEMAN ANNE SCOTT  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC01900  
COOLEY CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

1S116DA07003  
COOLEY CRAIG L & JUNE E  
21797 SW OAK-HILL LN  
TUALATIN OR 97062

1S116AD08000  
CORP OF PRESIDING BISHOP OF  
CHURCH OF JESUS CHRIST LDS  
TAX ADMIN FILE 536-2490  
50E NORTH TEMPLE  
SALT LAKE CITY UT 84150-2201

1S116AC05801  
CORSON JOHN D  
4850 SW MENLO DR  
BEAVERTON OR 97005

1S116AD07400  
CRITELLI FAMILY TRUST  
c/o CRITELLI TINA  
BY GARY A & SHARON L CRITELLI  
PO BOX 87058  
VANCOUVER WA 98687

1S116AD10300  
CRITELLI GARY A & SHARON L TRS  
PO BOX 87058  
VANCOUVER WA 98687

1S116DA01600  
CRITELLI GARY A & SHARON L TRS  
PO BOX 87058  
VANCOUVER WA 98687

1S116DB00200  
D'ALFONSO PAULA L  
4975 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD10500  
DAMIANA RONALD G  
GEORGIA M  
12870 SW 4TH  
BEAVERTON OR 97005

1S116DB00800  
DANEIL HOLLIS MARION  
13425 SW 6TH  
BEAVERTON OR 97005

1S116DB00900  
DANIEL AVERILL H/HOLLIS M  
13425 SW 6TH  
BEAVERTON OR 97005

1S116AC01500  
DANISH SEYMOUR R REV TRUST &  
BERRY HARRY L ET AL  
BY DANISH CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005

1S116AD03100  
D'SILVA FELIX & LOURDES  
8765 SW JAMIESON RD  
PORTLAND OR 97225

1S116DA01700  
EIMERS SANDRA P &  
EIMERS ANGELA  
6045 SW 167TH AVE  
ALOHA OR 97007

1S116DB03101  
ENGEL MICHAEL J &  
REBECCA C  
13210 SW 6TH ST  
BEAVERTON OR 97005

1S116AA05700  
ETON LANE LTD PARTNERSHIP  
WALKER HUBERT E TRUSTEE &  
ETON LANE LLC  
PO BOX 14746  
PORTLAND OR 97293

1S116AA05800  
ETON LANE LTD PARTNERSHIP  
WALKER HUBERT E TRUSTEE  
PO BOX 14746  
PORTLAND OR 97214

1S116AC04200  
EVANS CATHERINE B  
11775 SW 11TH STREET  
BEAVERTON OR 97005

1S116AC01200  
FISHBACK MERL E &  
GLORIA M TRUST  
FISHBACK MERL E/GLORIA M TR  
9700 NW GROVELAND RD  
HILLSBORO OR 97124

1S116DA02200  
FLOREN JANET M AND GREG E  
12870 SW 6TH AVE  
BEAVERTON OR 97005

1S116DB00300  
FRANKLIN JOHN A  
5025 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC04900  
FRIEZE REX D SHIRLEY L  
4720 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116DB01100  
GABRIEL KATHLEEN  
13470 SW 3RD ST  
BEAVERTON OR 97005

1S116AC05600  
GARCIA WAGNER  
4800 SW MENLO DR  
BEAVERTON OR 97005

1S116DA90025  
GARTLAND SHARIN  
5162 SW ERICKSON  
BEAVERTON OR 97005

1S116AD03000  
GARVER JON & REBECCA  
12855 SW 2ND ST  
BEAVERTON OR 97005

1S116AD02200  
GENEST VIRGINIA E  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD02100  
GENEST VIRGINIA E  
BY VALLEY DAIRY QUEEN  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD02700  
GHIM MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

1S116AD02600  
GHIM MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

1S116DB05000  
GLASNER GENE R  
c/o DREY BRIAN S & PAMELA R  
5225 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AC06000  
GLICKMAN TERRY L  
13555 SW THIRD ST  
BEAVERTON OR 97005

1S116DA01300  
GORMAN JOHN & PATRICIA A  
16555 SW HIGH HILL LN  
BEAVERTON OR 97007

1S116AC01000  
GRABHORN CHAD  
350 NW FREEMAN  
HILLSBORO OR 97124

1S116AC03700  
GREEN ERNESTINE RUTH  
4820 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

1S116AA02100  
GREER DAVID J & LISA D  
19370 SW SOUTHVIEW ST  
ALOHA OR 97007

1S116AC05300  
GREMAUX DEANE B AND  
PATRICIA A  
4755 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

1S116AB02100  
GRL INC  
13095 SW CANYON RD  
BEAVERTON OR 97005

1S116DB10800  
GUSTAFSON CONRAD W & ANNA M  
13245 SW PEARL ST  
BEAVERTON OR 97007

1S116AA02500  
GUTZLER CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116AA02600  
GUTZLER CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116AA02300  
GUTZLER THOMAS M & CAROLYN E  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116AA02400  
GUTZLER THOMAS-M AND  
CAROLYN E  
~~BY TOM'S PANCAKE HOUSE~~  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116DA90058  
HALL LAURA I  
12888 SW 5TH  
BEAVERTON OR 97005

1S116DA01900  
HALSTEN ELSIE M TRUSTEE  
12855 SW 6TH  
BEAVERTON OR 97005

1S116DA02000  
HAMREUS CALVIN E & JOYCE K  
5075 SW MAIN AVE  
BEAVERTON OR 97005

1S116DA07004  
HATTENHAUER CHRISTINE  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

1S116DA07002  
~~HATTENHAUER CHRISTINE~~  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

1S116DA01800  
HEDGES SALLY C  
12850 SW 5TH AVE  
BEAVERTON OR 97005

1S116DB04600  
HEISLER MARYANNE &  
NEUFELL PATRICIA J  
13250 SW PEARL  
BEAVERTON OR 97005

1S116DA01100  
HETU TED G  
5075 SW ANGEL AVE  
BEAVERTON OR 97005

1S116AD06000  
HILL RICHARD E AND FLORENCE E  
8405 SW MAVERICK PL  
BEAVERTON OR 97008

1S116AD10100  
HOUDROJ ALI  
4970 SW MAIN ST  
BEAVERTON OR 97005

1S116DA90028  
HUESMAN NICOLE K  
5134 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AA03000  
HUNKAPILLAR PATRICIA W  
BY THE TAGGART TRUST  
21185 NW EVERGREEN PKWY  
SUITE 101  
HILLSBORO OR 97124

1S116AA03100  
HUNKAPILLAR PATRICIA W  
SMITH HARRIET L AND GEORGE  
BY TAGGART TRUST THE  
21185 NW EVERGREEN PKW STE 101  
HILLSBORO OR 97124

1S116AC00800  
HUNT SHERYLL R/WILLIAM J &  
HUNT JONATHAN J  
13380 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116DA02100  
HUTCHENS DANA C & LORI E  
12820 SW SIXTH ST  
BEAVERTON OR 97005

1S116DA06700  
HUYGENS JOSEPH D &  
JULIE ANN HUTCHINSON  
7830 SW LAUREL ST  
PORTLAND OR 97225

1S116AD09901  
JOHNSON CAROL L R  
PO BOX 524  
SCAPPOOSE OR 97056

1S116AC04000  
JONES E CHANNING & PAULA KAY  
13420 SW 2ND AVE  
BEAVERTON OR 97005

1S116DA90052  
JONES JERRY C  
12912 SW 5TH 52  
BEAVERTON OR 97005

1S116AC03600  
JONES KRISTIN L  
4870 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA07000  
JUNELL BONNIE M &  
BABCOCK DOUGLAS C  
4820 SW 18TH PL  
PORTLAND OR 97201

1S116AD01600  
KABDEBO JOSEPH & CAMILLE  
725 SW VIEWMONT DR  
PORTLAND OR 97225

1S116AD01700  
~~KABDEBO JOSEPH & CAMILLE~~  
725 SW VIEWMONT DR  
PORTLAND OR 97225

1S116AD01800  
~~KABDEBO JOSEPH AND CAMILLA~~  
725 SW VIEWMONT DR  
PORTLAND OR 97225

1S116DB03202  
KAUTH JANE F  
5115 SW ERICKSON AVE  
BEAVERTON OR 97007

1S116DA07300  
KIKO JOHN R &  
ORTIZ INGRID  
5155 SW MAIN AVE  
BEAVERTON OR 97005

1S116AD04300  
KIM UN & SUE J  
14756 SW KILCHIS  
BEAVERTON OR 97007

1S116AA02700  
KIM YOUNG M & LINDA  
12855 SW CANYON RD  
BEAVERTON OR 97005

1S116DA06800  
KING CHARLES C & ROM N  
5150 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90024  
KLANECKY ARDITH ANN  
5164 SW ERICKSON  
BEAVERTON OR 97005

1S116DB10900  
KO JOON B  
13215 SW PEARL ST  
BEAVERTON OR 97005

1S116AD06800  
KRAEMER JOHN L & CLAUDIA R  
33713 NE WILLIAMS APT F-6  
SCAPPOOSE OR 97056

1S116DA90049  
KRIEG MARGIE J  
12924 SW 5TH ST  
BEAVERTON OR 97005

1S116DA90057  
LAMB KEVIN M &  
YANT SUSAN A  
12892 SW 5TH ST  
BEAVERTON OR 97005

1S116BD00100  
LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

1S116AA04690  
~~LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005~~

1S116AC00500  
LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

1S116BD00200  
~~LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005~~

1S116AC00600  
~~LANPHERE ROBERT  
ATTN DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005~~

1S116DB03100  
LEE SANDY  
344 NE 156TH AVE  
PORTLAND OR 97230

1S116DB02900  
LEIVA LUIS ALONSO & SANDRA J  
13320 SW 6TH ST  
BEAVERTON OR 97005

1S116AC04300  
LEONARD CLIFFORD A & SHARON B  
7345 SW 84TH AVE  
PORTLAND OR 97223

1S116AD06200  
LESHER SAM E/HELENA R &  
LESHER CARL H  
7725 SE 32ND AVE  
PORTLAND OR 97202

1S116AC06200  
LEWIS SUNNY M  
13425 SW 3RD  
BEAVERTON OR 97005

1S116DA90047  
LIDBERG MARY ALICE C  
12932 SW 5TH AVE  
BEAVERTON OR 97005

1S116AD08100  
LIEDTKE CATHERINE A  
17561 SW ALBERT CT  
BEAVERTON OR 97007

1S116DA01400  
LISICKI THOMAS D & RUTH S  
13332 SW BENCHVIEW TER  
TIGARD OR 97223

1S116DA07400  
LOCKARD DOROTHY J  
LIVING TRUST THE  
5125 SW SHERWOOD PL  
BEAVERTON OR 97005

1S116AC02800  
LONG PATRICIA C & JOE PAT  
5070 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116AC04800  
LOPEZ JORGE & CARMEN  
4700 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116DB03201  
LOY JOHN G  
5105 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90060  
LUCAS LAURIE G  
12880 SW 5TH AVE  
BEAVERTON OR 97005

1S116DA06200  
LUCKE WILLIAM T TRUSTEE  
5205 SW MAIN AVE  
BEAVERTON OR 97005

1S116AC06400  
LUND BRADLEY W  
4855 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116AC01601  
LUNDGREN TIMOTHY  
18213 PORTLAND AVE  
GLADSTONE OR 97027

1S116DA90041  
MACK TIMOTHY F  
12956 SW 5TH  
BEAVERTON OR 97005

1S116DA90048  
MATELICH RAE MARIE  
12928 SW 5TH AVE  
BEAVERTON OR 97005

1S116DA90054  
MAYES PATSY L  
12904 SW 5TH ST  
BEAVERTON OR 97005

1S116AD08200  
MERCADO NEVILLE & RENEE  
PO BOX 487  
BEAVERTON OR 97075

1S116DB02800  
MEYER JANE E & RICHARD J &  
JOHN M  
5450 SW 150TH CT  
BEAVERTON OR 97007

1S116AC03800  
MILLARD KATHLEEN  
4790 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

1S116AC03200  
MOHR PATRICIA ALICE  
WILLIAM RICHARD  
7495 SW 136TH AVE  
BEAVERTON OR 97008

1S116AD08300  
MORGAN WILLIAM L & EVE E  
20855 NW CHILOQUIN  
PORTLAND OR 97229

1S116DA01200  
MORRIS GLENN R AND ANITA S  
12755 SW 6TH  
BEAVERTON OR 97005

1S116AB02200  
NELSEN MARY G  
9965 SW 170TH  
BEAVERTON OR 97007

1S116AC00700  
NEUSCHWANDER FRED ANDREW &  
INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064

1S116AC00900  
~~NEUSCHWANDER FRED ANDREW &  
INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064~~

1S116AC02900  
NILSON GAIL ANN  
5020 SW FAIRMOUNT  
BEAVERTON OR 97005

1S116DA90050  
NORMAN CONSTANCE R  
12920 SW 5TH ST  
BEAVERTON OR 97005

1S116DA90051  
OKAMOTO BRETT K  
12916 SW 5TH  
BEAVERTON OR 97005

1S116AC06300  
OVERBY ARDON L  
PO BOX 704  
BEAVERTON OR 97075

~~1S116DB11000  
OVERBY ARDON L  
PO BOX 704  
BEAVERTON OR 97075~~

1S116AD00900  
PARKS LOREN E  
PO BOX 5669  
ALOHA OR 97006

1S116AD10000  
PEARCE JOSEPH J  
4920 SW MAIN  
BEAVERTON OR 97005

1S116AC06500  
PETERSEN DONALD E  
10240 SW HAWTHORNE LN  
PORTLAND OR 97225

1S116AD03500  
PHAM HIEN &  
NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006

1S116AD03400  
~~PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

~~1S116AD03200  
PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

1S116AD03300  
~~PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

1S116AD06600  
~~PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

1S116AD08400  
PIES SANDRA J & EVERETT M TRS  
29395 SW HILLECKE RD  
HILLSBORO OR 97123

1S116DA90046  
PODEBSKI ADAM P & GUTA H  
PODEBSKI BENNY  
12936 SW 5TH  
BEAVERTON OR 97005

1S116AC02001  
QUINN CAROLYN F  
13395 SW 2ND ST  
BEAVERTON OR 97005

1S116AD07500  
QUTUB WAHBI M &  
MATAR RIAD A  
7883 SW BARNARD DR  
BEAVERTON OR 97007

1S116AD02890  
RANDOLPH GLORIA  
2725 SW 185TH  
ALOHA OR 97006

~~1S116AD02800  
RANDOLPH GLORIA C  
2725 SW 185TH AVE  
ALOHA OR 97005~~

1S116DB00600  
REMCHER GEORGE & FLORENCE  
4240 SW 102ND AVE  
BEAVERTON OR 97005

1S116AC05400  
ROBERTS LEON &  
CARRIE  
4725 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA01500  
ROBINSON JACK A & PATRICIA A  
3830 SE BENTLEY AVE  
HILLSBORO OR 97123

1S116DB04700  
ROSENHOLM DAVID  
ROSENHOLM ANDREA  
13220 SW PEARL ST  
BEAVERTON OR 97005

1S116DB04800  
SANABRIA-BARILLAS JOSE ALFREDO  
ALVARADO JUANA  
13170 SW PEARL ST  
BEAVERTON OR 97005

1S116AC02700  
SCALONE DAVID  
13255 SW 6TH ST  
BEAVERTON OR 97005

1S116AD01500  
SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

1S116AD01400  
~~SCHOEN ENTERPRISES LLC~~  
~~3815 WAUNA VISTA DR~~  
VANCOUVER WA 98661

1S116DA90059  
SESSIONS KEITH W & LAVERNA M  
TRUSTEES  
12884 SW 5TH  
BEAVERTON OR 97005

1S116AB01901  
SKORO MARION & MIRELA  
19300 NW GILLIHAN LOOP RD  
PORTLAND OR 97231

1S116DB03500  
SMITH DONALD J PHYLLIS J  
13295 SW PEARL ST  
BEAVERTON OR 97005

1S116AD09900  
SMITH IAN C & NADINE T  
12750 SW 4TH ST  
BEAVERTON OR 97005

1S116DA90053  
SMITH RICHARD BRENDAN &  
HILARY ANTOINETTE MARY  
12908 SW 5TH ST  
BEAVERTON OR 97005

1S116AC05000  
SMOLENSKY G MARIKA  
PO BOX 1114  
BEAVERTON OR 97075

1S116DB00700  
SNODGRASS PHILIP R & PATRICIA  
13420 SW THIRD ST  
BEAVERTON OR 97005

1S116AC05800  
SOTH COLLEEN V TRUSTEE  
SOTH FORREST C TRUSTEE  
4890 SW MENLO DR  
BEAVERTON OR 97005

1S116AD04400  
SPRINGER JAMES F & SUE  
c/o BEAVERTON FUNERAL HOME INC  
4126 NORLAND AVE  
BURNABY BC CANADA  
V5G 358

1S116AD04501  
~~SPRINGER JAMES F & SUE~~  
~~c/o BEAVERTON FUNERAL HOME INC~~  
~~4126 NORLAND AVE~~  
~~BURNABY BC CANADA~~  
V5G 358

1S116DA90030  
SPURGEON JOAN P  
5130 SW ERICKSON  
BEAVERTON OR 97005

1S116DB00500  
ST CLAIR JERALD LEE  
SHIRLEY ANN  
13355 SW 6TH ST  
BEAVERTON OR 97005

1S116AC02600  
STARKE DONALD B EDNA A  
13155 SW 6TH  
BEAVERTON OR 97005

1S116AA02101  
STEICHEN NANCY & MCCOURT JOHN  
c/o HOVERSLAND GORDON & MYRNA L  
4350 SW CEDAR HILLS BLVD  
BEAVERTON OR 97005

1S116AD10600  
STEIGER JOHN M & LOU-WAYNE  
301 NW MURRAY BLVD  
PORTLAND OR 97229

1S116AC01100  
STOCKWELL KATHERINE R &  
BROWN GARY E  
13240 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116DA01000  
STOLTENBURG BARBARA  
c/o RAINEY WALLACE L & LESLIE C  
5025 SW ANGEL AVE  
BEAVERTON OR 97005

1S116AC02000  
SURBY PAUL W L FRANCES  
13375 SW 2ND  
BEAVERTON OR 97005

1S116AC03500  
THAO PANCHONG & MAO HER  
4920 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA90029  
THOMAS BARBARA A  
5132 SW ERICKSON  
BEAVERTON OR 97005

1S116AD04200  
THOMAS KAREN JO  
12650 SW 1ST  
BEAVERTON OR 97005

1S116AD06500  
THOMAS KELLY R &  
BOTHNE MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005

1S116AD06900  
~~THOMAS KELLY R &~~  
~~BOTHNE MELISSA~~  
~~12720 SW 2ND ST~~  
BEAVERTON OR 97005

1S116AA05001  
~~THOMPSON AND ADAMS~~  
~~4500 SW HALL BLVD~~  
BEAVERTON OR 97005

1S116AA05101  
THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

IS116AA05100  
~~THOMPSON AND ADAMS~~  
4500 SW HALL BLVD  
BEAVERTON OR 97005

IS116AA05102  
~~THOMPSON AND ADAMS~~  
4500 SW HALL BLVD  
BEAVERTON OR 97005

IS116DA90056  
THOMPSON DANIEL HARRISON &  
THOMPSON TOM G &  
THOMPSON MARGIE J  
12896 SW 5TH AVE  
BEAVERTON OR 97005

IS116AC06100  
THOMPSON MAURICE J & KIMBERLY  
13475 SW 3RD ST  
BEAVERTON OR 97005

IS116AC01700  
THOMPSON TOMMY G  
JEANNETTE L  
13305 SW 2ND ST  
BEAVERTON OR 97005

IS116DB04900  
TISCHLER STEPHEN & KATHLEEN  
5195 SW ERICKSON AVE  
BEAVERTON OR 97005

IS116AA02200  
TOSCO CORP  
PROPERTY TAX DEPT-DC17  
PO BOX 52085  
PHOENIZ AZ 85072

IS116AD07600  
TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

IS116AD07700  
~~TUALATIN HILLS PARK  
RECREATION DISTRICT~~  
15707 SW WALKER RD  
BEAVERTON OR 97005

IS116AD06700  
UNDERHILL MICHAEL  
12795 SW 3RD ST  
BEAVERTON OR 97005

IS116AA04900  
VADEN MICHAEL & JANE  
12740 SW CANYON RD  
BEAVERTON OR 97005

IS116AA05000  
~~VADEN MICHAEL & JANE~~  
12740 SW CANYON RD  
BEAVERTON OR 97005

IS116AA04800  
VADEN MICHAEL & JANE  
BY LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116DA02501  
VEAZEY THOMAS C AND  
ROXANNE T  
12770 SW 6TH ST  
BEAVERTON OR 97005

IS116DA90045  
VINK NANCY E  
12940 SW 5TH ST  
BEAVERTON OR 97005

IS116AC05100  
VOGEL MICHAEL J & D'ANNE T  
4770 SW FAIRMONT DR  
BEAVERTON OR 97005

IS116AC05200  
VON W METHENEY FAMILY TRUST  
4775 SW FAIRMONT DR  
BEAVERTON OR 97005

IS116DA90026  
VOSE BENJAMIN A  
PO BOX 73  
TOLOVANA PARK OR 97145

IS116AC03100  
WABNITZ STEVEN G  
2648 SW 153RD AVE  
BEAVERTON OR 97006

IS116AC00400  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

IS116AC00200  
~~WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744~~  
BEAVERTON OR 97075

IS116AC00100  
~~WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744~~  
BEAVERTON OR 97075

IS116AC00190  
~~WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744~~  
BEAVERTON OR 97075

IS116AD10800  
WELDON GEORGE E AND  
FLORENCE GRACE  
8783 SW 175TH AVE  
BEAVERTON OR 97007

IS116DA06801  
WENDT WILLIAM J CAROL  
5120 SW ERICKSON AVE  
BEAVERTON OR 97005

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WHITACRE SEAN M  
12952 SW 5TH ST  
BEAVERTON OR 97005

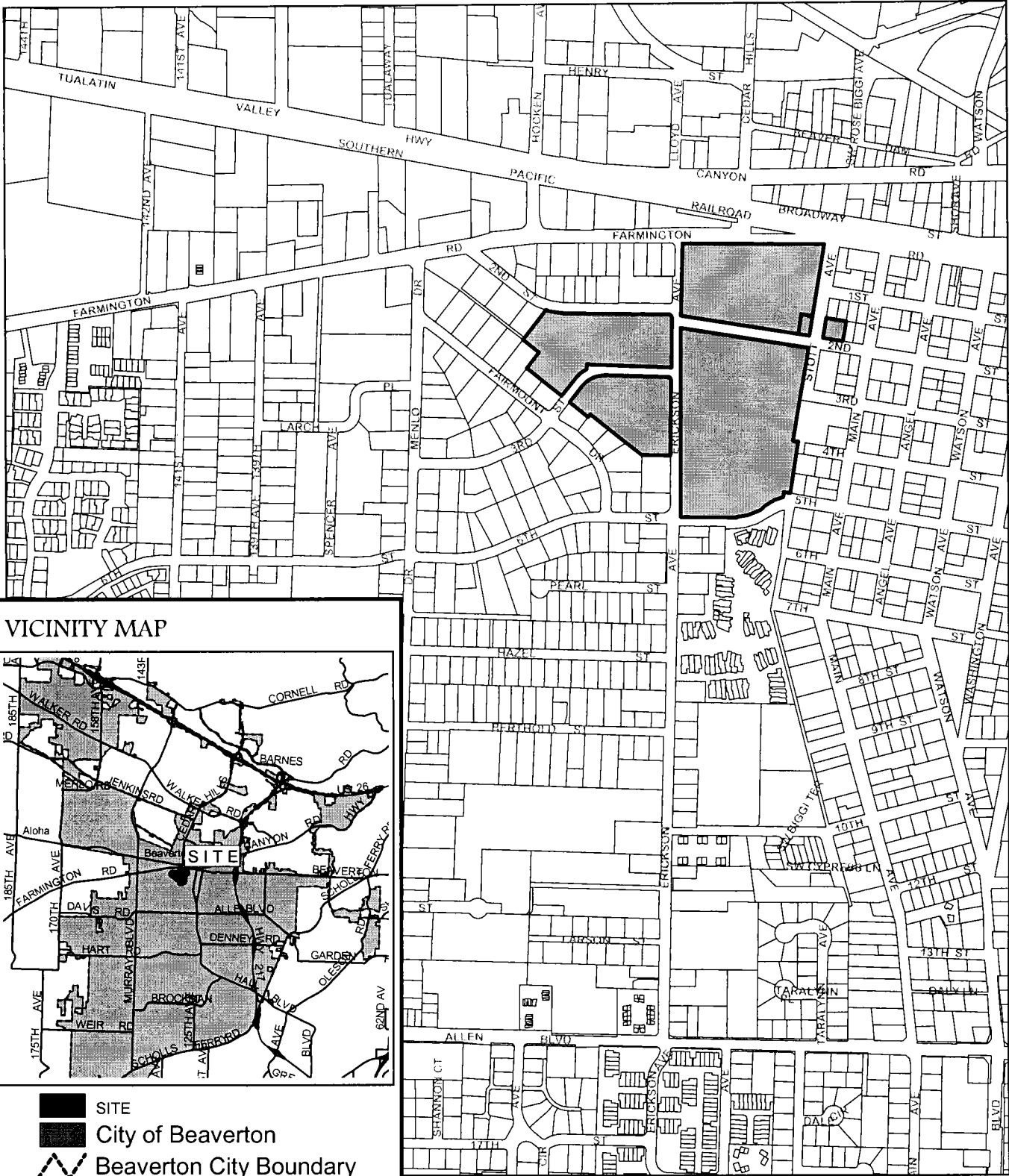
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WIESMANN LARRY J  
13450 SW 2ND ST  
BEAVERTON OR 97005

IS116AD10700  
WILD GENE D AND BETTY LOU  
4975 SW MAIN  
BEAVERTON OR 97005

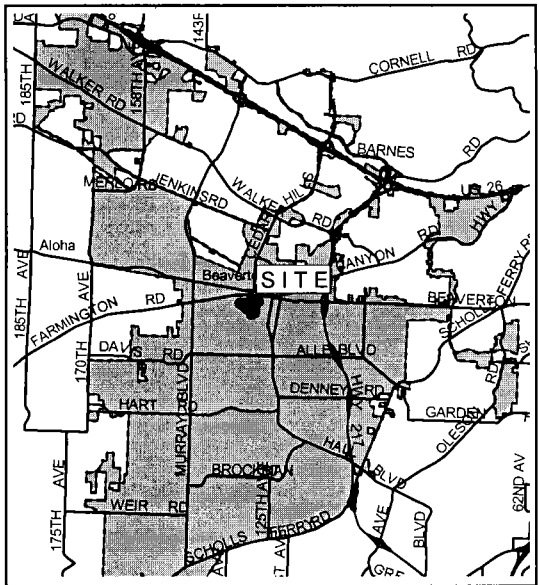
IS116AD10200  
YI HA YON &  
YI CHO SIM  
4975 SW ANGEL AVE  
BEAVERTON OR 97005




IS116AD03600  
YOSHIDA JUNKI  
8440 NE ALDERWOOD RD STE A  
PORTLAND OR 97220

# DETAIL MAP



VICINITY MAP



-  SITE
-  City of Beaverton
-  Beaverton City Boundary



City of Beaverton

BEAVERTON HIGH SCHOOL HISTORIC TREES  
**COMMUNITY DEVELOPMENT DEPARTMENT**  
 Development Services Division

11/27/01  
 Map Number  
 1S116AC  
 1S116AD  
 Application #  
 TPP 2001-0008







City of Beaverton  
Community Development Department  
4755 S.W. Griffith Drive  
P.O. Box 4755  
Beaverton, OR 97076

**BDR 2001-0213/CUP 2001-0031/VAR 2002-0002/TPP 2001-0008 BEAVERTON HIGH SCHOOL CAFETERIA AND PARKING LOT EXPANSION** **03/28/02**

**NOTICE TO MORTGAGEE, LIENHOLDER  
VENDOR OR SELLER: IF YOU RECEIVE THIS  
NOTICE, IT MUST BE PROPERLY  
FORWARDED TO PURCHASER**

**City of Beaverton  
DEVELOPMENT APPLICATION  
NOTICE OF REVIEW**

Notice Date: **February 5, 2002**

The Historic Resource Review Committee will conduct a public hearing on **March 21, 2002** at 6:30 pm. The Planning Commission will conduct a public hearing on **March 20, 2002** at 7 pm. The Board of Design Review will conduct a public hearing on **March 28, 2002**, at 6:30 p.m. The public hearings will be held to consider the following applications:

**Beaverton High School Cafeteria and Parking Lot Expansion**

The following land use applications have been submitted to construct a new two-level, 30,000-square foot cafeteria building located directly north of and connecting to the existing high school building. The development proposal is located at 13000 SW 2<sup>nd</sup> Street; Washington County Assessor's Map 1S1-16AD, on Tax Lots 11100, 11000, 02900, 07100 and 10900 and 1S116AC, on Tax Lots 02100 and 02500. The site is zoned Urban Low Density (R-10) and is approximately 27 acres in size.

**BDR2001-0213: Type III Design Review**

The applicant is requesting the Board of Design Review to approve construction of a new cafeteria building and parking area at the existing Beaverton High School location. The proposal includes construction of a new two-level, 30,000-square foot cafeteria building located directly north of and connecting to the existing high school building, new parking area, and associated landscaping. In taking action on the proposed development, the Board of Design Review shall base its decision on the approval criteria listed in Section 40.10.15.3.C.

**CUP2001-0031: Conditional Use Permit**

The applicant requests approval of a Conditional Use Permit (CUP) to build new cafeteria building and parking area at the existing Beaverton High School location. Educational institutions, including public, private or parochial academic schools are identified specifically as a conditional use within the R-10 zone. The proposed project is an expansion of an existing conditional use. The Planning Commission, during a public hearing, will review this request. In taking action on the proposed development, the Planning Commission shall base its decision on the approval criteria listed in Section 40.05.15.2.C.

**VAR2002-0002: Variance (Regular)**

The applicant requests a variance to reduce the amount of bicycle parking required at Beaverton High School. Beaverton Development Code requires the minimum parking ratio of one long-term bicycle space for every 18 students. Based on the potential enrollment of the school of 2,200 students, 122 long-term bicycle parking spaces would be required. The applicant requests a variance to reduce the total number to 56 spaces, 66 fewer than the required amount. The Planning Commission, during a public hearing, will review this request. In taking action on the proposed development, the Planning Commission shall base its decision on the approval criteria listed in Section 40.80.15.2.C.

**TPP2001-0008: Tree Preservation Plan (Historic Trees)**

Request for Historic Resource Review Committee approval for the removal of 20 trees the City of Beaverton has designated as Historical Trees. In taking action on the proposed development, the Historic Resource Review Committee shall base its decision on the approval criteria listed in Section 40.75.15.1.C.2.

The Public Hearings will be conducted at Beaverton City Hall in the Council Chambers located at **4755 S.W. Griffith Drive, Beaverton, Oregon**. These hearings will be conducted for the purpose of receiving testimony from interested persons on the proposed development described above. Further information may be obtained by contacting the Development Services Division at **503-526-2348**.

Any person may appear before the Planning Commission, Board of Design Review, and/or the Historic Resource Review Committee public hearings and be heard in support of or in opposition to the granting

of the request. Written testimony is acceptable if received by either of the two Commissions and/or Board prior to the meeting. However, the public is encouraged to submit their written comments by **February 27, 2002**. On that date, the Planning Director will convene the Facilities Review Committee to consider the proposed development's compliance with the technical criteria of Section 40.10.15.3.C.1. The Committee will then forward recommended findings and conditions on the technical approval criteria to the Board of Design Review. The Committee's recommendation to the Board of Design Review shall be whether to approve, approve with conditions, or deny the proposed development.

The Beaverton Development Code requires that a notice be sent to property owners within 500 feet of the property proposed for development and to the chairperson of the affected Neighborhood Association Committee (NAC). Affected parties may review plans that have been submitted for the project. Copies of the staff report are available for inspection at no cost seven (7) calendar days prior to the public hearing, and will be duplicated upon request for a reasonable cost.

Please note: The Planning Commission reviews and takes action on zone changes, conditional uses, variances, and other proposals which result from the application of the requirements of the City Development Code. The Board of Design Review hears requests for development on properties where zoning, land use, traffic impacts, drainage and other functional issues have already been evaluated and approved. The Board's jurisdiction is over design issues such as landscaping, materials, building designs, etc. The Historic Resource Review Committee hears requests for the alteration or removal of designated historic resources within the City.

Following the hearings, the Planning Commission, Board of Design Review, and Historic Resource Review Committee can each do one of the following:

- 1) Deny the application.
- 2) Approve the application.
- 3) Approve the application with conditions.
- 4) Continue the application.

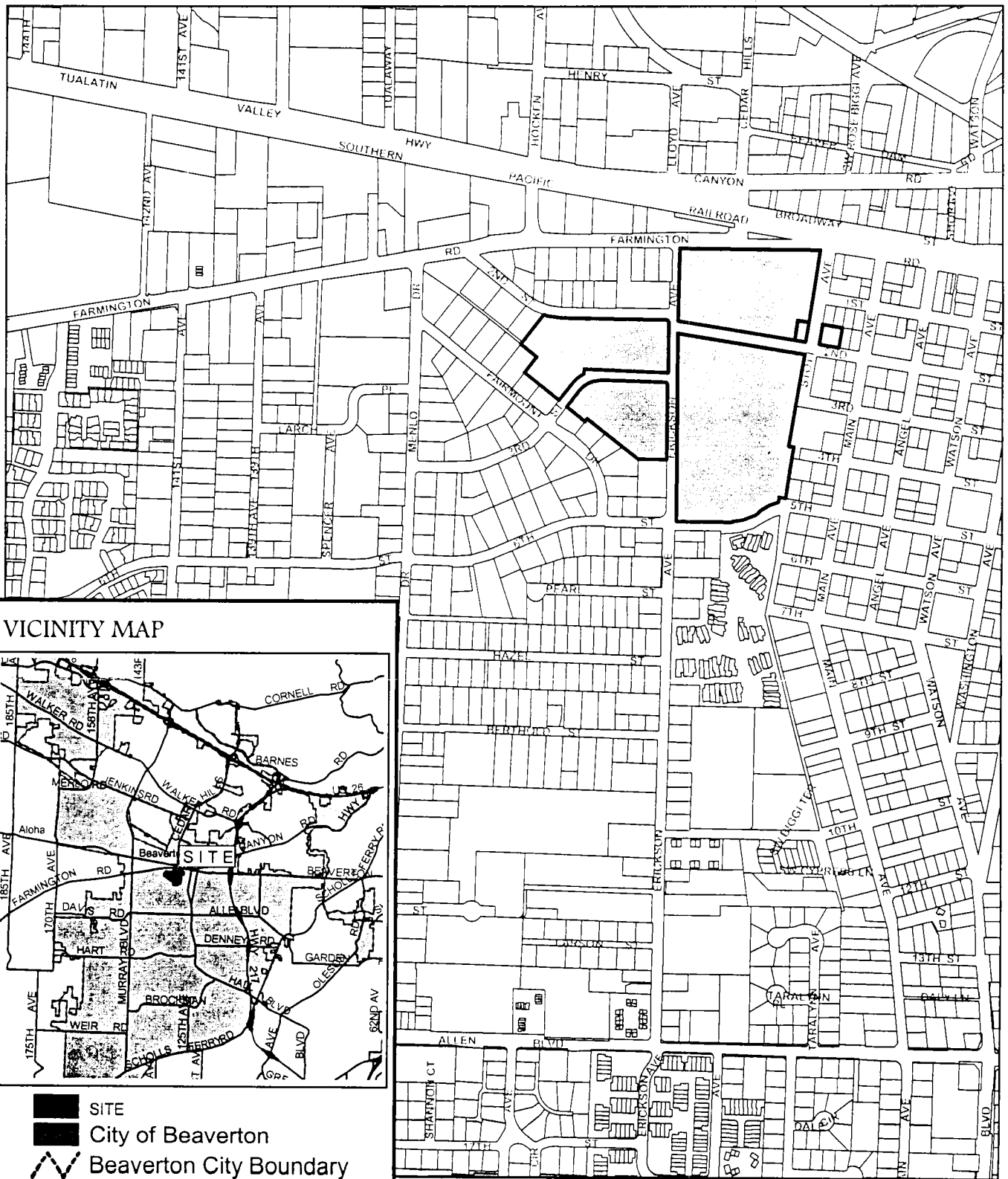
A participant in the hearing may request, before the close of the hearing, that the record remain open for at least seven days after the hearing.

Failure of an issue to be raised in person or by letter with sufficient specificity to afford decision makers an opportunity to respond to the issue precludes appeal to the Land Use Board of Appeals on that issue.

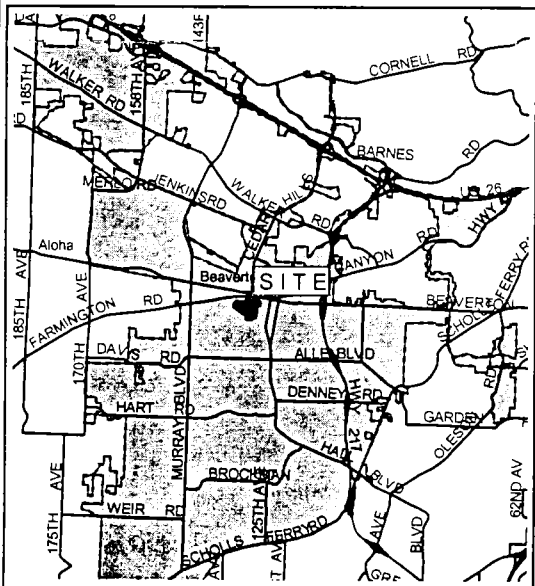
An applicant, aggrieved person, or the Mayor may appeal the action of the Planning Commission, Board of Design Review, or Historic Resource Review Committee to the City Council. The appeal must be filed within ten days of the Commission's or Board's action and be in writing. Appeal of any of the above specified applications must meet the requirements of Section 50.40.2 of the Development Code.




THIS INFORMATION IS AVAILABLE IN LARGE PRINT OR AUDIO TAPE UPON REQUEST. IN ADDITION, ASSISTED LISTENING DEVICES, SIGN LANGUAGE INTERPRETERS, OR QUALIFIED BILINGUAL INTERPRETERS WILL BE MADE AVAILABLE AT ANY PUBLIC MEETING OR PROGRAM WITHIN 72 HOURS ADVANCE NOTICE. TO REQUEST THESE SERVICES, PLEASE CALL 503-526-2222/VOICE/TDD.

# DETAIL MAP



## VICINITY MAP



-  SITE
-  City of Beaverton
-  Beaverton City Boundary



City of Beaverton

BEAVERTON HIGH SCHOOL CAFETERIA & PARKING LOTS

COMMUNITY DEVELOPMENT DEPARTMENT

Development Services Division

11/27/01

Map Number

1S116AC

1S116AD

Application #

BDR 2001-0213



# APPLICATION FOR COMPLETENESS

Completeness comments are due at Fac Rev on: ~~02/20~~ 1/9/02

<input checked="" type="checkbox"/> Development Services Planner (file)	Assigned Planner:  _____
<input checked="" type="checkbox"/> Jim Duggan	
<input checked="" type="checkbox"/> Don Gustafson	
	Phone: _____

## Project Information:

Project Number: TPP2001-0008

Project Type: Tree Preservation Plan

Project Name: Beaverton High School Historic Trees

Location: 13000 SW 2nd

Map & Tax Lot #s 1S1116AD10900, 11000, 11100

Zone: R-10

Owner/Applicant: Beaverton School Dist.

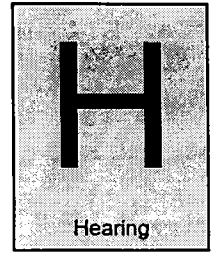
Comments prepared by: \_\_\_\_\_ Phone: \_\_\_\_\_

Complete  Incomplete

## Comments:

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

<input type="checkbox"/> BDR-3	<input type="checkbox"/> VAR-D	<input type="checkbox"/> CPA	<input type="checkbox"/> APP
<input type="checkbox"/> CUP-R	<input type="checkbox"/> VAR-R	<input type="checkbox"/> RZ	<input type="checkbox"/> HRRC
<input checked="" type="checkbox"/> TPP	<input type="checkbox"/> VAR-S	<input type="checkbox"/> TA	<input type="checkbox"/> _____



Community Input Meeting: _____	<input type="checkbox"/> BDR _____	<input type="checkbox"/> HRRC _____
	<input type="checkbox"/> PC _____	<input type="checkbox"/> CC _____

Project Number: TPP2001-0008

Project Name: Beaverton High School Historic Trees

See also: CUP 2001-0031; BDR 2001-0213

**Request for:**

Removal of historic trees along SW 2nd

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

The site is within the R-10 zone.

The site is located 13000 SW 2nd

and is approximately 17.69 acres in size.

Tax lot (s): 11100, 11000, 10900 Map 1S116AD

Owner/Applicant: Beaverton School District

**Routing**

From: L Kelly  
COUNCIL PLANNER

To: \_\_\_\_\_  
COMPLETENESS PLANNER

Assigned Planner: \_\_\_\_\_

**Approvals**

Draft Notice: \_\_\_\_\_  
ASSIGNED PLANNERS INITIALS

Final Notice: \_\_\_\_\_  
ASSIGNED PLANNERS INITIALS

**Items Received**

<input type="checkbox"/> Newspaper Notice	<input type="checkbox"/> Application copy
<input type="checkbox"/> Property list	<input checked="" type="checkbox"/> N.A.C. Central Beaverton
<input type="checkbox"/> Location map	<input type="checkbox"/> Other _____

<input checked="" type="checkbox"/> BDR-3	<input type="checkbox"/> VAR-D	<input type="checkbox"/> CPA	<input type="checkbox"/> APP
<input type="checkbox"/> CUP-R	<input type="checkbox"/> VAR-R	<input type="checkbox"/> RZ	<input type="checkbox"/> HRRC
<input type="checkbox"/> TPP	<input type="checkbox"/> VAR-S	<input type="checkbox"/> TA	<input type="checkbox"/>



Community Input Meeting:  BDR \_\_\_\_\_  HRRC \_\_\_\_\_  
 PC \_\_\_\_\_  CC \_\_\_\_\_

Project Number: BDR2001-0213  
 Project Name: Beaverton High School Cafeteria & Pkg Lots  
 See also: TPP 2001-0008; CUP 2001-0031

**Request for:**

[Empty space for request details]

NOTICE: IF THIS DOCUMENT IS LESS LEGIBLE THAN THIS NOTICE, IT IS DUE TO THE QUALITY OF THE ORIGINAL

The site is within the R10 zone  
 The site is located 13000 SW 2nd  
 and is approximately 17.69 acres in size  
 Tax lot (s): 11000, 11100, 10900 Map 15116AD  
 Owner/Applicant: Beaverton School Dist

**Routing**

From: [Signature]  
COUNTY PLANNER  
 To: \_\_\_\_\_  
COMPLETENESS PLANNER  
 Assigned Planner: \_\_\_\_\_

Approvals  
 Draft Notice: [Signature]  
ASSIGNED PLANNERS INITIALS  
 Final Notice: \_\_\_\_\_  
ASSIGNED PLANNERS INITIALS

**Items Received**

<input type="checkbox"/> Newspaper Notice	<input type="checkbox"/> Application copy
<input type="checkbox"/> Property list	<input checked="" type="checkbox"/> NAC <u>Central Beaverton</u>
<input type="checkbox"/> Location map	<input type="checkbox"/> Other _____

CHECKLIST

Project Number: BDR 2001-0213 / CUP 2001-0031 / VAR 2002-0002

Re-Notice. MAP Revision. TPP 2001-0008

Project Name: Beaverton High School Cafeteria + Parking lot expansion

Map Number: 1S116AC 1S116AD

Tax Lot(s): 02500 07100

Mailing Date: 3/11/02

Meeting Date(s): 3/20/02 / 3/28/02

RECEIVED: Request PC BDR (X)  
 Map (X)  
 Copy of Application (X)  
 Vicinity Map, Detail Map, Reference Map (X)  
 ArcView Labels (X)

COMPLETED: Notice to Newspaper (X)  
 Date Submitted 3/8/02 (X)  
 To Print 3/14/02 (X)  
 Newspaper Transmittal (X)  
 Labels for Owner, Applicant, Etc. (X)  
 500' Labels (Remove Duplicates) (X)  
 Request typed on PHN form (X)  
 Map and Criteria (X)  
 Reprographics form filled out (X)  
 20 Day Notice ( )  
 30 Day Notice ( )  
 Posting - Front Rack (X)  
 Affidavit (X)  
 Email (to PHN) (X)



SK

PC

BDR

FAC REV

CITY COUNCIL

HRRC

CHECKLIST

Project Number: BDR 2001-0213/CWP 2001-0031/VAR 2002-0022/FP 2001-0008

Project Name: Beaverton High School, Cafeteria & Parking Lots

Map Number: 151-16AD ; 151-16AC

Tax Lot(s): 11100; 11000; 2900; 7100; 10900; 2100; 2500

Mailing Date: 151-16AD ; 151-16AC  
FAC 2-27-02

Meeting Date(s): PC 3-20-02 ; HRRC 3-21-02 ; BDR 3-28-02

RECEIVED:

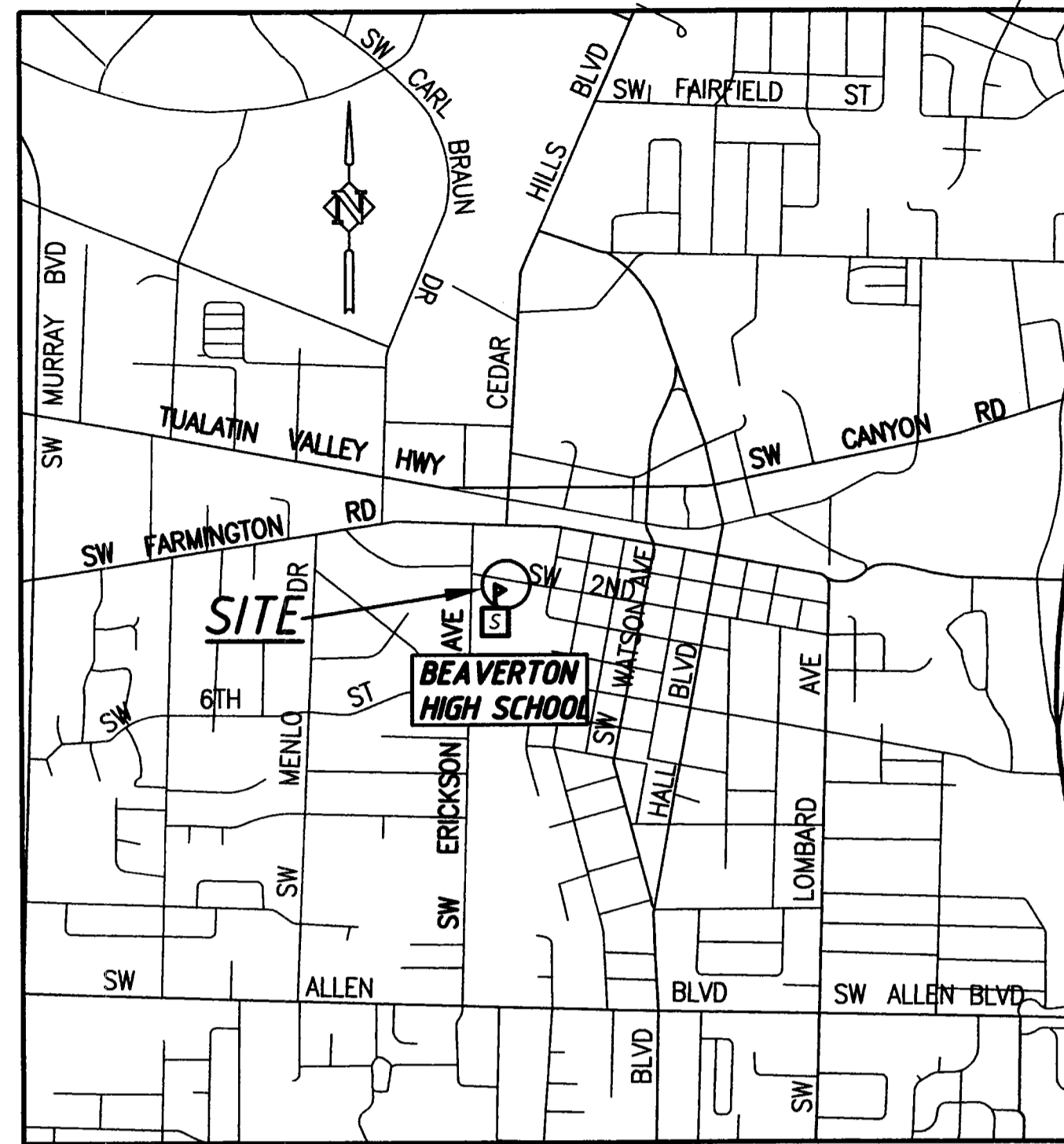
Request	(✓)
Map	(✓)
Copy of Application	(✓)
Vicinity Map, Detail Map, Reference Map	(✓)
ArcView Labels	(✓)

COMPLETED:

Notice to Newspaper	(✓)
Date Submitted <u>2-1-02</u>	(✓)
To Print <u>2-7-02</u>	(✓)
Newspaper Transmittal	(✓)
Labels for Owner, Applicant, Etc.	(✓)
500' Labels (Remove Duplicates)	(✓)
Request typed on PHN form	(✓)
Map and Criteria	(✓)
Reprographics form filled out	(✓)
20 Day Notice	(✓)
30 Day Notice	( )
Posting - Front Rack	(✓)
Affidavit	(✓)
Email (to PHN)	(✓)

TPP 2001-0008

# BEAVERTON HIGH SCHOOL CAFETERIA ADDITION

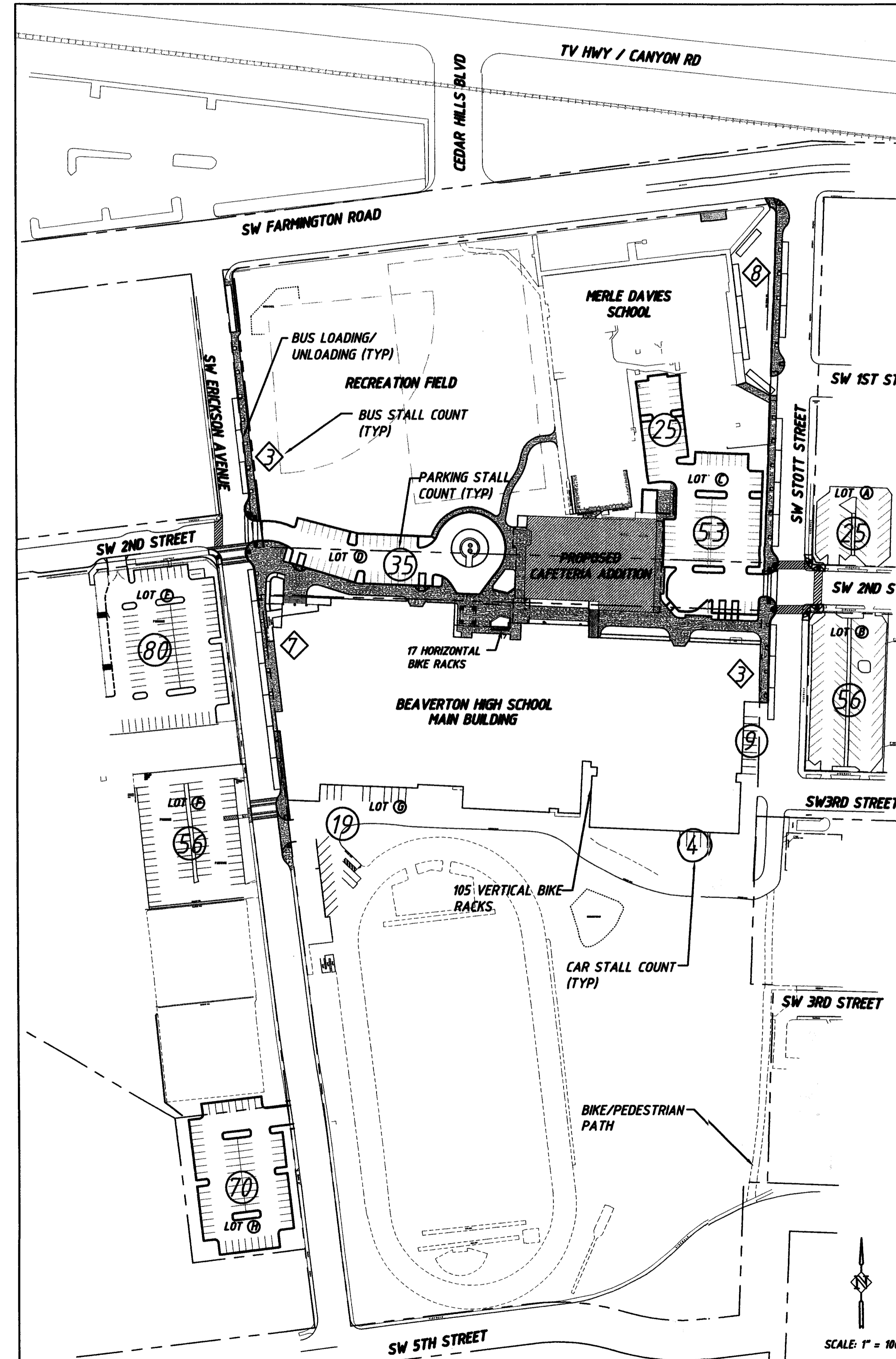


VICINITY MAP  
N.T.S.

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- 2 EXISTING CONDITIONS PLAN
- 3 FUTURE PARKING LOT EXISTING CONDITIONS
- 4 DIMENSION PLAN PUBLIC IMPROVEMENTS
- 5 DIMENSION PLAN PRIVATE IMPROVEMENTS (WEST)
- 6 DIMENSION PLAN PRIVATE IMPROVEMENTS (EAST)
- 7 FUTURE PARKING LOT DIMENSION GRADING AND UTILITY PLAN
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EXISTING PARKING / SIDEWALK AREA	90,000 SF (PUBLIC) 110,000 SF (PRIVATE)
EXISTING NUMBER OF PARKING SPACES	311 (PRIVATE), 66 (PUBLIC)
PROPOSED NUMBER OF PARKING SPACES	362 + 70 LOT H
PROPOSED PARKING/ SIDEWALK AREA	225,000 SF (PRIVATE)
EXISTING OPEN SPACE AREA	640,000 SF (PERVIOUS) 120,000 SF (IMPERVIOUS)
PROPOSED OPEN SPACE AREA	570,000 SF (PERVIOUS) 140,000 SF (IMPERVIOUS)
EXISTING BUILDING AREA	195,000 SF
PROPOSED BUILDING AREA	212,500 SF
EXISTING LANDSCAPED AREA	27,000 SF
PROPOSED LANDSCAPED AREA	44,000 SF
TOTAL SITE AREA	27 ACRES
EX % OPEN SPACE (PERVIOUS)/LANDSCAPE	56%
PRO% OPEN SPACE (PERVIOUS)/LANDSCAPE	48%



PARKING PLAN & SITE MAP

## GENERAL NOTES:

WORK SHALL CONFORM WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF BEAVERTON STANDARDS, CLEAN WATER SERVICES, THE UNIFORM BUILDING CODE (UBC), AND UNIFORM PLUMBING CODE (UPC).

EXISTING TOPOGRAPHIC INFORMATION AND UTILITIES SHOWN BASED ON AS-BUILT DRAWINGS AND TOPOGRAPHIC SURVEY COMPLETED MAY 2001 BY COMPASS ENGINEERING, PORTLAND, OREGON.

EXISTING MONUMENTS, PROPERTY CORNERS, AND SURVEY MARKERS SHALL BE PROTECTED. REPLACEMENT SHALL BE AT THE CONTRACTOR'S EXPENSE.

CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.

OREGON LAW REQUIRES YOU TO FOLLOW THE RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER AT 503-246-6699.

CONTRACTOR TO PROVIDE ENGINEER WITH COPY OF FIELD PLANS SHOWING ANY AND ALL DEVIATIONS FROM DESIGN FOR AS-BUILT SUBMITTAL TO OWNER AND CITY.

## STORM DRAINAGE

STORM DRAIN PIPE, BENDS, AND FITTINGS SHALL BE PVC (SDR 3034) OR SMOOTH INTERIOR HIGH DENSITY POLYETHYLENE CORRUGATED PIPE AS PRODUCED AND SPECIFIED BY ADS, PRODUCT NAME N12(1M), OR EQUIVALENT.

## EROSION CONTROL

### TEMPORARY EROSION CONTROL:

THE CONSTRUCTION NOTES AND DETAILS REFLECT RECOMMENDED PROCEDURES AS ADDRESSED IN THE "EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL" (DEC 2000) USA (EWS) AS ADOPTED BY THE CITY OF BEAVERTON.

IT IS THE INTENT OF THESE PLANS TO SHOW POSSIBLE APPLICATIONS OF RECOMMENDED PROCEDURES IN THE EVENT THAT EROSION CONTROL IS NEEDED. ALL RECOMMENDED PROCEDURES ARE DEPENDENT ON CONSTRUCTION METHODS, STAGING, SITE CONDITIONS, WEATHER AND SCHEDULING. THE PROCEDURES SHOWN ON THE PLANS ARE NOT INTENDED TO BE EXCLUSIVE OF ALL THE PROTECTION REQUIRED IN AN AREA AT A PARTICULAR INSTANT. THEY SHOULD BE USED AS A GUIDELINE ONLY.

THE CONTRACTOR IS RESPONSIBLE FOR CONTROL OF SEDIMENT TRANSPORT WITHIN PROJECT LIMITS DURING CONSTRUCTION. IF AN INSTALLED EROSION CONTROL SYSTEM DOES NOT ADEQUATELY CONTAIN SEDIMENT ON SITE, THEN THE EROSION CONTROL MEASURES MUST BE FIELD ADJUSTED BY THE CONTRACTOR AS NECESSARY FOR EXPECTED STORM EVENTS TO ENSURE THAT SEDIMENT LADEN WATER DOES NOT LEAVE THE SITE.

TEMPORARY EROSION CONTROL METHODS MUST REMAIN AND BE MAINTAINED UNTIL PERMANENT EROSION CONTROL METHODS ARE IN PLACE AND OPERATIONAL. THESE METHODS SHALL BE REMOVED ONCE SOIL STABILIZATION HAS BEEN ACHIEVED.

ADDITIONAL INTERIM MEASURES WILL INCLUDE, AT A MINIMUM, INSTALLATION OF SILT FENCES.

PERMANENT EROSION CONTROL - VEGETATION ON ALL EMBANKMENTS AND DISTURBED AREAS SHOULD BE REESTABLISHED AS SOON AS CONSTRUCTION IS COMPLETED.

THE EROSION CONTROL MEASURES SHALL BE INSPECTED DAILY BY CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.

ALL EXISTING AND NEWLY CONSTRUCTED STORM INLETS AND DRAINS SHALL BE PROTECTED UNTIL PAVEMENT SURFACES ARE COMPLETED AND/OR VEGETATION IS REESTABLISHED.

THE CONTRACTOR IS SOLELY RESPONSIBLE FOR PROTECTION OF ALL ADJACENT PROPERTIES AND DOWNSTREAM FACILITIES FROM EROSION AND SILTATION DURING THE COURSE OF THE WORK. ANY DAMAGE RESULTING FROM SUCH EROSION AND SILTATION SHALL BE CORRECTED AT THE SOLE EXPENSE OF THE CONTRACTOR.

STRAW IS TO BE PLACED ON ALL DISTURBED AREAS IMMEDIATELY AFTER COMPLETION OF EACH SECTION OF CONSTRUCTION ACTIVITY, UNTIL THE TEMPORARY EROSION CONTROL SEEDING HAS BEEN ESTABLISHED.

## PARKING SUMMARY

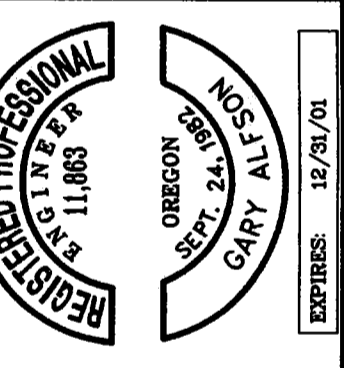
	EXISTING	PROPOSED	REQUIRED
AUTO	377	432	632*
BKE SHORT	40	0	0
BKE LONG	0	122	122

\* WITH 10% REDUCTION

RECEIVED  
MAY 17 2001  
City of Beaverton  
Engineering Department  
2001-0008

COVER SHEET  
B.H.S. CAFETERIA ADDITION  
BEAVERTON, OREGON

Harper Houf  
Righellis, Inc.  
ENGINEERS + PLANNERS  
5900 SW Macadam Avenue, Suite 580, Portland, OR 97201  
TEL: 503.221.1131 www.hhrvill.com FAX: 503.221.1171

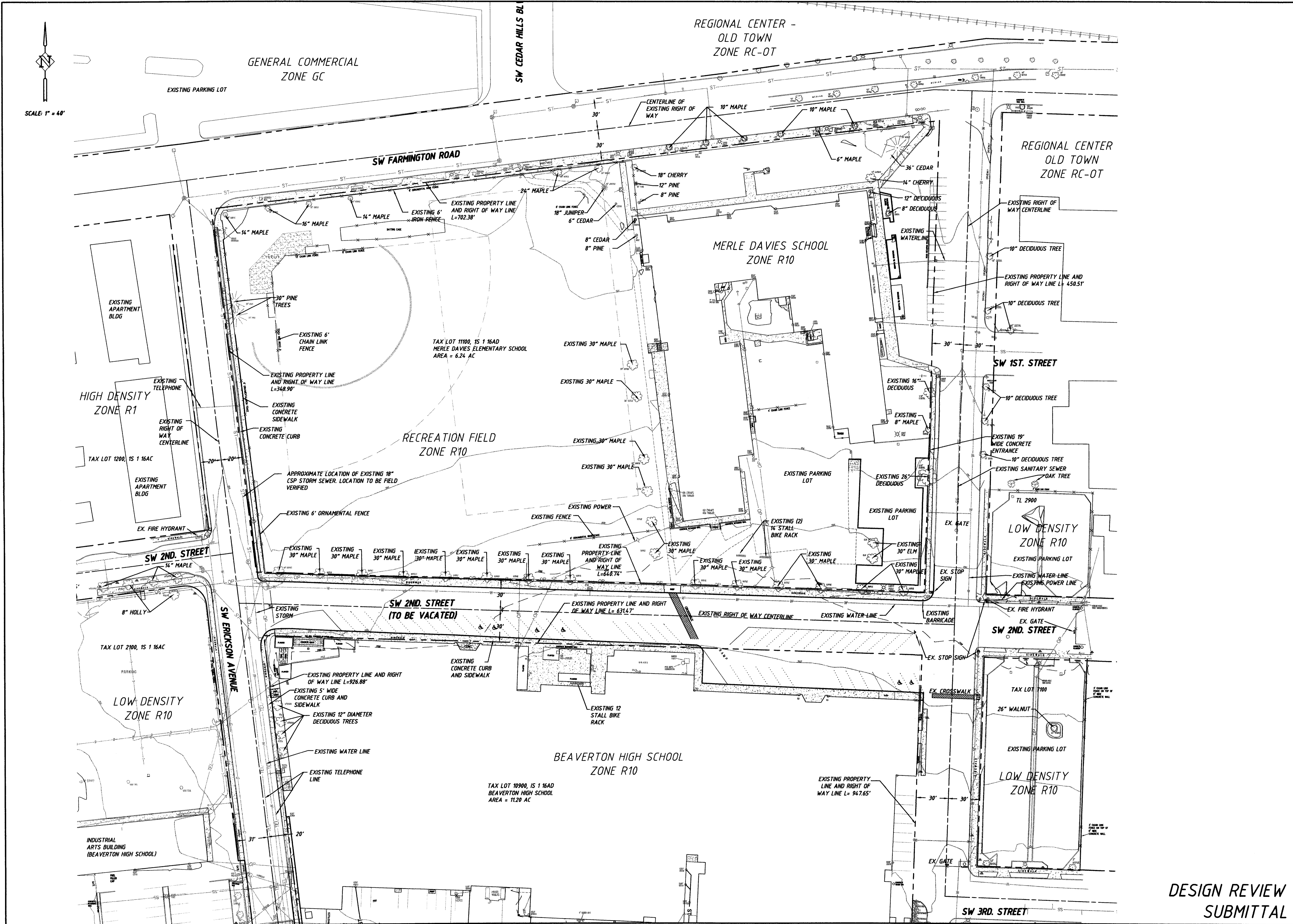


DESIGNED: GAA	DRAWN: ANG	CHECKED: GAA	DATE: 03-04-02
R E V I S I O N S			
SHEET NO.	1 of 17		JOB NO. 85D-23

DESIGN REVIEW SUBMITTAL

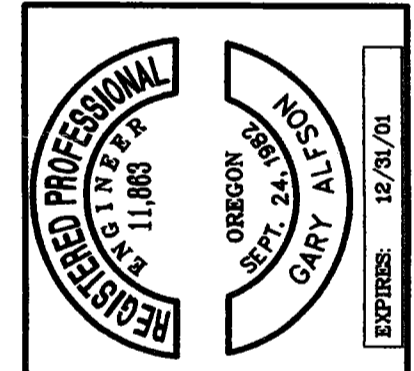


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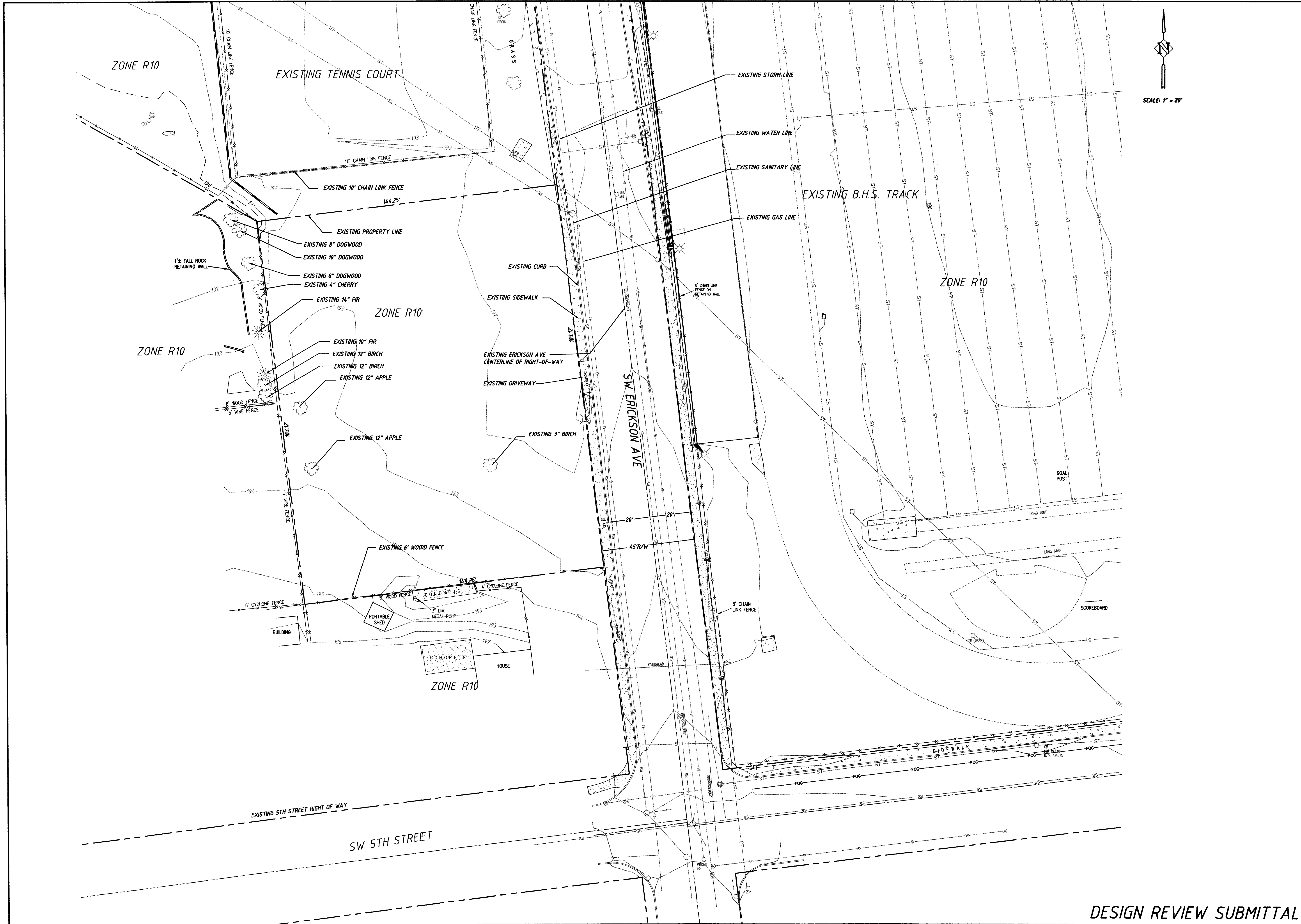
EXISTING CONDITIONS PLAN  
**B.H.S. CAFETERIA ADDITION**  
BEAVERTON, OREGON

**Harper Houff**  
**Righellis, Inc.**  
ENGINEERS - PLANNERS  
5200 SW MACADAM AVENUE, SUITE 380, PORTLAND, OR 97201  
TEL: 503.221.1131 www.hhrinc.com FAX: 503.221.1171



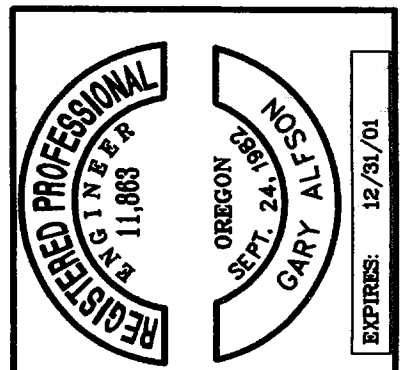
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DRAWN: AMG	
CHECKED: GAA	
R E V I S I O N S	
DATE	DESCRIPTION
SHEET NO.	2 of 17
JOB NO.	850-23

DESIGN REVIEW  
SUBMITTAL



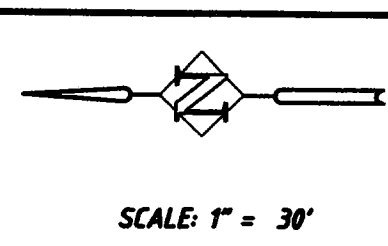
EXISTING CONDITIONS PLAN  
**B.H.S. FUTURE PARKING ADDITION**  
 BEAVERTON, OREGON

**Harper Houff**  
**Righellis, Inc.**  
 ENGINEERS & PLANNERS  
 5200 SW MACADAM AVENUE, SUITE 580, PORTLAND, OR 97201  
 TEL. 503.221.1131 www.harhoff.com FAX 503.221.1171

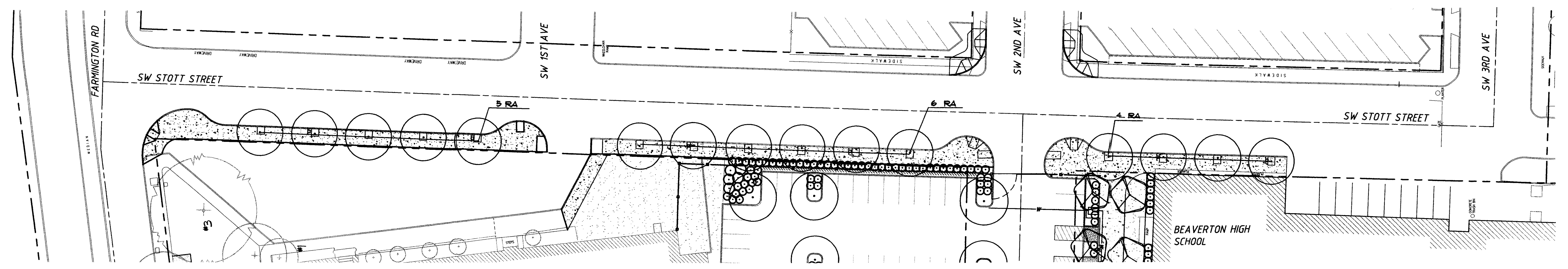


DESIGNED:	GAA	DRAWN:	AMG	CHECKED:	GAA	DATE:	03-05-02
R E V I S I O N S							
SHEET NO.	3	OF	17	DATE	NO.	DESCRIPTION	JOB NO.
							BSP-23

**DESIGN REVIEW SUBMITTAL**

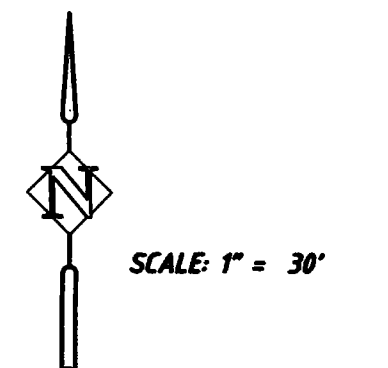


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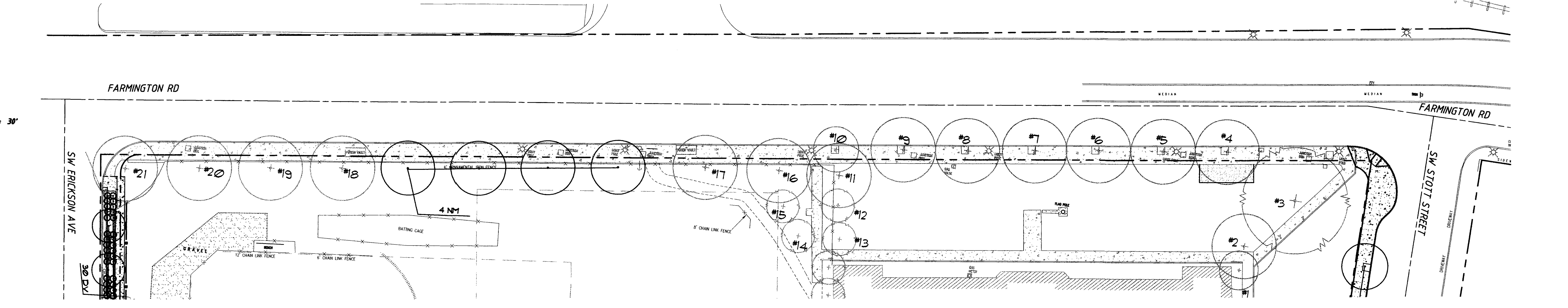


NOTE: SEE TREE TABLE SHEET 12

**SW STOTT STREET**

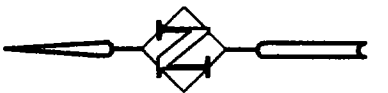


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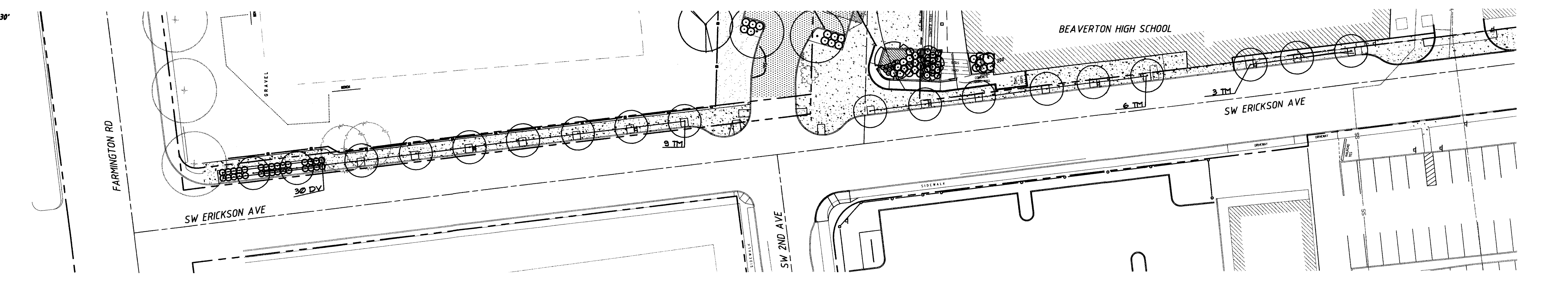


NOTE: SEE TREE TABLE SHEET 12

**SW FARMINGTON ROAD**



SCALE: 1" = 30'

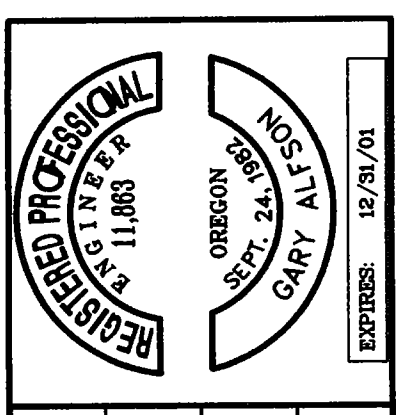


NOTE: SEE TREE TABLE SHEET 12

**SW ERICKSON AVE**

PUBLIC LANDSCAPING PLAN  
**B.H.S. CAFETERIA ADDITION**  
 BEAVERTON, OREGON

**Harper Houf**  
**Righellis, Inc.**  
 ENGINEERS & PLANNERS  
 5200 SW MACADAM AVENUE, SUITE 800, PORTLAND, OR 97201  
 TEL. 503.221.1131 WWW.HRCHILL.COM FAX 503.221.1171



DESIGNED: GAA	DATE: 02-05-02
DRAWN: AMG	
CHECKED: GAA	
R E V I S I O N S	
NO.	DESCRIPTION
SHEET NO. 11 OF 17	JOB NO. 850-23

DESIGN REVIEW SUBMITTAL

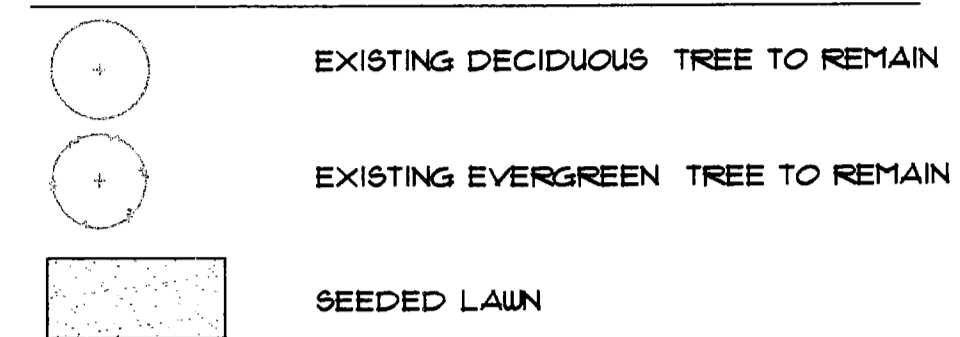
**LANDSCAPE NOTES**

- STREET TREES WITH UTILITY LINES ABOVE ARE SELECTED FOR APPROPRIATE HEIGHT.
- PROTECTION OF EXISTING TREES  
STAKE AND TAPE AREA 5' OUTSIDE OF DIRMLINE OF EACH TREE. ALLOW NO COMPACTION OF SOIL WITHIN PROTECTED ZONE. PROTECT FROM HEAVY EQUIPMENT.
- SEE CIVIL PLAN SHEET FOR BIO-SWALE PLANTING PLAN.

**IRRIGATION NOTES**

ALL LANDSCAPE AREAS WILL BE FULLY IRRIGATED BY AN AUTOMATED SYSTEM TO BE INTEGRATED WITH EXISTING IRRIGATION.

**LEGEND**



**PLANT SCHEDULE**

SYM	BOTANICAL NAME	COMMON NAME	QTY	SIZE	SPACING
TR	TREES				
VM	ACER CIRCINNATUM	VINE MAPLE	8	6"-1"	
RM	ACER PALMATUM 'ATROPURPUREUM'	RED JAPANESE MAPLE	3	2" CAL.	30' O.C.
NM	ACER FLATNOBES	NORWAY MAPLE	4	2" CAL.	30' O.C.
TA	ACER TATARICUM	TATARIC MAPLE	11	2" CAL.	30' O.C.
FR	FRAXINUS OXYCARPA 'RAYWOOD'	'RAYWOOD' ASH	30	2"	
AP	FRAXINUS AMERICANA 'JUNGINGER'	'AUTUMN PURPLE' ASH	6	2"	
CS	HALEBIA CAROLINA	CAROLINA SILVERBELLS	12	2" CAL.	
OU	XYDENDRON ARBOREUM	SOURWOOD	5	2"	
RC	SORBUS AMERICANA 'DUARFOROUN'	'RED CASCADE' MT. ASH	11	2" CAL.	
WO	QUERCUS ALBA	WHITE OAK	1	2" CAL.	
SH	SHRUBS				
CB	ARBITUS UNEDO 'COMPACTA'	'COMPACT STRAWBERRY TREE'	3	3 GAL.	
CA	AZALEA 'CORAL BELLS'	'CORAL BELLS' AZALEA	21	3 GAL.	
RG	BERBERIS THUNDERBERRY 'ROSY GLOW'	'ROSY GLOW' BARBERY	19	3 GAL.	
UG	BUXUS MICROPHYLLA 'WINTER GEM'	'WINTER GEM' BOXWOOD	22	5 GAL.	
ID	CORNUS SERICEA 'ISANTI'	'ISANTI DOGWOOD'	11	3 GAL.	
CO	CORNUS SERICEA 'KELSEY'	'KELSEY DOGWOOD'	61	3 GAL.	
BO	ILEX CORN 'BURKORD COMPACTA'	'BURKORD COMPACTA' HOLLY	69	5 GAL.	
CO	MAHONIA AQUAFOLIA 'COMPACTA'	'COMPACT OREGON GRAPE'	55	3 GAL.	
MY	MYRICA CALIFORNICA	PACIFIC WAXMYRTLE	5	5 GAL.	
ND	NANDINA DORI 'MOON BAY'	'MOON BAY' H. BAMBOO	12	3 GAL.	
FP	PIERIS MT. FIERI	'MT. FIERI' PIERIS	3	3 GAL.	
OL	FRUNUS LAURO 'OTTO LUYKEN'	'OTTO LUYKEN' LAUREL	41	3 GAL.	4' O.C.
AK	RHOODENDRON 'ANNA KRUSCHKE'	'ANNA KRUSCHKE' RHOD.	1	18"-21"	
GR	RHOD. CAT. GRANDIFOLIUM	CAT. GRANDIFOLIUM RHOD.	4	18"-21"	
SK	SKIMMIA JAPONICA	JAPANESE SKIMMIA	59	3 GAL.	4' O.C.
EH	VACCINIUM OVATUM	EVERGREEN HUCKLEBERRY	11	3 GAL.	
SB	VIBURNUM TINUS 'SPRING BOUQUET'	'SPRING BOUQUET' TINUS	1	3 GAL.	4' O.C.
MY	VIBURNUM FT. 'MARIESSI'	'MARIESSI' VIBURNUM	1	3 GAL.	
DV	VIBURNUM DAVIDII	DAVID VIBURNUM	32	3 GAL.	
GC	GROUND COVERS				
AW	AUGA REPENS 'ATROPURPUREA'	BRONZE BUGLEWEED	50	4" POT	18" O.C.
KK	ARCHTOSTAPHYLOS UVA-URSI	KINKINNICK	20	4" POT	18" O.C.
BP	VINCA MINOR 'BOULES'	'BOULES' FERWINKLE	275	4" POT	18" O.C.
SAL	GAULTHERIA SHALLON	SALAL	62	1 GAL.	24" O.C.
CM	MAHONIA REPENS	CREEPING MAHONIA	75	1 GAL.	24" O.C.
SL	SEEDED LAWN	70/2000 SF			

**BIOSWALE PLANT SCHEDULE**

SYMBOL	BOTANICAL NAME	COMMON NAME	MIXTURE # BY FL. S.	QUANTITY
RD	CORNUS SERICEA	RED-OSIER DOGWOOD	20	2 GAL.
DS	SPIREA DOUGLASSII	DOUGLAS SPIREA	40	7 GAL.
WM	WET/MOIST SEED MIX			850 SF.
	CAREX DEWEYANA	DEWEY SEDGE	20	
	CAREX OENIPTA	SLOUGH SEDGE	35	
	ELEOCHARIS PALUSTRIS	CREEPING SPRIKERUSH	10	
	ANGUS TENIS	SLENDER RUSH	10	
	SCRIPUS MICROCARPUS	SMALL-FRUITED BULRUSH	25	
DA	DRY AREA SEED MIX			500 SF.
	CALLIFORNIA BRODIE	CALLIFORNIA BRODIE	20	
	ELYNUS GLAUCUS	BLUE WILDRYE	20	
	PESTUCA RUBRA VAR. RUBRA	NATIVE RED FESCUE	50	
	LUPINUS POLYPHYLLUS	LARGE-LEAFED LUPINE	10	

**BIOSWALE PLANTING NOTES**

- WET/MOIST SEED MIX HYDROSEED APPLICATION RATE SHALL BE 45 LBS./ACRE
- DRY AREA SEED MIX HYDROSEED APPLICATION RATE SHALL BE 25 LBS./ACRE
- MULCH SEEDED AREAS WITH TWO INCHES OF APPROVED MULCH.

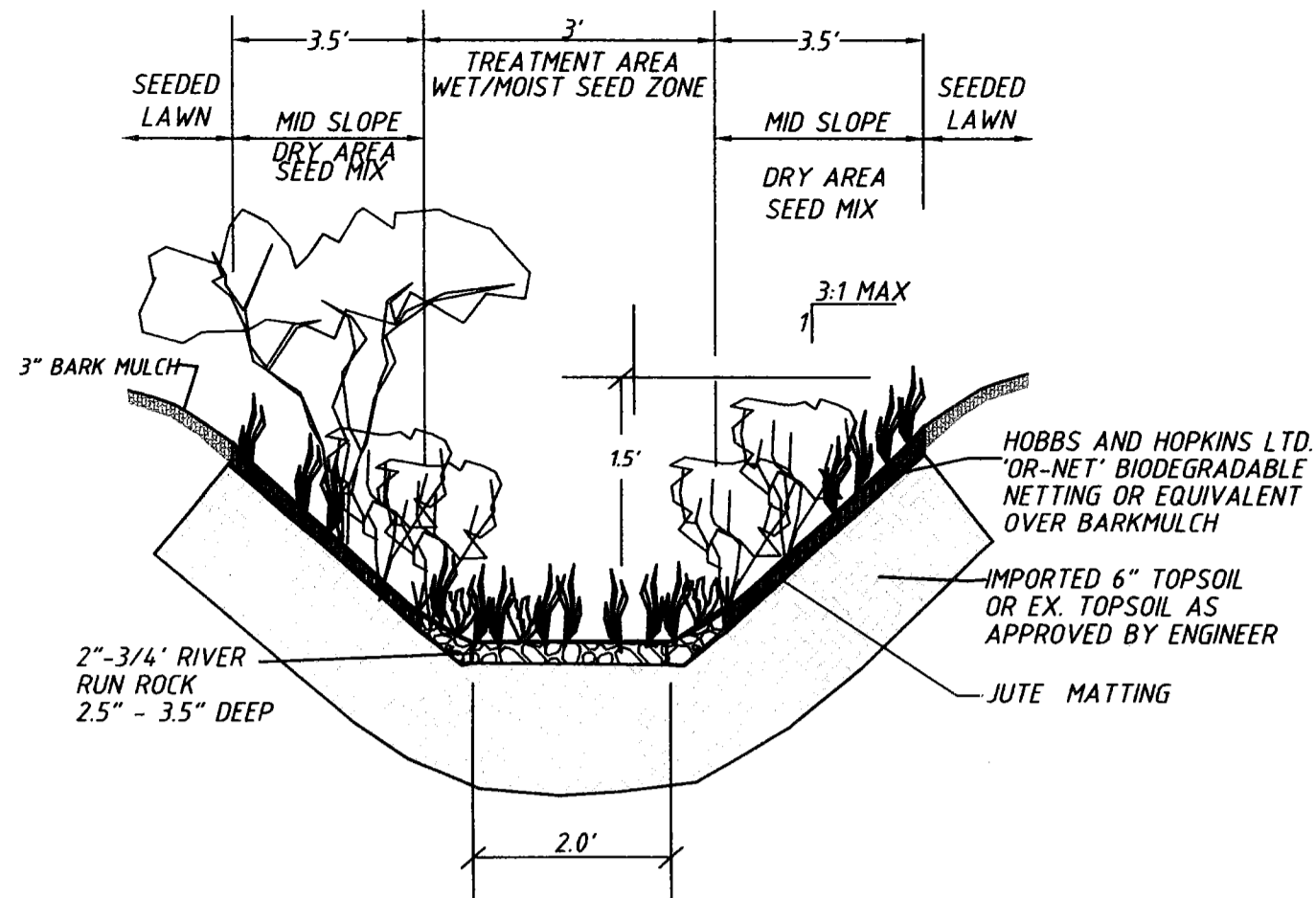
**EXISTING TREE INFORMATION**

**TREES TO BE PRESERVED**

NUMBER	SPECIES	SIZE (DBH)
1	SUGAR MAPLE (ACER SACCARINUM)	8.6"
2	NORWAY MAPLE	11.1"
3	BEECH	7.0"
4	CHERRY (PRUNUS AVIUM)	16.0"
5	DEODAR CEDAR (CEDRUS DEODAR)	4.0"
6	NORWAY MAPLE	10"
7	NORWAY MAPLE	8"
8	NORWAY MAPLE	10"
9	NORWAY MAPLE	10"
10	NORWAY MAPLE	10"
11	NORWAY MAPLE	10"
12	NORWAY MAPLE	10"
13	CHERRY (PRUNUS AVIUM)	18"
14	FINE (PINUS)	12"
15	FINE (PINUS)	8"
16	CEDAR	6"
17	CEDAR	6"
18	NORWAY MAPLE	24"
19	NORWAY MAPLE	24"
20	NORWAY MAPLE	14"
21	NORWAY MAPLE	16"
22	NORWAY MAPLE	16"
23	NORWAY MAPLE	14"
24	FINE (PINUS)	26"
25	FINE (PINUS)	26"
26	SYCAMORE	29"
27	SYCAMORE	29"
28	SYCAMORE	29"
29	SYCAMORE	29"

**LUMINAIRE SCHEDULE**

- SA1 EXTERIOR POLE TOP ACORN LUMINAIRE, ZTY. CAST ALUMINUM FITTER HOUSING WITH BOROSILICATE GLASS REFRACTOR. TYPE III DISTRIBUTION. 1-1/2" METAL HALIDE LAMP. ARCHITECTURAL LIGHTING ALN 418 SERIES.
- POLE: 8" ROUND POLE.
- SA2 SAME AS TYPE 'SA1' EXCEPT TYPE V DISTRIBUTION.
- SB1 EXTERIOR WALL MOUNTED SCORCE, ZTY. DIE CAST ALUMINUM HOUSING IN A DOME SHAPE. WIDE THRU DISTRIBUTION. 1-1/2" METAL HALIDE LAMP. GARCOO 106 SERIES.
- SB2 SAME AS TYPE 'SB1' EXCEPT MEDIUM THRU DISTRIBUTION.
- SC EXTERIOR RECESSED SQUARE LENSED DOWN LIGHT, ZTY. DIE CAST ALUMINUM DOOR FRAME WITH FLAT FRESNEL LENS. 1-1/2" METAL HALIDE LAMP. UL LISTED IET. LITHONIA LAH SERIES.
- SD EXTERIOR WALL MOUNTED GOOSENECK LUMINAIRE, ZTY. 1-1/2" METAL HALIDE LAMP. UL LISTED IET. ABOLITE BD SERIES.
- SE EXTERIOR IN-GRADE WELL LIGHT, ZTY. 1-1/2" METAL HALIDE LAMP. HYDREL 9300 SERIES.
- SG1 EXTERIOR POLE TOP METAL HALIDE LUMINAIRE. 22" DIAMETER BY 14" HIGH ROUND ALUMINUM HOUSING WITH FLAT TOP. DIE CAST ALUMINUM DOOR FRAME WITH IMPACT RESISTANT GLASS LENS. SEGMENTED REFLECTOR FORWARD THRU DISTRIBUTION. 1-1/2" METAL HALIDE LAMP. POLE: 2" TAPERED STEEL. GARCOO CP SERIES.
- SG2 SAME AS TYPE 'SG1' EXCEPT TYPE B DISTRIBUTION.

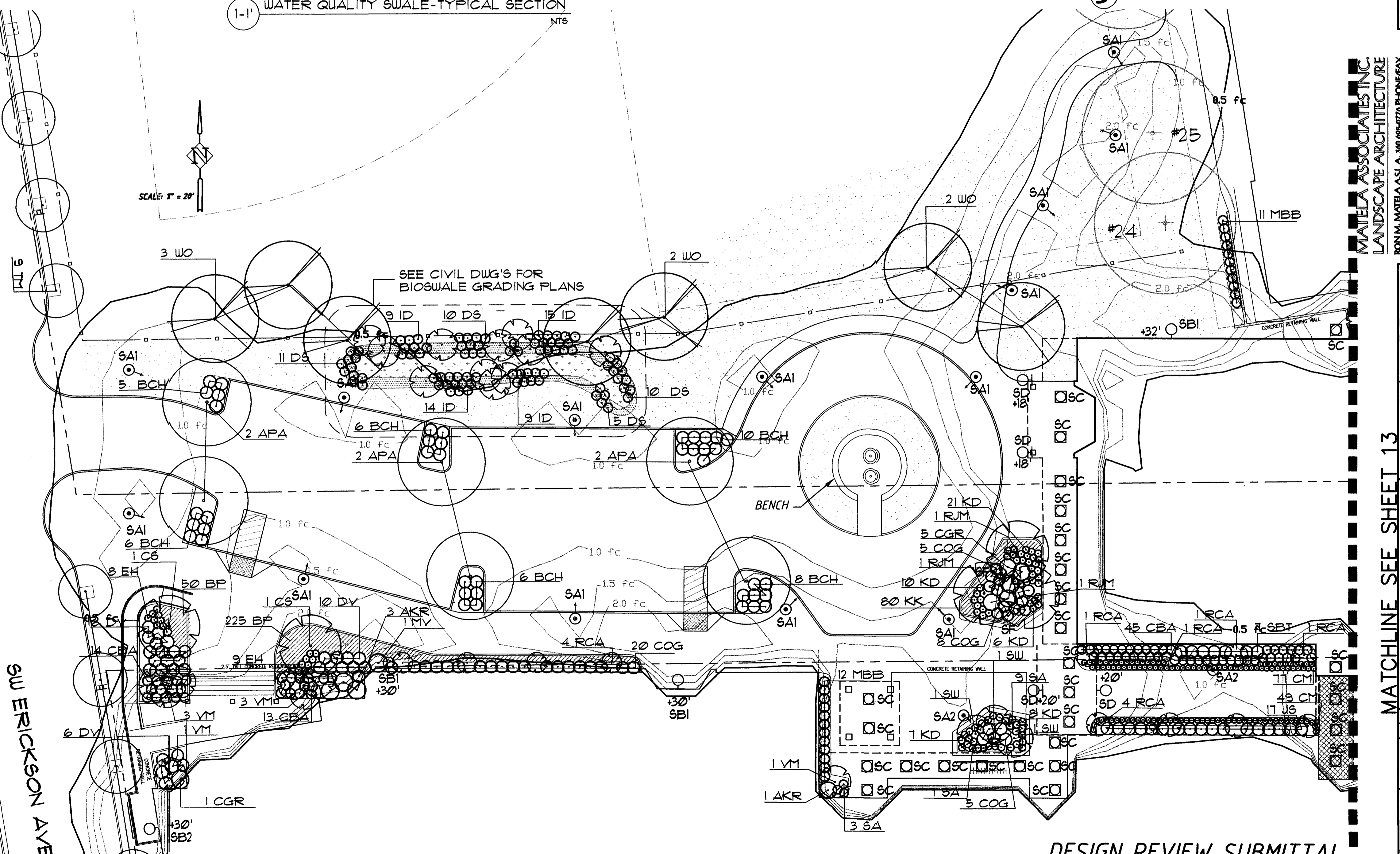
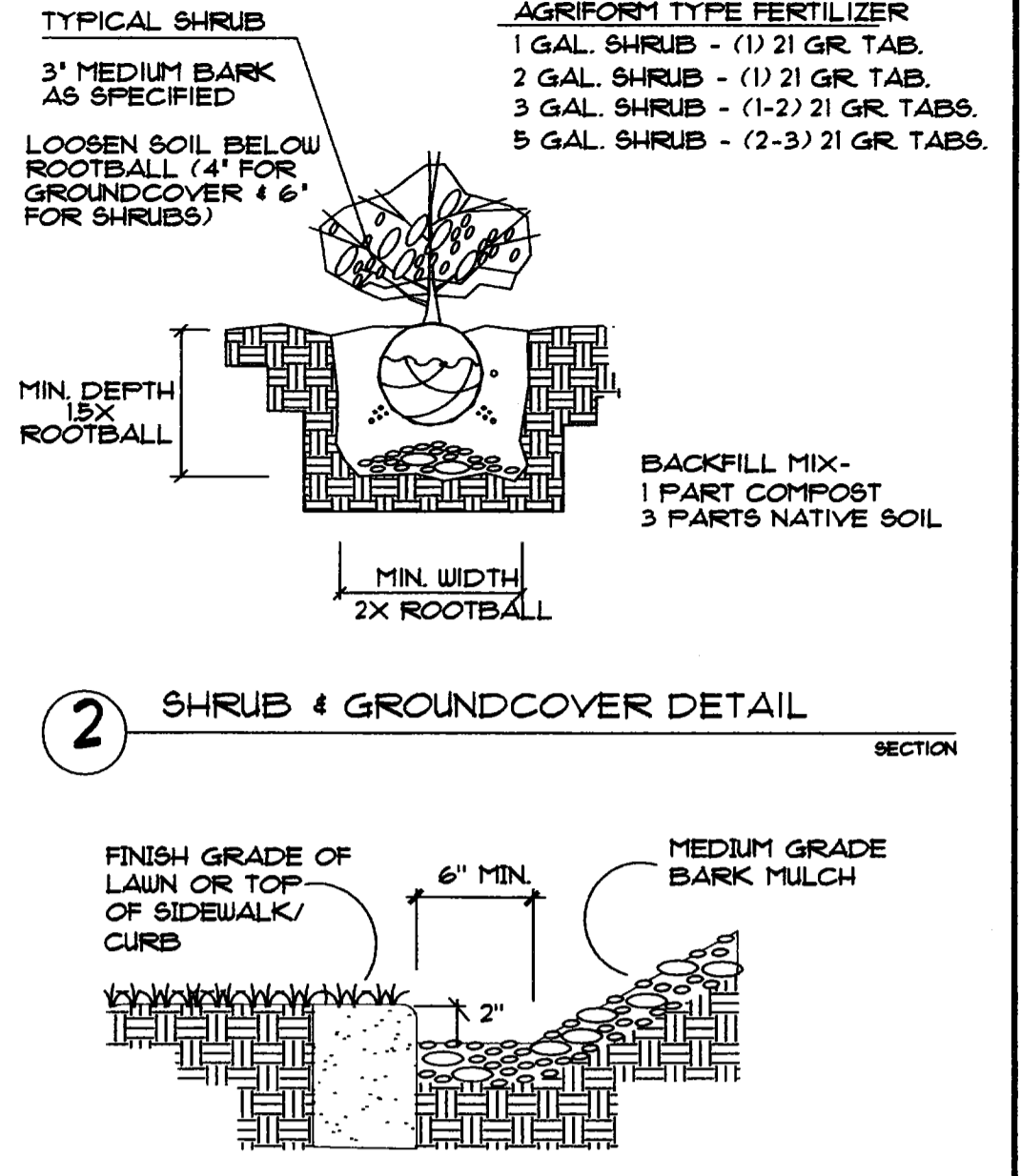
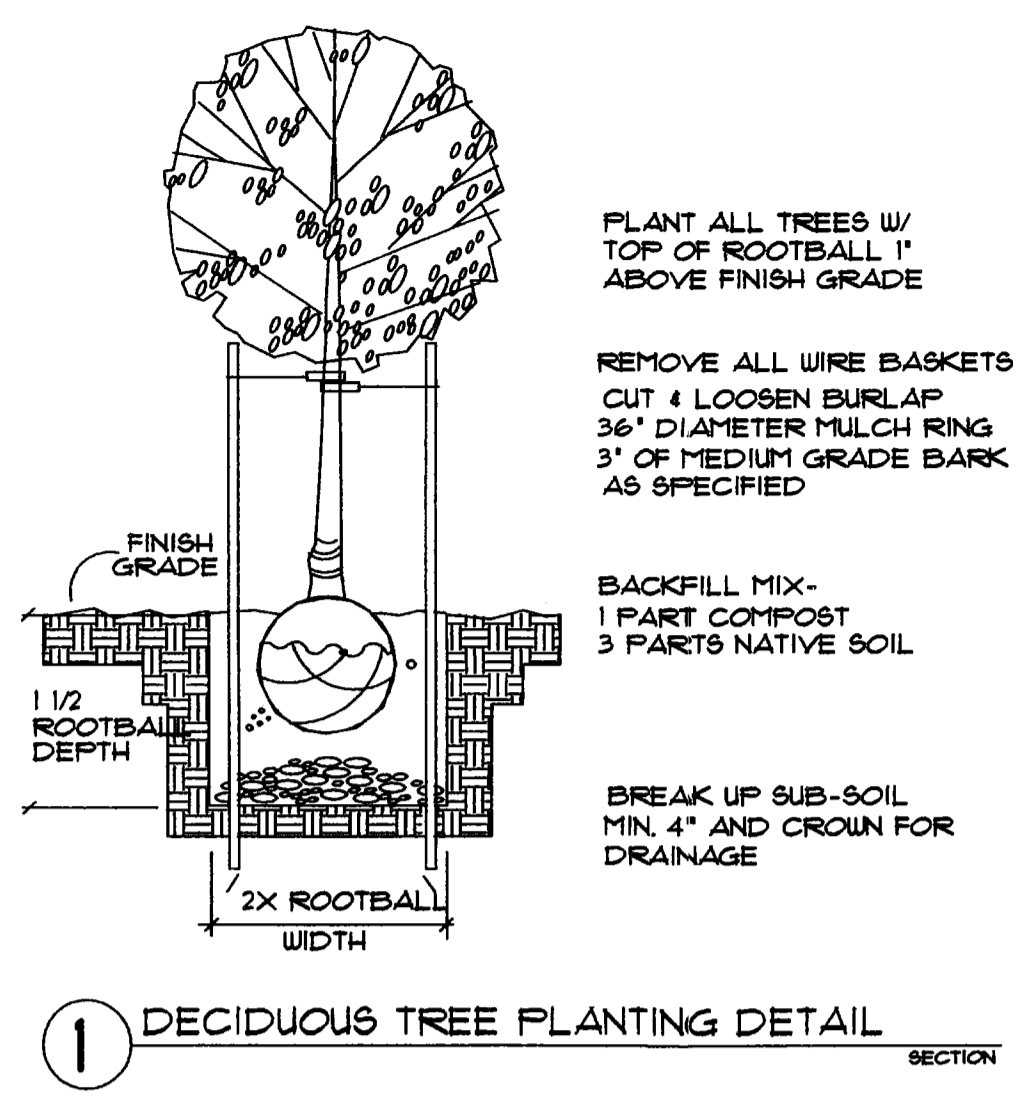
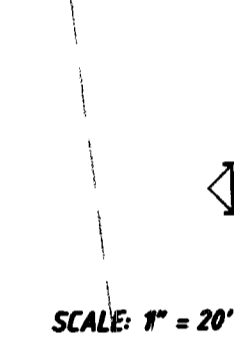


SLOPES OF WATER QUALITY BIO-SWALE SHALL BE SEEDDED WITH WETLAND SEED MIXTURE (PRO-TIME NO. 504 OR APPROVED EQUAL).

ALL EROSION CONTROL DEVICES SHALL BE IN PLACE PRIOR TO CONSTRUCTION TO PREVENT TRANSPORT OF SEDIMENT TO THE BIO-SWALE.

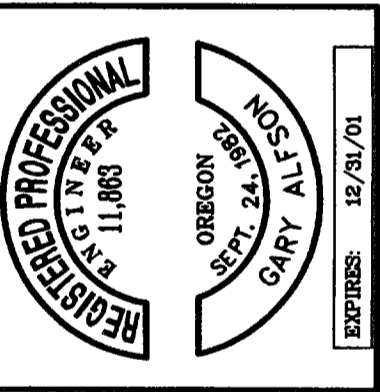
THE WATER QUALITY BIO-SWALE SHALL BE KEPT CLEAR OF DEBRIS AND SEDIMENT UNTIL THE DENSE GRASS COVER OF THE SLOPES AND BASIN FLOOR HAVE BEEN ESTABLISHED.

**1-1' WATER QUALITY SWALE-TYPICAL SECTION**



PRIVATE LANDSCAPING AND LIGHTING PLAN (WEST)  
**B.H.S. CAFETERIA ADDITION**  
BEAVERTON, OREGON

**MATELA ASSOCIATES INC.**  
LANDSCAPE ARCHITECTURE  
RON M. MATELA A.S.L.A. 340 995-9776 PHONE/FAX  
401 MAIN STREET #601 VANCOUVER, WA 98640  
**Harper Houf Righellis, Inc.**  
ENGINEERS & PLANNERS  
5200 SW MACADAM AVENUE, SUITE 560, PORTLAND, OR 97201  
TEL: 503.221.1131 www.hrrh.com FAX: 503.221.1171



**MATCHLINE SEE SHEET 13**

DESIGNED: GAA	DRAWN: ANG	CHECKED: GAA	DATE: 03-05-02
REVISIONS PER CITY COMMENTS	DATE	NO.	DESCRIPTION
7-26-01			

SHEET NO. **12** OF **17**  
JOB NO. B50-23

DESIGN REVIEW SUBMITTAL

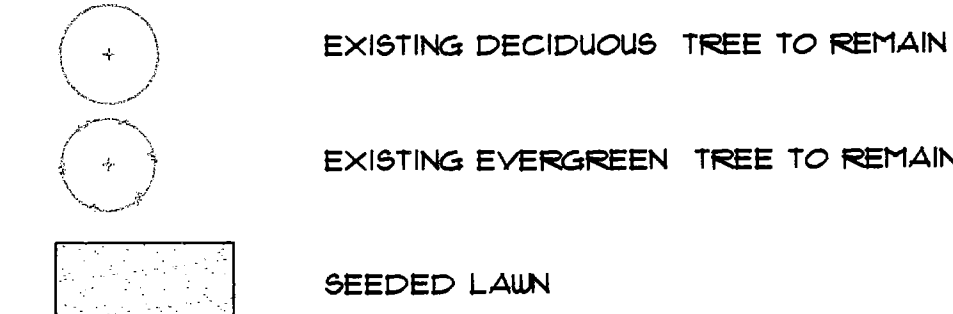
**LANDSCAPE NOTES**

- STREET TREES WITH UTILITY LINES ABOVE ARE SELECTED FOR APPROPRIATE HEIGHT.
- PROTECTION OF EXISTING TREES
- STAKE AND TAPE AREA 5' OUTSIDE OF DRIPLINE OF EACH TREE. ALLOW NO COMPACTION OF SOIL WITHIN PROTECTED ZONE. PROTECT FROM HEAVY EQUIPMENT.
- SEE CIVIL PLAN SHEET FOR BIO-SWALE PLANTING PLAN.

**IRRIGATION NOTES**

ALL LANDSCAPE AREAS WILL BE FULLY IRRIGATED BY AN AUTOMATED SYSTEM TO BE INTEGRATED WITH EXISTING IRRIGATION.

**LEGEND**



**PLANT SCHEDULE**

SYM	BOTANICAL NAME	COMMON NAME	QTY.	SIZE	SPACING
<b>TREES</b>					
VM	ACER CIRCINATUM	VINE MAPLE	8	6'-1"	
RJM	ACER PALMATUM 'ATROPURPUREUM'	RED JAPANESE MAPLE	3	2' CAL.	
NM	ACER PLATINOIDES	NORWAY MAPLE	4	2' CAL.	30' O.C.
TM	ACER TATARICUM	TATARICAN MAPLE	11	2' CAL.	30' O.C.
RA	FRAXINUS OXYCARPA 'RAYWOOD'	'RAYWOOD' ASH	30	2"	
AFPA	FRAXINUS AMERICANA 'JUNGINGER'	'AUTUMN PURPLE' ASH	6	2"	
CS	HALESIA CAROLINA	CAROLINA SILVERBELLS	12	2' CAL.	
SW	OXYDENDRON ARBOREUM	SOURWOOD	5	2"	
RCA	SCORBUS AMERICANA 'DIARPCROWN'	'RED CASCADE' MT. ASH	11	2' CAL.	
UO	QUERCUS ALBA	WHITE OAK	7	2' CAL.	
<b>SHRUBS</b>					
CST	ARBUTUS UNEDO 'COMPACTA'	COMPACT STRAWBERRY TREE	3	3 GAL.	
CBA	AZALEA 'CORAL BELLS'	'CORAL BELLS' AZALEA	21	3 GAL.	
RGB	BERBERIS THUNBERGI 'ROBY GLOW'	'ROBY GLOW' BARBERRY	19	3 GAL.	
UGB	BUXUS MICROPHYLLA 'WINTER GEM'	'WINTER GEM' BOXWOOD	22	5 GAL.	
ID	CORNUS SERICEA 'ISANTI'	'ISANTI' DOGWOOD	11	3 GAL.	
KD	CORNUS SERICEA 'KELSEY'	'KELSEY' DOGWOOD	61	3 GAL.	
BCH	ILEX CORN 'BURFORD COMPACTA'	'BURFORD' COMP. CHIN. HOLLY	69	5 GAL.	
COG	MAHONIA AQUAFOLIA 'COMPACTA'	COMPACT OREGON GRAPE	55	3 GAL.	
FUM	HYDRICIA CALIFORNICA	PACIFIC WAXMYRTLE	5	5 GAL.	
MBB	NANDINA DOM. 'MOON BAY'	'MOON BAY' H. BAMBOO	2	3 GAL.	
MFP	PIERIS 'MT. FIRE'	'MT. FIRE' PIERIS	3	3 GAL.	
OLL	FRUNUS LAURO 'OTTO LUYKEN'	'OTTO LUYKEN' LAUREL	41	3 GAL.	4' O.C.
AKR	RHODODENDRON 'ANNA KRUSCHKE'	'ANNA KRUSCHKE' RHOD.	7	18"-21"	
CGR	RHOD. CAT. GRANDIFOLIUM	CAT. GRANDIFOLIUM RHOD.	4	18"-21"	
JS	SKIMMIA JAPONICA	JAPANESE SKIMMIA	59	3 GAL.	4' O.C.
EH	VACCINIUM OVATUM	EVERGREEN HUCKLEBERRY	11	3 GAL.	
SBT	VIBURNUM TINUS 'SPRING BOUQUET'	'SPRING BOUQUET' TINUS	1	3 GAL.	4' O.C.
MY	VIBURNUM FT. MARIESSE	'MARIESS' VIBURNUM	1	3 GAL.	
DV	VIBURNUM DAVIDII	DAVID VIBURNUM	32	3 GAL.	
<b>GROUND COVERS</b>					
BW	AJUGA REPENS 'ATROPURPUREA'	BRONZE BUGLEWEED	50	4" POT	18" O.C.
IK	ARCHTOSTAPHYLOS UVA-URSI	KINKINNICK	80	4" POT	18" O.C.
BFP	VINCA MINOR 'BOULES'	'BOULES' PERIWINKLE	215	4" POT	30" O.C.
SAL	GAILTHERIA SHALLOON	SAL. GAILTHERIA	62	1 GAL.	30" O.C.
CH	MAHONIA REPENS	CREEPIING MAHONIA	15	1 GAL.	24" O.C.
	SEEDED LAWN	20,000 SF			

**BIOSWALE PLANT SCHEDULE**

SYMBOL	BOTANICAL NAME	COMMON NAME	% MIXTURE # BY P.L.S.	QUANTITY
RD	SHRUBS			
	CORNUS SERICEA	RED-OSIER DOGWOOD	20	2 GAL.
Ds	SPHIREA DOUGLASHII	DOUGLAS SPHIREA	40	2 GAL.
WET/MOIST BEED MIX				850 SF.
	CAREX DEWEYANA	DEWEY SEDGE	20	
	CAREX OBNUPA	SLOUGH SEDGE	35	
	ELEOCHARIS PALUSTRIS	CREEPIING SPKERUSH	10	
	JUNCUS TENUI	SLENDER RUSH	10	
DRY AREA BEED MIX				500 SF.
	BROTIS CARNATUS	CALLIFORNIA BROOME	20	
	ELYNUS GLAUCUS	BLUE WILDRYE	20	
	FESTUCA RUBRA VAR. RUBRA	NATIVE RED FESCUE	50	
	LUPINUS POLYPHYLLUS	LARGE-LEAFED LUPINE	10	

\* PURE LIVE BEED

**BIOSWALE PLANTING NOTES**

- WET/MOIST BEED MIX HYDROSEED APPLICATION RATE SHALL BE 45 LBS./ ACRE
- DRY AREA BEED MIX HYDROSEED APPLICATION RATE SHALL BE 25 LBS./ ACRE
- MULCH SEEDED AREAS WITH TWO INCHES OF APPROVED MULCH.

**EXISTING TREE INFORMATION**

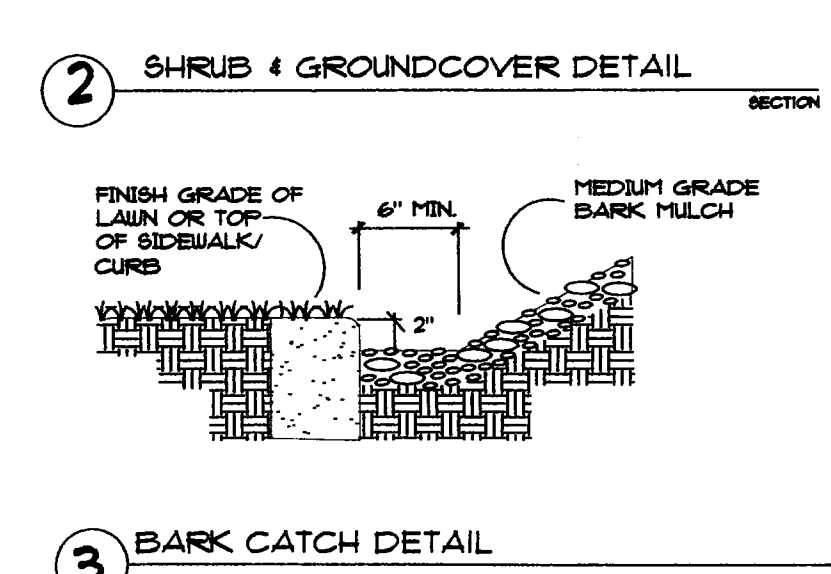
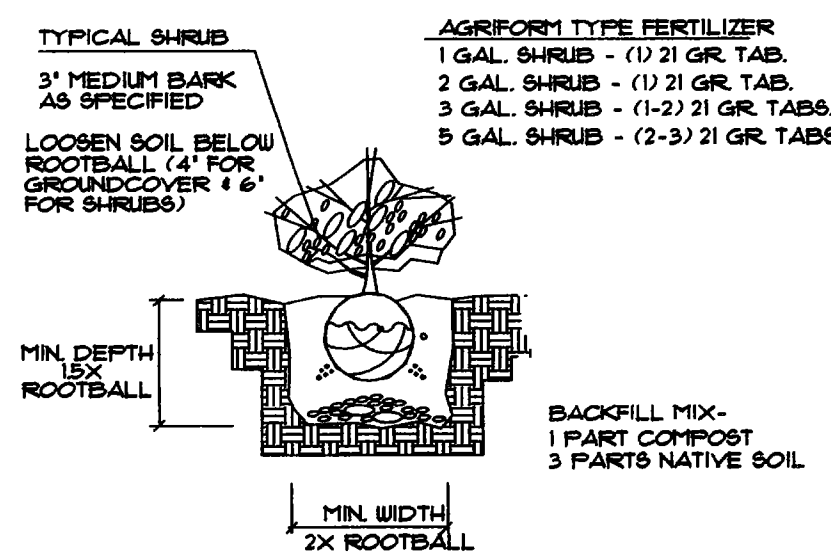
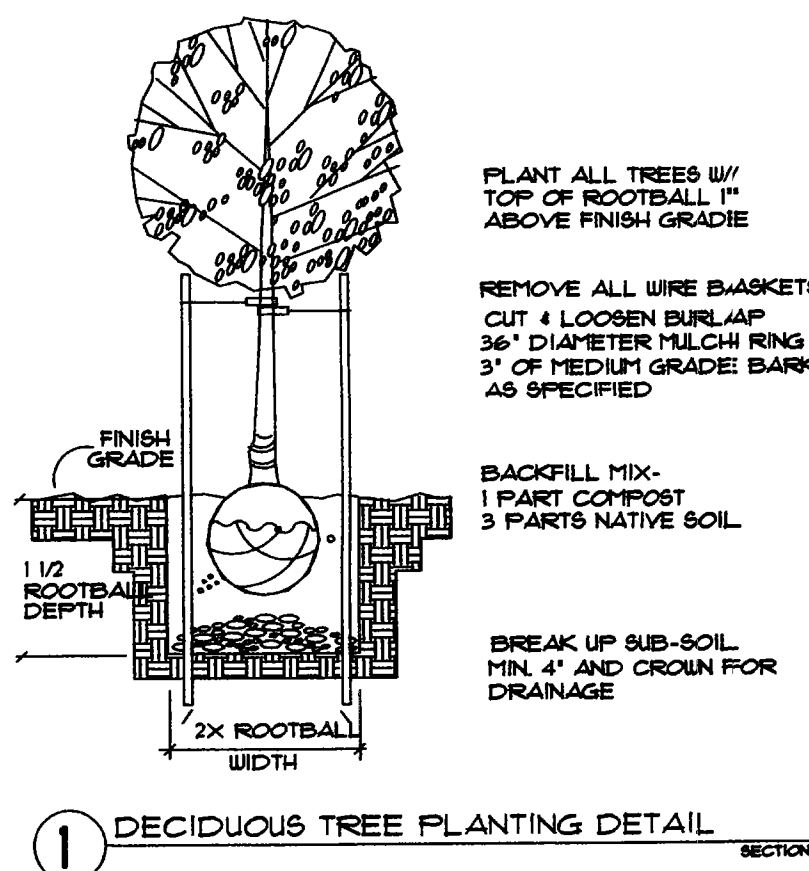
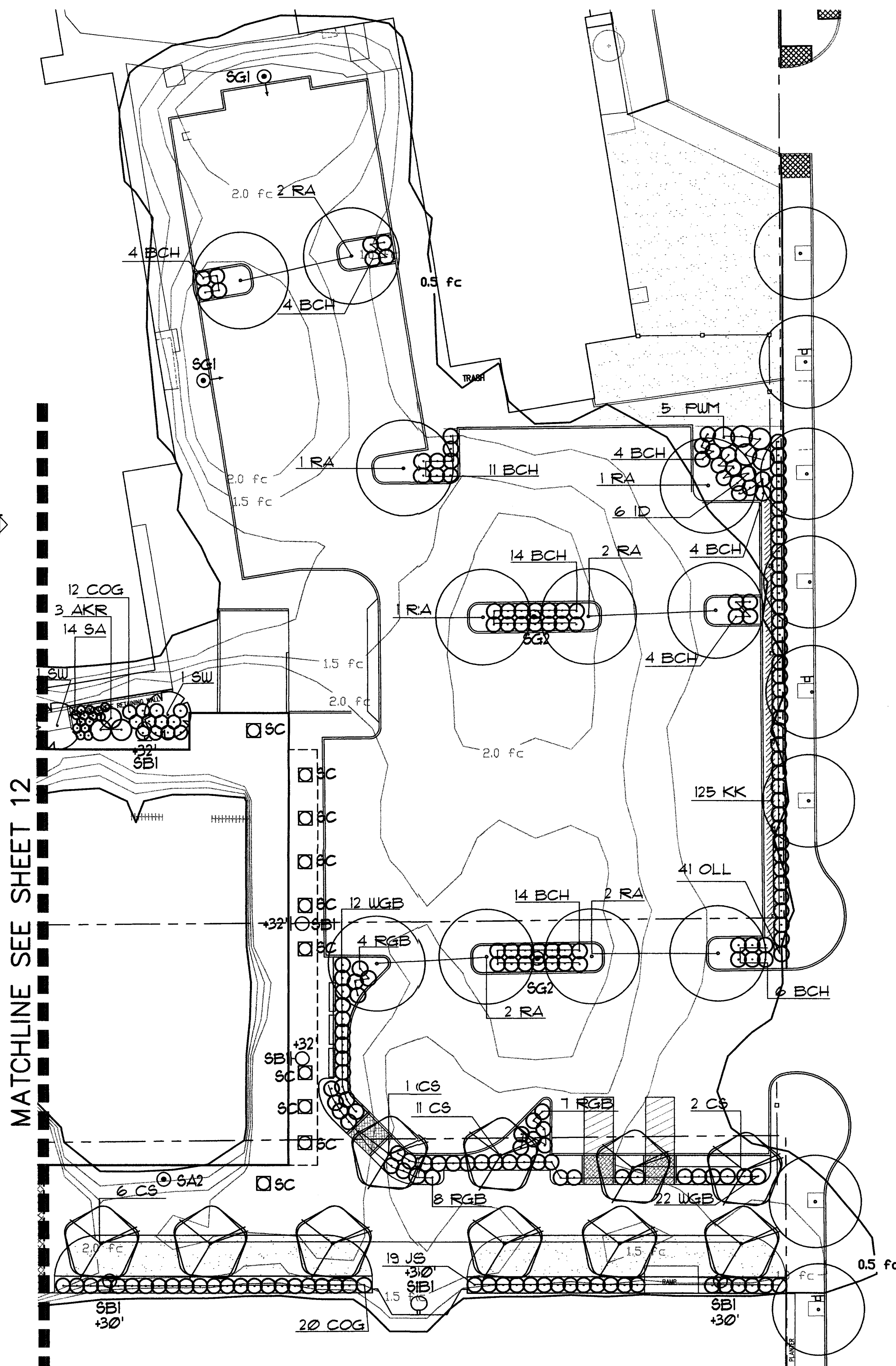
**TREES TO BE PRESERVED**

NUMBER KEY	SPECIES	SIZE (DBH)
1	SUGAR MAPLE (ACER SACCARINUM)	8.6'
2	NORWAY MAPLE	17.1'
3	BEECH	13.0'
4	CHERRY (FRUNUS AVIUM)	16.0'
5	DEODAR CEDAR (CEDRUS DEODAR)	4.0'
6	NORWAY MAPLE	10'
7	NORWAY MAPLE	10'
8	NORWAY MAPLE	10'
9	NORWAY MAPLE	10'
10	NORWAY MAPLE	10'
11	NORWAY MAPLE	10'
12	NORWAY MAPLE	10'
13	CHERRY (FRUNUS AVIUM)	18'
14	PINE (PINUS)	12'
15	PINE	8'
16	CEGAR	10'
17	CEGAR	10'
18	NORWAY MAPLE	24'
19	NORWAY MAPLE	24'
20	NORWAY MAPLE	14'
21	NORWAY MAPLE	16'
22	NORWAY MAPLE	16'
23	NORWAY MAPLE	14'
24	PINE (PINUS)	26'
25	PINE	26'
26	SYCAMORE	29'
27	SYCAMORE	29'
28	SYCAMORE	29'
29	SYCAMORE	29'

**LUMINAIRE SCHEDULE**

- SA1 EXTERIOR POLE TOP ACORN LUMINAIRE, 27V. CAST ALUMINUM FITTER HOUSING WITH BOROSILICATE GLASS REFRACTOR. TYPE III DISTRIBUTION. 1-TBU METAL HALIDE LAMP. ARCHITECTURAL LIGHTING ALN 418 SERIES.
- POLE: 6" ROUND POLE.
- SA2 SAME AS TYPE 'SA1' EXCEPT TYPE V DISTRIBUTION.
- SB1 EXTERIOR WALL MOUNTED SCONE, 27V. DIE CAST ALUMINUM HOUSING IN A DOME SHAPE. WIDE THROW DISTRIBUTION. 1-TBU METAL HALIDE LAMP. GARDCO 126 SERIES.
- SB2 SAME AS TYPE 'SB1' EXCEPT MEDIUM THROW DISTRIBUTION.
- SC EXTERIOR RECESSED SQUARE LENSED DOWN LIGHT, 27V. DIE CAST ALUMINUM DOOR FRAME WITH FLAT FRESNEL LENS. 1-12W METAL HALIDE LAMP. UL LISTED UET. LITHONIA LAH SERIES.
- SD EXTERIOR WALL MOUNTED GOOSENECK LUMINAIRE, 27V. 1-12W METAL HALIDE LAMP. UL LISTED UET. ABSOLUTE 80 SERIES.
- SE EXTERIOR IN-GRADE WELL LIGHT, 27V. 1-12W METAL HALIDE LAMP. HYDREL 9300 SERIES.
- SG1 EXTERIOR POLE TOP METAL HALIDE LUMINAIRE. 22" DIAMETER BY 14" HIGH ROUND ALUMINUM HOUSING WITH FLAT TOP. DIE CAST ALUMINUM DOOR FRAME WITH IMPACT RESISTANT GLASS LENS. SEGMENTED REFLECTOR. FORWARD THROW DISTRIBUTION. 1-400W METAL HALIDE LAMP. POLE: 2 1/2" TAPERED STEEL. GARDCO CP SERIES.
- SG2 SAME AS TYPE 'SG1' EXCEPT TYPE B DISTRIBUTION.

SCALE: 1" = 20'



DESIGN REVIEW SUBMITTAL

**MATELA ASSOCIATES INC.**  
LANDSCAPE ARCHITECTURE  
RON M. MATELA, A.S.L.A. 300 695-0776 PHONE/FAX  
601 MAIN STREET #201 VANCOUVER, WA 98660

**Harper Houf Righellis, Inc.**  
ENGINEERS • PLANNERS  
5200 SW Macadam Avenue, Suite 500, Portland, OR 97201  
TEL: 503.221.1131 www.hrhinc.com FAX: 503.221.1171

REVISIONS PER CITY COMMENTS

NO.	DATE	DESCRIPTION
7-26-01		

DESIGNED: GAA  
DRAWN: ANG  
CHECKED: GAA  
DATE: 03-05-02

SHEET NO. 13 of 17  
JOB NO. 650-23

PRIVATE LANDSCAPING AND LIGHTING PLAN (EAST)  
**B.H.S. CAFETERIA ADDITION**  
BEAVERTON, OREGON

**LANDSCAPE NOTES**

- 1 STREET TREES WITH UTILITY LINES ABOVE ARE SELECTED FOR APPROPRIATE HEIGHT.
- 2 ALL LANDSCAPE AREAS WILL BE FULLY IRRIGATED BY AN AUTOMATED SYSTEM TO BE INTEGRATED WITH EXISTING IRRIGATION.

**IRRIGATION NOTES**

- 1 ALL LANDSCAPE AREAS WILL BE FULLY IRRIGATED BY AN AUTOMATED SYSTEM TO BE INTEGRATED WITH EXISTING IRRIGATION.

**LEGEND**

- EXISTING TREE TO BE REMOVED
- SEEDED LAWN
- STREET LAMP

**PLANT SCHEDULE**

SYM	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
RA	TREES FRAXINUS OXYCARPA 'RAYWOOD'	'RAYWOOD' ASH	12	2'
TM	ACER TATARICUM	TATARICAN MAPLE	6	2' GAL.
<b>SHRUBS</b>				
ME	EUONYMUS KIALTSCHOVICA	MANHATTAN EUONYMUS	82	3 GAL.
BCH	ILEX CORN. 'BURFORD COMPACTA'	'BURFORD COMPACTA' HOLLY	68	3 GAL.
OLL	FRUNUS LAURO. 'OTTO LUYKEN'	'OTTO LUYKEN' LAUREL	35	3 GAL.
SBT	VIBURNUM TINUS 'SPRING BOUQUET'	'SPRING BOUQUET' TINUS	31	3 GAL.
<b>GROUND COVERS</b>				
BW	AJUGA REPENS 'ATROPURPUREA'	BRONZE BUGLEWEED	250	4' POT
KK	ARCHTOSTAPHYLOS UVA-URSI	KINKINICK	225	4' POT
	SEEDED LAWN	5,920 SF		

**EXISTING TREE DATA**

**TREES TO BE REMOVED**

NUMBER KEY	SPECIES	SIZE (DBH)
1	APPLE	12"
2	APPLE	12"

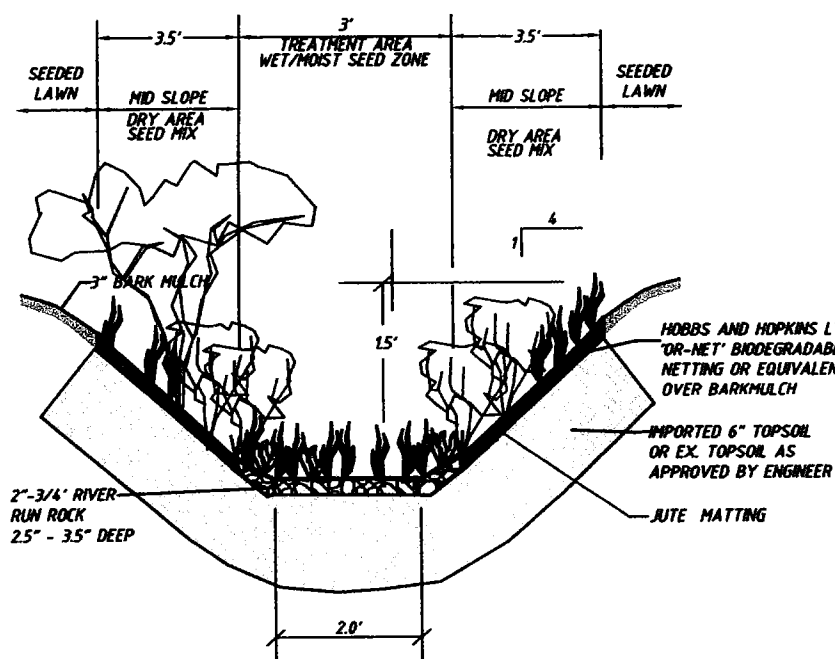
**BIOSWALE PLANT SCHEDULE**

SYMBOL	BOTANICAL NAME	COMMON NAME	% MIXTURE # EST. PLS.	QUANTITY
VM	TREES ACER CIRCINNATUM	VINE MAPLE	5	6'-8'
RD	SHRUBS CORNUS SERICEA	RED-OSIER DOGWOOD	35	2 GAL.
	SPHREA DOUGLASHI	DOUGLAS SPHREA	34	2 GAL.
WET/MOIST SEED MIX	CAREX DEBRYANA	DEWEY SEDGE	20	625 SF.
	CAREX CORNUPIA	SLOUGH SEDGE	35	
	ELEOCHARIS PALUSTRIS	CREEPING SPKERUSH	10	
	JUNCUS TENNIS	SLENDER RUSH	10	
	SCRIPUS MICROCARPUS	SMALL-FRUITED BULRUSH	25	
	DRY AREA SEED MIX	ERCTHUS CARNATUS	CALLIFORNIA BROOME	
ELYMUS GLAUCUS		BLUE WILDRYE	20	
PESTUCA RUBRA VAR. RUBRA		NATIVE RED FESCUE	50	
LUPINUS POLYPHYLLUS		LARGE-LEAFED LUPINE	10	

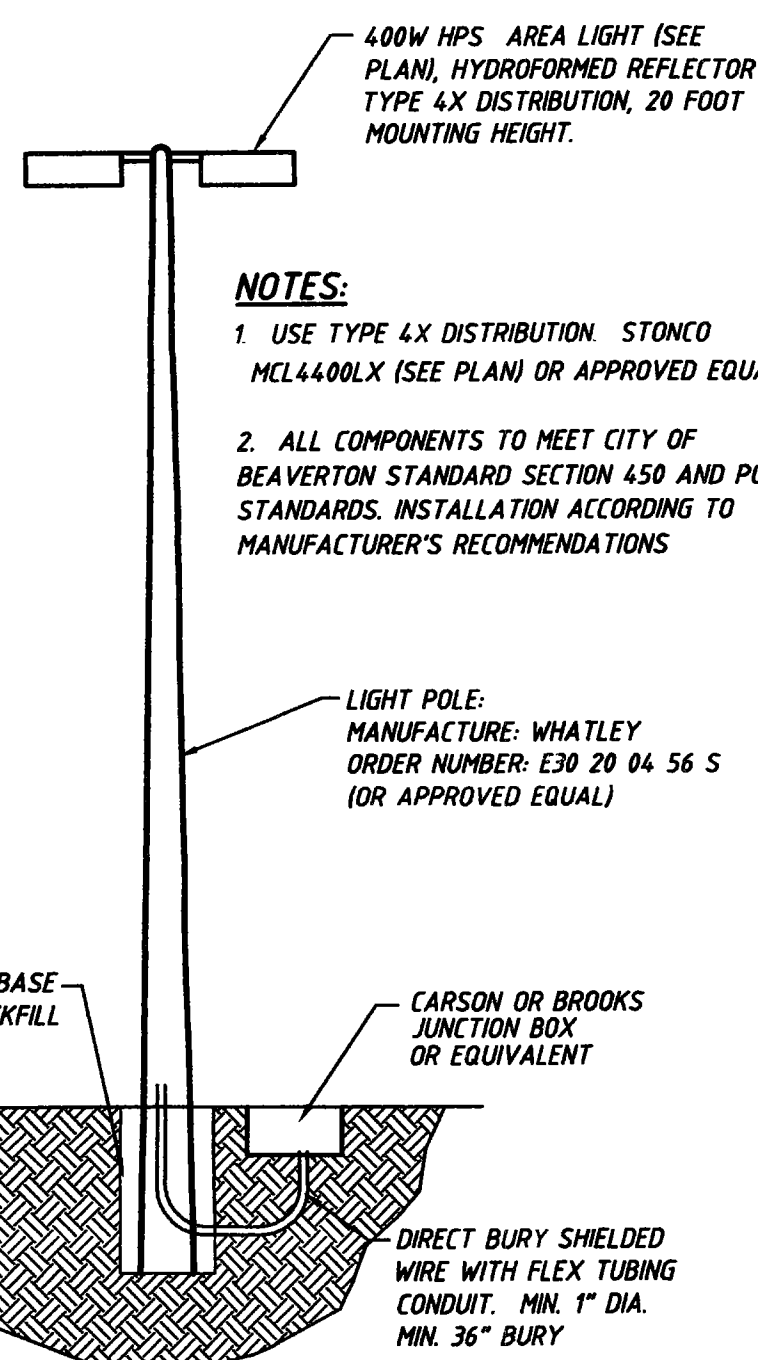
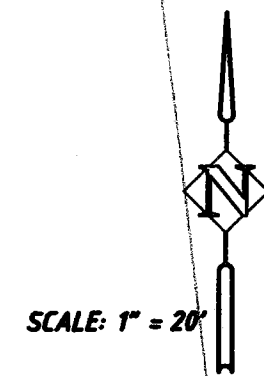
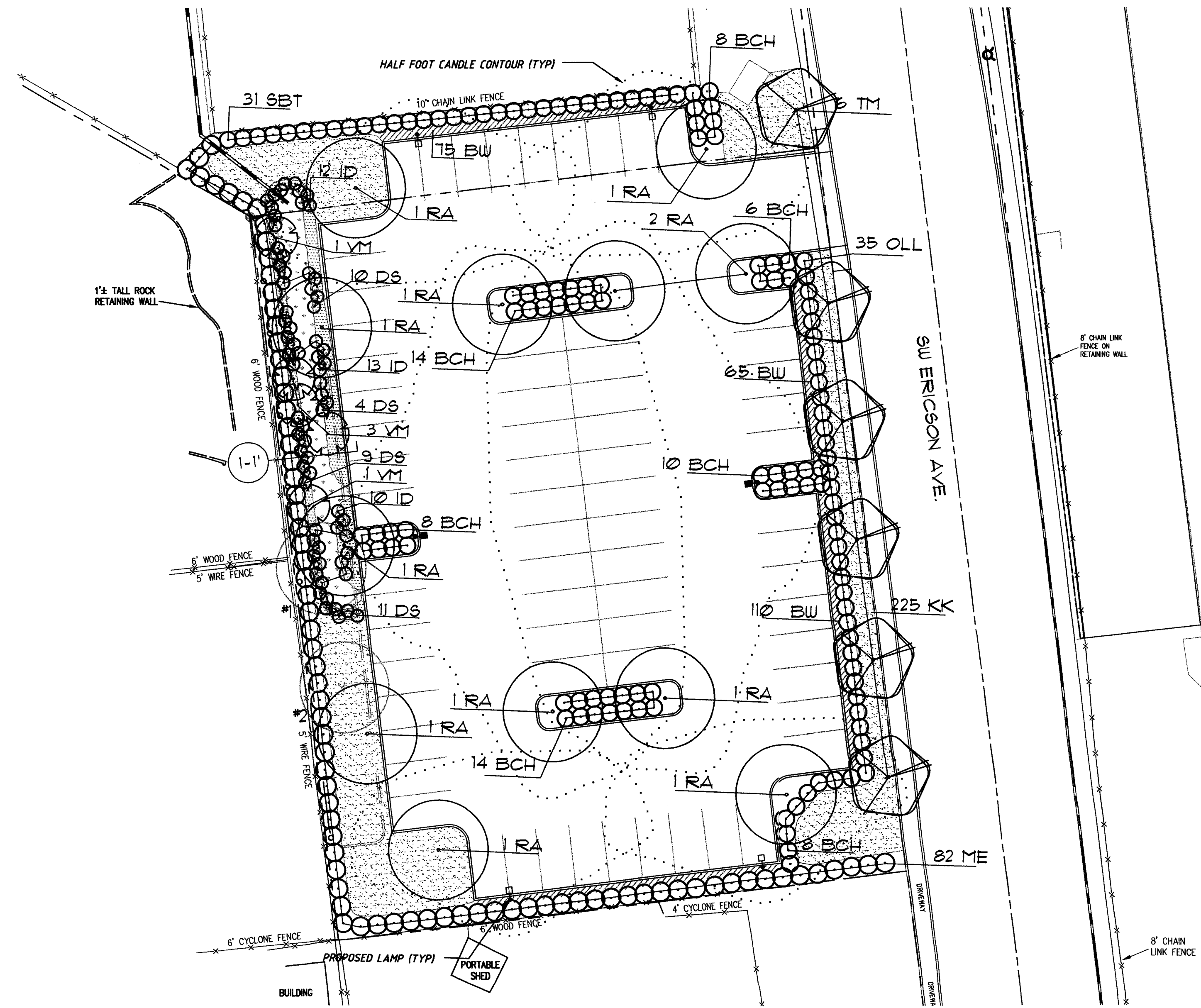
\* PURE LIVE SEED

**BIOSWALE PLANTING NOTES**

- 1 WET/MOIST SEED MIX HYDROSEED APPLICATION RATE SHALL BE 45 LBS./ACRE
- 2 DRY AREA SEED MIX HYDROSEED APPLICATION RATE SHALL BE 25 LBS./ACRE
- 3 MULCH SEEDED AREAS WITH TWO INCHES OF APPROVED MULCH.



**1 WATER QUALITY SWALE-TYPICAL SECTION**  
NTB



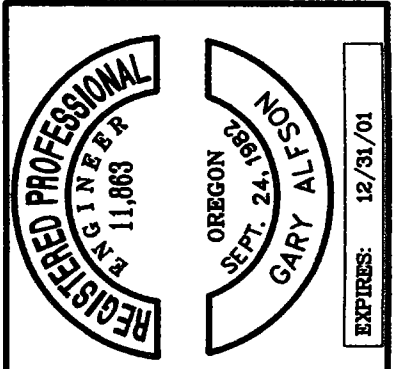
**STREET LIGHT DETAIL**  
NOT TO SCALE

- NOTES:**
- 1 USE TYPE 4X DISTRIBUTION STONCO HCL4400LX (SEE PLAN) OR APPROVED EQUAL.
  - 2 ALL COMPONENTS TO MEET CITY OF BEAVERTON STANDARD SECTION 450 AND PGE STANDARDS. INSTALLATION ACCORDING TO MANUFACTURER'S RECOMMENDATIONS

PRIVATE LANDSCAPING AND LIGHTING PLAN  
**B.H.S. FUTURE PARKING LOT**  
BEAVERTON, OREGON

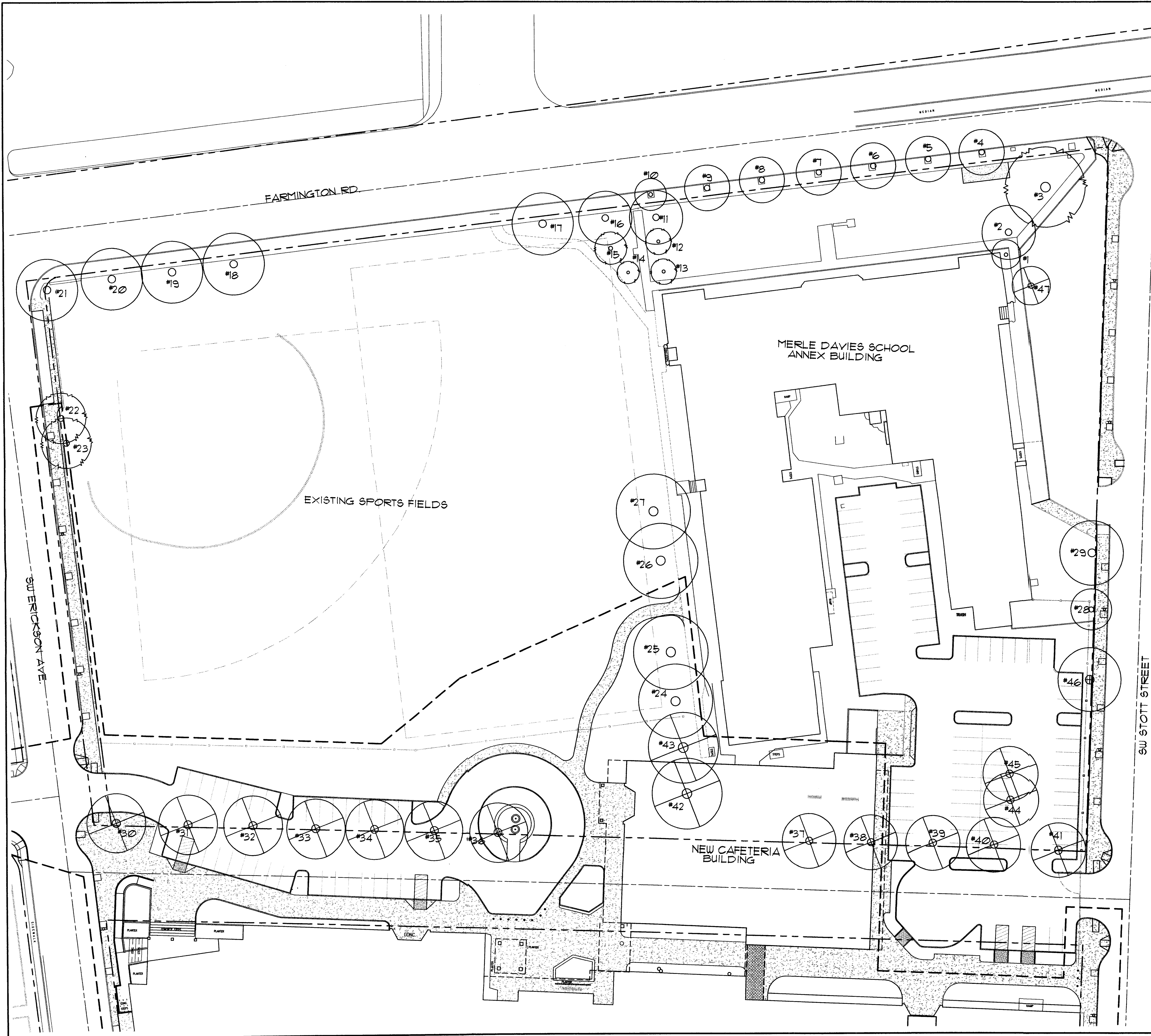
MATELA ASSOCIATES INC.  
LANDSCAPE ARCHITECTURE  
RON M. MATELA A.S.L.A. 560 698-0776 PHONE/FAX  
607 MAIN STREET #807 VANCOUVER, WA, 98640

Harper Houf  
Righellis, Inc.  
ENGINEERS + PLANNERS  
5200 SW MACADAM AVENUE, SUITE 580, PORTLAND, OR 97201  
TEL. 503.221.1131 www.hrcpl.com FAX 503.221.1171



DESIGNED: GAA	DATE: 03-05-02
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REVISIONS PER CITY COMMENTS	
7-26-01	
NO. DATE DESCRIPTION	
14 of 17	
SHEET NO.	JOB NO. B50-23









**GENERAL NOTES**

- 1 SEE TREE PRESERVATION NARRATIVE FOR ARBORIST REPORT INFORMATION ON EXISTING TREES.
- 2 TREE SYMBOL APPROXIMATES ACTUAL CANOPY DIMENSION.
- 3 PRESERVED TREES WITHIN THE LIMITS OF CONSTRUCTION SHALL BE PROTECTED BY APPROVED FENCING PLACED FIVE FEET OUTSIDE OF DRIP LINE OF TREE.
- 4 WHERE CONSTRUCTION OCCURS WITHIN THE DRIP LINE OF ANY TREE TO BE PRESERVED EXISTING GRADES SHALL NOT BE CHANGED WITHOUT APPROVAL OF LANDSCAPE ARCHITECT.
- 5 SEE CIVIL DRAWINGS FOR EXISTING AND PROPOSED UTILITY LOCATIONS.

**LEGEND**

-  EXISTING DECIDUOUS TREE TO REMAIN
-  EXISTING EVERGREEN TREE TO REMAIN
-  EXISTING DECIDUOUS TREE TO BE REMOVED
-  LIMITS OF CONSTRUCTION LINE

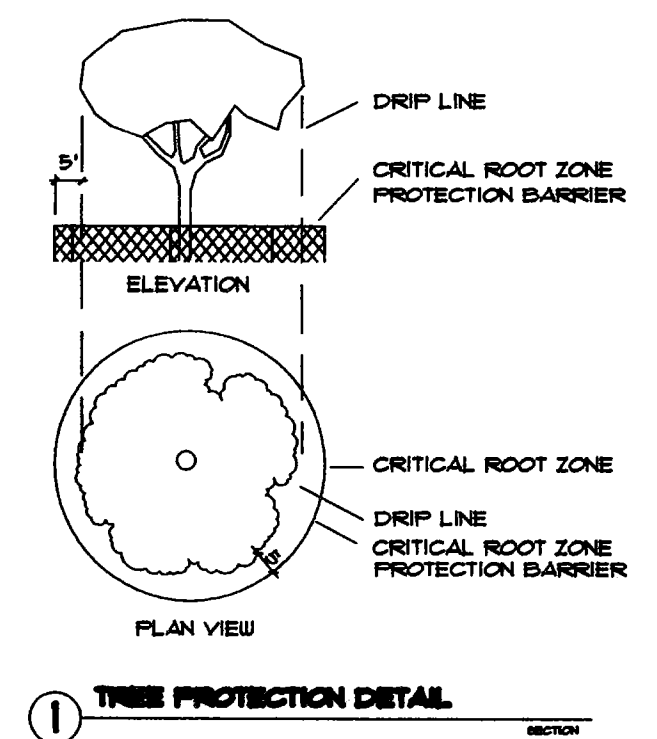
**EXISTING TREE DATA**

**TREES TO BE PRESERVED**

#	SPECIES	SIZE (DBH)
1	BEECH	18"
2	CHERRY (FRAXINUS AVIUM)	16.0"
3	DEODAR CEDAR (CEDRUS DEODAR)	40"
4	NORWAY MAPLE	10"
5	NORWAY MAPLE	8"
6	NORWAY MAPLE	10"
7	NORWAY MAPLE	10"
8	NORWAY MAPLE	10"
9	NORWAY MAPLE	10"
10	NORWAY MAPLE	10"
11	CHERRY (FRAXINUS AVIUM)	18"
12	PINE (PINUS)	12"
13	PINE (PINUS)	6"
14	CEDAR	6"
15	CEDAR	10"
16	NORWAY MAPLE	24"
17	NORWAY MAPLE	24"
18	NORWAY MAPLE	14"
19	NORWAY MAPLE	16"
20	NORWAY MAPLE	16"
21	NORWAY MAPLE	14"
22	PINE (PINUS)	26"
23	PINE (PINUS)	26"
24	SYCAMORE	29"
25	SYCAMORE	29"
26	SYCAMORE	29"
27	SYCAMORE	29"

**TREES TO BE REMOVED**

#	SPECIES	SIZE (DBH)
28	SUGAR MAPLE (ACER SACCARINUM)	8.6"
29	NORWAY MAPLE	11.1"
30	NORWAY MAPLE (ACER PLATINOIDES)	16.1"
31	NORWAY MAPLE	16.6"
32	NORWAY MAPLE	19.0"
33	NORWAY MAPLE	20.4"
34	NORWAY MAPLE	18.3"
35	NORWAY MAPLE	21.1"
36	NORWAY MAPLE	19.8"
37	NORWAY MAPLE	18.1"
38	NORWAY MAPLE	15.5"
39	NORWAY MAPLE	20.0"
40	NORWAY MAPLE	19.3"
41	NORWAY MAPLE	18.2"
42	SYCAMORE (PLATANUS ACERIFOLIA)	28.6"
43	SYCAMORE (PLATANUS ACERIFOLIA)	29.0"
44	ELM (ULMUS FUMILIA)	20.0"
45	ELM (ULMUS FUMILIA)	17.9"
46	NORWAY MAPLE	25.4"
47	RED MAPLE (ACER RUBRUM)	10.0"



**TREE PRESERVATION PLAN**  
**B.H.S. CAFETERIA ADDITION**  
 BEAVERTON, OREGON

**Harper Houf**  
**Righellis, Inc.**  
 ENGINEERS & PLANNERS  
 5200 SW MACADAM AVENUE, SUITE 500, PORTLAND, OR 97201  
 TEL. 503.221.1131 www.hhrh.com

DESIGNED:	DRAWN:	CHECKED:	DATE:
			03-05-02
DATE:	NO.	DESCRIPTION	REVISIONS
SHEET NO.	1 of 1		JOB NO. BSD-23



CITY OF BEAVERTON  
DEVELOPMENT SERVICES DIVISION  
4755 S.W. GRIFFITH DRIVE  
P.O. BOX 4755  
BEAVERTON, OR 97076  
PHONE: (503) 526-2420

# APPLICATION FOR DEVELOPMENT REVIEW

FILE NAME _____	SUBMIT: _____ REVIEW: _____
TYPE: _____	RESUBMIT: _____ REVIEW: _____
FILE NUMBERS: _____	APPLICATION MATERIAL RECEIVED: _____
	FEE RECEIVED: _____ FEE PAID: \$ _____
	CHECK #: _____ RECEIPT #: _____
	COMPLETENESS DATE: _____

**PRINT OR TYPE ALL INFORMATION BELOW**

**APPLICANT**  USE MAILING ADDRESS FOR MEETING NOTIFICATION

YOUR NAME <b>Frank Angelo</b>	PHONE # <b>503-227-3664</b>
BUSINESS NAME <b>Angelo Eaton &amp; Assoc.</b>	FAX # <b>503-227-3679</b>
ADDRESS <b>620 SW Main Suite 201</b>	SUITE
CITY, STATE <b>Portland, Oregon</b>	ZIP <b>97205</b>
SIGNATURE (ORIGINAL REQUIRED) <i>Frank Angelo</i>	DATE <b>12/18/01</b>

**PROPERTY/ZONING DATA**

**13000 SW 2nd**  
Address

**Beaverton High School**  
Location Description

**R-10**

CONSTRUCTION EXPECTED TO BEGIN: \_\_\_\_\_

MAP & TAX LOT (List one per line)	DO NOT USE LOT & BLOCK	SITE ACRES	ZONING	
			EXISTING	PROPOSED
15116AD - 11100		6.15	R10	R10
15116AD - 11000		.13	R10	R10
15116AD - 10900		11.41	R10	R10
-				
-				
TOTAL GROSS SITE:		ACRES <b>17.69</b>	SQ. FT. <b>770,576.40</b>	
TOTAL NET SITE (GROSS SITE MINUS DRIVEWAYS & R.O.W.)				

Attach separate sheet if more than one owner

**PROPERTY OWNER**

YOUR NAME <b>Mike Maloney</b>	PHONE # <b>503-591-4303</b>
BUSINESS NAME <b>Beaverton School Dist.</b>	FAX # <b>503-591-4484</b>
ADDRESS <b>16550 SW Merlo Rd</b>	SUITE
CITY, STATE <b>Beaverton OR</b>	ZIP <b>97006-5152</b>
SIGNATURE (ORIGINAL REQUIRED) <i>Mike Maloney</i>	DATE <b>12/19/01</b>

**COMMERCIAL/INDUSTRIAL DEVELOPMENT**

**Public High School**  
Use or Development Description

**ARCHITECT/DESIGNER**

YOUR NAME <b>Russ Pitkin/Eric Nielsen</b>	PHONE # <b>503-274-5432</b>
BUSINESS NAME <b>LSW Architecture</b>	FAX # <b>503-274-0085</b>
ADDRESS <b>1953 NW Kearny</b>	SUITE
CITY, STATE <b>Portland OR</b>	ZIP <b>97209</b>

BUILDING USE PROPOSED USES (LIST ONE PER LINE)	BLDG. AREA		PARKING
	GROSS SQ. FT.	% OF BLOG.	# SPACES REQ/PROP
<b>Cafeteria</b>			/
<b>Parking Lot</b>			/
			/
			/
TOTAL BUILDING AREA:		100%	/

**LANDSCAPE ARCHITECT**

YOUR NAME <b>Ron Matela</b>	PHONE # <b>360-695-0776</b>
BUSINESS NAME <b>Matela Associates</b>	FAX # <b>360-695-0776</b>
ADDRESS <b>601 Main St #201</b>	SUITE
CITY, STATE <b>Vancouver WA</b>	ZIP <b>98660</b>

**SUBDIVISION/LAND PARTITION**

# LOTS PROPOSED \_\_\_\_\_

PARTITION LOT SIZES IN SQUARE FEET

A	B	C
---	---	---

**ENGINEER/SURVEYOR**

YOUR NAME <b>Gary Alfson</b>	PHONE # <b>503-221-1131</b>
BUSINESS NAME <b>Harper Houf Righelli</b>	FAX # <b>503-221-1171</b>
ADDRESS <b>5200 SW Macadam Suite 580</b>	SUITE
CITY, STATE <b>Portland OR</b>	ZIP <b>97201</b>

**MULTI-FAMILY/P.U.D. DEVELOPMENT**

TOTAL BUILDING SPACE	GR. SQ. FT./% SITE	# UNITS
SITE AREA PER UNIT	/	PARKING SPACES
TOTAL OPEN SPACE	/	# COVERED
OPEN SPACE PER UNIT	/	# UNCOVERED
BUILDING COVERAGE	/	TOTAL SPACES
DRIVEWAY AREA	/	RV STORAGE YES <input type="checkbox"/> NO <input type="checkbox"/>
PARKING AREA	/	# SPACES
MAXIMUM BLDG HEIGHT	FEET/STORIES	
UNITS PER BEDROOM NO	S _____ 1 _____ 2 _____ 3 _____	
ATTACHED UNITS	RENTAL <input type="checkbox"/> CONDO <input type="checkbox"/> LOT OWNERSHIP <input type="checkbox"/>	

NOTE: ACCEPTABLE SUBMITTAL  
MAXIMUM SHEET SIZE 24 X 36  
FOLDED TO FIT LEGAL SIZE  
ASSEMBLED INTO SETS



# FILE COPY

City of Beaverton, OR

4755 SW GRIFFITH DR  
BEAVERTON, OR 97076

02/15/2002  
3:10:16PM

Permit Number  
**CUP2001-0031**

Receipt Number  
20020733

Paid By BEAVERTON SCHOOL DISTRICT

Contractor

Site Address 13200 SW 2ND

Description BEAVERTON HIGH SCHOOL CAFETERIA & PKG LO

Paid Date 02/15/2002

Pay Method Check

Check Number 268113

<b>FEES</b>	<b>Amount</b>	<b>Account</b>	<b>FEES</b>	<b>Amount</b>	<b>Account</b>
Development Services	1,199.00	001-03-20-331			

**Total Fees** 1,199.00



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## NOTICE OF DECISION

**June 6, 2002**

**Date of Final Decision: June 3, 2002**

The following project was on the June 3, 2002, City Council Agenda. This decision is final. Any appeal of this decision would be made to the State Land Use Board of Appeals.

**By Consent Agenda:**

1. CUP 2001-0031 BEAVERTON HIGH SCHOOL CAFETERIA AND PARKING LOT CONDITIONAL USE PERMIT

**How to Obtain Further Information:** This decision is available for review, and a copy may be obtained at a cost, at the following address:

City of Beaverton, Development Services Division  
Beaverton City Hall  
4755 S.W. Griffith Drive  
Beaverton, OR 97076  
Phone: (503) 526-2348

Sincerely,

Steven A. Sparks, AICP  
Development Services Manager



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## DECISION FINAL

May 7, 2002

To whom it may concern:

**RE: CUP 2001-0031 – BEAVERTON HIGH SCHOOL CAFETERIA AND  
PARKING LOT CONDITIONAL USE PERMIT**

Please be advised that the Planning Commission's decision of April 24, 2002, on the above referenced application was not appealed, therefore, the decision is final.

If you have any questions, please contact me at 526-2427.

Sincerely,

Steven A. Sparks, AICP  
Development Services Manager

C: Frank Angelo  
Russ Pitkin/Eric Nelsen  
Gary Alfson  
Carl Springer  
Shannon Starke  
Tyler Raddue  
Brad Roast  
Phil Healy  
Bill Avery

Mike Maloney  
Ran Matela  
Vlad Voytilla  
Edna Starke  
Henry Kane  
Jim Duggan  
Sue Nelson  
Jan Youngquist  
Project File



SK

## CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

### NOTICE OF DECISION

April 26, 2002

To Whom It May Concern:

Attached, please find a copy of the approved Land Use Order finalizing the Planning Commission's action on CUP 2001-0031 – BEAVERTON HIGH SCHOOL CAFETERIA AND PARKING LOT CONDITIONAL USE PERMIT.

An applicant or aggrieved person may appeal the order of the Planning Commission by delivering written notice to the City Recorder by 5:00 p.m. on Monday, May 6, 2002. In order to file an appeal, the notice of appeal must contain the following pursuant to Section 50.40.2.B of the Development Code:

1. A reference to the matter sought to be reviewed and the date of the Planning Commission order.
2. If the appellant is not the applicant, a statement of why the appellant is an "aggrieved person".
3. The specific grounds including ordinance and section numbers relied on for the appeal. To be effective the appeal must relate to the specific criteria on which the decision was based. If the notice of appeal is filed prior to the receipt by the appellant of the Commission's written order, the notice shall state this fact and indicate the notice of appeal will be supplemented with a statement of the specific grounds relied upon within 15 days of the date of the Planning Commission order. The Director or City Recorder, at the direction of the City Attorney, may reject the request if it does not meet the requirements of this ordinance. In such instances, the City Attorney shall provide written notice that the request for review has been rejected within 10 days of receipt of the request for review.

A filing fee of \$600.00 is due at the time the appeal is filed. The fee amount depends upon the action being appealed and the number of appeals being filed. Furthermore, if the appeal is requested to be on the record, a fee to cover the cost of preparing a transcript of the Planning Commission meeting(s) shall be paid within five (5) working days after the Planning Director estimates the cost of the transcript. Within ten (10) days of the notice of completion of the transcript, the appellant shall remit the balance due on the cost of the transcript. If the estimate exceeds the cost, the balance shall be refunded.

For further information about your rights to appeal, please contact the City Recorder at 526-2495.

Sincerely,



Steven A. Sparks, AICP  
Development Services Manager

- |                         |                |
|-------------------------|----------------|
| c: Frank Angelo         | Mike Maloney   |
| Russ Pitkin/Eric Nelsen | Ran Matela     |
| Gary Alfson             | Vlad Voytilla  |
| Carl Springer           | Edna Starke    |
| Shannon Starke          | Henry Kane     |
| Tyler Raddue            | Jim Duggan     |
| Brad Roast              | Sue Nelson     |
| Phil Healy              | Jan Youngquist |
| Bill Avery              | Project File   |

**BEFORE THE PLANNING COMMISSION**  
**FOR THE CITY OF BEAVERTON, OREGON**

IN THE MATTER OF A REQUEST FOR	)	
	)	ORDER NO. 1504
CONDITIONAL USE PERMIT APPROVAL TO	)	
	)	CUP 2001-0031
CONSTRUCT A CAFETERIA & PARKING	)	
	)	ORDER APPROVING
AREAS AT THE EXISTING BEAVERTON	)	
	)	REQUEST WITH
HIGH SCHOOL CAMPUS (BEAVERTON	)	
	)	CONDITIONS
HIGH SCHOOL CAFETERIA & PARKING	)	
	)	
LOT), FRANK ANGELO, APPLICANT	)	

This matter came before the Planning Commission on March 20, 2002, and was continued to April 24, 2002, on a request for approval of a Conditional Use Permit (CUP) for the construction of a two-level, 30,000 square foot cafeteria building and parking areas at the existing Beaverton High School campus. Educational institutions, including public, private or parochial academic schools are identified specifically as a conditional use within the R-10 zone. The proposal is located directly to the north of and connecting to the existing high school building at 13000 SW Second Street. The Beaverton High School campus is specifically described on Washington County Assessor's Map 1S1-16AD, Tax Lots 11100, 11000, 02900, 07100 and 10900, and Map 1S1-16AC, Tax Lots 02100 and 02500. The site is zoned Urban Low Density (R-10) and is approximately 27 acres in size.



Pursuant to Ordinance 2050 (Development Code), Section 40.05.10 and Section 50.30.1, the Planning Commission conducted a public hearing and considered testimony and exhibits.

Prior to commencement of the public hearings on March 20, 2002 and April 24, 2002, Vlad Voytilla, Planning Commission Chairman, recused himself from the proceedings due to his conflict of interest as being an employee of the Beaverton School District.

After holding the public hearing and considering testimony, the Planning Commission adopts the Staff Report dated March 13, 2002, as to applicable criteria contained in Section 40.05.15.2 of the Development Code. The Planning Commission also adopts the following supplemental findings, contained herein, in response to the primary issues of concern that were presented to the Planning Commission on April 24, 2002, during the period of oral testimony. Supplemental findings in response to key issues of concern, as identified herein, are as follows:

*1. Drop off facility for students.* The applicant stated that student drop-off would occur in the driveway areas on either side of the proposed cafeteria and buses would load and unload students along SW Erickson and SW Stott Avenues. The Commission was concerned with the delays to parents using the main driveway, off of SW Erickson Avenue from buses loading and from the number of students dropped off on SW Erickson Avenue in the morning. To provide an additional location for student drop off, the Commission adopts a condition to locate an area on SW Erickson Avenue for morning student drop off.

2. *Pedestrian Plaza.* The applicant stated that a pedestrian plaza would be constructed at the northeastern portion of the Beaverton High School campus. The pedestrian plaza was provided to allow a 10% reduction in the minimum parking requirement. The Commission was concerned that without a complete review of the proposed plaza, the elements of the pedestrian plaza as identified in Section 60.20.10.10 of the Development Code would not be met. The Commission adopts a condition that the applicant provide the pedestrian plaza meeting the requirements outlined in Section 60.20.10.10.A.2 of the Development Code.

3. *Design elements of the proposed parking area.* Testimony from Edna Starke and Shannon Starke conveyed concerns relating to the impacts to the abutting property south of parking lot 'H' from noise, lighting, and vehicle exhaust. Specifically, concerns were raised regarding an adequate barrier between the two properties, the proposed landscape materials provided, and the impacts to their property from the proposed lighting. The Commission agreed that a physical barrier between the two properties would provide some mitigation to the parking area for the abutting property owner; however, issues of lighting, fence design, and landscape plantings are discussed as part of the Design Review application. The Commission adopts a condition to provide a physical barrier between the Starke property and parking lot 'H'. The applicant also discussed concern with the proposed development lowering their property value. The Commission found that no evidence was provided to show that this would occur and staff clarified that in review of CUP applications, property value impacts are not evaluated in the approval criteria.

4. *The closure of SW 2<sup>nd</sup> Street.* Testimony from Henry Kane conveyed concern with the closure of SW 2<sup>nd</sup> Street and that Tualatin Valley Fire and Rescue (TVF&R) were not provided adequate emergency access to the campus. The Commission clarified that the application was to address a proposed expansion and that the issue of closing SW 2<sup>nd</sup> Street had been addressed in the Street Vacation application that was approved by City Council. Staff clarified that TVF&R had reviewed this application and found that the emergency access provided was adequate.

**IT IS HEREBY ORDERED** that CUP 2001-0031 is approved, based on the facts and findings of the Planning Commission, on April 24, 2002, subject to the following conditions:

1. Prior to obtaining a Site Development Permit, the applicant shall provide staff with a copy of a recorded lot consolidation of all parcels in the area of the proposed building in accordance with Washington County's requirements.
2. The permit granted shall run with the land and shall continue to be valid upon a change of ownership of the site or structure unless otherwise specified in conditions attached to the permit.
3. Prior to issuance of a building permit, the applicant shall, in accordance with Section 40.05.15.2.D.2 of the Beaverton Development Code, file a copy of this approved permit with the Washington County Department of Records and Elections.
4. Building permits must be secured prior to construction. For further information regarding building permits and/or related building code issues, please call 503-526-2403.
5. A Site Development Permit shall be secured prior to any site clearing, grading, parking lot paving and public works construction.
6. The Beaverton School District shall provide a morning student drop off area approximately 60 feet on SW Erickson Street, adjacent to Beaverton High School.

7. The applicant shall provide a pedestrian plaza, as defined in Development Code Section 60.20.10.10.A.2. This pedestrian plaza must be open to the public and be at least 300 square feet and provide landscaping and a trash receptacle, as well as a transit shelter, if required by Tri-Met.
8. The applicant shall provide a wooden barrier along the southern property line of parking lot 'H' to SW Erickson Street to provide screening for the adjacent property owners, the height and design of which is to be determined by the Board of Design Review.

Motion **CARRIED**, by the following roll call vote:

**AYES:** Maks, Young, Pogue and Barnard

**NAYS:** Bliss and Johansen.

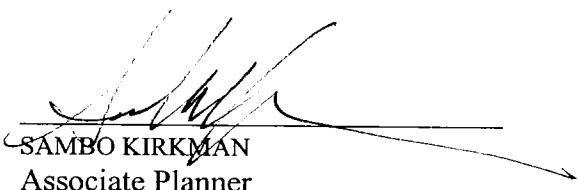
Dated this 26<sup>th</sup> day of April, 2002.


To appeal the decision of the Planning Commission, as articulated in Land Use Order No.1504, an appeal must be filed with the City of Beaverton Recorder's Office by no later than 5:00 p.m. on Monday, May 6, 2002.

PLANNING COMMISSION  
FOR BEAVERTON, OREGON

ATTEST:

APPROVED:

  
SAMBO KIRKMAN  
Associate Planner

  
ROBERT BARNARD  
Vice-Chairman

  
STEVEN A. SPARKS, AICP  
Development Services Manager

**AGENDA BILL**

Beaverton City Council  
Beaverton, Oregon

RECEIVED  
JUN 05 2002  
FILE COPY  
COMMUNITY DEVELOP DEPT

**SUBJECT:** CUP 2001-0031 Beaverton High School Cafeteria & Parking Lot Conditional Use Permit

**FOR AGENDA OF:** 06-03-02 **BILL NO:** 02173

**Mayor's Approval:** *[Signature]*

**DEPARTMENT OF ORIGIN:** CDD *[Signature]*

**DATE SUBMITTED:** 05-21-02

**CLEARANCES:** Devel Serv *[Signature]*

City Attorney *[Signature]*

**PROCEEDING:** Consent Agenda

**EXHIBITS:** Vicinity Map  
Land Use Order No.1504  
PC Minutes 03-20-02  
Draft PC Minutes 04-24-02  
Staff Report Dated 03-13-02

**BUDGET IMPACT**

EXPENDITURE REQUIRED \$	AMOUNT BUDGETED \$	APPROPRIATION REQUIRED \$0
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**HISTORICAL PERSPECTIVE:**

On March 20, 2002, the Planning Commission postponed a Public Hearing until April 24, 2002, at which time they approved a Conditional Use Permit for the construction of a cafeteria building and related parking areas at the existing Beaverton High School Campus (Beaverton High School Cafeteria & Parking Lot Conditional Use Permit). This decision has not been appealed.

**INFORMATION FOR CONSIDERATION:**

The site is located at 13000 SW 2<sup>nd</sup> Street, and is more specifically described as Tax Lots 11100, 11000, 02900, 07100 and 10900 on Washington County Assessor's Map 1S1-16AD; and Tax Lots 02100 and 02500 on Map 1S1-16AC. The property is zoned Urban Low Density (R-10) and is approximately 27 acres in size.

**RECOMMENDED ACTION:**

Consent to the approval of Land Use Order No. 1504.

SS:sp

1 **PLANNING COMMISSION MINUTES**

2  
3 **March 20, 2002**

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6 **CALL TO ORDER:**

Chairman Vlad Voytilla called the meeting to order at 7:00 p.m. in the Beaverton City Hall Council Chambers at 4755 SW Griffith Drive.

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10 **ROLL CALL:**

Present were Chairman Vlad Voytilla, Planning Commissioners Bob Barnard, Gary Bliss, Eric Johansen, Dan Maks, Bill Young and Shannon Pogue; and Alternate Planning Commissioner Steven Olson.

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Senior Planner John Osterberg, Associate Planner Sambo Kirkman, Associate Planner Veronica Smith, Transportation Engineer Don Gustafson, Assistant City Attorney Ted Naemura and Recording Secretary Sandra Pearson represented staff.

The meeting was called to order by Chairman Voytilla, who presented the format for the meeting.

**NEW BUSINESS:**

**PUBLIC HEARINGS:**

A. **BEAVERTON HIGH SCHOOL CAFETERIA & PARKING LOT EXPANSION**

The following land use applications have been submitted for the construction of a two-level, 30,000-square foot cafeteria building located directly north of and connecting to the existing high school building. The development proposal is located at 13000 SW 2<sup>nd</sup> Street, and is more specifically described on Washington County Assessor's Map 1S1-16AD, Tax Lots 11100, 11000, 02900, 07100 and 10900, and Map 1S1-16AC, Tax Lots 02100 and 02500. The site is zoned Urban Low Density (R-10) and is approximately 27 acres in size.

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1. **CUP 2001-0031 – CONDITIONAL USE PERMIT**

This application requests approval of a Conditional Use Permit (CUP) for the construction of a cafeteria building and parking area at the existing Beaverton High School location. Educational institutions, including public, private or parochial academic schools, are identified specifically as a conditional use within the R-10 zone. The proposed project is an expansion of an existing conditional use. A decision for action on the proposed application will be based upon the approval criteria listed in Section 40.05.15.2.C of the Development Code.

Commissioner Maks **MOVED** and Commissioner Johansen **SECONDED** a motion that this Public Hearing be continued to a date certain of April 24, 2002.

Motion **CARRIED**, unanimously.

**MISCELLANEOUS BUSINESS:**

The meeting adjourned at 9:25 p.m.

**DOB**

**PLANNING COMMISSION MINUTES**

**April 24, 2002**

**CALL TO ORDER:**

Chairman Vlad Voytilla called the meeting to order at 7:00 p.m. in the Beaverton City Hall Council Chambers at 4755 SW Griffith Drive.

**ROLL CALL:**

Present were Chairman Vlad Voytilla, Planning Commissioners Bob Barnard, Gary Bliss, Eric Johansen, Dan Maks, Bill Young and Shannon Pogue; and Alternate Planning Commissioner Steven Olson.

Development Services Manager Steven Sparks, Associate Planner Sambo Kirkman, Senior Planner Barbara Fryer, Principal Planner Hal Bergsma, Associate Planner Veronica Smith, Senior Transportation Planner Don Gustafson, Assistant City Attorney Ted Naemura and Recording Secretary Sandra Pearson represented staff.

The meeting was called to order by Chairman Voytilla, who presented the format for the meeting.

**OLD BUSINESS:**

Chairman Voytilla opened the Public Hearing and read the format for Public Hearings. There were no disqualifications of the Planning Commission members. No one in the audience challenged the right of any Commissioner to hear any of the agenda items, to participate in the hearing or requested that the hearing be postponed to a later date. He asked if there were any ex parte contact, conflict of interest or disqualifications in any of the hearings on the agenda. There was no response.

7:05 p.m. – Observing that he is a member of staff of the Beaverton School District, Chairman Voytilla recused himself from participating on tonight's agenda item, passed the gavel to Vice-Chairman Barnard, and stepped down from the dais.

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**CONTINUANCES:**

**A. CUP 2001-0031 – BEAVERTON HIGH SCHOOL CAFETERIA AND PARKING LOT EXPANSION**

*(Continued from March 20, 2002)*

The following land use applications have been submitted to construct a new two-level, 30,000-square foot cafeteria building located directly north of and connecting to the existing high school building. The proposed project is an expansion of an existing conditional use. Educational institutions, including public, private or parochial academic schools are identified specifically as a conditional use within the R-10 zone. The development proposal is located at 13000 SW 2<sup>nd</sup> Street; Washington County Assessor’s Map 1S1-16AD, on Tax Lots 11100, 11000, 02900, 07100 and 10900 and 1S116AC, on Tax Lots 02100 and 02500. The site is zoned Urban Low Density (R-10) and is approximately 27 acres in size. A decision for action on the proposed development shall be based upon the approval criteria listed in Section 40.05.15.2.C.

Commissioner Maks disclosed that he is a former member of the Beaverton School District’s Long-Range Facilities Group and had been involved in the projects involved in the last bond measure, adding that he is technically an elected official of the district. He clarified that his constituency is Southridge High School, noting that he serves as Chairman of the Southridge High School Local School Committee and was elected to this position in March 2001. Pointing out that his daughter is currently a student at Beaverton High School, he emphasized that none of these disclosures would affect his ability to be fair and impartial with regard to making a decision on this particular application.

Commissioner Pogue disclosed that he had attended the Incoming Freshman Night at Beaverton High School and that he had reviewed some of the drawings and information with regard to this project, observing that this would not affect his ability to make a fair and impartial decision with regard to this particular application.

On question, Associate Planner Sambo Kirkman advised Vice-Chairman Barnard that no film of the site is available.

Indicating that he had visited and been involved in several other land use actions involving this site, Commissioner Johansen pointed out that as a former student of Beaverton High School, he is generally familiar with the site.

Observing that he had attended a baseball game on the site this afternoon, Commissioner Young stated that he is familiar with the site.

Commissioner Bliss mentioned that he had visited the site and had no contact with any individual with regard to this application.

**OIO**

1 Noting that he had made several visits to the site prior to the recent Street  
2 Vacation application, Commissioner Maks noted that he drops off his daughter at  
3 the school on a regular basis and is very familiar with the site.  
4

5 Commissioner Pogue mentioned that he had visited and is familiar with the site  
6 and has had no contact with any individual with regard to this application.  
7

8 Vice-Chairman Barnard stated that he had visited the site prior to the recent Street  
9 Vacation application and on numerous other occasions and is very familiar with  
10 this site.  
11

12 Associate Planner Kirkman presented the Staff Report and briefly described the  
13 request for a Conditional Use Permit (CUP) for the expansion of the existing  
14 Beaverton High School Campus, including the construction of a 30,000-square  
15 foot two-story cafeteria building, three parking lots and other associated site  
16 modifications. Concluding, she recommended approval of the application and  
17 offered to respond to questions.  
18

19 Referring to Section 60.20.10.5 (Parking Tables) on page 10 of the Staff Report,  
20 Commissioner Johansen requested clarification of the projected numbers of  
21 students and staff with regard to the parking requirement.  
22

23 Ms. Kirkman clarified that the projected numbers are 2200 students and 200 staff.  
24

25 Senior Transportation Planner Don Gustafson explained that the determination of  
26 the parking requirements is based upon a student count that actually includes both  
27 students and staff.  
28

29 Commissioner Johansen referred to page 11 of the Staff Report with regard to the  
30 ten percent parking reduction, requesting clarification of whether the  
31 Development Code provides that approval of the design is only one of the criteria  
32 that must be addressed, emphasizing that other criteria must be met prior to the  
33 Commission even considering granting this ten percent parking reduction.  
34

35 Mr. Gustafson responded that the property owner is required to provide a parking  
36 analysis demonstrating to the City's satisfaction that the vehicle parking demand  
37 for the existing or proposed use will be met with the reduction in place.  
38

39 Commissioner Johansen expressed his opinion that this suggests that if the design  
40 is approved by both Tri-Met and the City of Beaverton, the ten percent parking  
41 reduction would be permitted with no further need to demonstrate compliance  
42 with any additional criteria. He pointed out that all of the criteria included in  
43 Section 60.20.10.10.A.2 must be met in order for the Commission to consider  
44 granting the parking reduction.  
45

**011**

1 Vice-Chairman Barnard questioned whether a conditional approval would be  
2 necessary based upon whether that action is actually met.

3  
4 Commissioner Maks clarified that historically, the adoption of a CUP is also  
5 adopting the parking and other associated issues as proposed by the applicant in  
6 the application, emphasizing that a specific Condition of Approval is not  
7 necessary unless the Commission desires to specify a minimum and maximum  
8 amount of parking spaces.

9  
10 Referring to Section 60.20.10.5 (Places of Assembly), Development Services  
11 Manager Steven Sparks pointed out that the parking ratio for a high school relates  
12 to the number of both FTE students and staff.

13  
14 Commissioner Johansen noted that he interprets this as indicating that the base-  
15 parking requirement, based upon 2400 total students and staff, is 480 parking  
16 spaces, as opposed to the 440 parking spaces indicated in the Staff Report.

17  
18 **APPLICANT:**

19  
20 **VLAD VOYTILLA**, representing the Facilities Department of the Beaverton  
21 School District, observed that the applicant has reviewed the Staff Report and  
22 concurs with the recommended Conditions of Approval. Noting that the applicant  
23 would provide a brief presentation, he introduced Frank Angelo, the Planning  
24 Consultant, adding that the Traffic Consultant, the Landscape Consultant and  
25 various other consultants are also available to respond to questions.

26  
27 **FRANK ANGELO**, representing *Angelo, Eaton & Associates* on behalf of the  
28 Beaverton School District, reiterated that the applicant concurs with staff's  
29 recommendation with regard to this application and expressed his opinion that the  
30 applicant's proposal complies with the City of Beaverton's requirements for a  
31 CUP. He provided a brief overview of the site plan, pointing that that one of the  
32 goals is to provide adequate parking for buses. He described the location of the  
33 existing Beaverton High School as it relates to the proposed cafeteria and parking  
34 lot expansion for this facility. Observing that only the access to the parking area  
35 on the east side would be gated and would be closed for campus security purposes  
36 throughout the school day, he clarified the location of the parking lots and number  
37 of parking spaces that would be available with this proposal. Pointing out that the  
38 proposed 432 parking spaces should be adequate for both students and staff, he  
39 expressed his opinion that the application meets the eligibility requirements for  
40 the ten percent parking reduction. He noted that the applicant is proposing a  
41 pedestrian amenity or shelter plaza at the intersection of SW Farmington Road  
42 and SW Stott Street, adding that there have been negotiations with Tri-Met with  
43 regard to the design of this facility, which would be approximately 300 square  
44 feet in size.

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012

1 Mr. Angelo discussed the parking of school buses, observing that this would  
2 occur both on SW Erickson Street and on SW Stott Street. He mentioned that ten  
3 of these buses would park on the east side of SW Erickson Street in the  
4 northbound direction and eleven buses would park in the west side of SW Stott  
5 Street in the southbound direction. Referring to an illustration depicting the  
6 proposed bus parking, he noted that the cross section on the left is SW Erickson  
7 Avenue at the entrance to the driveway. He mentioned that the applicant is  
8 providing bulb outs and curb extensions in an effort to provide some protection  
9 for the buses, adding that these vehicles would be parking very closely nose to  
10 end in order to provide no opportunity for students to run between the buses.  
11 Observing that crosswalks would be provided on SW Erickson Street, he  
12 explained that speed tables would operate as crosswalks as well. He described the  
13 raised speed humps, which would be flat on top, adding that these would be  
14 located at the intersection SW Erickson Avenue and the entrance to the new  
15 parking area, and as well as across from the parking lot at the entrance to the  
16 football stadium.

17  
18 Mr. Angelo explained that no parking would be allowed on the east side of SW  
19 Erickson Street during school hours, adding that the general public would be  
20 allowed to park in this area after school hours and on weekends. He pointed out  
21 that 122 parking spaces would be provided for bicycles, emphasizing that the  
22 overall intent of these improvements is the creation of an urban campus, with an  
23 attractive pedestrian crossing between SW Erickson Street and SW Stott Street, as  
24 well as an attractive streetscape. He provided copies of illustrations of the street  
25 sections, reiterating that the applicant has met the City of Beaverton's applicable  
26 requirements for a CUP. Concluding, he requested approval of the application  
27 and offered to respond to questions.

28  
29 Commissioner Young mentioned the 311 parking spaces is provided on-site,  
30 adding that the applicant is projecting to provide 432 parking spaces on-site as the  
31 change occurs. He requested clarification of whether the existing spaces that  
32 would be lost within the public right-of-way would be subtracted from the total of  
33 the increased parking spaces or whether these had already been subtracted.

34  
35 Mr. Angelo advised Commissioner Young that public street parking has not been  
36 included in any of the projections.

37  
38 Commissioner Young emphasized that he is attempting to determine the net gain  
39 in parking spaces.

40  
41 Vice-Chairman Barnard pointed out that public parking spaces could not be  
42 included within the required criteria, explaining that it is necessary for the  
43 applicant to demonstrate only on-site parking.

44  
45 Mr. Angelo noted that 311 parking spaces are currently available, adding that the  
46 net increase in parking is 121 spaces, for a total of 432 parking spaces. He

013

1 emphasized that this is on-site parking and does not include the parking available  
2 on SW Erickson Street or SW Stott Street.

3  
4 Commissioner Young reiterated that he is curious with regard to the net increase  
5 of on-site parking spaces, and requested clarification of how many public spaces  
6 would be lost with this proposal.

7  
8 Mr. Angelo advised Commissioner Young that he does not have this information.

9  
10 Mr. Voytilla pointed out that this is not included within the applicable criteria.

11  
12 Referring to the proposed pedestrian plaza on the corner of SW Farmington Road  
13 and SW Stott Street, Commissioner Young questioned whether this has been  
14 designed to the point where it is possible to determine what this facility would  
15 look like.

16  
17 Observing that Gary Alfson of *Harper Houf Righellis, Inc.* is working on this  
18 issue with Ben Baldwin of Tri-Met, Mr. Angelo pointed out that the design of the  
19 proposed pedestrian plaza has not yet been determined.

20  
21 Commissioner Young emphasized that he is curious with regard to whether there  
22 is any intention of providing access for individuals utilizing public transit from  
23 buses on SW Farmington Road traveling both westbound and eastbound.

24  
25 **GARY ALFSON**, representing *Harper Houf Righellis, Inc.* on behalf of the  
26 applicant, pointed out that the pedestrian plaza would primarily serve buses  
27 traveling eastbound on SW Farmington Road.

28  
29 Mr. Voytilla pointed out that this pedestrian plaza would be located directly east  
30 of the signalized intersection at SW Cedar Hills Boulevard, noting that this is not  
31 a controlled intersection. He noted that with a mid-block crossing on SW  
32 Farmington Road, a safe pedestrian route for individuals riding the westbound  
33 buses would not be available.

34  
35 Commissioner Bliss requested clarification of the discrepancy between the 436  
36 parking spaces referenced in the application and the 432 parking spaces  
37 mentioned in all of the testimony that has been received.

38  
39 Pointing out that there had been some revisions, Mr. Angelo clarified that the  
40 correct number of parking spaces is 432, including the requested ten percent  
41 parking reduction.

42  
43 Commissioner Johansen requested information with regard to public transit that  
44 would serve the proposed pedestrian plaza.  
45

014

1 Mr. Angelo advised Commissioner Johansen that the headways on SW  
2 Farmington Road are 15 minutes.

3  
4 Commissioner Johansen questioned whether any evidence indicates that the  
5 presence of this proposed pedestrian plaza would generate any transit usage along  
6 this route, noting that this could reduce the parking demand.

7  
8 Mr. Voytilla clarified that one of the features of this proposed facility, beyond  
9 utilization as a cafeteria, is that it is large enough to accommodate conferences,  
10 adding that the applicant intends individuals attending conferences and meetings  
11 scheduled at this facility would have the option of utilizing public transit.

12  
13 Commissioner Johansen questioned whether a parking analysis based upon this  
14 different use had been prepared.

15  
16 Mr. Voytilla informed Commissioner Johansen that this issue had been  
17 considered, emphasizing that this option is available for any of the district's  
18 facilities that provide for multiple uses.

19  
20 Commissioner Johansen requested information with regard to current utilization  
21 of public transit by both students and staff.

22  
23 Mr. Voytilla pointed out that although this information is available, he does not  
24 have it with him at this time.

25  
26 Referring to the Development Code, Commissioner Johansen mentioned that one  
27 of the requirements for consideration of the parking reduction is parking analysis  
28 demonstrating adequate demand, adding that he had not found any parking  
29 analysis within any of the documents that had been provided.

30  
31 Mr. Angelo explained that the overall parking requirements had been reviewed,  
32 noting that the projected 2200 students had been there historically prior to the  
33 opening of Southridge High School and pointed out that parking at that time had  
34 been adequate.

35  
36 Commissioner Johansen questioned whether adequate is intended to indicate that  
37 the parking had not spilled out of the district's lots into the neighborhoods.

38  
39 Mr. Angelo responded that it is his understanding that the parking at the site had  
40 not gone beyond the district's lots, expressing his opinion that the additional 122  
41 parking spaces is more than adequate to accommodate the projected enrollment  
42 and employment at the site.

43  
44 Mr. Voytilla noted that at that time, the district had also utilized portable  
45 classrooms, observing that these structures were located on the parking lots,

1 emphasizing that these particular parking spaces were not effectively in use at the  
2 time.

3  
4 Commissioner Johansen expressed his recollection of Friday night football games  
5 is that parking definitely spills over down SW 6<sup>th</sup> Street, adding that he has  
6 personally observed and heard comments from some of the neighbors with regard  
7 to this issue.

8  
9 Referring to the Development Code with regard to parking requirements at high  
10 schools, Mr. Voytilla pointed out that many of the high schools in the  
11 metropolitan area do not have on-site parking that is adequate to accommodate  
12 sporting events, particularly with regard to a team that is experiencing a good  
13 season.

14  
15 Commissioner Johansen expressed his opinion that a parking reduction should be  
16 based upon a real issue, rather than a pedestrian plaza that would not contribute  
17 towards a reduction in the parking demand.

18  
19 Mr. Angelo pointed out that in terms of the proposed parking reduction, the  
20 school has a permit process that provides them with the ability to control the  
21 number of students permitted to utilize the parking lots. He mentioned that the  
22 district's requirement to provide busing to all students who reside a mile or more  
23 from the school creates alternative transportation and reduces the demand on the  
24 parking.

25  
26 Expressing his appreciation of the parking permit process, Commissioner Bliss  
27 pointed out that this involves on-site parking, adding that once all of the permits  
28 are issued, there would still be additional students who wish to drive their own  
29 vehicles to and from school.

30  
31 Observing that this would involve parking on public property, Mr. Angelo noted  
32 that the district has no means to address this issue.

33  
34 Commissioner Bliss emphasized that he has a problem with approving a parking  
35 reduction without having access on all information with regard to the proposal,  
36 specifically the pedestrian plaza. He noted that if this pedestrian plaza is not  
37 functional, then the Commission would have approved a proposal based upon  
38 something that is not going to occur.

39  
40 Commissioner Johansen mentioned that the applicant has stated that parking for  
41 ten buses would be available northbound on SW Erickson Street and requested  
42 clarification of the number of buses that would travel through that area during the  
43 a.m. peak period.

44  
45 Mr. Voytilla explained that while the district has not reached the anticipated  
46 student level of 2,200 students, adding that the buses could be purposely

016

1 staggered to make certain that only several of the buses load or unload at any  
2 given time. He noted that parking has been proposed for ten buses on SW Stott  
3 Street and eleven buses on SW Erickson Street, adding that the district has a  
4 variety of options available for controlling the bus traffic and parking. On  
5 question, he informed Commissioner Johansen that parking would be available to  
6 accommodate all of the buses traveling northbound on SW Erickson Avenue.  
7

8 Commissioner Maks requested clarification of why it is necessary to provide  
9 space for all ten buses at the same time, observing that in exchange for not  
10 stopping traffic, public parking is being eliminated. He pointed out that because  
11 all of these buses should not be at that location at the same time, it should only be  
12 necessary to provide adequate room for three or four buses.  
13

14 Mr. Voytilla explained that there would be occasions in which it would be  
15 necessary for all ten buses to park in that location at the same time, emphasizing  
16 that the district is also required to provide the ability to remove the students from  
17 the school in the event of an emergency situation.  
18

19 Commissioner Johansen questioned whether the Traffic Study has fully accounted  
20 for the buses that would be traveling north on SW Erickson Street and turning  
21 either left or right, rather than traveling east on SW 2<sup>nd</sup> Street. Referring to Figure  
22 2 of the Traffic Study with regard to peak hour traffic volumes, he pointed out  
23 that one additional left turn and 17 additional right turns would be created and that  
24 at least ten additional bus trips accessing that intersection would be generated.  
25

26 **CARL SPRINGER**, representing *DKS Associates* on behalf of the applicant,  
27 responded to Commissioner Johansen's question, indicating that only eight  
28 additional non-bus-related trips would be accessing that particular intersection, as  
29 opposed to traveling east on SW 2<sup>nd</sup> Street.  
30

31 Commissioner Johansen expressed his opinion that while he has no evidence to  
32 the contrary, eight non-bus-related trips appears to be a low figure considering the  
33 amount of traffic currently traveling this same route at that time.

34 Commissioner Maks referred to the discussion with Mr. Springer with regard to  
35 the Street Vacation, observing that due to the locations of the additional parking  
36 and the new parking on the other side, the flow of the traffic has shifted, creating  
37 more route and more vehicles traveling on SW 5<sup>th</sup> Street towards SW Stott Street,  
38 resulting in a decrease in the amount of traffic on SW Erickson Street.  
39

40 Commissioner Johansen mentioned that there would also be traffic patterns  
41 created by the vehicles dropping off individuals, adding that these traffic patterns  
42 would not be dictated by the parking lots.  
43

44 Mr. Springer explained that there would be a net difference of a very small  
45 amount of vehicles to the movement, pointing out that ten buses are generally  
46 going to turn right, rather than left.

017



1 Commissioner Maks expressed concern that the majority (70%) of the drop-offs  
2 occur on SW Erickson Street.

3  
4 Commissioner Johansen observed that this intersection is Level of Service "F" for  
5 the northbound left turn movement, with a delay greater than 50 seconds. He  
6 questioned whether the Traffic Model takes into account that buses, rather than  
7 cars, are involved, emphasizing that these vehicles take longer to pull in and pull  
8 out and stack further back through the intersection and could block access to the  
9 left turn lane.

10  
11 Mr. Springer advised Commissioner Johansen that the analysis had indicated a  
12 change in the vehicle queue, observing that this had involved one additional  
13 vehicle. He emphasized that the majority of the buses would be turning right,  
14 pointing out that a left turn at this location would be difficult and would not be  
15 appropriate for the district's routing.

16  
17 Observing that a certain amount of cost is involved in operating each of these  
18 buses, Mr. Voytilla pointed out that the district makes every effort to utilize these  
19 resources efficiently.

20  
21 Mr. Springer expressed his opinion that one additional vehicle to a queue is not  
22 significant.

23  
24 Mr. Voytilla mentioned that the City of Beaverton has been suffering for a long  
25 time due to incremental additions to failing intersections, emphasizing that at  
26 some point, this does become a significant issue.

27  
28 Commissioner Maks questioned where the special education buses would be  
29 traveling.

30  
31 Referring to an illustration, Mr. Angelo indicated that the special education buses  
32 would be entering off of SW Erickson Street and loading and unloading students  
33 within the traffic circle.

34  
35 Commissioner Maks requested clarification of where the buses would be located  
36 on SW Erickson Street.

37  
38 Mr. Angelo advised Commissioner Maks that three buses would be located north  
39 of the entrance to the driveway and eight buses would be located to the south  
40 between the stadium and the entrance to the driveway.

41  
42 Commissioner Maks emphasized that the majority of the students at Beaverton  
43 High School do not utilize public transit, pointing out that 70% of the road is  
44 outside of the attendance area. He noted that he questions what is needed for  
45 parking as it relates to what is required by the Development Code, adding that he  
46 is not certain that he approves of eliminating public parking from 7:30 a.m. until

**OIB**

1 2:30 p.m. Observing that a primary issue with him involves the drop off, he noted  
2 that although this could probably be addressed, he had been less concerned when  
3 it had appeared that only 110 cars would be traveling through this area. Noting  
4 that the horizontal parking situation adds to traffic congestion, he questioned the  
5 number of parking spaces located within this turnaround area.  
6

7 Mr. Springer informed Commissioner Maks that 35 horizontal parking spaces are  
8 located within the turnaround area.  
9

10 Commissioner Maks pointed out that these 35 parking spaces provides 35  
11 opportunities for vehicles to take up to two minutes to park within a parking stall,  
12 emphasizing that the peak traffic time at a high school is a span of approximately  
13 20 minutes. He mentioned that the school could address this issue by allowing  
14 these parking spaces to be utilized prior to 7:15 a.m., adding that this parking is  
15 actually restricting the adequate functioning of that turnaround area. Observing  
16 that a student would be attempting to park a Buick in a space that barely has  
17 adequate space for a sub-compact car, he noted that there would also be the mini-  
18 buses that provide transportation for the students with special needs, who often  
19 require additional time to reach their destinations, which also delays the traffic  
20 pattern through that turnaround area.  
21

22 On question, Mr. Voytilla informed Commissioner Maks that he is not certain of  
23 the number of special needs buses that would be utilized at the site, adding that he  
24 is sure that less than six special needs buses would be there. He pointed out that  
25 the geometry of the cul-de-sac has been designed to make certain that the buses  
26 could park at the curb and traffic would be able to travel around the buses. He  
27 noted that a drop-off is also located at the southern end of the cul-de-sac, adding  
28 that to the east of the proposed cafeteria, there is a similar curved curb line that  
29 will also function as a drop-off area, adding that this area is being enhanced in an  
30 effort to encourage drop-offs at both ends.

31 Emphasizing that the students would not utilize the crosswalks as intended,  
32 Commissioner Maks pointed out that they walk diagonally and sideways and that  
33 active enforcement is the only way to get the students to use the crosswalks. He  
34 mentioned that he would prefer a right-hand turn lane, rather than the pedestrian  
35 bulb, and questioned whether there is adequate room to install a right-hand turn  
36 lane with a queuing capacity for three vehicles, which could effectively relieve  
37 some of the congestion.  
38

39 Observing that he is not comfortable guessing on this type of issue, Mr. Springer  
40 noted that he understands what Commissioner Maks is attempting to accomplish.  
41

42 Commissioner Maks expressed his opinion that the issues could be appropriately  
43 addressed by leaving adequate room for vehicles without being required to travel  
44 through the bulb to drop off students.  
45

DM

1 Mr. Voytilla addressed Commissioner Maks' concern with the proposed  
2 elimination of some of what is now public parking on SW Erickson Avenue in  
3 order to provide parking for buses, observing that this issue had been debated. He  
4 pointed out that the applicant had considered restricting this area for the morning  
5 and afternoon periods when the buses arrive, emphasizing that there had been  
6 concern with the possibility that some individuals might not comply with this  
7 restriction. He explained that it had been determined that the easiest and safest  
8 solution would be to restrict this area during school hours.

9  
10 Commissioner Maks suggested conditioning this restriction for a shorter period of  
11 time, pointing out that there are signs at Raleigh Hills Elementary School  
12 prohibiting dropping off and allowing buses only during this period of time. He  
13 noted that he agrees that parents fail to read signs, observing that the parents of  
14 the Southridge High School students began obeying the "No Parking" signs when  
15 their cars were towed.

16  
17 Mr. Voytilla pointed out that the district is attempting to address the situations  
18 they have identified at the school that are not working, emphasizing that the  
19 parking situation would be monitored and that any necessary revisions would be  
20 addressed.

21  
22 Emphasizing that he has no concern with the parking for the sake of parking,  
23 Commissioner Maks requested clarification of how the vehicles that are currently  
24 entering and exiting the site to drop off students would continue to do so.

25  
26 Referring to Figure 4 of the Traffic Report, Mr. Springer pointed out that the  
27 number of vehicles turning right heading north on SW Erickson Street is not very  
28 significant. Observing that there is no real need for a right-hand turn lane, he  
29 noted that this would not serve much purpose.

30  
31 Commissioner Maks suggested a 60-foot drop-off area on SW Erickson Street.  
32 Observing that he would like to discuss this with the engineer, Mr. Springer  
33 pointed out that there could be a way to provide a drop-off area adequate for two  
34 or three vehicles to the south of the entrance.

35  
36 Commissioner Maks suggested a Condition of Approval requiring that the  
37 Beaverton School District would provide a student drop-off area, 60-feet in length  
38 (three car lengths), at some location on SW Erickson Street. Emphasizing that  
39 this area is supposed to function appropriately, he noted that the diagonal parking  
40 he had proposed would help to serve this function.

41  
42 Commissioner Bliss mentioned that while the cul-de-sac bulb provides an  
43 adequate turning radius for buses, there are up to six special education buses  
44 parked at the curb.

45

020

1 Mr. Voytilla pointed out that these special education buses are not all on the site  
2 at the same time, adding that their arrivals are staggered and they are only at the  
3 site for a brief period of time.  
4

5 Vice-Chairman Barnard reminded Mr. Springer and Mr. Voytilla that it is  
6 necessary to complete and submit yellow testimony cards.  
7

8 Mr. Voytilla clarified that because Beaverton High School is a closed campus,  
9 students would not be allowed to park in undesignated areas, emphasizing that the  
10 students are required to park at a location on the campus. He further explained  
11 that student cars are within a gated area during school hours, adding that the  
12 ungated parking areas are for visitors and staff only.  
13

14 **PUBLIC TESTIMONY:**  
15

16 **EDNA STARKE** mentioned that her home is located to the south of the proposed  
17 parking lot, adding that she is concerned with health issues that could potentially  
18 be created by the exhaust fumes emitted by the buses. She pointed out that  
19 because her small wooden fence does not extend all the way to SW Erickson  
20 Street, she would like the applicant to install a tall fence to provide security and  
21 screening for her yard. Emphasizing that she had actually moved here in 1968  
22 due to air pollution issues that were affecting her husband's health, she noted that  
23 she is concerned with the proposal to locate a parking lot near her back yard. She  
24 observed that some sort of screening is necessary to prevent the lights from  
25 shining into her home, adding that she is also concerned with the size of the  
26 proposed shrubs at the time of planting. Observing that she had discussed her  
27 concerns with both Ms. Kirkman and Mr. Angelo, she expressed her opinion that  
28 it does not appear that her concerns are being addressed appropriately.  
29

30 At the request of Vice-Chairman Barnard, Ms. Edna Starke's daughter-in-law,  
31 **SHANNON STARKE**, indicated the location of her home on the map. She  
32 expressed concern that her mother-in-law would be visited by her grandchildren,  
33 who have allergies and asthma-related illnesses.  
34

35 Vice-Chairman Barnard requested that Ms. Shannon Starke also complete and  
36 submit a yellow testimony card.  
37

38 Ms. Edna Starke reiterated her concerns with a fence, observing that she would  
39 like the applicant to provide at least a ten-foot concrete or brick wall fence along  
40 the property line.  
41

42 Ms. Shannon Starke explained that the existing fence does not extend all of the  
43 way to SW Erickson Street, expressing her opinion that the fence should extend  
44 all of the way to the sidewalk to prevent the students from driving on her mother-  
45 in-law's lawn in order to exit the parking area.  
46

021

1 Commissioner Maks requested clarification of what purpose the requested tall  
 2 wall along the property line would serve.

3  
 4 Ms. Edna Starke advised Commissioner Maks that she is requesting this tall wall  
 5 along her property line to prevent the noise and exhaust fumes from entering her  
 6 home.

7  
 8 Commissioner Maks assured Ms. Edna Starke that the City's design standards  
 9 would regulate the design of the lights in such a way that they would not intrude  
 10 significantly upon her property. He further explained that the design aspect of  
 11 this particular proposal would be considered at a meeting of the Board of Design  
 12 Review, adding that it would be in her best interests to address her concerns at  
 13 that Public Hearing as well. On question, he advised Ms. Shannon Starke that it is  
 14 practically the standard operating procedure to design the lighting in a way that  
 15 would not intrude upon the adjacent properties.

16  
 17 Observing that the Planning Commission reviews the proposal for the conditional  
 18 use in a general manner, Vice-Chairman Barnard explained that the Board of  
 19 Design Review would then review and condition the specifics with regard to  
 20 issues such as the lighting and the size and type of shrubs to be planted. He  
 21 requested that staff make certain that Ms. Edna Starke is provided with the  
 22 necessary information with regard to the date and time of the Public Hearing  
 23 before the Board of Design Review.

24  
 25 Commissioner Johansen requested clarification of how far west of the site Ms.  
 26 Starke's property is located.

27  
 28 Ms. Shannon Starke indicated the location of Ms. Edna Starke's property on the  
 29 illustration

30  
 31 Commissioner Johansen pointed out that there is another property located to the  
 32 west on the south side adjacent to proposed parking lot, requesting clarification of  
 33 whether this property would be impacted as well.

34  
 35 Ms. Edna Starke expressed her opinion that her home is the only one that would  
 36 be significantly impacted by this proposed parking lot, adding that this could  
 37 potentially decrease the value of her property.

38  
 39 Vice-Chairman Barnard informed Ms. Edna Starke that it is necessary to provide  
 40 documented information indicating that the proposal would decrease the value of  
 41 her property, adding that this information could either be provided to the Planning  
 42 Commission at this time or to the Board of Design Review during their Public  
 43 Hearing.

44  
 45 HENRY KANE addressed the Planning Commission's denial of the SV 2001-  
 46 0003 – Beaverton High School Street Vacation, which is related to this particular

022

1 proposal, expressing his opinion that the City of Beaverton appears to be  
2 struggling with the preparation of an ordinance that would stand scrutiny by the  
3 Circuit Court that would not be appealed to the Land Use Board of Appeals  
4 (LUBA). He pointed out that the public interest statute provides that the street can  
5 be closed only if the proposal is within the public interest. Emphasizing that this  
6 issue involves a 27-acre site, five acres of which only appears to be utilized for  
7 grass, he pointed out that while the buses are currently stacking up on SW 2<sup>nd</sup>  
8 Street, the proposed closure would create a mess on SW Erickson Street and  
9 create severe damage to the traffic circulation. Observing that no action can be  
10 taken until Tualatin Valley Fire & Rescue (TVF&R) provides permission, he  
11 noted that the State Fire Code requires that equal access must be provided.  
12 Noting that their first concern is with public health and safety, he expressed his  
13 opinion that it is doubtful that TVF&R would agree to block a through street.  
14

15 Vice-Chairman Barnard advised Mr. Kane that the decision with regard to the  
16 Street Vacation has already been made, emphasizing that this is not the subject of  
17 this Public Hearing.  
18

19 Mr. Kane stated that he is aware that the decision with regard to the Street  
20 Vacation has already been made and is not the subject of this Public Hearing,  
21 adding that TVF&R is duty bound by the State Fire Code to insist upon  
22 appropriate access, which can only be provided by this street. He pointed out that  
23 it is not appropriate for a cafeteria to block the highway without access for fire  
24 and safety vehicles.  
25

26 **TYLER RADDU** mentioned that he is a former school bus driver for Beaverton  
27 High School and described the sizes of the school buses, pointing out that while  
28 the older school buses are 40-42 feet long, approximately the length of 2½ cars,  
29 the new buses are 45 feet long, approximately three car lengths. Observing that  
30 loading and unloading wheelchair ramps takes time, he pointed out that those  
31 mini-buses would be parked there for quite a while and leave at approximately the  
32 same time. He noted that while there were formerly two columns of 13 buses, for  
33 a total of 26 buses, now there are 11 and 10 buses, for a total of 21 buses, adding  
34 that there appears to be several buses missing. He mentioned that because the  
35 buses are parked in different spots every day, the students would need time to  
36 locate their buses. He discussed the team buses, observing that these buses arrive  
37 early to pick up the teams that would be traveling to various games, as well as the  
38 field trip buses that arrive late due to afternoon traffic issues, emphasizing that  
39 this would all create additional traffic and parking complications on SW Stott  
40 Street and SW Erickson Street.  
41

42 Expressing his appreciation to Mr. Raddu for his information, Commissioner  
43 Pogue requested clarification of how long ago Beaverton High School utilized 26  
44 buses.  
45

023

1 Mr. Raddu advised Commissioner Pogue that Beaverton High School had utilized  
2 26 buses a year ago, noting that Southridge High School had been operating at the  
3 time.

4  
5 Commissioner Johansen requested clarification of what time the buses for Merlo  
6 Station High School and C. E. Mason (Arts and Communication) High School  
7 arrive at Beaverton High School.

8  
9 Mr. Raddu explained that these buses pick up the students in the morning at their  
10 home bus stops and drop them off at Merlo Station High School and C. E. Mason  
11 High School, noting that in the afternoon, both of these schools are dismissed at  
12 2:10 p.m., allowing for only 20 minutes travel time for the buses to reach  
13 Beaverton High School in order for these students to transfer to the appropriate  
14 buses to take them home. He emphasized that because the buses from Merlo  
15 Station High School and C. E. Mason High School don't always reach Beaverton  
16 High School in time, these students often are unable to make the connection and  
17 transfer to the buses that take them to their homes.

18  
19 Vice-Chairman Barnard expressed his appreciation to Mr. Raddu for the  
20 information he provided.

21  
22 **APPLICANT REBUTTAL:**

23  
24 Mr. Angelo referred to the comments of Ms. Edna Starke and Ms. Shannon  
25 Starke, pointing out that the lighting issue is addressed within the Development  
26 Code. He noted that the applicant has nothing more to add with regard to their  
27 issues, observing that these issues would be addressed at the Board of Design  
28 Review Hearing scheduled for May 8, 2002. Referring to the existing wooden  
29 fence which does not extend to SW Erickson Street, he mentioned that the  
30 Beaverton School District is willing to extend this six-foot wooden fence up to  
31 SW Erickson Street, as requested by Ms. Edna Starke. He discussed the  
32 landscaping materials between the proposed development and Ms. Edna Starke's  
33 property, observing that the applicant is willing to consider a different variety of  
34 plant materials, most likely a hardier shrub that would grow faster and more  
35 aggressively than the existing vegetation, and that this would be reviewed prior to  
36 the Board of Design Review Hearing. He noted that the plans have been  
37 coordinated with TVF&R, adding that they had been involved in the Facilities  
38 Review process and had indicated that they were process with the proposed access  
39 for both parking lots.

40  
41 Mr. Angelo discussed Mr. Raddu's comments, pointing out that the plans had  
42 been reviewed with the transportation staff of the Beaverton School District in  
43 terms of circulation and bus parking. Emphasizing that the high school serves as  
44 both an activity center and community center that attracts students and visitors, he  
45 noted that the applicant is comfortable with the proposed distribution of bus  
46 parking.

024

1 Commissioner Young requested clarification of the typical number of passengers  
2 on a school bus.

3  
4 Mr. Voytilla explained that while he does have the information with regard to the  
5 exact number of students served by each bus, the number quoted has been  
6 adequate for 2,200 students projected for campus.

7  
8 Commissioner Johansen questioned the height of the fence by Ms. Edna Starke's  
9 property.

10  
11 Mr. Angelo described Ms. Edna Starke's fence as a six-foot wooden alternating  
12 board, good neighbor type fence, observing that the existing fence is in relatively  
13 good condition.

14  
15 Commissioner Maks mentioned that the shrubbery placed along the fence and the  
16 school parking lot would typically screen headlights and prevent glare from  
17 spilling onto adjoining properties, emphasizing that often plant materials that  
18 grow higher and quicker do not actually serve as a screen.

19  
20 **PUBLIC REBUTTAL TO APPLICANT'S REBUTTAL:**

21  
22 Vice-Chairman Barnard pointed out that public rebuttal of applicant's rebuttal IS  
23 restricted to new matters raised in applicant's rebuttal to public testimony.

24  
25 Ms. Shannon Starke mentioned that she would like to address the issue with  
26 regard to continuing the fence out to SW Erickson Street, expressing her opinion  
27 that this would not be adequate without extending the fence all the way around  
28 SW Erickson Street and towards SW 5<sup>th</sup> Street.

29  
30 Ms. Edna Starke expressed her opinion that a wooden fence would not screen  
31 noise, pollution or lights, emphasizing that she would prefer a concrete fence.

32  
33 Ms. Kirkman referred to Development Code Section 60.20.10.6, and clarified that  
34 the exceeded parking ratios have been addressed, observing that while the  
35 Planning Commission is authorized to require additional parking for a CUP, less  
36 would require a Variance. She explained that the Board of Design Review would  
37 address the fencing issue, observing that property values is not addressed within  
38 the approval criteria. Concluding, she pointed out that TVF&R has reviewed the  
39 application and determined that adequate emergency access is available.

40  
41 Mr. Gustafson noted that the raised concrete pedestrian crossings that have been  
42 proposed do not quite meet applicable standards, clarifying that specifying that  
43 these crossings be raised would only create further difficulties and that it would  
44 be appropriate for the applicant to propose a better means of addressing this issue.  
45 Referring to the proposed pedestrian plaza, he noted that such a facility does  
46 encourage public transit for students and members of the public.



1 On question, City Attorney Ted Naemura indicated that he had no comments with  
2 regard to this application.

3  
4 The public portion of the Public Hearing was closed.

5  
6 8:56 p.m. to 9:03 p.m. -- break.

7  
8 Commissioner Young stated that he had reviewed and concluded that the findings  
9 in the Staff Report are appropriate, adding that he would support a motion for  
10 approval of the application, although he is interested in additional Conditions of  
11 Approval.

12  
13 Commissioner Bliss pointed out that while he is in favor of the proposed  
14 expansion, he is unable to approve a proposal that does not meet the Development  
15 Code. He mentioned that he would be willing to approve the application subject  
16 to an agreement with Tri-Met with regard to the ten percent parking reduction. In  
17 response to Mr. Naemura's offer to address this issue, he stated that he would like  
18 this to be addressed after hearing from his fellow Commissioners.

19  
20 Emphasizing that many issues had been raised during the Public Hearing for the  
21 Street Vacation, Commissioner Johansen observed that he had not supported that  
22 application due to the potential impact on the intersection of SW Erickson Street  
23 and SW Farmington Road. Expressing his opinion that the applicant has not met  
24 the criteria with regard to the burden of proof for the ten percent parking  
25 reduction, he stated that he is unable to approve the application, which he feels is  
26 inconsistent with regional goals. He pointed out that it is obvious that parking  
27 demand is not affected by the availability of public transit or a pedestrian plaza,  
28 adding that although he understands the difficulty in expanding this particular site,  
29 the proposal is to build out, rather than up, which is not consistent with an urban  
30 type of high school.

31  
32 Commissioner Pogue expressed his agreement with Mr. Johansen's statements  
33 with regard to the pedestrian plaza, adding that although he supports the  
34 application, he would like to hear from the City Attorney prior to making a  
35 decision. Observing that he is also in favor of Commissioner Moks' suggestion  
36 for allocating a student drop-off location, he pointed out that he is not in favor of  
37 limiting the availability of the parking space, adding that the use of this area  
38 should be determined by the Beaverton School District.

39  
40 Commissioner Moks concurred with the comments of his fellow Commissioners,  
41 observing that with regard to the traffic, this application involves a CUP for a  
42 cafeteria. Pointing out that he understands concern with additional traffic, he  
43 expressed his disagreement with Commissioner Johansen and stated that the  
44 Street Vacation, rather than the CUP, would create this situation. He noted that  
45 the proposal would enhance both Beaverton High School and the community.  
46 Concluding, he expressed his support of the application, emphasizing that he

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1 would like to include some additional Conditions of Approval to address certain  
2 issues.

3  
4 Vice-Chairman Barnard expressed his support of the application, adding that he is  
5 also in favor of the proposed pedestrian plaza. Referring to Ms. Edna Starke's  
6 request for a fence, he noted that while he appreciates good interaction between  
7 neighbors, the request should be proportionate to what is being done at the site.  
8

9 Mr. Naemura responded to concerns with regard to the proposed pedestrian plaza,  
10 referring to a two-step analysis of this issue, as follows:

- 11  
12 1. It is evident that this is language appropriate for a Condition of Approval,  
13 and the Planning Commission is responsible to make certain that the  
14 condition is actually possible. Testimony from Mr. Angelo illustrates that  
15 there is space on the site plan that is large enough to accommodate a 300-  
16 square foot facility, as provided within the Development Code  
17 requirement, indicating that this condition is actually feasible.  
18  
19 2. With regard to providing for the actual language of the condition, since it  
20 appears that this facility could be adequately accommodated on the site  
21 plan, it is necessary to create a Condition of Approval that contains  
22 language that appropriately addresses the intent of the Planning  
23 Commission and the Development Code.  
24

25 Mr. Sparks suggested providing a Condition of Approval with regard to the  
26 proposed pedestrian transit plaza, adding that this should specify a size of no less  
27 than 300 square feet, while leaving the design details up to the discretion of the  
28 Board of Design Review. He also suggested that a Condition of Approval for a  
29 fence should provide general language for a structural or physical separation  
30 between the proposed parking lot and Ms. Edna Starke's property, adding that the  
31 design issues for this fence also be left up to the discretion of the Board of Design  
32 Review.  
33

34 Vice-Chairman Barnard expressed his agreement with Mr. Sparks' suggestion  
35 with regard to the proposed pedestrian transit plaza and Ms. Edna Starke's request  
36 for a fence.  
37

38 Commissioner Maks disagreed with Mr. Sparks' suggestion with regard to the  
39 proposed pedestrian transit plaza and Ms. Edna Starke's request for a fence.  
40

41 Mr. Sparks discussed compatibility and design issues with regard to Ms. Edna  
42 Starke's request for a fence, observing while that creating a barrier between the  
43 proposed parking lot and the residence involves compatibility, the appearance and  
44 materials involves design issues.  
45

1 Expressing his agreement with Mr. Sparks, Commissioner Maks pointed out that  
2 the Planning Commission is responsible for determining proportionality and  
3 compatibility. He noted that while the Board of Design Review might determine  
4 that a cement wall would provide an appropriate barrier, because this would not  
5 be proportionate to the proposed development or necessary for reasonable  
6 compatibility, in his opinion, the Planning Commission has the authority to  
7 determine the height and material of this barrier or fence.  
8

9 Commissioner Johansen commented that while access is an issue for a CUP  
10 whether it is a new or expanded use, it is within the purview of the Planning  
11 Commission to require additional access. He pointed out that he does not agree  
12 with Commissioner Maks' statement that the traffic issues that have been  
13 addressed by the Street Vacation are not subject to consideration with this CUP  
14 application. He expressed his opinion that a certain level of creativity exists with  
15 attempting to demonstrate that there is a justifiable basis for granting parking  
16 reduction based upon available transit service.  
17

18 Commissioner Bliss stated that he finds it difficult to condition a parking  
19 reduction based upon something that might not actually occur, emphasizing that  
20 other applicants are required to provide all of the necessary information and  
21 background data that has not been provided with this application. He referred to  
22 Development Code Section 6.20.10.10.A.2(e) and Section 6.20.10.10.A.3.f,  
23 pointing out that any applicant requesting this parking reduction should provide  
24 the appropriate supporting documentation. He emphasized that he is concerned  
25 with the functionality of this proposed pedestrian plaza, rather than the color or  
26 size of the benches or other design issues. Concluding, he stated that although he  
27 supports Beaverton High School, he is opposed to this application.  
28

29 Commissioner Maks **MOVED** and Commissioner Young **SECONDED** a motion  
30 to approve CUP 2001-0031 – Beaverton High School Cafeteria and Parking Lot  
31 Expansion Conditional use Permit, based upon the testimony, reports and  
32 exhibits, new evidence presented during the Public Hearing on the matter and  
33 upon the background facts, findings and conclusions found in the Staff Report  
34 dated March 13, 2002, including Conditions of Approval Nos. 1 through 5, and  
35 including additional Conditions of Approval Nos. 1 through 4, and adding  
36 additional Conditions of Approval, as follows:  
37

- 38 6. The Beaverton School District shall provide a student drop off of  
39 approximately 60 feet on SW Erickson Street, adjacent to Beaverton High  
40 School.  
41
- 42 7. The applicant shall provide a pedestrian plaza, as defined in Development  
43 Code Section 60.20.10.A.2. This pedestrian plaza must be open to the  
44 public and be at least 300 square feet and provide landscaping and a trash  
45 receptacle, as well as a transit shelter, if required by Tri-Met.  
46

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- 1           8. The applicant shall provide a wooden barrier along the southern property  
2           line of the southern parking lot to SW Erickson Street to provide screening  
3           for the adjacent property owners, the height and design of which is to be  
4           determined by the Board of Design Review.  
5

6           Mr. Sparks requested clarification of Condition of Approval No. 8, specifically  
7           whether the fence would extend only to SW Erickson Street or also down to SW  
8           5<sup>th</sup> Street, as requested by Ms. Edna Starke.  
9

10          Commissioner Moks clarified that this fence would extend only to SW Erickson  
11          Street.  
12

13          Commissioner Johansen **MOVED** and Commissioner Bliss **SECONDED** a  
14          motion to amend the motion to include Condition of Approval No. 9, as follows:  
15

- 16           9. The number of outside parking spaces will be no fewer than 480.  
17

18          Commissioner Johansen explained the purpose of his proposed amendment and  
19          additional Condition of Approval, observing that this would allow this issue to be  
20          addressed in a future action at the option of the applicant.  
21

22          Commissioner Moks suggested that conditioning 480 additional parking spaces  
23          would require the applicant to appear again before the Planning Commission to  
24          modify the CUP, expressing his opinion that this Condition of Approval would  
25          not accomplish anything.  
26

27          Mr. Naemura observed that an inherent conflict exists between the main motion  
28          and the proposed amendment to the motion, adding the active motion-making  
29          process could not address and resolve this issue.  
30

31          Vice-Chairman Barnard requested a two-minute session with the City Attorney  
32          for clarification purposes.  
33

34          Commissioner Moks suggested that this issue could be resolved by calling the  
35          question on the proposed amendment.  
36

37          Commissioner Johansen requested clarification of the conflict of the main motion  
38          and the proposed amendment.  
39

40          Mr. Naemura pointed out that the amendment to the motion challenges the  
41          sufficiency of the motion to stand on evidence, observing that this involves what  
42          he referred to as “dueling motions”.  
43

44          Commissioner Johansen expressed his opinion that this does not involve a  
45          “dueling motion”.  
46

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1 Mr. Naemura reiterated that there is a significant conflict between the main  
2 motion and the proposed amendment.

3  
4 Motion on the amendment to the main motion providing for the proposed  
5 additional Condition of Approval No. 9 **FAILED** by the following roll call vote:

6  
7 **AYES:** Bliss and Johansen.

8  
9 **NAYS:** Barnard, Maks, Pogue and Young.

10  
11 Commissioner Young discussed the potential impacts of Street Vacations,  
12 observing that the decision with regard to the related application for a Street  
13 Vacation has been made. He pointed out that the request for the proposed  
14 reduction in parking should demonstrate that the criteria had been adequately  
15 addressed.

16  
17 Commissioner Johansen expressed his agreement with Commissioner Young,  
18 noting that the parking analysis must demonstrate that vehicle parking demand  
19 would be adequately met with the reduced parking. He expressed his concern that  
20 it is dangerous to disregard this particular requirement in approving a reduction in  
21 parking.

22  
23 Vice-Chairman Barnard acknowledged Commissioner Johansen's comments,  
24 clarifying that the issue now involves the main motion, including the three  
25 additional Conditions of Approval, as proposed by Commissioner Maks.

26  
27 Motion **CARRIED**, by the following roll call vote:

28  
29 **AYES:** Barnard, Maks, Pogue and Young

30  
31 **NAYS:** Bliss and Johansen.

32  
33 **MISCELLANEOUS BUSINESS:**

34  
35 The meeting adjourned at 10:00 p.m.

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# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## STAFF REPORT

TO: Planning Commission

STAFF REPORT DATE: Wednesday, March 13, 2002

STAFF: Sambo Kirkman, Associate Planner *SK*

APPLICATION: **CUP2001-0031 (Beaverton High School Cafeteria and Parking Lot)**

LOCATION: 13000 SW 2<sup>nd</sup> Street  
Map 1S1-16AD; Tax Lots 11100, 11000, 02900, 07100 and 10900  
Map 1S1-16AC, on Tax Lots 02100 and 02500

ZONING: Urban Low Density (R-10)

REQUEST: The applicant request approval of a Conditional Use Permit to build a new cafeteria building and parking area at the existing Beaverton High School location. Educational institutions, including public, private or parochial academic schools are identified specifically as a conditional use within the R-10 zone.

PROPERTY OWNER: Beaverton School District  
16550 SW Merlo Road, Beaverton, OR 97006

APPLICANT: Frank Angelo, Angelo Eaton & Associates  
620 SW Main, Suite 201, Portland, OR 97205

AUTHORIZATION: Ordinance 2050, The Development Code

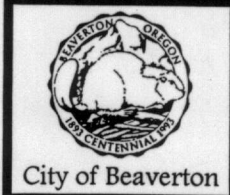
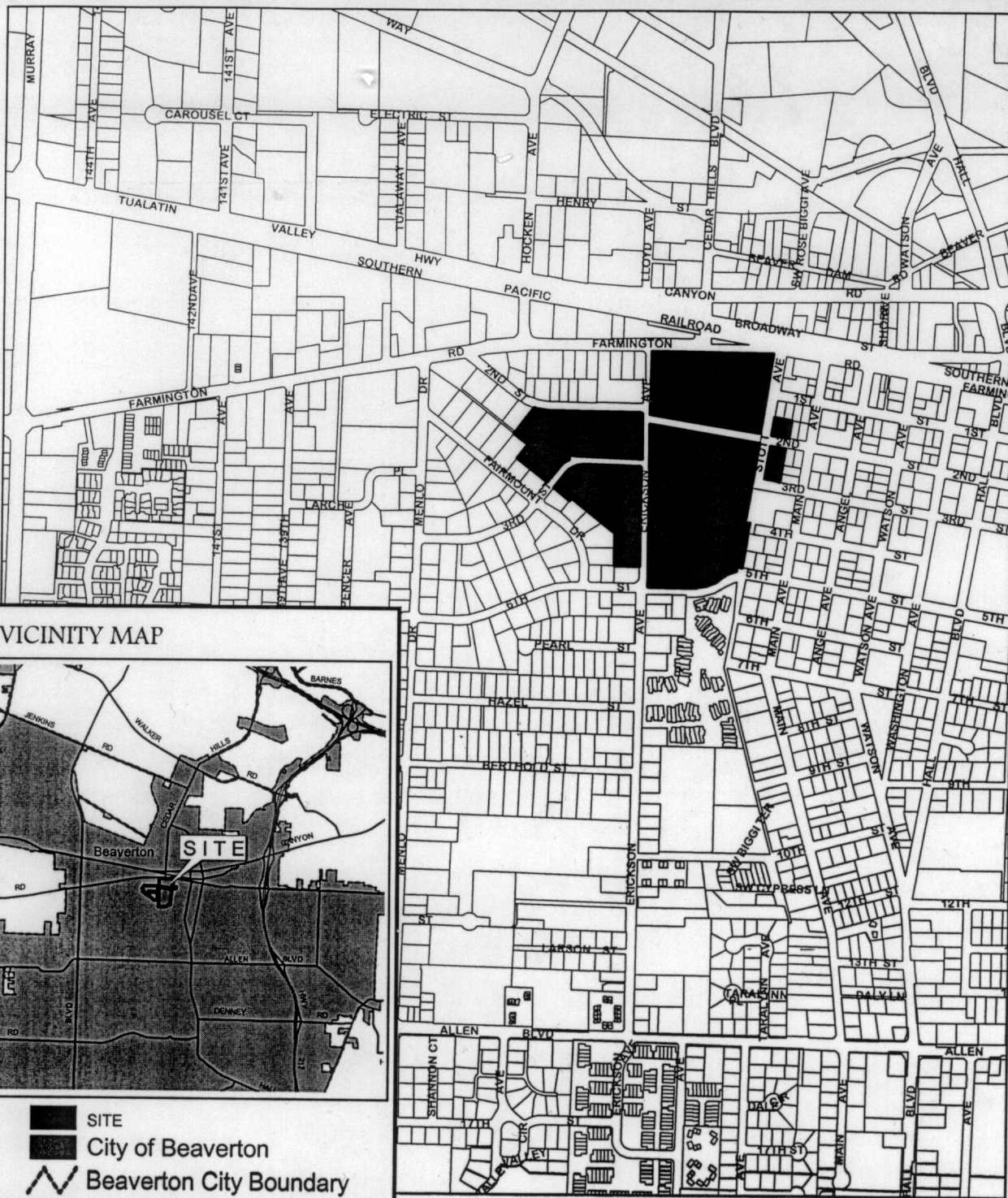
APPROVAL CRITERIA: Development Code, Section 40.05.15.2

HEARING DATE: Wednesday March 20, 2002

RECOMMENDATION: **APPROVAL CUP2001-0031 (Beaverton High School Cafeteria and Parking Lot)**, subject to conditions identified at the end of this report.

# DETAIL MAP

# EXHIBIT #1



BEAVERTON HIGH SCHOOL CAFETERIA & PARKING LOTS  
 COMMUNITY DEVELOPMENT DEPARTMENT  
 Development Services Division

01/02/02  
 Map Number  
 1S116AD  
 Application #



BDR 2001-0213/CUP 2001-0031  
 VAR 2002-0002/TPP 2001-0008

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## APPLICATION PROCESSING

### Subject Application:

The project as proposed requires a Conditional Use Permit (CUP) application. The CUP requires a public hearing before the Planning Commission (Commission). Notice shall be given in accordance with Sections 50.30.1.C of the Development Code.

The Commission may approve, approve with conditions, or deny the application for a Conditional Use Permit. In permitting a CUP, the Commission may impose conditions found necessary to protect the best interests of the surrounding property or neighborhood, or the City as a whole. These conditions may include, but are not limited to, requirements increasing the required lot size or yard dimensions, increasing street widths, providing for the construction of public improvements, controlling the location and number of vehicular access points to the property, increasing the number of off-street parking or loading spaces required, limiting the number or size of signs, limiting the coverage or height of buildings, limiting or prohibiting openings in sides of buildings or structures, requiring screening and landscaping, and requirements under which any future enlargement or alteration of the use shall be reviewed by the City and new conditions imposed. A CUP shall not grant variances to the regulations prescribed by this ordinance.

### Appeals:

Appeal of a Commission decision shall be to the City Council. The procedure for filing such an appeal and the manner of the hearing shall be governed by the provisions of Section 50.40 of Ordinance 2050, The Development Code. The appeal request must be made in writing and delivered to the City within 10 calendar days from the date of the Land Use Order memorializing the Commission's final decision. In addition, there is a non-refundable \$600.00 fee, which must accompany the request for hearing.

To be effective the notice of appeal shall contain:

- A. A reference to the matter sought to be reviewed and the date the Planning Commission made the decision.
- B. If the appellant is not the applicant, a statement of why the appellant is an "aggrieved person".
- C. The specific grounds including ordinance and section numbers relied on for the appeal. To be effective the appeal must relate to the specific criteria on which the decision was based. If the notice of appeal is filed prior to the receipt by the appellant of the Commission's written order, the notice shall state this fact and indicate the notice of appeal will be supplemented with a statement of the specific grounds relied upon within 15 days of the date of the Planning Commission order. The Director or City Recorder, at the direction

of the City Attorney, may reject the request if it does not meet the requirements of this ordinance. In such instances, the City Attorney shall provide written notice that the request for review has been rejected within 10 days of receipt of the request for review. (ORD 3739).

**120 Day Requirement:**

In accordance with State statute, all land use applications must have a final decision within 120 days from the date the application was made complete. Completeness of the application was perfected on January 30, 2002. Therefore, the City must take final action on the application on or before May 30, 2002. The Commission's hearing date of March 20, 2002 is day 49 of the review process.

**Public Notice:**

1. Property was posted in two locations on February 7, 2002.
2. Mailed notice to property owners within 500 feet on February 5, 2002 and March 11, 2002.
3. Legal notice was published in the Valley Times on February 7, 2002.
4. In addition, notice was posted at the Library, City Hall, and Post Office.

**Previous Actions:**

BDR2001-0085 Beaverton High School Parking Lot Improvement  
CUP2001-0018 Beaverton High School Parking Lot Improvement  
BDR99-00083 Beaverton High School Repaint  
BDR91-00091 Beaverton High School Two Portable Classrooms  
CUP91-00027 Beaverton High School Two Portable Classrooms  
BDR3-84 Beaverton High School Parking Lot  
BDR31-84 Beaverton High School Batting Cage  
BDR39-79 Beaverton High School Addition  
CUP4-70 Beaverton High School Expansion

**Other Current Applications:**

**CPA2001-0022 Beaverton High School Comprehensive Plan Amendment**  
Application submitted November 15, 2001  
**SV2001-0003 Beaverton High School Street Vacation**  
Application submitted November 15, 2001  
**BDR2001-0213 Beaverton High School Cafeteria and Parking Lot**  
Application submitted December 21, 2001  
**TPP2001-0008 Beaverton High School Historic Tree Removal**  
Application submitted December 21, 2001  
**VAR2002-0002 Beaverton High School Bicycle Parking Variance**  
Application submitted January 30, 2002. On March 4, 2002 the application was withdrawn.

## **BACKGROUND INFORMATION**

### **Summary:**

The proposal is to expand the existing high school to include a new two-story building located between the main high school building and the Merle Davies building. The proposed building is to be approximately 30,000 square feet in size with a sky bridge connecting the proposed building with the main building. The proposed development includes the construction of three parking lots. Two located on either side of the proposed building and one south of the existing tennis courts on the west side of SW Erickson Avenue. The proposal includes improvements to the pedestrian streetscape making them more ADA compliant, the construction of raised sidewalks along SW Erickson and extended corners, and the construction of a pedestrian plaza.

### **Major Issues:**

The applicant is proposing to expand the existing Beaverton High School (BHS) campus to include an additional parking lot and a new two-story cafeteria facility. The location of the cafeteria is within the current public right-of-way of SW 2<sup>nd</sup> Street. The approval of this Conditional Use Permit and the associated Design Review and Tree Preservation application are contingent upon the approval to vacate SW 2<sup>nd</sup> Street. Prior to approval of the Street Vacation, the applicant will need to have approved a Comprehensive Plan Amendment to remove SW 2<sup>nd</sup> Street from the Street Functional Classification Plan as a neighborhood route.

### **Public Comment:**

As of the date of this staff report no written comments have been received.

### **Comprehensive Plan Designation:**

#### **Land Use:**

The Comprehensive Plan identifies the parcels as Public Facilities – Schools and Parks, except for one parcel, located south of the existing tennis courts along SW Erickson Street. This parcel is designated as Residential – Urban Low.

#### **Street Functional Classification Plan:**

SW 2<sup>nd</sup> Avenue is designated on the Functional Classification Plan Map as a Neighborhood Route. The applicant has requested a Comprehensive Plan Amendment to remove this designation on SW 2<sup>nd</sup> Street between SW Stott and SW Erickson Avenues.

SW Farmington Road (State Highway 10) is designated as an Arterial.

SW Erickson Street and SW 5<sup>th</sup> Street are designated as “Collector” streets.

SW Stott Avenue is designated as a local street.

#### **Street Improvement Master Plan:**

The Street Improvement Master Plan Map illustrates the future street widening of SW Farmington Road west of SW Hocken.

**Traffic Signals Master Plan:**

There are no future traffic signals identified for abutting streets on the Traffic Signals Master Plan. There are two existing traffic signals located on SW Farmington within close proximity to the project area located at SW Hocken and at SW Cedar Hills Boulevard.

**Bicycle Master Plan and Action Plan:**

SW Erickson is designated as a street where bike lanes are proposed. The applicant, as a part of the Design Review application includes a Street Design Modification requesting not to add bicycle lanes along SW Erickson Avenue.

**Zoning:**

Subject Property: Residential – Urban Low Density (R-10)

Surrounding Area: North: Regional Center Old- Town (RC-OT) and General Commercial (GC)  
South: Urban Standard Density (R-5) and Urban Low Density (R-10)  
East: Regional Center – Old Town (RC-OT)  
West: Urban Low Density (R-10)

**Existing Land Uses:**

Subject Property: Public High School

Surrounding Area: North: Retail  
South: Residential  
East: Residential  
West: Residential

**Existing Site Conditions:**

The Beaverton High School (BHS) campus consists of seven parcels approximately 27 acres in size. There are three existing buildings on the site, the main building, the Merle Davies Building, and a building located west of the main building across SW Erickson Avenue. The Merle Davies building along with the landscaping surrounding this building is listed in the City's Historic Inventory. There are athletic fields located north of the main building and west of the Merle Davies building, south of the main building, and on parcels located along the west side of SW Erickson Avenue. The applicant has identified 311 private and 77 public parking spaces on the BHS campus and 57% open space on the existing campus. The site is relatively flat with an overall grade change of less than 10 feet. SW Erickson Avenue, SW Stott Avenue, and SW 2<sup>nd</sup> Street are located within the BHS campus, providing access to the buildings. SW 2<sup>nd</sup> Street and SW Stott Avenue have gates closing the roadway to vehicular traffic during school hours.

**CONDITIONAL USE  
CRITERIA, FACTS and FINDINGS**

**SECTION 40.05.15.2.C APPROVAL CRITERIA**

*In order to grant a Conditional Use Permit, the Planning Commission shall make findings of fact to support the following conclusions:*

- (1). "The proposed conditional use will comply with the purpose of this section and with all of the applicable provisions of this ordinance."*
- (2). "The proposed development will comply with the Comprehensive Plan"*
- (3). "That the location, size, design, and functional characteristics of the proposed use are such that it can be made reasonably compatible with and have a minimum impact on the livability and appropriate development of other properties in the surrounding neighborhood."*

The following section provides a response to the criteria for approving the Conditional Use Permit. Findings are made in response to each approval criterion and Code section to establish that the criterion has been met.

**Criterion (1):**

*The proposed conditional use will comply with the purpose of this section and with all of the applicable provisions of this ordinance.*

**Facts and Findings:**

*20.05.10 Single Family District (Urban Low Density (10, 000 square feet))*

- 1. Purpose. The purpose of this zone is to allow one dwelling per lot of record. (ORD 3293; November, 1982) The R-10 is intended to establish standard urban density residential home sites where a minimum land area of 10,000 square feet is available for each dwelling unit, and where full urban services are provided.*
- 2. District Standards and Uses. R-10 districts and uses shall comply with the following:*

*B. Conditional Uses: (Subject to Section 40.05)*

*The following uses and their accessory uses may be permitted subject to the approval of a Conditional Use Permit (CUP):*

- 5. Educational institutions, including public, private or parochial academic schools, colleges, universities, vocational and trade schools. (See also Special Use Regulations Section, Uses Requiring Special Regulation - Portable Classrooms.)*

The applicant is requesting a Conditional Use Permit based on the Development Code requirement that a Public School is a Conditional Use in the R-10 zone and that substantial modification to an existing conditional use requires a new Conditional Use Permit. Staff find the proposal is in conformance with this provision of the Development Code.

*Standard Low Density (R-10) Zone Site Development Requirements*

*20.05.50. Site Development Requirements.*

*1. Minimum Lot Area and Minimum Land Area: (per dwelling unit)*

*A. Single Family Residential*

*R-10 10,000 square feet for each lot*

*2. Minimum Lot Dimensions: (in feet)*

*A. Width R-10*

*1. Corner lots 90*

*2. Interior lots 80*

*B. Depth: as specified, provided however that no lot depth shall be more than 2 1/2 times the lot width.*

*1. Corner lots 110*

*2. Interior lots 120*

*3. Minimum Yard Setbacks: (in feet)*

*Minimum yard setback in feet for all dwellings constructed after the effective date of the ordinance; dwellings in existence on the effective date which do not meet the following setback requirements shall be exempt from the requirements and may be reconstructed, remodeled, or additions made thereto, providing setback regulations in force and effect at the effective date of this ordinance are followed and no further encroachment into the setback area required by those regulations takes place.*

*A. Front R-10*

*1. Dwelling or building 25*

*B. Side*

*1. One Side of dwelling or building 9*

*2. Opposite side of dwelling or building 5*

*C. Rear*

*1. Dwelling, building, garage 20*

**Setbacks:** Site Development Standards of the R-10 zone, as listed above, are primarily intended for construction of single-family homes. In this case the applicant is requesting the expansion of an existing public school building. The proposed cafeteria expansion is to be located between two existing buildings to its north and south. It is located approximately 300 feet from SW Erickson Avenue

and approximately 120 feet from SW Stott Avenue. The proposed building is located in the area of the current SW 2<sup>nd</sup> Street right-of-way. The applicant is currently processing a Street Vacation request. If the request is approved, the area of the existing public right-of-way will be incorporated into the properties abutting the roadway, which is owned by the BSD and will be part of the BHS campus. The proposed building would be located on top of two parcels. Staff recommend that to prevent a building from being constructed on more than one parcel and to show how the building will meet the required setbacks of the zone, the parcels should be consolidated. Therefore, staff recommend the adoption of Condition #1.

4. *Maximum Building Height: (in feet)*

A.	<i>Maximum Height without at Conditional Use Permit, except as provided in subsection B. below.</i>	<u>R-10</u> 30
----	---	-------------------

*The method of measuring building height for structures built under this section is set out in Chapter 90, Definitions.*

**Building Height:** The applicant's elevation plans identify the height of the proposed cafeteria as 39 feet in their narrative and 34 feet in their architectural elevation. In discussions with the applicant, it was identified that the proposal is to construct the building at a height of 34 feet. The main building of the High School is 31 feet in the area of the proposed cafeteria, and the main school building at its highest point is 60 feet in the area of the auditorium. Therefore, the proposed cafeteria will not exceed the existing height of the school building.

*60.20.10.5: Parking Tables*

Pursuant to Section 60.20.10.5 of the Development Code, off-street parking required for educational institutions such as high schools is based on the ratio of 0.2 of a parking stall per FTE students and staff. The applicant states that the existing counts are 1800 students and 150 staff at the facility and the projected numbers are 2000 students and 200 staff. At the existing population of the campus, the number of required parking for the 1950 people is 390 stalls and for the projected numbers, the required parking for 2200 people is 440 stalls. The proposed development will include the construction of three parking lots, two east and west of the proposed cafeteria building and one south of the existing tennis courts on the west side of SW Erickson Avenue. With the proposed parking lots, the BHS campus will provide only 436 parking stalls, not meeting the minimum parking stall requirements for the projected population of the school.

However, the applicant has requested the option to utilize the Development Code's 10% parking credit for construction of a pedestrian plaza next to the transit route on SW Farmington to reduce the minimum parking space requirement. The proposed plaza and details of its construction are being finalized with Tri-Met and are subject to the approval of City staff and Tri-Met prior to issuance of the site

development permit. The Development Code Section 60.20.10.A.2, requires that the pedestrian plaza be open to the public, be at least 300 square feet, exclusive of connecting walkways, and provide a transit shelter (if required by Tri-Met and the City), provide landscaping, and include a trash receptacle as part of the pedestrian plaza. The Code requires that the property owner provide a parking analysis demonstrating to the City's satisfaction that the vehicle parking demand for the existing or proposed use will be met with the reduction in place.

If the design of the proposed pedestrian plaza is approved by Tri-Met and the City of Beaverton, the 10% reduction would be allowed and the 432 spaces will therefore meet the required minimum number of parking spaces and will also not exceed the maximum parking spaces.

Bicycle parking requirements shall be met. Short-term spaces are not required for educational institutions such as high schools; however 1 long-term bicycle parking space per 18 people is required. Therefore the applicant is required to provide 122 bicycle spaces. The applicant has submitted a variance application to reduce the required number of bicycle parking spaces. The applicant has since then withdrawn their Variance application and has shown two locations in which the parking will be provided, 17 at the front entrance of the building and 105 along the south elevation of the building. Therefore the long-term bicycle parking space requirements will be met.

#### *60.20.20: Off-Street Parking Lot Design*

Technical and design review of the parking lot design will occur with a required Board of Design Review application. Staff find that the proposal is in conformance with this requirement of the Development Code.

#### *Chapter 40.05 Conditional Use Permits*

*Section 40.05.05. Conditional Use Permit: Purpose. Certain types of uses require special consideration prior to being permitted in a particular district. The reasons for requiring special consideration involves among other things, the size of the area required, the nature of the traffic problems, the effect such uses have on any adjoining land uses and on the growth and development of the community as a whole. The uses are listed in each use district as a Conditional Use.*

As previously mentioned, educational institution are specifically listed as a Conditional Use within the R-10 zoning district. The expansion proposal requires the BHS campus to receive conditional use approval through a public hearing process. Special considerations, as noted in Section 40.05.05, will be considered with this request for Conditional Use expansion. Staff find that the proposal is in conformance with this provision of the Development Code.



*Section 40.05.10. The Planning Commission or Planning Director may approve, approve with conditions, or deny the application for a Conditional Use Permit. In permitting a conditional use, the Planning Commission or Planning Director may impose conditions found necessary to protect the best interests of the surrounding property or neighborhood, or the City as a whole. These conditions may include, but are not limited to, requirements increasing the required lot size or yard dimensions, increasing street widths, providing for the construction of public improvements, controlling the location and number of vehicular access points to the property, increasing the number of off-street parking or loading spaces required, limiting the number or size of signs, limiting the coverage or height of buildings, limiting or prohibiting openings in sides of buildings or structures, requiring screening and landscaping, and requirements under which any future enlargement or alteration of the use shall be reviewed by the City and new conditions imposed. A Conditional Use Permit shall not grant variances to the regulations prescribed by this ordinance.*

In review of the proposed Conditional Use expansion, staff believes that conditions of approval, specific to lot size, yard dimensions, street widths, public improvements, vehicular access points, off-street parking, signs, lot coverage, building height and building entrances, will not be necessary. More specific conditions of approval, with respect to landscape, building design and lighting may be required with the Board of Design Review application, which has been submitted. A Design Review request for the building expansion and site design must satisfy the Design Review technical and design standards shall be approved prior to the issuance of site development and building permits.

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**SUMMARY OF FINDINGS for Section 40.05.15.C.1 Conditional Use Approval Criterion:**

*(1). The proposed conditional use will comply with the purpose of this section and with all of the applicable provisions of this ordinance.*

**FINDING:** Staff find that by meeting condition #1, the proposal meets this approval criterion.

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**Criterion (2):**

*The proposed development will comply with the Comprehensive Plan.*

**Facts and Findings:**

The Beaverton Comprehensive Plan is not intended to be a site-specific document. Therefore, the Planning Commission must determine whether a particular specific use request, such as an expansion of an existing public high school facility in the R-10 zone and within a Plan designation of "Schools and Parks," would be in conformance with the Comprehensive Plan.

**The following objectives and policies of the Beaverton Comprehensive Plan relate directly to this request:**

**3.3.1 Land Use Standards and Design Objectives**

- a. *Community appearance should become a major concern and the subject of a major effort in the area. Street tree planting and landscaping, sign regulation, and building improvement and painting programs all contribute to an improved environment.*
- b. *Recognize the future development of the Central Business District as the major element in creating an individual identity for Beaverton. The image of the City should protect the new and the old relating to:*
  - *the Central Business District as a major regional employment and commercial center; and*
  - *the City's past reflected in its older buildings.*
- c. *Concerted efforts should be made to improve the appearance of the community.*
- d. *Use the appearance of the community as a principal means of establishing an individual identity in the metropolitan area.*
- e. *Beaverton should capitalize on its tree-filled environment in establishing a unique urban identity.*

The applicant describes the identify of the campus as a "urban high school campus." The proposed development intends to integrate the campus into a more cohesive unit by vacating SW 2<sup>nd</sup> Street and proposing improvements within the area of the public right-of-way to include the construction of a new two-story building. The design of the building appears to match in design, color, and material to the existing main building with minor differences such as "light control appurtenances" along the elevations, and the materials used to screen the trash / recycling and mechanical units which are perforated metal panels. These panels will also be added to the new canopies over the entrances to the proposed and existing buildings adjacent to the proposed sky bridge. Additionally landscaping and improved pedestrian streetscapes are included with this proposal to improve the overall look of the new and old buildings. Canopy trees, some sight-obscuring hedges and other landscape materials are proposed to provide site buffering at the perimeter and to mitigate the visual impacts to the surrounding parcels.

FINDING: Staff find that the proposal is consistent with these Plan Objectives.

### **3.3.2 Land Use Policies**

*c. Older sections of the community should be carefully evaluated and efforts made to rehabilitate or redevelop those areas requiring special treatment.*

The BHS campus is located in an area adjacent to the city's downtown area and an established residential neighborhood. It provides a connection between the multiple uses both commercial and residential of the downtown area with low-density residential homes. The main building and the Merle Davies building, in fact has been a part of this established neighborhood for decades and encompasses a large portion of the neighborhood. Any development within the campus will need to incorporate the design elements of the existing building to encourage continuity. The applicant has stated "The proposed cafeteria and associated parking will be developed with recognition of the existing historic Merle Davies building and the existing Beaverton High School building and the surrounding area." The proposed building is to be located between the main high school building and Merle Davies. The proposed building is designed as a two-story facility and is to be made of a stucco finish similar to the main high school building. The proposal includes a sky bridge connecting the main building to the proposed cafeteria building. The design of the cafeteria building appears to provide a continuation to the design of the existing main building. The proposed location of the building is at a minimum 12 feet from the Merle Davies building, an identified historic building. Although the materials are not similar to those found on the Merle Davies building, the applicant has provided materials that will complement in color to the brick façade of this historic building. Additionally the applicant is not proposing any changes to the existing Merle Davies building, leaving the historic design of the building in tact. The proposed development adequately reflects the existing design of the BHS campus and the urban character of this section of downtown Beaverton.

The applicant is also proposing the construction of three parking lots. The two parking lots located east and west of the existing cafeteria will provide additional buffering from the building to the surrounding properties and provide a modified entrance to the existing facility. Cars entering the facility will be provided with drop-off and pick-up areas on both sides of the cafeteria as well as additional parking. The location of the third new parking lot is south of the existing tennis courts. The building location and the proposed parking lots will be reviewed by the Board of Design Review whereby a more detailed review of the design elements will occur.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

- f. Efforts should be made to obtain ten-foot landscape strips along either side of Canyon Road, the Beaverton-Hillsdale Highway, the Tualatin Valley Highway, Murray Boulevard, Cedar Hills Boulevard, and along other major streets in the planning area which are bordered by commercial or industrial developments. Landscaped median strips should also be installed in these streets wherever possible.*

No additional right-of-way is being provided with this development on SW Farmington Road. The applicant has stated that the existing landscaping and shade trees along the northern boundary of the BHS campus, abutting SW Farmington Road will not be altered by this development. Although the trees along SW Farmington Road are not affected, the proposed location of the pedestrian plaza will require that landscaping be removed in the vicinity of the pedestrian plaza at the northeast area of the campus. The removal of existing grass area is not expected to affect the established trees in the area and the impacts to the open space and landscaping to the overall campus is minimal.

However, staff find this land use policy does not address the BHS site because it does not abut the roadways identified in the policy or is located along a major street bordered by commercial or industrial development. This portion of SW Farmington has the railroad tracks located to the north.

FINDING: Staff find that the Plan Policy is not applicable to the proposal.

### **3.4.2 Residential Objectives**

- 3.4.2.1 The primary focus of residential development should be towards maintaining or creating maximum livability and promoting quality living areas.*
- 3.4.2.2 All residential areas should be provided with services and facilities necessary for safe, healthful, convenient urban living.*
- 3.4.2.3 Residential areas should be developed in ways which are consistent with the geographic features so as not to create health or erosion hazards.*

The proposed development is an expansion of an existing use. The proposed building expansion is located between two existing buildings on the existing BHS campus. The applicant has indicated that the development would include improved pedestrian and bicycle facilities, specifically an east-west connection located on what is currently SW 2<sup>nd</sup> Street right-of-way, which would provide improved pedestrian connection to the surrounding neighborhood. The safety features including the raised crosswalks and the extended curbs are to improve pedestrian safety of the students and surrounding neighborhood using the buildings and the existing recreational fields. These improvements would also improve the connections to pedestrian and bicyclist who use other parts of the neighborhood through the BHS campus. The proposed development is an expansion of the BHS campus to improve the existing facility for both students and the community who

uses the facility after hours. The applicant has stated that the proposed development site is essentially flat in terms of elevation and that the development would not create any undue health or erosion hazards, and would be built to federal, state, and building code standards for schools. Issues regarding the technical design of the application are evaluated as part of the Design Review application.

FINDING: Staff find that the proposal is consistent with these Plan Objectives.

***Goal 6.2.2: A balanced transportation system.***

***Policies***

- a. *Develop and implement public street standards that recognize the multi-purpose nature of the street right-of-way for utility, pedestrian, bicycle, transit, truck, and auto use, and recognize these streets as important to community identity as well as providing a needed service.*
- b. *Provide connectivity to each area of the City for convenient multi-modal access.*
- c. *Develop a safe, complete, attractive, and efficient system of pedestrian ways and bicycle ways, including bike lanes, shared roadways, off-street pathways, and sidewalks according to the pedestrian and bicycle system maps.*

If the Street Vacation request is approved, development of the campus will occur in the existing public right-of-way. Currently there are pedestrian, bicycle, and utility uses found within the SW 2<sup>nd</sup> Street right-of-way. The applicant has stated that the use of the area within the SW 2<sup>nd</sup> Street right-of-way for the proposed development does not preclude its use by utilities, pedestrians and bicyclists. The proposed development identifies improvements including new bicycle and pedestrian streetscapes that will be more ADA compliant. A pedestrian plaza is proposed with this development at the northeast corner of the campus along SW Farmington Road to provide improved facilities for transit users in the area.

The issues of connectivity and street spacing of SW 2<sup>nd</sup> Street are addressed with the Street Vacation application and not with the proposed development. Approval of the CUP request assumes the approval of the Street Vacation by City Council.

FINDING: Staff find that the proposal is consistent with these Plan Policies.

- d. *Design arterial and collector streets to accommodate pads for public transit.*

As part of the development application, the proposal includes the addition of a pedestrian plaza at the northeast area of the campus. The applicant has included the facility in order to request a 10% reduction in the minimum required parking spaces for the site. The pedestrian plaza is identified as being 300 square feet in size. The applicant is working with Tri-Met in the design of the pedestrian plaza.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

**6.2.3. Goal: A safe transportation system.**

*6.2.3.e) Designate routes to schools for each school and to and from any new residential project.*

Although the proposed development is for the expansion of the BHS campus and not a new residential development, the applicant has identified safety improvements that were incorporated with the proposed pedestrian development to improve the outer edges of the campus that connect to routes in the surrounding area. The improvements include raised crosswalks along SW Erickson Avenue, a turn around area west of the cafeteria, and the pedestrian facilities will be well lit in the evening. As identified in their response, if the Street Vacation is approved, vehicular access along SW 2<sup>nd</sup> Street is removed, improving the safety pedestrian and bicycle access on this portion of SW 2<sup>nd</sup> Street. Pedestrian and bicycle routes along SW 2<sup>nd</sup> Street are to remain, but modified to accommodate the proposed cafeteria building and enhanced with an improved streetscape. The applicant also states "Pedestrian facilities will be designed to enhance access and connectivity for pedestrians with wheelchairs, including curb and intersection treatments and easier access to the front of the high school building, the auditorium, athletic fields, and the proposed cafeteria."

The applicant has proposed having buses provide drop-off and pick-up along SW Erickson and SW Stott Avenue, with special needs buses using the turn around in the main driveway entrance off of SW Erickson. The proposed drop off areas would provide a safe location for students to be loaded and unloaded, as they would have access to the school from the improved streetscapes proposed with the development.

Staff does agree that the improvements identified by the applicant will assist in providing improved safety to buses, pedestrians and bicyclists using the site, which will meet the need created by the proposal.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

**6.2.4 Goal: An efficient transportation system that reduces the number of trips and limits congestion.**

**Policies:**

- a. *Support trip reduction strategies developed regionally, including employment, tourist, and recreational trip programs.*

The applicant has stated that students predominately arrive by school bus. Staff agrees with the applicant that school busing does provide a method of substantially reducing trips on the transportation system and reduces parking. Additionally the proposal to include a pedestrian plaza along SW Farmington provides an amenity to

the surrounding area to encourage using buses along SW Farmington Road. The proposed development provides amenities to support trip reductions to the BHS campus.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

b. *Limit the provision of parking to meet regional and state standards.*

The City's Development Code establishes minimum and maximum parking standards and opportunities to reduce these standards with transit amenities that meet Metro's Urban Growth Management Functional Plan Title 2 requirements. As identified in criterion #1, the applicant has shown how the proposed development will meet the minimum and maximum parking standards with the 10% reduction allowed by the Development Code with the construction of a pedestrian plaza.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

c. *Maintain level of service consistent with regional goals. Reduce traffic congestion and enhance traffic flow through such measures as intersection improvements, intelligent transportation systems, signal synchronization, and other similar measures.*

The applicant has stated that according to the DKS traffic analysis dated December 2001 provided with this application "...the existing street system can adequately service the incremental shift in traffic volumes that would occur as a result of the street vacation." The traffic analysis addresses the vacation of SW 2<sup>nd</sup> Street with a worse case scenario of a school population of 2200 people. If the street vacation were approved the level of service even with the proposed development will remain adequate based on the traffic analysis. The applicant also states that the proposed development will not add student capacity beyond levels that have historically been found at the school.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

#### **Section 6.3.4 Transportation Policies:**

a) *Each parcel of property shall be required to provide and maintain safe and reasonably convenient access to the public street system.*

The BHS campus is accessed from SW 2<sup>nd</sup> Street, SW Erickson and SW Stott Avenue. If the proposed Street Vacation is approved, then vehicular access is removed on SW 2<sup>nd</sup> Street. The applicant has proposed certain public street improvements such as crosswalks, extended curbs and improved sidewalks to provide additional safety measures for the pedestrians using the campus. The location of the two access points along SW Stott and SW Erickson will distribute

the number of students being dropped off or driving to the campus. The access will be subject to the improvements required as conditions of approval. The location of the access provides reasonably safe and convenient access to the public street system. Staff find that Policy “a” for safe and reasonably convenient access to public streets has been met.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

- c) In residential areas, driveway access onto minor collector streets shall be discouraged; access shall primarily be by local streets.***

The BHS campus abuts four public streets, SW 2<sup>nd</sup> Street, SW 3<sup>rd</sup> Street, SW Erickson and SW Stott Avenues. Access to the campus from SW Stott Street and SW 3<sup>rd</sup> Street, the only local streets are provided along with access from SW Erickson. If the proposed Street Vacation requested by the applicant is approved, access to the other public streets remains. The two access points from SW Stott and SW Erickson to the proposed building is to remain, with an additional access pointed added to parking lot ‘H’ along SW Erickson Street. Access from SW Erickson to SW 2<sup>nd</sup> Street, on the east side of SW Erickson, will be changed with the proposed street vacation and the proposal. The access point to the school will be moved north and will be constructed as the main driveway of the school aligning with SW 2<sup>nd</sup> Street to the west of SW Erickson. Parking lot ‘H’ is to provide the campus with needed parking spaces.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

- d) Non-residential access to arterial and major streets shall be primarily via the non-residential collector street system in order to preserve the primary function of the arterial street, which is to move traffic through the community. Where access to the collector system is not feasible, access shall be limited; and means of reducing access points, such as combining multiple points of access or developing frontage drives and roadways, shall be utilized where feasible.***

The existing and proposed facilities on the BHS campus do not access SW Farmington Road, the only arterial abutting the campus. With the exception of the parking facility located south of the existing tennis courts, the campus site layout will make use of the access points from SW 2<sup>nd</sup> Street in the redesign of the driveway and parking lots. The proposal includes improved access connection at the intersection of SW 2<sup>nd</sup> and SW Erickson. With approval of the Street Vacation, the applicant will shift the existing access from Erickson onto SW 2<sup>nd</sup> to the north thus aligning the proposed driveway with the portion of SW 2<sup>nd</sup> Street not being vacated.

FINDING: Staff find that the proposal is consistent with this Plan Policy.



- n) *In order to alleviate congestion on the public street system, each private property owner shall be responsible for providing adequate on-site parking, loading, and internal circulation based upon adopted standards in the city's development code.*

The Development Code Section 60.20.10.5 *Off-Street Parking* identifies the parking space requirements for educational institutions. As addressed in criterion #1, the applicant is requesting a 10% reduction in the minimum number of parking spaces required by providing a pedestrian plaza. The 10% reduction would allow the proposed development to meet the minimum and maximum parking standards. On either side of the proposed cafeteria, the proposed development includes a parking lot with areas for student drop-offs and pick-ups. These turn-around areas will improve the internal circulation of the campus by providing a convenient area, in front of the main entrance of the school for quick drop-offs and pick-ups. Additionally with the removal of the gates that currently close the campus during school hours, the main driveway will have parking spaces open to those needing to visit the BHS campus during school hours. Staff find that the proposed internal circulation of the campus would be improved with the proposed development.

FINDING: Staff find that the proposal is consistent with this Plan Policy.

### ***7.2.1 Parks and Recreation Planning Objectives***

- a. *Schools, park, and recreation development should be considered as a vital part of the future livability of the area, and efforts should be made to keep these facilities abreast of growth.*
- b. *Provide for schools, parks, and natural open space throughout the city in locations that are easily accessible to those they are intended to serve.*
- c. *Help offset higher residential densities and intense commercial developments in the community with parks and open spaces.*

The proposed development is within the campus of an existing high school on a site designated by the Comprehensive Plan for use by "Schools and Parks". The applicant is proposing to expand the site to provide improve use of the facility for the community and the students who use the campus.

The capacity of the school is projected to be at student enrollment levels found prior to the opening of the recently completed Southridge high school in south Beaverton. The proposed development is intended to provide a modern educational facility with improved ADA compliant community accessible space. The proposed cafeteria building would provide additional space in the existing building for increased educational opportunities.

Pedestrian and bicycle facilities would be enhanced through the campus and the surrounding area with the proposed development. Safety amenities are provided along SW Stott and SW Erickson which include raised and marked crosswalks and extended curbs to improve the accessibility of students and other users of the campus and those accessing other parts of the downtown or residential areas through the campus.

There are existing open spaces and athletic fields located along the west, south, and north sides of the campus. The applicant has shown on their plans that with the proposed development, 48% of the campus will be made up of open space area, mainly existing recreational fields. Although development will result in an 8% percent reduction, the campus will still provide useful open space area for the students, the surrounding neighborhood, and the community.

FINDING: Staff find that the proposal is consistent with these Plan Objectives.

### **7.3.1. Cultural and Historic Resources Objectives**

- a) *Protect, where possible, cultural and historic resources in order to enhance and perpetuate landmarks and districts representing or reflecting elements of the city's cultural, social, economic, political and architectural history.*
- b) *Stabilize and improve property values in historic districts and other designated landmarks.*
- c) *Foster civic pride in the beauty and noble accomplishments of the past.*

The proposed development includes the addition of a new two-story building located between the main building and the Merle Davies School. Merle Davies is a historic landmark. The proposed development is located 12 feet from the Merle Davies building and does not include any modifications to the building. However, the proposed development does include the removal of the historic trees along SW 2<sup>nd</sup> Street. The removal of the trees is for the construction of the cafeteria building and the new driveway entrance off of SW 2<sup>nd</sup> Street, and parking areas. A request to remove the historic trees is being reviewed by the Historic Resource Review Committee with a Tree Preservation Plan application.

As stated by the applicant, the proposed development will not affect the property value as the proposed development is located within an area incorporated into the BHS campus and does not alter the existing historic building, Merle Davies.

FINDING: Staff find that the proposal is consistent with these Plan Objectives.

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**SUMMARY OF FINDINGS for Section 40.05.15.C.2 Conditional Use  
Approval Criterion:**

**(2) *“The proposed development will comply with the Comprehensive Plan.”***

**FINDING:** Staff find that by satisfying the conditions identified at the end of this report, the proposal meets this approval criterion.

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**Criterion (3):**

***That the location, size, design, and functional characteristics of the proposed use are such that it can be made reasonably compatible with and have a minimum impact on the livability and appropriate development of other properties in the surrounding neighborhood.***

**Facts and Findings:**

**Location:** The subject property is zoned Urban Low Density Residential R-10. Although the site is zoned residential it is used as an education institution consistent with the Plan designation of “Schools and Parks”. The BHS campus has some residential uses on the south, east and west side of the campus, which the school provides buffering for some residential homes in the area from the commercial uses north of the BHS campus and from SW Farmington Road. The proposed development includes a two-story cafeteria building with two parking facilities and driveways east and west of the building. The cafeteria building would be located between two existing buildings, the main high school building and Merle Davies. The proposed development will have the greatest visual impacts to properties to the east and west of the proposed building. However the impacts are minimized with the proposed building located approximately 300 feet from SW Erickson and approximately 120 feet away from SW Stott Street.

The parcel on the west side of SW Erickson, south of the existing tennis courts is proposed for use as a parking lot. The parking lot is surrounded by vegetation, such as canopy trees, continuous hedges, and other plant materials to minimize the visual impacts of the cars using the facility. Since no buildings are proposed on this parcel, there are less visual impacts to the surrounding residential area.

The height of the proposed addition is 34 feet, three feet higher than the high school building in close proximity. However, the auditorium building, which is 60 feet in height, is substantially higher than the proposed cafeteria. With the current setback of the existing buildings and the setback provided for the proposed building, visual impacts will be minimal to the surrounding residential area.

Size: The proposed expansion, at approximately 30,000 square feet, will be a separate building connected to the main high school building by a sky bridge. Existing and proposed landscaping and the design of the streetscape are to improve the current layout of the BHS campus to effectively connect the buildings together. Landscape mitigation will assist in buffering and minimizing the impact of parking lot 'H' to the surrounding residential area and trees and other vegetation are proposed in the surrounding area of the new building. The Board of Design Review will review the proposed Landscape Plan during the Design Review application.

Design: The applicant has submitted a conceptual drawing of the elevations of the proposed building addition. The main building is made up of stucco and the Merle Davies building is brick. The design of the proposed building is such that it is consistent with the design of the main building and does not result in alterations to the historic Merle Davies building. The elevation of the proposed building provides a number of windows that compliment the windows found on both existing structures. The colors and the awnings proposed are similar to the color of the existing building and compliment the color of the brick façade on the Merle Davies building. Building and site design of the proposed expansion is subject to Board of Design Review approval.

Functional Characteristics: In response to the applicant's request for Conditional Use expansion, staff recognizes the primary use of the facility is a public high school. However, as identified by the applicant the proposed facility is used by the community during after-school hours. This request for Conditional Use approval will expand existing school facilities. However, staff believes that such expansion will not significantly expand existing after school and public uses of the facility that are not already found at the site to create an impact on existing streets that serve the site area. Therefore, staff believes that the functional characteristics of the proposed expansion, assuming that the SW 2<sup>nd</sup> Street Vacation is recommended by the Planning Commission to the City Council for approval, will have only minimal impacts on neighboring residential properties.

Noise: No significant issues related to noise have been identified with the proposed building expansion. The proposed building expansion will be completely enclosed.

Hours of Operation: Although the school is open five days a week during school hours, the facilities are used for after school programs and community events in the evenings and weekend as well. These uses already occur on the site and are not a result of the proposed development. The proposed development intends to improve the facility currently being used by students and the community.

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**SUMMARY OF FINDINGS for Section 40.05.15.C.3 Conditional Use Approval Criterion:**

*(3) "That the location, size, design, and functional characteristics of the proposed use are such that it can be made reasonably compatible with and have a minimum impact on the livability and appropriate development of other properties in the surrounding neighborhood."*

**FINDING:** Staff find that the proposal meets this approval criterion.

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**CONDITIONAL USE CONCLUSION**

Based on the facts and findings presented, staff conclude that by satisfying the conditions of approval, the proposal, **CUP2001-0031 (Beaverton High School Cafeteria and Parking Lot)**, meets the criteria.

**RECOMMENDATION**

Based on the facts and findings presented, staff recommend **APPROVAL** of **CUP2001-0031 (Beaverton High School Cafeteria and Parking Lot)**, subject to the following conditions:

**CONDITIONS OF APPROVAL**

1. Prior to obtaining a Site Development Permit, the applicant shall provide staff with a copy of a recorded lot consolidation of all parcels in the area of the proposed building in accordance with Washington County's requirements.
2. The permit granted shall run with the land and shall continue to be valid upon a change of ownership of the site or structure unless otherwise specified in conditions attached to the permit.
3. Prior to issuance of a building permit, the applicant shall, in accordance with Section 40.05.15.2.D.2 of the Beaverton Development Code, file a copy of this approved permit with the Washington County Department of Records and Elections.

4. Building permits must be secured prior to construction. For further information regarding building permits and/or related building code issues, please call 526-2403.
5. A Site Development Permit shall be secured prior to any site clearing, grading, parking lot paving, and public works construction.

### EXHIBITS

- Exhibit 1. Vicinity Map (located on page 2 of the Staff Report)
- Exhibit 2. Applicant's Material Packet
- Exhibit 3. Reduced Plans
- Exhibit 4. DKS Associates Traffic Impact Study dated December 2001
- Exhibit 5. DKS Associates Memorandum dated January 25, 2002
- Exhibit 6. Tri-Met Letter dated February 15, 2002

**Revised Conditional Use Application  
Beaverton High School Improvements**

- New Cafeteria
- New Parking Lots
- Height of Cafeteria

**Beaverton High School  
13000 SW 2<sup>nd</sup> Street  
Beaverton, Oregon 97005**

**RECEIVED**  
**MAR 11 2002**  
City of Beaverton  
Development Services

Prepared for:

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**Revised January 2002**

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3. REVISED EXISTING CONDITIONS PLAN	13. REVISED LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (EAST)
4. REVISED DIMENSION PLAN PUBLIC IMPROVEMENTS	14. REVISED LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS WITH LOT H (EAST)
5. REVISED DIMENSION PLAN PRIVATE IMPROVEMENTS (WEST)	15. REVISED DETAIL SHEET FOR WATER QUALITY CONSTRUCTION
6. REVISED DIMENSION PLAN PRIVATE IMPROVEMENTS (EAST)	16. REVISED DETAIL SHEET
7. REVISED DIMENSION PLAN PRIVATE IMPROVEMENTS WITH LOT H (EAST)	17. REVISED DETAIL SHEET BIKE RACK DETAIL
8. REVISED GRADING, STORM AND EROSION CONTROL PUBLIC IMPROVEMENTS	18. REVISED TREE PRESERVATION PLAN
9. REVISED GRADING, STORM AND EROSION CONTROL PRIVATE IMPROVEMENTS (WEST)	19. REVISED ARCHITECTURAL ELEVATIONS
10. REVISED GRADING, STORM AND EROSION CONTROL PRIVATE IMPROVEMENTS (EAST)	

### APPENDICES

- APPENDIX A  
APPLICATION FOR DEVELOPMENT REVIEW
  
- APPENDIX B  
FOUR-PAGE CONDITIONAL USE PERMIT SUBMITTAL CHECKLIST
  
- APPENDIX C  
NEIGHBORHOOD REVIEW MEETING INFORMATION
  
- APPENDIX D  
CLEAN WATER SERVICES SENSITIVE AREAS CERTIFICATION FORM
  
- APPENDIX E  
WATER QUALITY REPORT, HARPER HOUF RIGHELLIS



## I. Summary

**Applicant:**

Beaverton School District #48  
16550 SW Merlo Road  
Beaverton, Oregon 97006-5152  
Mike Maloney  
Phone: (503) 591-4303  
Fax: (503) 591-4484  
Mike\_Maloney@beavton.k12.or.us

**Applicant's Representative:**

Frank Angelo  
Angelo Eaton & Associates  
620 SW Main Street, Suite 201  
Portland, Oregon 97205  
Phone: (503) 227-3664  
Fax: (503) 227-3679  
fangelo@angeloeaton.com

**Applicant's Architect:**

Russ Pitkin  
LSW Architects, P.C.  
1953 NW Kearney Street  
Portland, Oregon 97209  
Phone: (503) 274-5432  
Fax: (503) 274-0085  
rpitkin@lsw-architects.com

**Applicant's Engineer:**

Gary Alfson  
Harper Houf Righellis, Inc.  
5200 SW Macadam  
Suite 580  
Portland, Oregon 97201  
Phone: (503) 221-1131  
Fax: (503) 221-1171  
gary@hrcivil.com

**Applicant's Landscape Architect:**

Ron Matela  
Matela Associates Inc.  
601 Main Street #201  
Vancouver, WA 98660  
Phone: (360) 695-0776  
Fax: (360) 695-0776  
rmatela@pacifier.com

**Site Address/Location:** Beaverton High School  
13000 SW 2<sup>nd</sup> Street  
Beaverton, Oregon 97005

**Map and Tax Lot #:** 1S116AD tax lots 11100, 11000, 02900, 07100  
and 10900  
1S116AC tax lots 02100 and 02500

**Zoning:** R-10

**Site Size:** 23.63 acres

**Request:** Conditional Use for (1) a School Use and parking in the R-10 District and (2) proposed building height higher than the maximum building height allowed in the R-10 District.

**Surrounding Properties:** Land to the south is used as the Beaverton High School campus. Land to the east is campus and residential. Land to the north, across Farmington Road, is primarily commercial. Some of the land to the east is owned by the school district and some is residential.

## **II. Introduction**

The Beaverton School District (BSD) is seeking revisions to the existing Beaverton High School (BHS) campus (Revised Site Improvement Plan, Figure 1) in order to provide a new cafeteria and to create a more urban high school campus that is integrated with the surrounding Beaverton urban area. As part of the funded improvement program, the improvements will include:

- A new cafeteria building located north of the existing BHS main building;
- Renovation to the interior of the existing high school to enhance educational facilities;
- Additional parking; and
- Improvements to the front and entrance to BHS.

The improvements at BHS are being designed not only to provide a modern educational facility but also to provide the community accessible space through pedestrian plazas and open space. The improvements will include measures to improve compliance with the Americans with Disabilities Act (ADA), such as easily accessible parking near the school entrance and auditorium and pedestrian system enhancements. The development will include the addition of one new building to be used for expanded cafeteria facilities. The new building will be located immediately north of the existing high school structure. In recent years Beaverton High School student population has ranged from 1,800 to 2,200 students (prior to the opening of Southridge High School). The Beaverton School District expects that enrollment will not exceed 2,200 in the future. The proposed improvements will enable the Beaverton School District to effectively accommodate curriculum for this range in student population. The campus improvements are intended to enable the District to enhance the educational facilities at BHS and improve the campus environment for students and the overall community.

Improvements will be made to the front of BHS to enhance the pedestrian environment and to clearly identify the main entrance. A more urban streetscape will be developed along the northern face of BHS. These improvements will emphasize pedestrian and bicycle access to and through the site. A strong pedestrian and bicycle connection between SW Stott and SW Erickson will be promoted through the streetscape improvements.

The cafeteria addition, renovations to the existing high school, and the new parking lots are funded improvements through a school bond measure approved by district voters. There are no other funded improvements at BHS that will require land use review at this time.

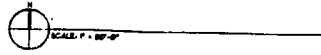
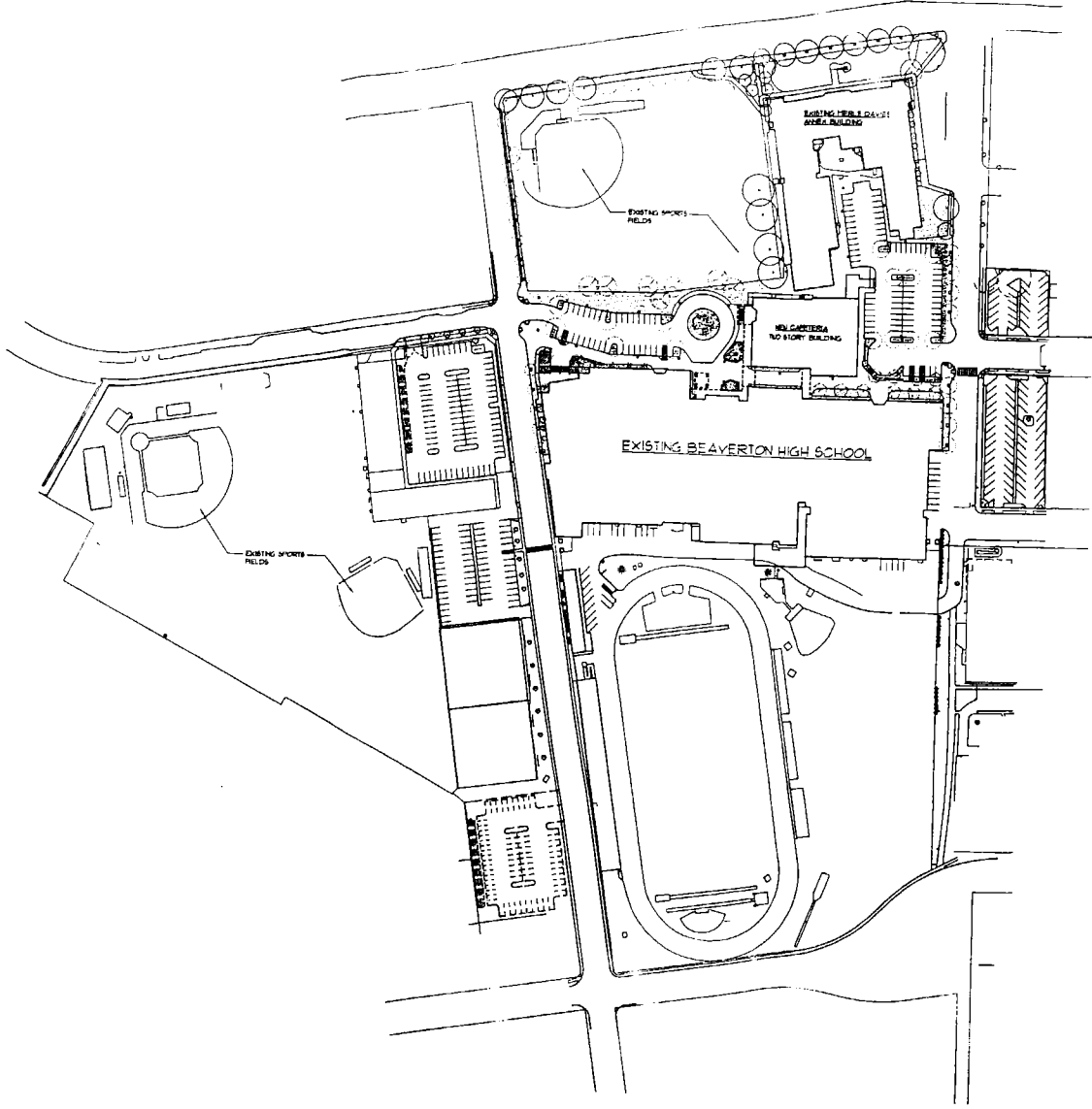
### **Conditional Use Application**

This Revised Conditional Use application and companion land use applications include the immediate development of a two-level, 30,000-square foot cafeteria building located directly north of and connecting to the existing high school building. This revised application is requesting Conditional Use Permit approval for the following improvements:

- New Cafeteria Building

A Conditional Use Permit is required for a new cafeteria building in an R-10 zone per Beaverton Development Code Section 20.05.10.2.B(5) – (Educational Institutions in an R-10 zone). The proposed cafeteria would form part of the Beaverton High School campus, which is an educational institution, which is not permitted outright in the R-10 zoning district. The

P:\1151-202P-444  
 Date: 01-23-07  
 Time: 3:30 PM



**ARCHITECTURE**

PLANNING/INTERIORS

Tel (503) 224-6432  
Fax (503) 224-0085

1053 NW Kearney St.  
Portland, OR 97209

**PRELIMINARY**

**B.H.S. CAFETERIA ADDITION**  
**BEAVERTON SCHOOL DISTRICT #48**  
 1,3000 S.W. SECOND ST.  
 BEAVERTON, OREGON 97005

Title: \_\_\_\_\_  
 Revision: \_\_\_\_\_  
 Date: 1-9-07  
 Job No: 0151  
 Drawn by: EGN  
 Checked by: JOW

Scale: **2.0**

**Figure 1. Revised Site Improvement Plan**

new cafeteria building would be located immediately north of and connected to the existing BHS building on a vacated portion of SW 2<sup>nd</sup> Street (see Street Vacation/Comprehensive Plan Amendment application, filed November 15, 2001). It is necessary to place the cafeteria at this location to take advantage of student circulation patterns and because the existing campus site is constrained in terms of where facility development can occur. There are no development opportunities to the east or west of the existing BHS building. Development of facilities to the south would eliminate athletic facilities that could not be replaced on campus and would disrupt current physical plant operations.

- **New Vehicle Parking Areas**

Three new parking lots will be constructed as part of the proposed improvements. The parking areas require a Conditional Use Permit as accessory uses to an Educational Institution in an R-10 zone, per Beaverton Development Code Section 20.05.10.2.B. Parking Lot C (Revised Cover Sheet, Attachment 1) will provide 79 standard parking spaces and three handicapped spaces. This lot is located immediately to the east of the new cafeteria building. Parking Lot D, noted on the same drawing, will provide 31 standard parking spaces and four handicapped spaces. This lot will be located in the vacated portion of SW 2<sup>nd</sup> Street. These spaces will replace the existing 11 parking spaces in the SW 2<sup>nd</sup> Street right-of-way. A third lot, Parking Lot H (Revised Cover Sheet, Attachment 1), will provide an additional 70 parking spaces on property currently owned by the Beaverton School District located directly south of the tennis courts adjacent to SW Erickson Avenue. Student vehicle parking is controlled through a permit system.

The addition of the three new parking lots (C, D, and H) will raise the total parking on the Beaverton High School campus to 436 spaces (426 standard spaces and 10 handicapped spaces). Based on the existing (2000-2001) student enrollment of 1,800 students and employment of 150 staff, the parking requirement at 0.2 spaces per FTE student and staff is 390 spaces. The proposed number of parking spaces (436) meets the parking requirement based on current student enrollment and staff numbers (390).

However, based on a potential capacity of 2,200 students and 200 staff, the parking requirement at 0.2 spaces per FTE student and staff is 480 spaces. The proposed number of parking spaces would not meet the parking requirement if student enrollment was to increase to 2,200 and the number of staff was to increase to 200. The campus is directly served by Tri-Met (on Farmington Road). As part of the Revised Design Review Application, the Beaverton School District is seeking to utilize the City's 10 % parking reduction credit for construction of a 300-square foot pedestrian plaza and transit improvements as described in Beaverton Development Code Section 60.20.10.10.A.2. The Beaverton School District would locate these improvements at the existing location of the Tri-Met bus stop on Farmington Road, adjacent to the school campus (see Revised Dimension Plan Public Improvements, Attachment 4). Discussions with Tri-Met have indicated that Tri-Met is supportive of the proposed transit stop enhancement.

The 10% parking reduction credit would reduce the parking requirement for Beaverton High School from 390 to a minimum of 351 spaces based on current enrollment and staff numbers (1,800 students and 150 staff), and would reduce the parking requirement for Beaverton High School from 480 to a minimum of 432 spaces based on potential enrollment (2,200 students and 200 staff). The proposed addition of Parking Lots C, D and H, combined with the 10% parking requirement reduction for parking amenities, enables Beaverton High School to meet

the City's parking requirements. These parking lots will all be screened from adjacent land uses with a site-obscuring hedge, fence or planting per Beaverton Development Code Section 60.20.20.2.A. Due to school bus maneuverability needs, 19 on-street parking spaces along SW Street and SW Erickson Avenue will be used from 7:00am to 4:00pm on school days for school bus transportation purposes. These areas will be appropriately signed.

**Beaverton High School Vehicle Parking**

**Table 1**

<b>Lot</b>	<b>Current Number of Parking Stalls</b>	<b>Proposed Number of Parking Stalls</b>
A	25	25
B	56	56
C	51	82
D*	0	35
E	80	80
F	56	56
G	32	32
H*	0	70
2 <sup>nd</sup> Street	11 **	0
<b>TOTAL</b>	<b>311</b>	<b>436</b>

\* These are new lots

\*\* This lot will be removed, pending street vacation along 2<sup>nd</sup> Street (stalls outside right-of-way).

NOTE: Current parking within the 2<sup>nd</sup> Street right-of-way is 61 spaces, which will be removed pending the street vacation along 2<sup>nd</sup> Street.

- A Conditional Use Permit is required for the height of the proposed cafeteria building in an R-10 zone per Beaverton Development Code Section 20.05.10.2.B(12) – (Structures over 30 feet in height in an R-10 zone). In order to accommodate a two-story cafeteria structure that complements the existing Beaverton High School facility, the proposed height of the cafeteria building is 39.6 feet. Height for the existing Beaverton High School building reaches a 50-foot maximum height over the auditorium.

**Additional Applications**

Aside from the Conditional Use Permit application, the proposed improvements at the Beaverton High School campus also require submittal of the following applications:

- Street Vacation Permit Application for the vacation of SW 2<sup>nd</sup> between SW Stott and SW Erickson;
- Comprehensive Plan Amendment to remove the Neighborhood Route Designation of SW 2<sup>nd</sup> between SW Stott and SW Erickson from the City's Functional Classification Plan Map;
- Design Review Type 3 Application;
- Tree Preservation Plan/Historical Tree Review;
- Bicycle Parking Variance-Public Hearing.

The Street Vacation Permit Application and the Comprehensive Plan Amendment applications were submitted to the City of Beaverton on November 15, 2001. The Revised Design Review and Revised Tree Preservation Plan/Historical Tree Review applications have been re-submitted concurrently with this Revised Conditional Use Permit application. A Bicycle Parking Variance-Public Hearing application for the reduction of the number of bicycle parking spaces required on

the Beaverton High School campus has also been submitted concurrently with this Revised Conditional Use application.

Appendix A includes the Application for Development Review and Appendix B includes the four-page conditional use permit submittal checklist. Appendix C includes Neighborhood Review Meeting information from the Neighborhood Review meeting held on September 20, 2001, as required per Beaverton Development Code Section 50.10.B.5(a-g) for a Conditional Use Permit – Public Hearing application submittal. Appendix D includes a Clean Water Services Sensitive Areas Certification Form and Appendix E includes a Water Quality Report.

### III. Applicable Development Code/Conditional Use Approval Criteria

The Conditional Use Permit – Public Hearing Written Narrative Requirements Submittal Checklist identifies three general approval criteria, as listed in Beaverton Development Code Section 40.05.15.2.C:

- 1) *The proposed conditional use will comply with the purpose of this section and with all of the applicable provisions of this ordinance.*
- 2) *The proposed development will comply with the Comprehensive Plan.*
- 3) *That the location, size, design, and functional characteristics of the proposed use are such that it can be made reasonably compatible with and have minimum impact on the livability and appropriate development of other properties in the surrounding neighborhood.*

The following responses to the three general approval criteria include more specific criteria that were identified as applicable to this Revised Conditional Use application as a result of an August 21, 2001 Pre-Application Conference held with Beaverton staff.

#### **General Approval Criterion #1. The proposed conditional use will comply with the purpose of this section and with all of the applicable provisions of this ordinance.**

**RESPONSE:** This criterion refers to Beaverton Development Code Section 40.05.15.2.C. and applicable provisions of the Beaverton Development Code. The proposed conditional use will comply with the purpose of Section 40.05.15.2.C. The proposed cafeteria and parking will enhance the educational environment for students at Beaverton High School as well as provide an amenity for the community in terms of public meeting space and an improved pedestrian and bicycle facility to and through the Beaverton High School campus. The new cafeteria building will free up space currently used as a cafeteria in the existing high school building, which will offer additional space for modernization and further development of educational facilities.

The Conditional Use Permit application will follow the specifications as identified in Beaverton Development Code Section 40.05.15.2 for a Conditional Use Permit – Public Hearing. The proposed conditional use will also comply with other applicable provisions of the development ordinance; the proposed cafeteria building and parking will comply with standards included in Beaverton Development Code Section 20.05.10 (R-10 Single-Family District) as well as other site standards.

The area within the project limits is currently designated R-10 (Urban Low Density, 10,000 square feet) per the City of Beaverton Development Code. Pursuant to Section 20.05.10 of the Development Code, the R-10 zoning district identifies educational institutions (including public schools) as a conditional use, subject to Section 40.05 of the Code. City staff stated in the Pre-Application Conference Letter (October, 8, 2001) that the following sections of the Development Code are applicable to the project:

- Setbacks
- Landscaping



- Vehicular parking
- Bike parking

Compliance with Beaverton Development Code Section 20.05. is addressed as follows:

**Setbacks:** The minimum front setback in the R-10 Zone is 25 feet. As illustrated on the Revised Existing Conditions Plan, Attachment 2, current and proposed development on the site meets this requirement.

**Landscaping:** According to Code, the minimum amount of landscaping on the site is 15% of the total site area. The Revised Landscape and Lighting Plan Public and Private Improvements Plans, Attachments 11, 12, 13 and 14, illustrate that this proposal provides 44,000 square feet or 68% of the site. This exceeds the amount required by the Code. Landscaping for all parking lots will be in conformance with Section 60.20.20.2A Off Street Parking.

**Vehicular and Bike Parking:** Three new parking lots will be constructed as part of the proposed improvements. Parking Lot C (Revised Cover Sheet, Attachment 1) will provide 79 standard parking spaces and three handicapped spaces. This lot is located immediately to the east of the new cafeteria building. Parking Lot D, noted on the same drawing, will provide 31 standard parking spaces and four handicapped spaces. This lot will be located in the vacated portion of SW 2<sup>nd</sup> Street. These spaces will replace the existing 11 parking spaces in the SW 2<sup>nd</sup> Street right-of-way. A third lot, Parking Lot H (Revised Cover Sheet, Attachment 1), will provide an additional 70 parking spaces on property currently owned by the Beaverton School District located directly south of the tennis courts adjacent to SW Erickson Avenue. Student vehicle parking is controlled through a permit system.

The addition of the three new parking lots (C, D, and H) will raise the total parking on the Beaverton High School campus to 436 spaces (426 standard spaces and 10 handicapped spaces). Based on the existing (2000-2001) student enrollment of 1,800 students and employment of 150 staff, the parking requirement at 0.2 spaces per FTE student and staff is 390 spaces. The proposed number of parking spaces (436) meets the parking requirement based on current student enrollment and staff numbers (390).

However, based on a potential capacity of 2,200 students and 200 staff, the parking requirement at 0.2 spaces per FTE student and staff is 480 spaces. The proposed number of parking spaces would not meet the parking requirement if student enrollment was to increase to 2,200 and the number of staff was to increase to 200. The campus is directly served by Tri-Met (on Farmington Road). As part of the Revised Design Review Application, the Beaverton School District is seeking to utilize the City's 10 % parking reduction credit for construction of a 300-square foot pedestrian plaza and transit improvements as described in Beaverton Development Code Section 60.20.10.10.A.2. The Beaverton School District would locate these improvements at the existing location of the Tri-Met bus stop on the corner of Farmington Road and SW Stott Street, adjacent to the school campus (see Revised Dimension Plan Public Improvements, Attachment 4). Discussions with Tri-Met have indicated that Tri-Met is supportive of the proposed transit stop enhancement.

The 10% parking reduction credit would reduce the parking requirement for Beaverton High School from 390 to a minimum of 351 spaces based on current enrollment and staff numbers (1,800 students and 150 staff), and would reduce the parking requirement for Beaverton High School from 480 to a minimum of 432 spaces based on potential enrollment (2,200 students and

200 staff). The proposed addition of Parking Lots C, D and H, combined with the 10% parking requirement reduction for parking amenities enables Beaverton High School to meet the City's parking requirements. Due to school bus maneuverability needs, 19 on-street parking spaces along SW Street and SW Erickson Avenue will be used from 7:00am to 4:00pm on school days for school bus transportation purposes. These areas will be appropriately signed.

**Beaverton High School Parking**

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<b>TOTAL</b>	<b>311</b>	<b>436</b>

\* These are new lots

\*\* This lot will be removed, pending street vacation along 2<sup>nd</sup> Street (stalls outside right-of-way).

NOTE: Current parking within the 2<sup>nd</sup> Street right-of-way is 61 spaces, which will be removed pending the street vacation along 2<sup>nd</sup> Street.

The proposed bicycle parking for the Beaverton High School campus will be located on the north side of the school (see Revised Cover Sheet, Attachment 1). This places the bicycle parking in close proximity to the entrance of the school. As shown on the Revised Cover Sheet, Attachment 1, and the Revised Detail Sheet Bike Rack Detail, Attachment 17, the campus will include 56 long-term bicycle parking spaces. According to Beaverton Development Code Section 60.20.10.5, the minimum parking ratio requirements for bicycle parking for an Educational Institution-High School is one long-term space per 18 students. Based on a potential maximum enrollment of 2,200 students, the long-term bicycle parking requirement for Beaverton High School would be 122 spaces. A Variance-Public Hearing application requesting a reduction in the long-term bicycle parking requirement from 122 to 56 was submitted to the City of Beaverton in January 2002, concurrently with this Revised Design Review application. Bicycle parking on the Beaverton High School campus will comply with long-term bicycle parking design standards per Beaverton Development Code 60.60.65.4.

**General Approval Criterion #2. The proposed development will comply with the Comprehensive Plan.**

The following Comprehensive Plan policies and objectives have been determined to apply to this application, as identified in the pre-application notes dated October 8, 2001 and in the fax entitled BDR2001-0213 (Beaverton High School Cafeteria and Parking Lot) dated January 18, 2002.

### **3.3.1 Land Use Standards and Design Objectives**

*a) Community appearance should become a major concern and the subject of a major effort in the area. Street tree planting and landscaping, sign regulation, and building improvement and painting programs all contribute to an improved environment.*

*b) Recognize the future development of the Central Business District as the major element in creating an individual identity for Beaverton. The image of the City should protect the new and the old relating to:*

- *the Central Business District as a major regional employment and commercial center; and*
- *the City's past reflected in its older buildings.*

*c) Concerted efforts should be made to improve the appearance of the community.*

*d) Use the appearance of the community as a principal means of establishing an individual identity in the metropolitan area.*

*e) Beaverton should capitalize on its tree-filled environment in establishing a unique urban identity.*

**RESPONSE:** The proposed cafeteria and parking for the existing Beaverton High School campus are designed to recognize the importance of building and facility appearance and compatibility with surrounding development, including the high school campus' relationship to downtown Beaverton. The proposed cafeteria building will be located immediately north of and attached to the existing Beaverton High School building, just south of the Merle Davies building, which is used as a Beaverton High School annex for additional classroom and office space. The proposed cafeteria building will be located on a portion of vacated SW 2<sup>nd</sup> Street.

The proposed cafeteria design is intended to make the campus site more urban and cohesive. The Merle Davies building is a former elementary school constructed in the Half Modern style. The building is generally a one-story, U-shaped brick structure with many windows. The existing Beaverton High School building is taller and has a more modern appearance. The proposed cafeteria building will be very similar in terms of design and material selection to the existing Beaverton High School building for several reasons. The proposed elevation of the cafeteria is to match that of the high school building so that the majority of students can easily access the cafeteria at lunchtime. The existing high school building accommodates more students than the Merle Davies annex, and is the primary high school building. Therefore, more students will need to access the cafeteria from the main high school building than from the Merle Davies annex. The cafeteria will be connected to the main building via a sky bridge and other ADA accessible walkways. There will be exterior rampways and stairs to connect the Merle Davies building with the cafeteria.

The new cafeteria building and associated development will meet all federal ADA requirements for access. On-going work on the Beaverton High School site, including the addition of an elevator to the main campus building and the provision of handicapped bathrooms and shower facilities, has brought the existing campus closer to compliance with ADA standards. External modifications have included the construction of a ramping system and other pedestrian facilities

to enhance access and connectivity for pedestrians with wheelchairs and to provide easier access to the front of the building and other areas.

The painted concrete stucco finish is planned to closely match the existing high school building. Fenestration is similar in size, location, and configuration with the exception of light control appurtenances to both shade and reflect light. This will lessen heat gain, add natural light and reduce dependence on light fixtures to save energy. The square geometric design near the existing high school auditorium entry would be mimicked at the south stair projection of the proposed cafeteria building. The trash/recycling and mechanical unit screening would introduce a new material, perforated metal panels, in this same geometric design. Metal panels would also be used at the new canopies over the new and existing building entries and pedestrian bridge canopy.

The proposed height of the cafeteria (39.6 feet) would provide a height transition between the Merle Davies annex building (36 feet) and the existing high school building (maximum height of 50 feet over the auditorium). The proposed cafeteria building has been designed to match the spatial arrangement of the existing Beaverton High School building (Architectural Elevations, Attachment 16). To make the buildings compatible, the height of the new building would be increased nine feet above the maximum allowed within the R-10 zoning district. The proposed height would also facilitate the construction and implementation of an ADA accessible sky bridge, which would be designed to link the second floor of the cafeteria structure with the existing building. The proposed height does not exceed existing building height in other locations and affords flexibility for eliminating mechanical units from line of sight.

It is intended that the new cafeteria and associated improvements will help to establish an identity for Beaverton High School as an urban high school campus. Once the campus is unified, it will better integrate with the surrounding community and downtown Beaverton. Enhanced bicycle and pedestrian facilities will work to integrate the campus with the surrounding area.

The entrance to the High School (immediately west of the proposed cafeteria building) will be enhanced via landscaping and pedestrian treatments. A landscaped island will be located in the middle of the proposed turnaround directly in front of the school, and the parking area along what is proposed to be removed for the pending SW 2<sup>nd</sup> Street vacation (Parking Lot D, Revised Cover Sheet, Attachment 1) will be landscaped with parking lot landscape islands with canopy trees at least every 12 parking spaces (as referenced in the October 8, 2001 pre-application notes regarding Board of Design Review standard; shown on Revised Landscaping and Lighting Plan Private Improvements (West), Attachment 12). Trees have been located to shade hard surfaces and parking areas. These changes are intended to provide a more defined "gateway" to the school to help establish school identity. The entrance to the Beaverton High School auditorium at the corner of SW 2<sup>nd</sup> Street and Erickson Avenue will also be enhanced with landscape treatments and improved ADA access.

The proposed parking facility east of the cafeteria (Parking Lot C, Revised Cover Sheet, Attachment 1) will include treatments that integrate it both with the high school campus and the surrounding community. The parking facility will be landscaped with parking lot landscape islands with canopy trees at least every 12 parking spaces (Revised Landscape and Lighting Plan Private Improvements (West), Attachment 12, and Landscape and Lighting Plan Private Improvements (East), Attachments 13 and 14). The parking lot will also feature landscaping that separates the lot from SW Stott Street to the east, and pedestrian facilities on the perimeter of the lot. Proposed Parking Lot H, located south of the tennis courts (see Revised Cover Sheet, Attachment 1), will also be landscaped with canopy trees and landscaping that separates the lot

from adjacent land uses and SW Erickson Avenue. Lots C, D and H will be screened with site-obscuring hedges, fences, or planting per Beaverton Development Code Section 60.20.20.2.A.

### **3.3.2 Land Use Policies**

*c) Older sections of the community should be carefully evaluated and efforts made to rehabilitate or redevelop those areas requiring special treatment.*

*f) Efforts should be made to obtain ten-foot landscape strips along either side of Canyon Road, the Beaverton-Hillsdale Highway, the Tualatin Valley Highway, Murray Boulevard, Cedar Hills Boulevard, and along other major streets in the planning area which are bordered by commercial or industrial developments. Landscaped median strips should also be installed in these streets wherever possible.*

**RESPONSE:** The proposed cafeteria and associated parking will be developed with recognition of the existing historic Merle Davies building and the existing Beaverton High School building and the surrounding area. The proposed development is intended to enhance the entire Beaverton High School site, and thereby the neighborhood, which is located in a relatively older area of Beaverton. The proposed improvements will enhance the area as a whole through improved pedestrian and bicycle access and visual improvements. The current landscaping and shade trees along Farmington Road, which bound the north edge of the high school campus, would not be altered according to the proposed development plans.

### **3.4.2 Residential Objectives**

*3.4.2.1. The primary focus of residential development should be towards maintaining or creating maximum livability and promoting quality living areas.*

*3.4.2.2. All residential areas should be provided with services and facilities necessary for safe, healthful, convenient urban living.*

*3.4.2.3. Residential areas should be developed in ways which are consistent with the geographic features so as not to create health or erosion hazards.*

**RESPONSE:** The proposed development at Beaverton High School would complement an already-existing high school facility. Although located in a residential zone, the site of the proposed cafeteria and parking is currently considered part of the Beaverton High School campus. The proposed development would include improved pedestrian and bicycle facilities, specifically an east-west connection located on what is currently SW 2<sup>nd</sup> Street right-of-way, which would enhance pedestrian and bicycle connectivity for the surrounding neighborhood. The proposed development would also offer additional public meeting space within the cafeteria that could be used after school hours by the community, as well as an enhanced access to the high school auditorium, including ADA treatments and landscaping. The proposed improvements would work to promote livability for the neighborhood and community by providing a more integrated educational facility for Beaverton High School students, many of whom live in the immediate area. The proposed development does not impact any of the school's existing public meeting space or athletic facilities that currently serve the public's recreational and civic needs. The proposed development would not create any undue health or erosion hazards and the new building would be built to federal, state, and local standards and would be compliant with

standards for building design (safety) and erosion. The proposed development site is predominantly level in terms of elevation.

### **6.2.2 Goal: A Balanced Transportation System**

#### ***Policies:***

- a) *Develop and implement public street standards that recognize the multipurpose nature of the street right-of-way for utility, pedestrian, bicycle, transit, truck, and auto use, and recognize these streets as important to community identity as well as providing a needed service.*

*Action: Develop and maintain a series of system maps and design standards for motor vehicle, bicycle, pedestrian, transit, and truck facilities in Beaverton.*

- b) *Provide connectivity to each area of the City for convenient multi-modal access.*

*Actions: Require the provision of an adequate local public street system for both residential and non-residential development. Give particular attention to large blocks of commercially developed properties to assure that local circulation has adequate public streets and is not forced to use only private parking and driveway areas or the major street systems to conduct local trips. Develop and maintain appropriate on-site loading, parking, and internal circulation standards for private development based on adopted standards in the City's development code.*

- c) *Develop a safe, complete, attractive, and efficient system of pedestrian ways and bicycle ways, including bike lanes, shared roadways, off-street pathways, and sidewalks according to the pedestrian and bicycle system maps.*

*Actions: Use the City of Beaverton Engineering Design Manual standards in design of facilities. Conform to the design guidelines set forth in the "Guide for Development of New Bicycle Facilities" (latest edition) as published by the American Association of State Highway and Transportation Officials (AASHTO) and the Oregon Bicycle and Pedestrian Plan adopted by the Oregon Transportation Commission. Coordinate with Washington County, Metro, Beaverton School District, Oregon Department of Transportation (ODOT) and the Tualatin Hills Park and Recreation District (THPRD). Bicycle and pedestrian facilities should be provided and designed to accommodate the unique requirements of various user groups and trip types (including school trips, commuter trips, neighborhood circulation trips, and recreation trips). Pathways should be located to provide the "shortest path" between origins and destinations. Accommodate non-automobile movements specifically by bicyclists and pedestrians within neighborhoods. Sidewalks will continue to be the responsibility of fronting property owners. Maintain the opportunity for resident groups to fund pathway improvements through the local improvement district process. Continue to recognize the importance of walking and bicycling as forms of transportation and recreation. Develop a network of "Major Pedestrian Routes" in areas of the city where development occurs at higher levels of*

*intensity, density, or both. Develop standards for sidewalk designs and standards for development on adjacent properties to be included in the Development Code.*

**RESPONSE:** The proposed development does not preclude use of the SW 2<sup>nd</sup> Street right-of-way for utilities, bicyclists, or pedestrians; in fact, the facilities for those users will be enhanced with the proposed improvements. Utility providers have been contacted regarding the proposed development (see Street Vacation/Comprehensive Plan Amendment Application, dated November 15, 2001), and all concur with the vacation of the street as long as their utility placement needs are coordinated and met by the school district. The proposed development will maintain bicycle and pedestrian connectivity between SW Stott and SW Erickson in front of BHS, and planned enhancements will improve the pedestrian and bicycle facilities via streetscape and curb and intersection treatments for pedestrians with wheelchairs. An ADA-compliant pedestrian plaza will be located at the existing Tri-Met transit stop adjacent to the campus on Farmington Road. The development of the 300 square-foot plaza will enhance pedestrian connectivity and multi-modal connections and provide for public open space. The plaza will enable Beaverton High School to reduce the requirement for on-site parking by 10% (per Beaverton Development Code Section 60.20.10.10.A.2), thereby minimizing impervious surfaces used for parking lots on campus. The plaza will be constructed in compliance with the standards described in Section 60.20.10.10.A.2 of the Beaverton Development Code.

According to the Beaverton High School 2<sup>nd</sup> Street Vacation Traffic Impact Study prepared for the City of Beaverton by DKS Associates (December 2001), the existing street system can adequately service the incremental shift in traffic volumes that would occur as a result of the street vacation (see Street Vacation/Comprehensive Plan Amendment application, filed November 15, 2001 – Traffic Impact Study). The proposed development plan will make only one change to the existing street spacing system near the high school. Existing traffic on SW Erickson that wants to go to the east would be limited (in terms of vehicular connectivity) to SW Farmington Road or SW 5<sup>th</sup> Street, with a distance between the two of approximately 1,350 feet. However, this access distance is already currently enforced during Beaverton High School school hours (6:30am to 2:30pm), as eastbound vehicular traffic is not allowed on SW 2<sup>nd</sup> (SW Stott – SW Erickson) during those times.

North and southbound travel on SW Stott Street would not be affected by the street vacation in terms of connectivity. This segment of SW 2<sup>nd</sup> is currently not open to westbound travel from SW Stott Street because it is a one-way street. SW 2<sup>nd</sup> Street is currently closed during the AM peak hour to eastbound traffic, the primary peak hour direction in Beaverton. The street is open to eastbound traffic during the PM peak hour, but at that time the predominant peak hour travel direction is westbound. Therefore, the vacation of SW 2<sup>nd</sup> Street between SW Stott and SW Erickson would only affect eastbound vehicular connectivity on weekends and non-school hours. According to the DKS Traffic Impact Analysis, the amount of both weekend and school day traffic activity that would be shifted away from SW 2<sup>nd</sup> Street could be accommodated by the existing street system (see Street Vacation/Comprehensive Plan Amendment application, filed November 15, 2001 – Traffic Impact Study).

**6.2.3: Goal: A Safe Transportation System**

- d) *Designate routes to schools for each school and to and from any new residential project.*

*Action: The City should work with the school district and community in developing safe bus, pedestrian, and bicycle routes to schools.*

**RESPONSE:** The pedestrian, bicycle and school bus routes to and from Beaverton High School are carefully integrated into the proposed overall campus site plan in recognition of the importance of those modes in an urban campus setting. Tri-Met bus service along Farmington Road also serves the campus; Tri-Met Route 52 serves the transit stop on the north edge of the campus every 15 minutes on weekdays from approximately 6:30am to 6:30pm in both eastbound and westbound directions. Pedestrian and bicycle routes on SW 2<sup>nd</sup> Street are proposed to remain through-routes to connect the school with adjacent residential and urban areas, and will be enhanced via improved streetscape and design. Pedestrian facilities will be designed to enhance access and connectivity for pedestrians with wheelchairs, including curb and intersection treatments and easier access to the front of the high school building, the auditorium, athletic fields, and the proposed cafeteria.

Raised crosswalks will be constructed on SW Erickson Avenue to calm vehicle traffic and provide a greater feeling of security for pedestrians and bicyclists (see Revised Dimension Plan Public Improvements, Attachment 4, and Appendix H). The pedestrian facilities will also be well lighted for use during evening hours. Existing pedestrian and bicycle routes will not be impacted. School bus circulation will be accommodated through a combination of on-site and on-street drop-off and pick-up areas along SW Erickson Avenue, SW Stott Street, and at the turnaround area at the school entrance (accessed via SW Erickson Avenue) for students with special needs. The proposed bus areas offer safe and efficient student access and vehicle maneuverability while maintaining public pedestrian facilities along the streets. The fact that SW 2<sup>nd</sup> Street will be closed to vehicular travel actually improves the safety for bicyclists and pedestrians along that street segment. The pedestrian and bicycle facility would also be well lighted for evening use (Revised Landscape and Lighting Plans, Attachments 11, 12, 13 and 14).

The proposed bicycle parking for the Beaverton High School campus will be located on the north side of the school (see Revised Cover Sheet and Revised Detail Sheet Bike Rack Detail, Attachments 1 and 17). This places the bicycle parking in close proximity to the entrance of the school. The campus will include 56 long-term bicycle parking spaces. According to Beaverton Development Code Section 60.20.10.5, the minimum parking ratio requirements for bicycle parking for an Educational Institution-High School is one long-term space per 18 students. Based on a potential maximum enrollment of 2,200 students, the long-term bicycle parking requirement for Beaverton High School would be 122 spaces. A Variance-Public Hearing application requesting a reduction in the long-term bicycle parking requirement from 122 to 56 was submitted to the City of Beaverton in January 2002, concurrently with this Revised Design Review application. Bicycle parking on the Beaverton High School campus will comply with long-term bicycle parking design standards per Beaverton Development Code 60.60.65.4.



**6.2.4: Goal: An Efficient Transportation System that Reduces the Number of Trips and Limits Congestion**

- a) *Support trip reduction strategies developed regionally, including employment, tourist, and recreational trip programs.*

*Actions: Encourage implementation of travel demand management programs, which reduce the number of single occupant vehicle trips per capita. Shift traffic to off-peak travel hours. Coordinate trip reduction strategies with Washington County, Metro, Westside Transportation Alliance, ODOT, Tri-Met, and Department of Environmental Quality. Seek to raise PM peak average vehicle occupancy (AVO) to 1.3 AVO or more in the evening peak and/or move 50 percent of standards evening peak trip generation outside the peak hour. Educate business groups, employees, and residents about trip reduction strategies, and work with business groups, residents, and employees to develop and implement travel demand management programs.*

- b) *Limit the provision of parking to meet regional and state standards.*

*Actions: Meet Metro Urban Growth Management Functional Plan Title 2 requirements. Establish maximum and minimum parking requirements. Use research conducted by Department of Environmental Quality for guidance in determining demand. Work to reduce parking per capita by ten percent or more relative to prior parking standards in Beaverton, and minimize impacts to neighborhoods.*

- c) *Maintain level of service consistent with regional goals. Reduce traffic congestion and enhance traffic flow through such measures as intersection improvements, intelligent transportation systems, signal synchronization, and other similar measures.*

*Actions: In adopting standards for the City, level of service D, Highway Capacity Manual, Chapter 11 is recommended to balance provision of capacity with level of service and funding. Continue to monitor Washington County's and Metro's work to develop a regional level of service standard.*

**RESPONSE:** The proposed development at Beaverton High School will not specifically add capacity for increases in student population beyond student population levels that have historically occurred at the school and are projected by the Beaverton School District (1,800 to 2,200 students). Students will continue to arrive predominantly by school bus. School bussing is an effective means of reducing trips on the overall transportation system and reducing parking needs at the campus. Students also have the option to walk or bicycle to school. According to the DKS traffic study, the existing street system can adequately service the incremental shift in traffic volumes that would occur as a result of the street vacation. All unsignalized intersections will continue to operate at current LOS levels (see Street Vacation/Comprehensive Plan Amendment application, filed November 15, 2001 – Traffic Impact Study).

### **7.2.1: Parks and Recreation Planning Objectives**

- a) *Schools, park, and recreation development should be considered as a vital part of the future livability of the area, and efforts should be made to keep these facilities abreast of growth.*
- b) *Provide for schools, parks, and natural open space throughout the city in locations that are easily accessible to those they are intended to serve.*
- c) *Help offset higher residential densities and intense commercial developments in the community with parks and open spaces.*

**RESPONSE:** The proposed cafeteria and parking will improve the efficiency and student circulation of the existing Beaverton High School campus, which is currently located in an R-10 zone. Criteria presented in 7.2.1 state that school facilities should be considered vital and should be located in easily accessible locations throughout the city. The current high school campus offers educational and recreational opportunities and open space to complement existing nearby residential and commercial uses. The proposed development would help to enhance the role the campus plays in the community by offering the school a chance to modernize its facilities, improving pedestrian and bicycle access, and providing public meeting space opportunities.

The proposed development would help to enhance and modernize the Beaverton High School campus, which is located within walking and biking distance of residential and commercial areas. The school currently provides both an educational institution and a place of recreation for community members, functions that would not be changed by the proposed development. The new cafeteria structure would provide the school with a modern cafeteria facility, which would free up space in the existing building for modernized and increased educational opportunities. The new cafeteria building could also offer additional public meeting space in a central downtown location, for community members to use during non-school hours.

Bicycle and pedestrian facilities along SW 2<sup>nd</sup> Street right-of-way would be enhanced, which would allow safer and more direct community access to the school facility, both for students and other community members wishing to attend events held at the campus or to use athletic fields when not in use by students. The pedestrian and bicycle improvements would also offer access through campus, particularly for those residents living to the west of the school wishing to walk or bike to downtown Beaverton (east of the campus). The current Beaverton High School campus contains open space and recreation areas, including several sports fields, tennis courts, and a track. These areas are accessible to the public when they are not in use by students. The proposed development will not affect the placement or size of these open space areas, which provide important recreational opportunities for the community during non-school hours in addition to serving high school students.

### **7.3.1: Cultural and Historical Resource Objectives**

- a) *Protect, where possible, cultural and historic resources in order to enhance and perpetuate landmarks and districts representing or reflecting elements of the city's cultural, social, economic, political and architectural history.*
- b) *Stabilize and improve property values in historic districts and other designated landmarks.*
- c) *Foster civic pride in the beauty and noble accomplishments of the past.*

**RESPONSE:** The proposed development minimizes impacts to historic structures. The Merle Davies building will not be altered as part of the proposed site improvement plan (Figure 1). The proposed development will work to enhance the integration of both the Merle Davies building and the existing high school building into a comprehensive, urban high school campus. The proposed development will not negatively affect property values on the site or in the surrounding neighborhood, as (1) the site already is considered part of the Beaverton High School campus, (2) the proposed development is intended to enhance the visual and physical cohesion of the campus, and (3) the proposed development will improve the pedestrian and bicycle facilities on SW 2<sup>nd</sup> Street right-of-way. The current uses of the proposed site location are a street (SW 2<sup>nd</sup> Street), which is open only during non-school hours, and a paved parking/loading area. The improved bicycle and pedestrian facilities and the new cafeteria building will provide public amenities for surrounding neighborhoods via improved access and public meeting area and parking for potential public events.

**General Approval Criterion #3. That the location, size, design, and functional characteristics of the proposed use are such that it can be made reasonably compatible with and have a minimum impact on the livability and appropriate development of other properties in the surrounding neighborhood.**

**RESPONSE:** As discussed in the previous responses to Comprehensive Plan elements, the proposed location of the cafeteria and parking is already considered part of the Beaverton High School campus, which has existed for many years in the surrounding neighborhood. The school expansion is intended to (1) make the Beaverton High School campus more urban and integrated, and (2) provide a modern cafeteria facility in order to free up additional space in the existing high school building for more modern and expanded educational opportunities. The proposed development is intended to accommodate the current student population as well as student populations projected by the Beaverton School District; student population projections do not exceed the historical student population range at Beaverton High School (1,800 to 2,200 students). The proposed building and parking would need to be located in the proposed location due to campus constraints to the east and west and athletic facilities to the south.

The proposed development includes improvements to the pedestrian and bicycle facilities on SW 2<sup>nd</sup> Street, as well as landscaping and ADA treatments at the primary school entrance, the entrance to the high school auditorium, and the new parking facilities. These improvements would help connect the school campus to the surrounding community as well as offering a safer through-route for pedestrians and bicyclists. The proposed development would also offer the community a potential location for centrally located public meeting space.

The proposed development would minimize impact to historic buildings. The proposed cafeteria and parking facilities also would not require removal of any of the existing Beaverton High School building or athletic facilities.

The proposed height of the cafeteria building, though higher than the maximum height allowed in an R-10 zoning district, is necessary to efficiently accommodate student circulation from the main high school building to the cafeteria facility. The height would allow the cafeteria to be built to the same elevation as the main high school building, allowing for effective ADA access, including a sky bridge connecting the second floors of the buildings.

#### **IV. Proposed Operations and Site Analysis Information**

##### *Proposed Operations*

Hours of Operation: Beaverton High School is open from 6:30am to 2:30pm for classroom activities. After-school activities can run until 10:00m, depending on the activity. School facilities are available for a number of users and are not limited to Beaverton School District programs. Tualatin Hills Parks and Recreation District and community groups conducting classes or events are able to use the facilities at BHS. These events are scheduled through the school. As with all District schools, BHS is a community facility.

Total Number of Employees:	150/200
Maximum Number of Employees per Shift:	150/200
Total Number of Students:	1,800 (9/30/01)/2,200

##### *Site Analysis Information*

Existing Building Area (footprint):	195,000 square feet
Proposed Building Addition or Subtraction (footprint):	+18,000 square feet
Proposed Total Building Area (footprint):	213,000 square feet
Proposed New Building Area (first floor):	18,000 square feet
Proposed New Building Area (second floor):	12,000 square feet
Total New Building Area: _	30,000 square feet
Existing Building Height:	50 feet maximum (existing Beaverton High School building) 36 feet (Merle Davies Building)
Proposed Building Height:	39.6 feet (proposed cafeteria building)
Existing Vehicle Parking/Sidewalk Area:	110,000 square feet
Existing Number of Vehicle Parking Spaces:	311 spaces
Proposed Vehicle Parking/ Sidewalk Addition or Subtraction:	+135,000 square feet
Proposed Number of Vehicle Parking Spaces:	436 spaces
Proposed Use:	Educational Institution (Cafeteria and Parking Facilities)
Vehicle Parking Requirement:	390/480 spaces

Proposed Number of Bicycle Parking Spaces:	56 spaces
Bicycle Parking Requirement:	122 spaces
Existing Landscaped Area:	27,000 square feet
Proposed Landscape Addition or Subtraction:	+17,000 square feet
Percentage of Site:	56%
Existing Open Space Area:	640,000 square feet (pervious)/140,000 square feet (impervious)
Proposed Open Space Addition or Subtraction:	-70,000 square feet (pervious)
Existing % Open Space (Pervious)/Landscape:	56%
Proposed % Open Space (Pervious)/Landscape:	48%

**Beaverton High School Parking  
Table 2**

<b>Lot</b>	<b>Current Number of Parking Stalls</b>	<b>Proposed Number of Parking Stalls</b>
A	25	25
B	56	56
C	51	82
D*	0	35
E	80	80
F	56	56
G	32	32
H*	0	70
2 <sup>nd</sup> Street	11**	0
<b>TOTAL</b>	<b>311</b>	<b>436</b>

\* These are new lots

\*\* This lot will be removed, pending street vacation along 2<sup>nd</sup> Street (stalls outside right-of-way).

NOTE: Current parking within the 2<sup>nd</sup> Street right-of-way is 61 spaces, which will be removed pending the street vacation along 2<sup>nd</sup> Street.

## V. Additional and Other Requirements

A Traffic Impact Study was prepared for the Beaverton High School 2<sup>nd</sup> Street Vacation by DKS Associates (December 2001), and was included as part of the Street Vacation/Comprehensive Plan Amendment application, submitted on November 15, 2001. According to the Pre-Application notes dated October 8, 2001, no additional requirements for the Conditional Use Permit application are necessary.

## VI. Neighborhood Review Meeting

The City of Beaverton requires that an applicant conduct a Neighborhood Review Meeting prior to submitting a Conditional Use application, per Development Code Section 50.10.B.5(a-g). Required submittal materials associated with the Neighborhood Review meeting held on September 20, 2001 are included in Appendix C.

## **VII. Conclusion**

The Beaverton School District's request for Conditional Use approval for the new cafeteria, parking areas and height requirement complies with the review criteria. The proposed improvements will enhance the overall Beaverton High School campus and central Beaverton, enable the School District to more efficiently provide educational and core facility services and enhance the campus environment to create more of an urban high school facility. The proposed cafeteria does not pose any adverse effects for the natural or neighborhood environment, and would provide additional public meeting space in the evening hours with improved accessibility for the community.

Revised  
ATTACHMENTS

1. COVER SHEET
2. EXISTING CONDITIONS PLAN
3. DIMENSION PLAN PUBLIC IMPROVEMENTS
4. DIMENSION PLAN PRIVATE IMPROVEMENTS (WEST)
5. DIMENSION PLAN PRIVATE IMPROVEMENTS (EAST)
6. GRADING, STORM AND EROSION CONTROL PUBLIC IMPROVEMENTS
7. GRADING, STORM AND EROSION CONTROL PRIVATE IMPROVEMENTS (WEST)
8. GRADING, STORM AND EROSION CONTROL PRIVATE IMPROVEMENTS (EAST)
9. LANDSCAPE AND LIGHTING PLAN PUBLIC IMPROVEMENTS
10. LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (WEST)
11. LANDSCAPE AND LIGHTING PLAN PRIVATE IMPROVEMENTS (EAST)
12. DETAIL SHEET FOR WATER QUALITY CONSTRUCTION
13. DETAIL SHEET FOR WATER QUALITY CONSTRUCTION
14. DETAIL SHEET FOR WATER QUALITY CONSTRUCTION AND BIKE PARKING DETAIL
15. PROPOSED TREE PROTECTION PLAN
16. ARCHITECTURAL ELEVATIONS

## **APPENDICES**

**APPENDIX A**  
APPLICATION FOR DEVELOPMENT REVIEW

**APPENDIX B**  
FOUR-PAGE CONDITIONAL USE PERMIT SUBMITTAL CHECKLIST

**APPENDIX C**  
NEIGHBORHOOD REVIEW MEETING  
INFORMATION

**APPENDIX D**  
CLEAN WATER SERVICES SENSITIVE AREAS CERTIFICATION FORM

**APPENDIX E**  
WATER QUALITY REPORT, HARPER HOUF RIGHELLIS



## **APPENDIX A**



CITY OF BEAVERTON  
DEVELOPMENT SERVICES DIVISION  
4755 S.W. GRIFFITH DRIVE  
P.O. BOX 4755  
BEAVERTON, OR 97076  
PHONE: (503) 526-2420

# APPLICATION FOR DEVELOPMENT REVIEW

FILE NAME: _____	SUBMIT: _____ REVIEW: _____
TYPE: _____	RESUBMIT: _____ REVIEW: _____
FILE NUMBERS: _____	APPLICATION MATERIAL RECEIVED: _____
	FEE RECEIVED: _____ FEE PAID: \$ _____
	CHECK #: _____ RECEIPT #: _____
	COMPLETENESS DATE: _____

**PRINT OR TYPE ALL INFORMATION BELOW**

APPLICANT  USE MAILING ADDRESS FOR MEETING NOTIFICATION

YOUR NAME <b>Frank Angelo</b>	PHONE # <b>503-227-3664</b>
BUSINESS NAME <b>Angelo Eaton &amp; Assoc.</b>	FAX # <b>503-227-3679</b>
ADDRESS <b>620 SW Main Suite 201</b>	SUITE
CITY, STATE <b>Portland, Oregon 97205</b>	ZIP
SIGNATURE (ORIGINAL REQUIRED) <i>Frank Angelo</i>	
DATE <b>12/18/01</b>	

**PROPERTY/ZONING DATA**

**13000 SW 2nd**  
Address

**Beaverton High School**  
Location Description

**R-10**

CONSTRUCTION EXPECTED TO BEGIN: \_\_\_\_\_

MAP & TAX LOT (list one per line)	DO NOT USE LOT & BLOCK	SITE		ZONING	
		ACRES	EXISTING	PROPOSED	
15116AD - 11100		6.15	R10	R10	
15116AD - 11000		.13	R10	R10	
15116AD - 10900		11.41	R10	R10	
-					
TOTAL GROSS SITE:		ACRES 17.69	SO FT 770,576.40		
TOTAL NET SITE (GROSS SITE MINUS DRIVEWAYS & R.O.W.)					

PROPERTY OWNER  Attach separate sheet if more than one owner

YOUR NAME <b>Mike Maloney</b>	PHONE # <b>503-591-4303</b>
BUSINESS NAME <b>Beaverton School Dist.</b>	FAX # <b>503-591-4484</b>
ADDRESS <b>16550 SW Merlo Rd</b>	SUITE
CITY, STATE <b>Beaverton OR 97006-5152</b>	ZIP
SIGNATURE (ORIGINAL REQUIRED) <i>Mike Maloney</i>	
DATE <b>12/19/01</b>	

**COMMERCIAL/INDUSTRIAL DEVELOPMENT**

**Public High School**  
Use or Development Description

BUILDING USE PROPOSED USES (LIST ONE PER LINE)	BLDG. AREA		PARKING # SPACES REQ/PROP
	GROSS SQ. FT.	% OF BLDG.	
Cafeteria			/
Parking Lot			/
			/
			/
TOTAL BUILDING AREA:		100%	/

ARCHITECT/DESIGNER

YOUR NAME <b>Russ Pitkin/Eric Nielsen</b>	PHONE # <b>503-274-5432</b>
BUSINESS NAME <b>LSW Architecture</b>	FAX # <b>503-274-0085</b>
ADDRESS <b>1953 NW Kearny</b>	SUITE
CITY, STATE <b>Portland OR 97209</b>	ZIP

LANDSCAPE ARCHITECT

YOUR NAME <b>Ron Matela</b>	PHONE # <b>360-695-0776</b>
BUSINESS NAME <b>Matela Associates</b>	FAX # <b>360-695-0776</b>
ADDRESS <b>601 Main St #201</b>	SUITE
CITY, STATE <b>Vancouver WA 98660</b>	ZIP

ENGINEER/SURVEYOR

YOUR NAME <b>Gary Alfson</b>	PHONE # <b>503-221-1131</b>
BUSINESS NAME <b>Harper Houf Righellis</b>	FAX # <b>503-221-1171</b>
ADDRESS <b>5200 SW Macadam Suite 580</b>	SUITE
CITY, STATE <b>Portland OR 97201</b>	ZIP

NOTE: ACCEPTABLE SUBMITTAL  
MAXIMUM SHEET SIZE 24 X 36  
FOLDED TO FIT LEGAL SIZE  
ASSEMBLED INTO SETS

**SUBDIVISION/LAND PARTITION**

# LOTS PROPOSED: \_\_\_\_\_

PARTITION LOT SIZES IN SQUARE FEET:  
A: \_\_\_\_\_ B: \_\_\_\_\_ C: \_\_\_\_\_

**MULTI-FAMILY/P.U.D. DEVELOPMENT**

TOTAL BUILDING SPACE	GR. SQ. FT./% SITE	# UNITS
SITE AREA PER UNIT	/	PARKING SPACES
TOTAL OPEN SPACE	/	# COVERED
OPEN SPACE PER UNIT	/	# UNCOVERED
BUILDING COVERAGE	/	TOTAL SPACES
DRIVEWAY AREA	/	RV STORAGE YES <input type="checkbox"/> NO <input type="checkbox"/>
PARKING AREA	/	# SPACES
	FEET/STORIES	
MAXIMUM BLDG HEIGHT	/	
UNITS PER BEDROOM NO.	S _____ 1 _____ 2 _____ 3 _____	
ATTACHED UNITS	PENTAL <input type="checkbox"/> CONDO <input type="checkbox"/> LOT OWNERSHIP <input type="checkbox"/>	

## APPENDIX B



## CITY OF BEAVERTON SUBMITTAL CHECKLIST

### CONDITIONAL USE PERMIT – PUBLIC HEARING

#### Written Narrative Requirements

- X   A. Please provide one completed copy of this four page checklist.
- X   B. **Description of proposal:** Please describe what changes are proposed to the site, structure, landscaping, parking, and land use. Provide findings verifying that the intended use is allowed by the City's Development Code (ORD 2050).
- X   C. **Approval criteria findings:** Please provide a narrative that evaluates and verifies the proposal meets each of the approval criteria identified below (*Section 40.05.15.2.C.*, Development Code).
- 1. The proposed conditional use will comply with the purpose of this section and with all of the applicable provisions of this ordinance.*  
Please note when making findings, the applicant shall address all applicable Development Code requirements. Specify conformance or proposed variance request from the requirements of the Development Code.
  - 2. The proposed development will comply with the Comprehensive Plan.*  
Please note when making findings, the applicant shall address all applicable Comprehensive Plan policies.
  - 3. That the location, size, design, and functional characteristics of the proposed use are such that it can be made reasonably compatible with and have minimum impact on the livability and appropriate development of other properties in the surrounding neighborhood.*
- X   D. **Proposed Operations:** Please provide the hours of operation, total number of employees, and maximum number of employees per shift.
- X   E. **Additional Requirements:** Please be advised that special studies, investigations and reports may be required to ensure that the proposal does not adversely affect the surrounding community, does not create hazardous conditions for persons or improvements on the site. These studies may include investigations and reports on noise attenuation, air quality, traffic control, soil conditions, flooding of waters and storm water run-off, natural resources, tree preservation, and other concerns.

X   F. **Site Analysis Information:** Please include the following information, if applicable:    See attached

- |   |       |         |
|---|-------|---------|
| 1. Existing building area:                  | _____ | sq. ft. |
| Proposed building addition or subtraction:  | _____ | sq. ft. |
| 2. Existing building height:                | _____ | ft.     |
| Proposed building height:                   | _____ | ft.     |
| 3. Existing parking area:                   | _____ | sq. ft. |
| Existing number of parking spaces:          | _____ | # sp.   |
| Proposed parking addition or subtraction:   | _____ | sq. ft. |
| Proposed number of parking spaces:          | _____ | # sp.   |
| Proposed use:                               | _____ |         |
| Parking requirement:                        | _____ |         |
| 4. Existing landscaped area:                | _____ | sq. ft. |
| Percentage of site:                         | _____ | %       |
| Proposed landscape addition or subtraction: | _____ | sq. ft. |
| Percentage of site:                         | _____ | %       |

  X   G. **Other Requirements:** Please provide documentation that the requirements of other agencies, jurisdictions, or both for your proposal are permitted OR submit your schedule for application and approval of the required permits.

  X   H. **Neighborhood Review Meeting** information required (Development Code Section 50.10.1):

- \_\_\_\_\_ 1. A copy of the notice sent to surrounding property owners and the NAC Representative.
- \_\_\_\_\_ 2. A copy of the mailing list used to send out meeting notices.
- \_\_\_\_\_ 3. A written statement containing the information posted on the property.
- \_\_\_\_\_ 4. An affidavit of mailing and posting notices.
- \_\_\_\_\_ 5. Representative copies of written materials and plans presented at the Neighborhood Review Meeting.
- \_\_\_\_\_ 6. Notes of the meeting, including the meeting date, time, and location, the names and addresses of those who attended, and oral and written comments received.
- \_\_\_\_\_ 7. Documentation verifying that notes have been provided to the NAC.



# CITY OF BEAVERTON SUBMITTAL CHECKLIST

## CONDITIONAL USE PERMIT – PUBLIC HEARING

### Plan Requirements

All plans, except architectural renderings, elevations or both, shall be presented at a minimum of 1" = 20'. Architectural elevations may be presented at an architectural scale. Each of the following plans and drawings shall be submitted on 24" x 36" separate sheets. Please include all of the following information for each plan.

#### A. Existing Conditions Plan:

- \_\_\_\_\_ 1. North arrow, scale and date of plan.
- \_\_\_\_\_ 2. Vicinity map.
- \_\_\_\_\_ 3. The entire lot(s), including area and property lines dimensioned.
- \_\_\_\_\_ 4. Points of existing access, interior streets, driveways, and parking areas.
- \_\_\_\_\_ 5. Location of all existing buildings and structures, including refuse storage locations, pedestrian and bike paths, or both, swimming pools, tennis courts, and tot lots.
- \_\_\_\_\_ 6. Existing right-of-way and improvements.
- \_\_\_\_\_ 7. Dimension from centerline to edge of existing right-of-way.
- \_\_\_\_\_ 8. Existing topographical information, showing 2 ft. contours and appropriate spot elevations for features such as walls, retaining walls (top and bottom elevations), catch basins, stairs, sidewalks, and parking areas.
- \_\_\_\_\_ 9. Surrounding development and conditions within 100 ft. of the property; such as zoning, land uses, buildings, driveways, and trees.
- \_\_\_\_\_ 10. Location of existing public and private utilities, easements, and 100-year floodplain.
- \_\_\_\_\_ 11. Natural Resource Areas, Significant trees, and Historic trees, as established by the City of Beaverton's inventories.
- \_\_\_\_\_ 12. Sensitive areas, as defined by the Unified Sewerage Agency standards.
- \_\_\_\_\_ 13. Wetland boundaries, upland wooded area boundaries, riparian area boundaries, rock out-croppings, and streams. *Wetlands must be professionally delineated.*
- \_\_\_\_\_ 14. Existing trees larger than 6" in dbh (diameter at breast height), including genus, species and size. Dbh is measured at 54" above grade.

B. Dimensioned Site Plan:


- \_\_\_\_\_ 1. North arrow, scale and date of plan.
- \_\_\_\_\_ 2. The entire lot(s), including area, property lines dimensioned and labeled "front," "side," and "rear."
- \_\_\_\_\_ 3. Points of access, interior streets, driveways, and parking areas.
- \_\_\_\_\_ 4. Location of buildings and structures, including refuse storage locations, pedestrian and bike paths, or both, swimming pools, tennis courts, and tot lots.
- \_\_\_\_\_ 5. Proposed right-of-way, dedications and improvements.
- \_\_\_\_\_ 6. Dimension from centerline to edge of proposed right-of-way.
- \_\_\_\_\_ 7. Dimensions of all improvements, including setbacks, parking spaces, driveways, and distance between buildings.
- \_\_\_\_\_ 8. Location of storm water quality and detention facilities, or both.
- \_\_\_\_\_ 9. Boundaries of development phases, if applicable.
- \_\_\_\_\_ 10. Natural Resource Areas, Significant trees, and Historic trees, as established by the City of Beaverton's inventories.
- \_\_\_\_\_ 11. Sensitive areas, as defined by the Unified Sewerage Agency standards.
- \_\_\_\_\_ 12. Wetland boundaries, upland wooded area boundaries, riparian area boundaries, rock out-croppings, and streams. *Wetlands must be professionally delineated.*
- \_\_\_\_\_ 13. Location of storm water quality and detention facilities, or both.
- \_\_\_\_\_ 14. Topographical information for surrounding properties within 100 ft. of the property.
- \_\_\_\_\_ 15. Location and schematic massing of proposed buffering, screening, and landscaped areas.
- \_\_\_\_\_ 16. Other pertinent landscape features, including walls, retaining walls, berms, fences, and fountains.
- \_\_\_\_\_ 17. Existing trees, larger than 6" dbh, proposed to be saved. Include genus, species, and size.

C. Architectural Renderings, Elevations, or both: Please provide information which identifies the general character of the buildings and structures, by indicating dimensions, materials, colors, and textures proposed. This includes buildings, retaining walls, refuse storage facilities, play structures, accessory facilities, and fences.

I have provided the items required in this four page submittal checklist. I understand that any missing information, omissions or both may deem my project incomplete, which may lengthen the time to process the request.

Frank Angelo  
Print name

503-227-3664  
Telephone Number

  
Signature

December 19, 2001  
Date

## APPENDIX C



620 SW Main, Suite 201  
Portland, Oregon 97205Tel: 503.224.6974  
Fax: 503.227.3679

August 28, 2001

RE: NEIGHBORHOOD REVIEW MEETING  
PROPOSED DEVELOPMENT AT BEAVERTON HIGH SCHOOL

Dear Resident:

Angelo Eaton & Associates is representing the Beaverton School District (BSD), the owner of the Beaverton High School (BHS) property located at 13000 SW Second Street. The BSD is considering the expansion of the BHS campus via the proposed construction of a new school building north of the existing BHS building. The new building would include 34 classrooms and a cafeteria. The property involved with this proposed development is located both north and south of SW Second Street between SW Erickson Avenue and SW Stott Street, shown in greater detail on the attached site plan. The property located north of SW Second Street (tax lot identification 1S116AD11100 and 1S116AD11000) is 6.28 acres, and the property south of SW Second Street (tax lot identification 1S116AD10900) is 11.41 acres. The property is zoned R-10 (single family residential, one dwelling per 10,000 square foot lot).

The BSD is seeking several land use reviews and approvals: a street vacation of SW Second Street, a Master Plan/Conditional Use approval (schools are conditional uses on R-10 land), and a Design Review for development of the cafeteria. This proposal would also require the removal of the Merle Davies school, and therefore, will require review by the City of Beaverton's Historic Resource Review Committee. Prior to applying to the Community Development Department for the City of Beaverton, we would like to discuss the proposal in more detail with you.

The purpose of this meeting is to provide a forum for the applicant and surrounding property owners/residents to review the proposal and identify any issues for consideration before the formal applications are submitted to the City. We will attempt to answer questions that may be relevant to meeting development standards consistent with the City Development Code.

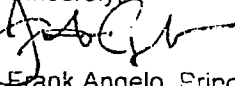
Pursuant to the City of Beaverton Development Code Section 50.10.1, you are invited to attend a meeting as part of the Central Neighborhood Association Committee (NAC) on:

**September 20, 2001  
8:00 – 8:30 p.m.****Beaverton Community Center, Community Room  
12350 SW 5<sup>th</sup> Street, Suite 100  
(across from the Beaverton City Library)  
Beaverton, OR 97005****Contact: Susan Kozak, Chairperson Central Beaverton NAC, (503) 643-3818**

Please note this will be an informational meeting on preliminary development plans, which may be altered prior to submittal of the application. In addition to this public meeting notice, you may receive official notice from the City of Beaverton after the application is submitted for you to either participate with written comments and/or an opportunity to attend a public hearing.

We look forward to discussing the proposal with you in greater detail. Please feel free to call me at (503) 227-3664 or fax me at (503) 227-3679 if you have questions.

Sincerely,



Frank Angelo, Principal, Angelo Eaton &amp; Associates

Frank Angelo

Chris Eaton, AICP

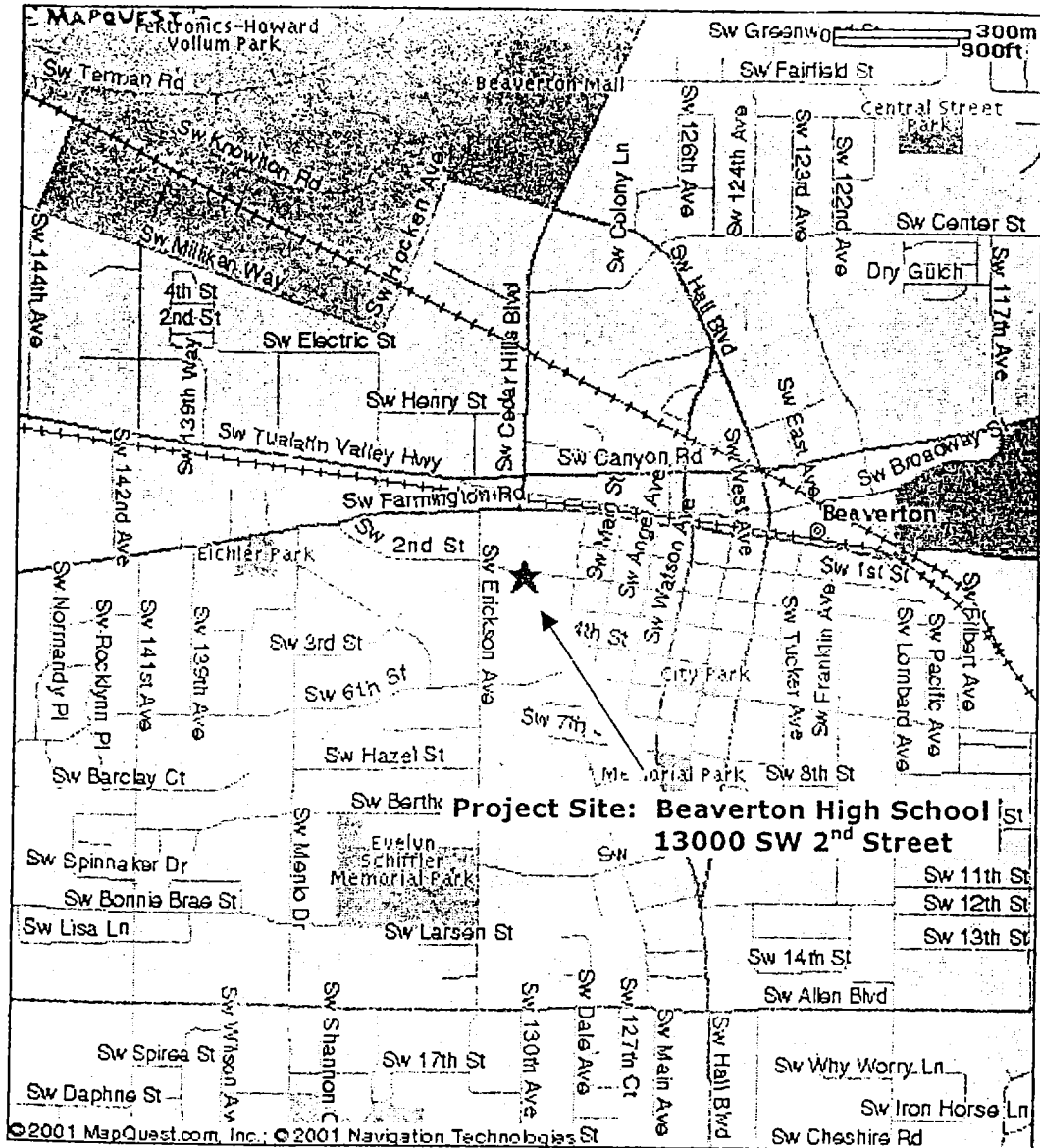
Catherine Corliss, AICP

Jean D'Agostino, AICP

Karen Siegel

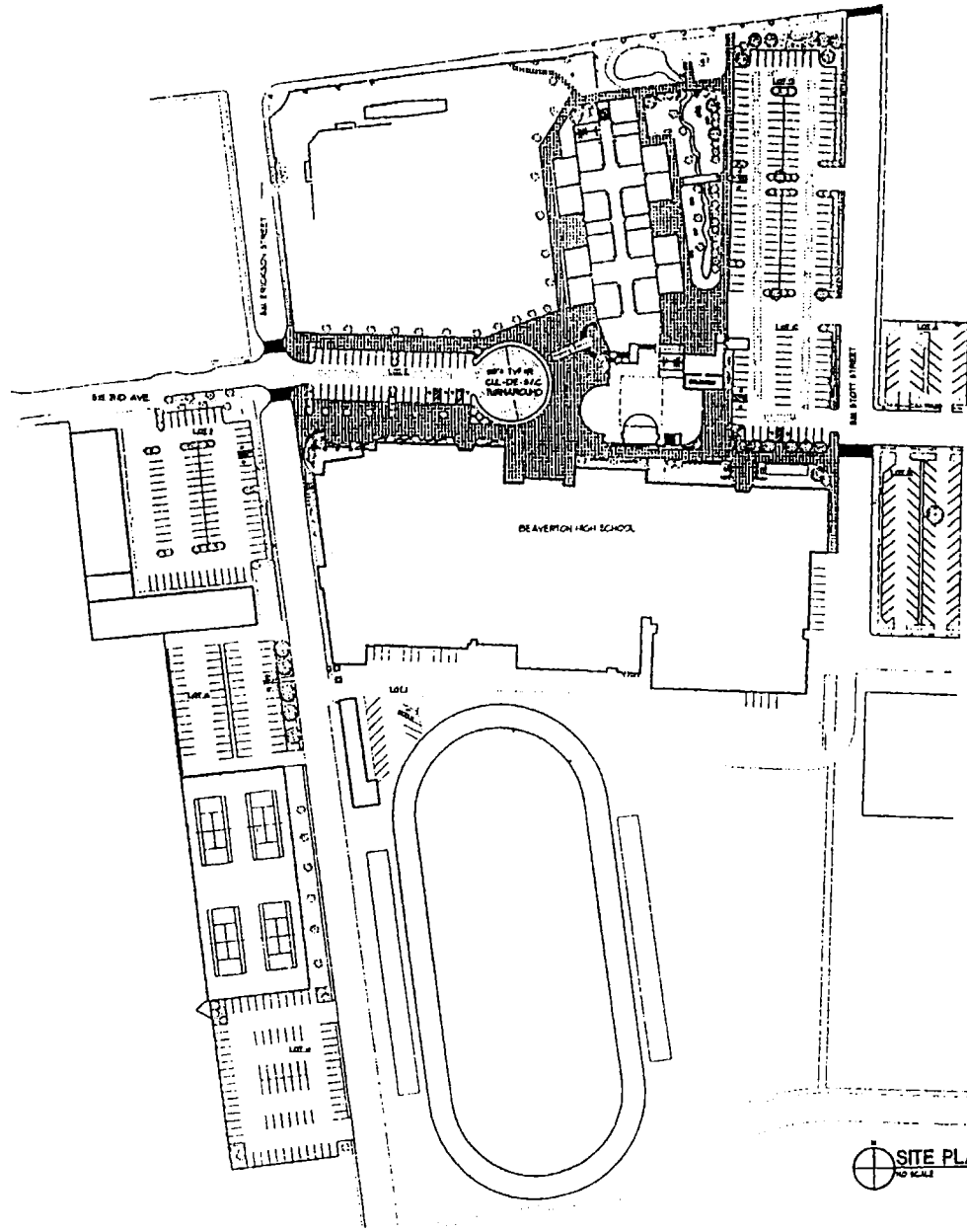


# Vicinity Map



\*PLEASE NOTE\*

Neighborhood Meeting to be held:  
September 20, 2001  
8:00 - 8:30 p.m.  
Beaverton Community Center, Community Room  
12350 SW 5<sup>th</sup> Street, Suite 100  
(across from the Beaverton City Library)  
Beaverton, OR 97005



PARKING SPACES		
LOT	ADA	TOTAL
1	1	1
2	1	1
3	1	1
4	1	1
5	1	1
6	1	1
7	1	1
8	1	1
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94	1	1
95	1	1
96	1	1
97	1	1
98	1	1
99	1	1
100	1	1
TOTAL	100	100



ARCHITECTS  
 11400 N.W. 24th St.  
 Portland, OR 97228  
 TEL (503) 374-54  
 FAX (503) 374-44  
 1979 N.W. Gateway  
 Portland, OR 97228

PRELIMINAR

BEAVERTON HIGH SCHOOL  
 CAFETERIA  
 BEAVERTON SCHOOL DISTRICT # 48  
 11400 N.W. 24th St. BEAVERTON, OREGON 97005

DATE: 11/11/00  
 DRAWN BY: J. L. JONES  
 CHECKED BY: J. L. JONES  
 APPROVED BY: J. L. JONES

5.0

Site Plan

1S116AA04700  
ADAMS-HAGLUND, JENNIFER  
2605 NE ROBERTS PL  
GRESHAM OR 97030

1S116AD02500  
ADOLPHSON LOVING TRUST  
BY RICHARD & SHIRLEY ADOLPHSON  
25505 SW LONE FIR LANE  
WEST LINN OR 97068

1S116AD02501  
ADOLPHSON LOVING TRUST  
25505 SW LONE FIR LN  
WEST LINN OR 97068

1S116AD02300  
ADOLPHSON, R G  
ADOLPHSON, R G JR  
BY A-2 ENTERPRISES  
4555 SW MAIN ST  
BEAVERTON OR 97005

1S116DB00400  
AFOURKEEFF, ANJA T  
5075 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD07000  
AHN, DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

1S116AD07001  
AHN, DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

1S116AC04700  
ANDERSON, ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC05500  
ANDERSON, ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DB03400  
ARLT, LINDA L  
13275 SW PEAR ST  
BEAVERTON OR 97005

1S116AC01800  
ARMSTRONG, JAMES R  
LINDA L  
13325 SW 2ND  
BEAVERTON OR 97005

1S116DA90027  
BABB, DORIS LEE  
5136 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90023  
BAKER, VIRGIL A & MARIE  
5166 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AC04500  
BARTLETT, CLIFFORD L  
13500 SW 2ND  
BEAVERTON OR 97005

1S116AA05200  
BASEEL, PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

1S116AA05301  
BASEEL, PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

BEA  
Beaverton  
PO BOX 4755  
BEAVERTON OR 97076

Beaverton  
PERSEY, JIM  
12345 SW DAVIES ROAD  
BEAVERTON OR 97008

1S116AB03100  
BEAVERTON FOODS INC  
PO BOX 687  
BEAVERTON OR 97005

1S116AC04400  
BEAVERTON PROPERTIES  
13470 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD11100  
BEAVERTON SCHOOL DISTRICT #48  
MERLE DAVIS ELEMENTARY  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02100  
BEAVERTON SCHOOL DISTRICT #48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD11000  
BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD02900  
BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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BEAVERTON SCHOOL DISTRICT #48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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BEAVERTON SCHOOL DISTRICT #48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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BEAVERTON SCHOOL DISTRICT #48  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD11200  
BEAVERTON, CITY OF  
000000

1S116AC04600  
BEAVERTON, CITY OF  
4950 SW HALL BLVD  
BEAVERTON OR 97005

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BEAVERTON, CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

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BEAVERTON, CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

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BEAVERTON, CITY OF  
PO BOX 4755  
BEAVERTON OR 97076

1S116AA06000  
BENNETT, CLEVE E & LOIS W TRUS  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

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BENNETT, CLEVE E AND  
LOIS W TRUSTEES  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

1S116AD10400  
BENNETT, TERRI LOUISE  
12820 SW 4TH ST  
BEAVERTON OR 97005

1S116AC01500  
BERRY, HARRY L AND  
DANISH, CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005

1S116BD00801  
BERRY, HARRY L AND  
DANISH, CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005

1S116AA02900  
BIGGI, STEVE AND LAURA  
3843 SW HALL BLVD  
BEAVERTON OR 97005

1S116DB00100  
BLOOMER, DANIEL D MARY L  
4925 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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BOYER, ROBERT M &  
ARRUDA, JANA R  
13370 SW 6TH ST  
BEAVERTON OR 97005

1S116DA90055  
BYRNES, DONALD B & B JERI  
12900 SW 5TH ST  
BEAVERTON OR 97005

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CAFFEE CENTER JOINT VENTURE  
BY ANDERSON & ASSOCIATES  
PO BOX 3821  
BELLEVUE WA 98009

1S116DB03000  
CARSON, JERRY/PAULA  
13270 SW 6TH  
BEAVERTON OR 97005

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CASSIDY, MARI  
4820 SW MENLO  
BEAVERTON OR 97005

1S116DA02500  
CASSON, KATHLEEN ANN  
3818 SE 9TH AVE  
PORTLAND OR 97202

1S116DB03700  
CASTANETTE, JOHN P AND  
KRISTI M  
13355 SW PEARL STREET  
BEAVERTON OR 97005

1S116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

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CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

1S116AC01300  
CHIA, CHIH CHIANG & YEA HUEY  
13200 SW FARMINGTON RD  
BEAVERTON OR 97006

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CHIA, FUO FUH &  
WU, CHUN ER  
13150 SW FARMINGTON RD  
BEAVERTON OR 97005

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COLEMAN, ANNE S  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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COLEMAN, ANNE SCOTT  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC01900  
COOLEY, CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

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COOLEY, CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

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CORP OF PRESIDING BISHOP OF  
CHURCH OF JESUS CHRIST LDS  
TAX ADMIN FILE: 536-249C  
50E NORTH TEMPLE  
SALT LAKE CIT' UT 84150-2201

1S116AD07400  
CRITELLI FAMILY TRUST  
c/o CRITELLI, TINA  
BY GARY A & SHARON L CRITELL  
PO BOX 87058  
VANCOUVER WA 98687

1S116AD10300  
CRITELLI, GARY A & SHARON L TR  
PO BOX 87058  
VANCOUVER WA 98687

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CRITELLI, GARY A & SHARON L TR  
PO BOX 87058  
VANCOUVER WA 98687

1S116DB00200  
D'ALFONSO, PAULA L  
4975 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD10500  
DAMIANA, RONALD G  
GEORGIA M  
12870 SW 4TH  
BEAVERTON OR 97005

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D'SILVA, FELIX & LOURDES  
8765 SW JAMIESON RD  
PORTLAND OR 97225

1S116AA05700  
ETON LANE LTD PARTNERSHIP  
WALKER, HUBERT E TRUSTEE &  
ETON LANE LLC  
PO BOX 14746  
PORTLAND OR 97293

1S116AC01200  
FISHBACK, MERL E &  
GLORIA M TRUST  
FISHBACK, MERL E/GLORIA M TR  
9700 NW GROVELAND RD  
HILLSBORO OR 97124

1S116AC04900  
FRIEZE, REX D SHIRLEY L  
4720 SW FAIRMONT DR  
BEAVERTON OR 97005

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GARTLAND, SHARIN  
5162 SW ERICKSON  
BEAVERTON OR 97005

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GENEST, VIRGINIA E  
BY VALLEY DAIRY QUEEN  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD02600  
GHIM, MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

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GORMAN, JOHN & PATRICIA A  
16555 SW HIGH HILL LN  
BEAVERTON OR 97007

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GREER, DAVID J & LISA D  
19370 SW SOUTHVIEW ST  
ALOHA OR 97007

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DANEIL, HOLLIS MARION  
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BEAVERTON OR 97005

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EIMERS, ANGELA  
6045 SW 167TH AVE  
ALOHA OR 97007

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ETON LANE LTD PARTNERSHIP  
WALKER, HUBERT E TRUSTEE  
PO BOX 14746  
PORTLAND OR 97214

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FLOREN, JANET M AND GREG E  
12870 SW 6TH AVE  
BEAVERTON OR 97005

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GABRIEL, KATHLEEN  
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BEAVERTON OR 97005

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GARVER, JON & REBECCA  
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BEAVERTON OR 97005

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GERETY, JEAN  
4750 SW FAIRMONT DR  
BEAVERTON OR 97005

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GLASNER, GENE R  
c/o DREY, BRIAN S & PAMELA R  
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BEAVERTON OR 97005

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350 NW FREEMAN  
HILLSBORO OR 97124

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PATRICIA A  
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BEAVERTON OR 97005

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13425 SW 6TH  
BEAVERTON OR 97005

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ENGEL, MICHAEL J &  
REBECCA C  
13210 SW 6TH ST  
BEAVERTON OR 97005

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EVANS, CATHERINE B  
11775 SW 11TH STREET  
BEAVERTON OR 97005

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FRANKLIN, JOHN A  
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BEAVERTON OR 97005

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BEAVERTON OR 97005

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BEAVERTON OR 97005

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GLICKMAN, TERRY L  
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BEAVERTON OR 97005

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GREEN, ERNESTINE RUTH  
4820 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

1S116AB02100  
GRL INC  
13095 SW CANYON RD  
BEAVERTON OR 97005

IS116DB10800 GUSTAFSON, CONRAD W & ANNA M 13245 SW PEARL ST BEAVERTON OR 97007	IS116AA02500 GUTZLER, CAROLYN E BY TOM'S PANCAKE HOUSE 12925 SW CANYON RD BEAVERTON OR 97005	IS116AA02600 GUTZLER, CAROLYN E BY TOM'S PANCAKE HOUSE 12925 SW CANYON RD BEAVERTON OR 97005
IS116AA02300 GUTZLER, THOMAS M & CAROLYN E 12925 SW CANYON RD BEAVERTON OR 97005	IS116AA02400 GUTZLER, THOMAS M AND CAROLYN E BY TOM'S PANCAKE HOUSE 12925 SW CANYON RD BEAVERTON OR 97005	IS116DA90058 HALL, LAURA I 12888 SW 5TH BEAVERTON OR 97005
IS116DA01900 HALSTEN, ELSIE M TRUSTEE 12855 SW 6TH BEAVERTON OR 97005	IS116DA02000 HAMREUS, CALVIN E & JOYCE K 5075 SW MAIN AVE BEAVERTON OR 97005	IS116DA07004 HATTENHAUER, CHRISTINE 261 SW ASHDOWN CIR WEST LINN OR 97068
IS116DA07002 HATTENHAUER, CHRISTINE 261 SW ASHDOWN CIR WEST LINN OR 97068	IS116DA01800 HEDGES, SALLY C 12850 SW 5TH AVE BEAVERTON OR 97005	IS116DB04600 HEISLER, MARYANNE & NEUFELL, PATRICIA J 13250 SW PEARL BEAVERTON OR 97005
IS116BD00400 HESS, EVE SINGER, SHARON 2150 NW 139TH PL PORTLAND OR 97229	IS116DA01100 HETU, TED G 5075 SW ANGEL AVE BEAVERTON OR 97005	IS116AD10100 HOUDROJ, ALI 4970 SW MAIN ST BEAVERTON OR 97005
IS116DA90028 HUESMAN, NICOLE K 5134 SW ERICKSON AVE BEAVERTON OR 97005	IS116AA03100 HUNKAPILLAR, PATRICIA W SMITH, HARRIET L AND GEORGE BY TAGGART TRUST, THE 21185 NW EVERGREEN PKW STE 101 HILLSBORO OR 97124	IS116AA03000 HUNKAPILLAR, PATRICIA W BY THE TAGGART TRUST 21185 NW EVERGREEN PKW SUITE 101 HILLSBORO OR 97124
IS116AC00800 HUNT, SHERYLL R/WILLIAM J & HUNT, JONATHAN J 13380 SW FARMINGTON RD BEAVERTON OR 97005	IS116DA02100 HUTCHENS, DANA C & LORIE 12820 SW SIXTH ST BEAVERTON OR 97005	IS116DA06700 HUYGENS, JOSEPH D & JULIE ANN HUTCHINSON 7830 SW LAUREL ST PORTLAND OR 97225
IS116AC05801 JEFFERS, STEPHEN R & REAGAN N 4850 SW MENLO DR BEAVERTON OR 97005	IS116AD09901 JOHNSON, CAROL L R PO BOX 524 SCAPPOOSE OR 97056	IS116AC04000 JONES, E CHANNING & PAULA KAY 13420 SW 2ND AVE BEAVERTON OR 97005
IS116DA90052 JONES, JERRY C 12912 SW 5TH #52 BEAVERTON OR 97005	IS116AC03600 JONES, KRISTIN L 4870 SW FAIRMOUNT DR BEAVERTON OR 97005	IS116DA07000 JUNELL, BONNIE M & BABCOCK, DOUGLAS C 4820 SW 18TH PL PORTLAND OR 97201
IS116AD01600 KABDEBO, JOSEPH & CAMILLE 725 SW VIEWMONT DR PORTLAND OR 97225	IS116AD01700 KABDEBO, JOSEPH & CAMILLE 725 SW VIEWMONT DR PORTLAND OR 97225	IS116AD01800 KABDEBO, JOSEPH AND CAMILLA 725 SW VIEWMONT DR PORTLAND OR 97225

IS116BD00500  
KADEL, RICHARD A & CAROL L  
9350 SW TIGARD ST  
TIGARD OR 97223

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KADEL, RICHARD A & CAROL L  
9350 SW TIGARD ST  
TIGARD OR 97223

IS116BD00700  
KADEL, RICHARD A & CAROL L  
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TIGARD OR 97223

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KAUTH, JANE F  
5115 SW ERICKSON AVE  
BEAVERTON OR 97007

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NIKO, JOHN R &  
ORTIZ, INGRID  
5155 SW MAIN AVE  
BEAVERTON OR 97005

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KIM, YOUNG M & LINDA  
12855 SW CANYON RD  
BEAVERTON OR 97005

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KING, CHARLES C & ROM N  
5150 SW ERICKSON AVE  
BEAVERTON OR 97005

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KLANECKY, ARDITH ANN  
5164 SW ERICKSON  
BEAVERTON OR 97005

IS116DB10900  
KO, JOON B  
13215 SW PEARL ST  
BEAVERTON OR 97005

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KRAEMER, JOHN L & CLAUDIA R  
33713 NE WILLIAMS APT F-6  
SCAPPOOSE OR 97056

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KRASAUSK, PAUL &  
MAHMOOD, WALLY  
8061 SW 168TH ST  
BEAVERTON OR 97007

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KRIEG, MARGIE J  
12924 SW 5TH ST  
BEAVERTON OR 97005

IS116BD00800  
L L & F INVESTMENT CO  
13549 SW ASCENSION DR  
TIGARD OR 97223

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LAMB, KEVIN M &  
YANT, SUSAN A  
12892 SW 5TH ST  
BEAVERTON OR 97005

IS116AA04690  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116AC00500  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

IS116BD00100  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116BD00200  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116BD00300  
LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116AC00600  
LANPHERE, ROBERT  
ATTN: DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

IS116BD00902  
LEE, EUN SANG/MIN JA &  
LEE, MICHAEL S &  
LEE, THOMAS S  
13773 SW FARMINGTON RD  
BEAVERTON OR 97005

IS116DB03100  
LEE, SANDY  
344 NE 156TH AVE  
PORTLAND OR 97230

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LEIVA, LUIS ALONSO & SANDRA J  
13320 SW 6TH ST  
BEAVERTON OR 97005

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LEONARD, CLIFFORD A & SHARON B  
7345 SW 84TH AVE  
PORTLAND OR 97223

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LEWIS, SUNNY M  
13425 SW 3RD  
BEAVERTON OR 97005

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LIDBERG, MARY ALICE C  
13932 SW 5TH AVE  
BEAVERTON OR 97005

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LIEDTKE, CATHERINE A  
17561 SW ALBERT CT  
BEAVERTON OR 97007

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LISICKI, THOMAS D & RUTH S  
13332 SW BENCHVIEW TER  
TIGARD OR 97223

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LOCKARD, DOROTHY J  
LIVING TRUST, THE  
5125 SW SHERWOOD PL  
BEAVERTON OR 97005

IS116AC02800  
LONG, PATRICIA C & JOE PAT  
5070 SW FAIRMONT DR  
BEAVERTON OR 97005



IS116AD06600  
PHAM, HIEN & NGUYEN, THAO &  
NGUYEN, GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006

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PILS, SANDRA J & EVERETT M TRS  
29395 SW HILLECKE RD  
HILLSBORO OR 97123

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PODEBSKI, ADAM P & GUTA H  
PODEBSKI, BENNY  
12436 SW 5TH  
BEAVERTON OR 97005

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PRESERVE SPENCER HOUSE INC  
BY TUALATIN VALLEY HOUSING PAR  
14355 SW ALLEN #130  
BEAVERTON OR 97005

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QUINN, CAROLYN F  
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BEAVERTON OR 97005

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QUTUB, WAHBIM &  
MATAR, RIAD A  
7883 SW BARNARD DR  
BEAVERTON OR 97007

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RANDOLPH, GLORIA  
2725 SW 185TH  
ALOHA OR 97006

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RANDOLPH, GLORIA C  
2725 SW 185TH AVE  
ALOHA OR 97005

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REMCHEK, GEORGE & FLORENCE  
4240 SW 102ND AVE  
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ROBERTS, LEON &  
CARRIE  
4725 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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ROBINSON, JACK A & PATRICIA A  
3830 SE BENTLEY AVE  
HILLSBORO OR 97123

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ROSENHOLM, DAVID  
ROSENHOLM, ANDREA  
13220 SW PEARL ST  
BEAVERTON OR 97005

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SANABRIA-BARILLAS, JOSE ALFRED  
ALVARADO, JUANA &  
MAX, MARTHA L PAR  
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SCALONE, DAVID  
13255 SW 6TH ST  
BEAVERTON OR 97005

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SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

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SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

SDBEA  
BEAV SCH DIST-JAN YOUNGQUIST,  
16550 SW MERLO RD  
BEAVERTON OR 97006

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SESSIONS, KEITH W & LAVERNA M  
TRUSTEES  
12884 SW 5TH  
BEAVERTON OR 97005

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SKORO, MARION & MIRELA  
19300 NW GILLIHAN LOOP RD  
PORTLAND OR 97231

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SMITH, DONALD J PHYLLIS J  
13295 SW PEARL ST  
BEAVERTON OR 97005

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SMITH, IAN C & NADINE T  
12750 SW 4TH ST  
BEAVERTON OR 97005

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SMITH, RICHARD BRENDAN &  
HILARY ANTOINETTE MARY  
12908 SW 5TH ST  
BEAVERTON OR 97005

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SNODGRASS, PHILIP R & PATRICIA  
13420 SW THIRD ST  
BEAVERTON OR 97005

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SOTH, COLLEEN V TRUSTEE  
SOTH, FORREST C TRUSTEE  
4890 SW MENLO DR  
BEAVERTON OR 97005

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SOUTHERN PACIFIC TRANSPORTATIO  
BY UNION PACIFIC RAILROAD CO  
1700 FARNAM ST, 10TH FLOOR SOU  
OMAHA NE 68102

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SPURGEON, JOAN P  
5130 SW ERICKSON  
BEAVERTON OR 97005

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ST CLAIR, JERALD LEE  
SHIRLEY ANN  
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STARKE, DONALD B EDNA A  
13155 SW 6TH  
BEAVERTON OR 97005

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STEICHEN, NANCY & MCCOURT, JOH  
c/o HOVERSLAND, GORDON & MYRNA  
4350 SW CEDAR HILLS BLVD  
BEAVERTON OR 97005

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STEIGER, JOHN M & LOU-WAYNE  
301 NW MURRAY BLVD  
PORTLAND OR 97229

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STOCKWELL, KATHERINE R &  
BROWN, GARY E  
13240 SW FARMINGTON RD  
BEAVERTON OR 97005

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STOLTENBURG, BARBARA  
c/o RAINEY, WALLACE L & LESLIE  
5025 SW ANGEL AVE  
BEAVERTON OR 97005

1S116AC02000  
SURBY, PAUL W L FRANCES  
13375 SW 2ND  
BEAVERTON OR 97005

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THAO, PANCHONG & MAO HER  
4920 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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THOMAS, BARBARA A  
5132 SW ERICKSON  
BEAVERTON OR 97005

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THOMAS, KELLY R &  
BOTHNE, MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005

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THOMAS, KELLY R &  
BOTHNE, MELISSA  
12720 SW 2ND ST  
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THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

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THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

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4500 SW HALL BLVD  
BEAVERTON OR 97005

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THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

1S116DA90056  
THOMPSON, DANIEL HARRISON &  
THOMPSON, TOM G &  
THOMPSON, MARGIE J  
12896 SW 5TH AVE  
BEAVERTON OR 97005

1S116AC06100  
THOMPSON, MAURICE J & KIMBERLY  
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BEAVERTON OR 97005

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THOMPSON, TOMMY G  
JEANNETTE L  
13305 SW 2ND ST  
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TISCHLER, STEPHEN & KATHLEEN  
5195 SW ERICKSON AVE  
BEAVERTON OR 97005

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TOSCO CORP  
PROPERTY TAX DEPT-DC17  
PO BOX 52085  
PHOENIZ AZ 85072

1S116AD07600  
TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

1S116AD07700  
TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

1S116BD02100  
TUALATIN HILLS PARK &  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97006

TVFR  
T-V FIRE & RESCUE  
7401 SW WASHO CT  
TUALATIN OR 97062

1S116AD06700  
UNDERHILL, MICHAEL  
12795 SW 3RD ST  
BEAVERTON OR 97005

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VADEN, MICHAEL & JANE  
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VADEN, MICHAEL & JANE  
12740 SW CANYON RD  
BEAVERTON OR 97005

1S116AA04800  
VADEN, MICHAEL & JANE  
BY LANPHERE ENTERPRISES INC  
ATTN: DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

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VEAZEY, THOMAS C AND  
ROXANNE T  
12770 SW 6TH ST  
BEAVERTON OR 97005

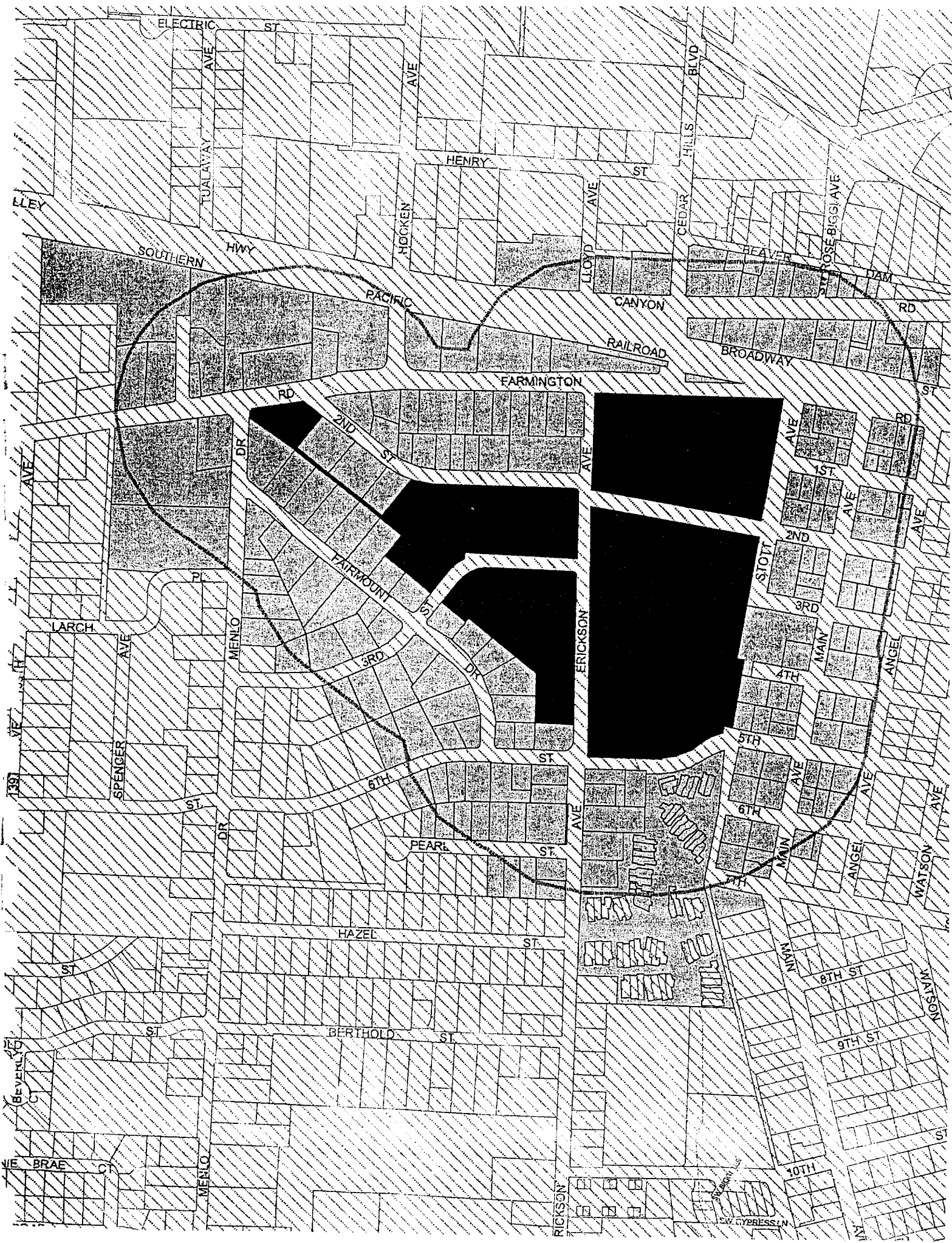
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4770 SW FAIRMONT DR  
BEAVERTON OR 97005

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VON W METHENEY FAMILY TRUST  
4775 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA90026  
VOSE, BENJAMIN A  
PO BOX 73  
TOLOVANA PAF OR 97145

1S116AC03100  
WABNITZ, STEVEN G  
2648 SW 153RD AVE  
BEAVERTON OR 97006



ELECTRIC ST

HENRY ST

SOUTHERN HWY

FARMINGTON ST

RAILROAD

PACIFIC

FARMINGTON

FAIRMOUNT ST

3RD

6TH

PEARL ST

HAZEL ST

BERTHOLD ST

TUALAY AVE

HOCKEN AVE

LLOYD AVE

HILLS BLVD

JOSE BIGGS AVE

BROADWAY

WILEY

AVE

AVE

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AVE

AVE

LARCH

SPENCER AVE

MENLO DR

DR

ERICKSON AVE

STOTT

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3RD

4TH

5TH

6TH

7TH

8TH ST

9TH ST

10TH

ANGEL AVE

WATSON AVE

WATSON

WATSON

CYPRESS LN

**ANGELO EATON**

& Associates

620 SW Main, Suite 201  
Portland, Oregon 97205

NOT DELIVERABLE  
AS ADDRESS  
UNABLE TO FORWARD  
BEAVERTON OR 97005

IS116AC05600  
GARCIA, WAGNER  
4800 SW MENLO DR  
BEAVERTON OR 97005



**ANGELO EATON**

& Associates

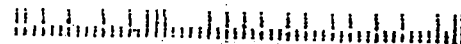
620 SW Main, Suite 201  
Portland, Oregon 97205

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HUNT, SHERYLL R/WILLIAM J &  
HUNT, JONATHAN J  
13380 SW FARMINGTON RD  
BEAVERTON OR 97005

BEAVERTON OR 97005-2005 1000 10 03/31/01  
FORWARD TIME EXP RIN TO SEND  
HUNT  
14230 SW 117TH AVE  
TIGARD OR 97224-2807

RETURN TO SENDER

37605-2807



**ANGELO** ♦ **EATON**

& Associates

620 SW Main, Suite 201  
Portland, Oregon 97205

  
RETURN TO  
SENDER  
Not Deliverable as Addressed  
UNABLE TO FORWARD  
BEAVERTON OR 97005-9998

1S116AC04600  
BEAVERTON, CITY OF  
4950 SW HALL BLVD  
BEAVERTON OR 97005


*Handwritten initials*



**ANGELO** ♦ **EATON**

& Associates

620 SW Main, Suite 201  
Portland, Oregon 97205

  
RETURN TO  
SENDER  
NOT DELIVERABLE  
AS ADDRESS  
UNABLE TO FORWARD  
BEAVERTON

1S116DA01800  
HEDGES, SALLY C  
12850 SW 5TH AVE  
BEAVERTON OR 97005



## NOTICE SIGN MOCK-UP

## PUBLIC MEETING

On A  
Preliminary Development Proposal  
Affecting

13000 SW 2nd Street /  
13000 SW Farmington Rd.

PROPOSED

Street Vacation / Conditional Use

A meeting to discuss the preliminary  
development proposal is scheduled for

September 20, 2001 5-8:30 PM  
Beaverton Community Ctr - 12350 SW 5th St.

ALL INTERESTED PERSONS MAY ATTEND

FOR MORE INFORMATION  
CONTACT:

Frank Angelo (503) 227-3664  
Kirsten Pennington (503) 227-3669

[Use this form and fill in the blank spaces with the information you provided on the notice sign posted at the site if you used the signs available from the City]

H:\FORMS\NEIGHBORHOOD MEETING\MOCKUP.DOC

DEVELOPER OR AGENT: Amalgam Edition + Associates  
PROJECT LOCATION: 13000 SW 2nd St

**AFFIDAVIT OF POSTING NOTICE**

\*\*\*\*\*

I, Kirsten Pennington, being first duly sworn; say that I am (represent) the party intended to submit an application to the City of Beaverton for a proposed Street Vacation/Conditional Use affecting land located at 13000 SW 2nd St., and that pursuant to Ordinance 2050, Section 50, and the guidelines set out by the Planning Director, did on the 30 day of August, 2001, personally post public notice on the proposed development site.

*Sign and Date in the presence of a Notary Public. Certain City staff are Notary Publics and are available for witnessing.*

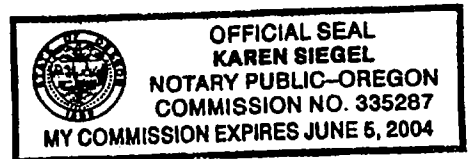
Signature: Kirsten Pennington

Dated this 31 day of August, 2001.

Subscribed and sworn to before me this 31st day of August, 2001.

Karen Siegel  
Notary Public for the State of Oregon

My Commission expires: June 5, 2004



TURN PAGE OVER FOR POSTING INSTRUCTIONS

DEVELOPER OR AGENT: Angelo Eaton & Associates

PROJECT LOCATION: 13000 SW 2nd Street

**AFFIDAVIT OF MAILING NOTICE**

\*\*\*\*\*

I, Robin Scholetzky, being first duly sworn; say that I am (represent) the party intended to submit an application to the City of Beaverton for a proposed street vacation/Conditional Use affecting land located at 13000 SW 2nd Street, and that pursuant to Ordinance 2050, Section 50, and the guidelines set out by the Planning Director, did on the 28th day of August, 2001, personally mail notice to affected property owners and NAC's within 500 feet of the proposed development site.

*Sign and Date in the presence of a Notary Public. Certain City staff are Notary Publics and are available for witnessing.*

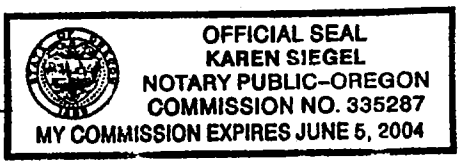
Signature: [Handwritten Signature]

Dated this 29th day of August, 2001.

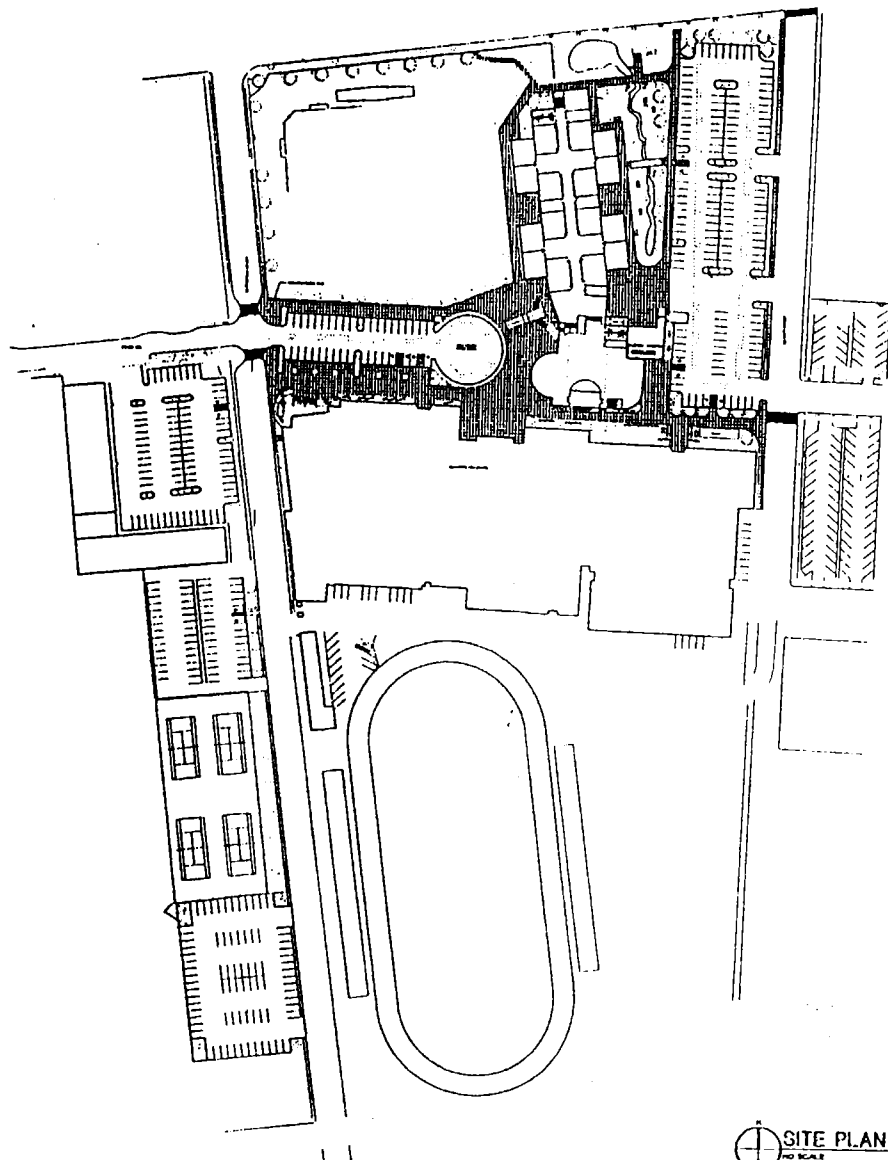
Subscribed and sworn to before me this 29th day of August, 2001.

Karen Siegel  
Notary Public for the State of Oregon

My Commission expires: June 5, 2004







SITE PLAN  
NO SCALE



ARCHITECTURE  
PLANNING/INTERIORS  
TEL: (503) 274-5555  
FAX: (503) 274-0888  
411 NW Frontiers St.  
Beaverton, OR 97005

PRELIMINARY

BEAVERTON HIGH SCHOOL  
CAFETERIA  
BEAVERTON SCHOOL DISTRICT # 48  
13000 S.W. 2nd ST., BEAVERTON, OREGON 97005

Drawn by: J.C.P.  
Checked by: J.C.P.  
Drawing Title: CAFETERIA

Sheet No.  
5.0

11/22/05

## MEETING MINUTES

Beaverton School District, Beaverton High School Street Vacation and Master Plan  
Neighborhood Meeting, Central Beaverton Neighborhood Association Committee  
September 20, 2001

8:00 – 8:30

Beaverton Community Center, Community Room  
12350 SW 5<sup>th</sup> Street, Suite 100  
Beaverton, OR 97005

Presented by: Frank Angelo, Angelo Eaton & Associates  
Mike Maloney, Beaverton School District  
Russ Pitkin, LSW Architects  
Vlad Voytilla, Beaverton School District

### I. Overview of Project (Frank Angelo)

- Described the street vacation, SW 2<sup>nd</sup> Street, between Stott Street and Erickson Avenue
- Description of proposed overall site master plan (new building to house cafeteria and classroom facilities, to be located north of existing high school building)
- New parking lot at SW 5<sup>th</sup> Street and Erickson Avenue and other locations for a total of 500 new parking spaces
- Proposed for development in two Phases

#### *Phase 1*

- Vacation of SW 2<sup>nd</sup> Street
- Temporary bus loading on SW 2<sup>nd</sup> Street
- Pedestrian connections to the surrounding area
- New cafeteria addition
- Modification of Merle Davies Building
- Funded by school board levy

#### *Phase 2*

- 60,000 square feet of class space
  - Expansion of parking facilities
  - Removal of Merle Davies building
  - Additional parking on Erickson
  - Enhanced pedestrian connection
  - Creation of an urban campus
- The application submitted will contain an application for a street vacation and a Master Plan
  - In terms of timing, the Phase 1 funding has been secured and will be completed as soon as possible with Phase 2 to follow when funding is available

### II. Questions (Project Team)

(note: Questions and Answers are not verbatim).

1. What type of community space will be offered?

- A. Cafeteria space will be available for residents to use free of charge.
2. Removal of the Merle Davies building—can it be avoided, what about the existing food court and when will that be removed as part of the demolition?
  - A. The Merle Davies building would be very expensive to rehabilitate as a high school building. Removal of the building is necessary to provide the improvements.
3. Where will busses load and unload after the second phase is completed?
  - A. There are two bus loading options, loading and unloading on either Erickson and Stott or they will locate the bus loading area in the Erickson parking area—adjacent to the parking area.
4. Historic Building demolition—I'm very disappointed.
  - A. Comment noted.
5. (Regarding the closure of 2<sup>nd</sup> Street)...I'm concerned about the closure of another street in Beaverton; it is already tough to get around.
  - A. The street vacation is seen as a way to connect the campus and also to connect the surrounding area with the school. The street vacation is the best way to use the existing land on the campus. Other options were considered, but this was the best way to utilize the space available to the School District. Pedestrian and bicycle access will continue to be provided through the campus.
6. Erickson is a busy street---What about safety concerns?
  - A. The City Engineer and the Beaverton School District Traffic Engineer are reviewing options to address traffic circulation and safety concerns. It is likely that they City Engineer will ask for some street improvements and safety precautions.
7. How will bus loading and bus unloading operate?
  - A. The BSD won't know until the decision has been made on where to locate the facilities. We know that there are safety concerns and that the intersections are difficult due to the bus traffic. We will be working with the City to develop the best solution for traffic. We know that traffic concerns are important for the community.
8. What is the number of busses currently running? How many students take the bus?
  - A. At least two thirds of the students take the bus and there are currently about 11 busses being used each day. BSD will confirm this number.
9. As a resident, I hate to see 1700 students being crammed into one school—I like the idea of having smaller magnet schools.
  - A. BSD has school within a school programs and the extra classroom space that we are asking for helps to facilitate this. The School District has some magnet schools, but a lot of small campuses are difficult to administrate and cost much more for janitors, cafeteria service etc.

10. There is a need in Beaverton for historic buildings -- I hate to see one being torn down and replaced with a parking lot.
  - A. The building is listed on the City's Goal 5 Historic inventory but not on any national lists. It is a concern to remove the building, however, economically, it's just not feasible to retrofit the building as a high school structure. It would be very expensive. The architect will be incorporating elements of the older building within the new construction, such as windows.
  
11. Why is the new cafeteria on two floors?
  - A. The cafeteria is on two floors to match the Phase 2 building and to match the existing construction, which has two floors. It also provides an opportunity for using the High School space as meeting space for the community and a place for the school to have dances, etc. The school doesn't have any place like this right now. It is important to centralize the space instead of having three separate cafeterias.
  
12. What is the number of total parking spaces?
  - A. There are a total of 508 total parking spaces. This will bring the site into compliance with City code. We expect the spaces to be completely utilized on event nights.
  
13. Why does Phase 2 add a total of 34 classrooms?
  - A. This is because of the anticipated increase in student population to the year 2010. The School District is using 2,200 students as the anticipated high school student population for the District's high schools.
  
14. What about the overall size of the school as being dehumanizing?
  - A. The new design will create small spaces within the school in order to be more comfortable and to provide a better learning environment.

**Beaverton Central Neighborhood Association Meeting**  
**Neighborhood Review Meeting for Beaverton School District Application**  
**September 20, 2001 8:00 to 8:30 PM**

PLEASE PRINT YOUR INFORMATION BELOW

Name	Address	Phone
JOSE GALANTE	4925 SW 14th Ave Beaverton OR 97005	503 642 1111
William J. [unclear]	13570 SW Clackamas Ave. 97005-2431	503 642 1111
Carol Franklin	5025 SW Fairmount Beaverton 97005	
Jack	" " " "	" "
W. A. Tolson	13755 S.W. Bonn Bee Ct Beaverton 97005	
Tacob D Godden	11815 SW 5th (503) 762-1026	
AL HAMREUS	5075 S.W. MAIN	644
Joe King	4925 SW 14th Ave 97005	
Deanna [unclear]	14660 SW Madras Ave Trout 97239	
Randy Kaiser	4570 SW COMPTON 503 350 0868	



Beaverton High

Beaverton Central Neighborhood Association Meeting  
Neighborhood Review Meeting for Beaverton School District Application  
September 20, 2001 8:00 to 8:30 PM

PLEASE PRINT YOUR INFORMATION BELOW

Name	Address	Phone
Jim & Linda Christensen	13325 SW 2nd	503-643-0433
Vicki Van Buren	9908 NW Abbey Rd Pld	503-296-4380
Mike Howser	3536 NE 25th Ave Pld	503-254-3022
Laurie Fishback	9700 NW Greenwood Rd Hillsboro 97124	503-477-7931
RUSSEL FISHBACK	1953 SW 25th Ave, Beaverton 97005	503-296-4380
RUSSEL L. PITKIN	1953 NW KEARNEY PORTLAND 97209	503-274-5432
Laurie Gundersen	The Oregonian	
Louis HAGG	12636 NW Barnes Rd	503-469-8300

Central Beaverton Neighborhood Association Committee  
Regular Board Meeting - Sept. 20, 2001

Call to order and greeting 6:30 PM

Board Members Present: Sue Kozak, Chair, Carol Franklin, Vice Chair, Jack Franklin, Rose Galante, Jacob Godden, Freddie McBride, D.A. 'Tobie' Tobiason, Randy Kaiser

Board Members Absent: Dorothy Fisher(excused), Rita McCormick (excused)

Others Present: Steve Enyart, Beaverton Police Dept., Louis Haga, Larry Eisenberg, Frank Angelo, Ginny Kingsley, Kathy Aulwes, Cal Hamrens, Louise Penion, Lyle Stevenson, Jim Armstrong, Linda Armstrong, Vickie Van Buren, Mike Howser, Gloria Fishback, Loren Fishback, Russel Pitkin, Laura Gunderson, Robin Scholetzky, Alisa Brodhay Pyszka

Presentation: Robin Scholetzky, Angelo Eaton & Associates-Proposed construction of a loading dock at the Washington County Elections Office. Handouts of the 9/20/01 meeting agenda and the drawings from DiLoreto Architects were given out. The loading dock is needed for handling the mail during election times. Kathy Aulwes of DiLoreto Architects presented the model and discussed screening and landscaping materials. Completion is due Spring 2002; approx. cost \$75-100K at completion.

Presentation: Alisa Brodhay Pyszka, WRG Design Inc. Proposed Comprehensive Plan Amendment on the Round. Drawings and discussion of changes in plans for the Round. Addition of green spaces, landscaping, removal of Esplanade Road and adding a parking structure. Discussion and questions keyed in on changing from proposed condo units to proposed rental units. Parking will have 256 parking stalls in the first phase. Removal of Esplanade Road. General discussion revealed concerns for safety crossing Canyon Road to get to the Round.

Presentation: Frank Angelo, Angelo Eaton and Associates. Proposed Development at Beaverton High School. Overview and discussion centered on added classroom and cafeteria space. Merle Davies School is to be demolished to accommodate those changes plus added parking spaces. Phase 2 is not funded yet. Jack Franklin received two calls from citizens concerned about the removal of Merle Davies School, which is on the City's register of Historical Buildings. Concern was raised as to the buses unloading/loading on Erickson. Questions about traffic flow and the safe unloading of school busses could not be responded to until the City Engineer made the decision on the best approach.

1. Beaverton Police Report: Officer Enyert provided crime statistics for August 2001. Again we are in the busiest district for calls taken. A list of details was provided for the Board to review.
2. TVF&R Report: Not present / no report this month.
3. Minutes: Approved as submitted
4. Treasurer's report: CBNAC received a check for \$340 for City Clean-Up volunteers. Check will be given to the Treasurer for deposit.
5. CCI Report:-Last meeting was a general discussion; Washington County is asking for applicants for 2 vacancies on Land Use Ordinance Advisory Commission; County Planning Division is planning 3 open houses in September. One coming up 9/24; Beaverton Resource Center open house is 9/29; Wa. Co. Historical Museum fundraiser is on 10/20; study group started on the Farmington Road improvement project.
6. Announcements:
  - All CBNAC mail received is on the table for viewing by all.
  - Discussion on upcoming election of CBNAC officers. Nominations for offices will be taken at the Oct meeting and elections will be held at the General Meeting in Nov.
7. New Business/Discussion and concerns:
  - Schiffler Park Report- Dorothy Fisher sent minutes of last meeting. Discussion regarding CBNAC's plan to help the park with available City matching funds. Pricing on suggested projects was not available. We will need more details to make a firm decision.
  - Merle Davies School-There should be a public hearing regarding taking the school off the Historic Sites list. Cal asked that the CBNAC support keeping it on the list. Motion to support was made and seconded. Passed unanimously. CBNAC will write a letter supporting keeping Merle Davies School on the list of Historic Building in Beaverton and modifying the expansion of Beaverton H.S. so as not to demolish it.
  - Randy made a motion that some CBNAC funds be contributed to the NYC Disaster relief efforts. Discussion led to the decision that NW Medical Teams would be the best recipients of those funds for that purpose. Motion was amended to have \$500 contributed to NW Medical Teams to be used for the NY Relief effort as they see fit. Vote was taken and passed unanimously.

Meeting adjourned 9PM

Submitted by Rose Galante (Temporary Recorder)



& Associates

620 SW Main Suite 201  
Portland OR 97205  
503-224-6974  
503-227-3679 – FAX

# TRANSMITTAL

**To:** Susan Kozak

**From:** Robin Scholetzky  
Kirsten Pennington

**Firm:** Chair, Central Beaverton NAC

**Date:** 10/15/01

**Address:** 4925 SW 141<sup>st</sup> Ave  
Beaverton, OR 97005

**RE:** September 20, 2001  
Neighborhood Meeting Notes  
1) Washington Co. Elections Division  
2) BHS Street Vacation/General Plan

**Phone:** 503-643-3818

**Cc:** Frank Angelo, Angelo Eaton & Associates  
Vlad Voytilla, Beaverton School District  
Larry Eisenberg, Washington County

**Urgent**     **For Review**     **Please Comment**     **Please Reply**     **Please Recycle**

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Hi Susan,

We have enclosed copies of the meeting notes from the September 20, 2001 Central Beaverton Neighborhood Association Committee (NAC), specifically for (1) the Washington County Elections Division loading dock project and (2) the Beaverton School District Beaverton High School street vacation and general plan project. As per Beaverton Development Code Section 50.10.1.B.5, we will be submitting these notes as part of the applications to the City of Beaverton.

Per Section 50.10.1.B.5, the meeting notes include the meeting date, time, and location in addition to meeting content. We have also attached copies of the meeting sign-in sheets.

Please do not hesitate to contact us if you have additional questions or comments. Thank you for the opportunity to share our planning process with you and your neighbors.

Angelo Eaton & Associates



## **APPENDIX D**

### Sensitive Areas Certification Form

**Property Owner**

Name BEAVERTON SCHOOL DISTRICT

Address 16550 SW Merlo Road

City/State/Zip BEAVERTON OR 97006

Telephone 503 591-4467 Fax 503 591-4469

E-mail vlad\_voytilla@beavton.k12.or.us

**Authorized Agent**

Name Harper Houf Righellis, INC GARY ALFSON

Address 5200 SW Macadam suite 580

City/State/Zip PORTLAND OR 97201

Telephone 503 221-1131 Fax 503 221-1171

E-mail gary@hrcivil.com

**Project Location**

Street, road, or other descriptive location 2ND Street between STOTT & ERICKSON

**Legal Description:**

Quarter	Section	Township	Range
	-	15 1	
In or near (city or town)	County	Tax Map #	Tax Lot #
BEAVERTON	WASHINGTON	16AD	10900 & 11100
Waterway	River Mile	Latitude	Longitude

**Adjacent Property Information:**

Street, road, or other descriptive location

**Legal Description:**

Quarter	Section	Township	Range
In or near (city or town)	County	Tax Map #	Tax Lot #
Waterway	River Mile	Latitude	Longitude

File Number

An on-site, water-quality-sensitive area reconnaissance was completed on:

Date 12/17/01 By Chris Alfano PE Title Project Manager Company Harper Hoof Righelli's

A. Existence of Water-Quality-Sensitive Areas

- As defined in the District's Design and Construction Standards, water-quality-sensitive areas:
[ ] do [X] do not exist on site (check appropriate box).
[ ] do [X] do not exist within 200' on adjacent properties, or [ ] unable to evaluate adjacent property (check appropriate box).

- If water-quality-sensitive areas exist, complete Section B below.
If water-quality-sensitive areas do not exist, skip Section B, sign this form and submit to the District with plan approval package.

B. Types of Water-Quality-Sensitive Areas

The type(s) of water-quality-sensitive area(s) that occur on site or within 200 ft on adjacent properties are (check all that apply):

- [ ] wetland(s) [ ] spring(s) [ ] intermittent stream(s) [ ] perennial stream(s) [ ] ponds

Sign this form and submit to the District with plan approval package and one (1) copy of the Natural Resources Assessment Report (information and forms are available through the District).

- The Natural Resources Assessment Report includes:
Wetland Delineation Report per DSL / Corps reporting requirements (if wetlands present).
Rapid Stream Assessment Technique Form and maps or other District-approved assessment (if construction or discharge is proposed into, through, or across an intermittent or perennial streams).
Vegetated corridor documentation, including a base map and photographs showing the surveyed location of all sensitive areas, vegetated corridors, and vegetated corridor condition.

I certify that I am familiar with the information contained in this document, and to the best of my knowledge and belief, this information is true, complete, and accurate.

Property Owner:

Print/Type Name: Wesley Votaw
Signature: [Handwritten Signature]

PROJECT MANAGER
PEARSON SCHOOL DISTRICT
Print/Type Title
Date: 12/18/01

## **APPENDIX E**

Water Quality Report for:

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**Beaverton High School**

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**Project Location:**

TAX MAP 1N-1 16AD  
TAX LOT 11100  
TAX LOT 10900  
WASHINGTON COUNTY, OREGON

**Prepared For:**

BEAVERTON SCHOOL DISTRICT #48  
16550 SW MERLO ROAD  
BEAVERTON, OREGON 97006

**Prepared By:**

**Harper Houf Righellis, Inc.**

5200 SW MACADAM AVENUE - SUITE 580  
PORTLAND, OREGON 97201  
(503) 221-1131  
Fax (503) 221-1171

**December 2001**

## **Summary**

The objective of this report will be to analyze and document the hydrologic and hydraulic characteristics of the proposed development. This report will outline a plan to meet City of Beaverton's requirements for management of stormwater quality.

Second Street between Stott Street and Erickson shall be redeveloped. Currently Second Street is a paved street with parking. Proposed shall be new parking and a new building for the school. The additional impervious area created from the redevelopment shall be treated for water quality using a vegetated swale.

## **Calculations**

The additional new impervious area is 10,000 sq ft. Using the vegetated swale worksheet, the water quality flow for the additional new impervious area is 0.021 cfs.

The vegetated swale shall have the following characteristics, length 100', bottom width 2', slope 0.5%, and side slopes 3:1. See vegetated swale worksheet.

## VEGETATED SWALE WORKSHEET

Project Name:	Beaverton High School
HHR Project Number:	BSD-23
Description:	Water Quality Swale
Computed By:	JLB
Company:	Harper Houf Righellis, Inc.
Address:	5200 SW Macadam Suite 580
	Portland, Oregon 97201
Phone Number:	503-221-1131

### Impervious Area Calculation.

#### Existing Impervious Area:

Buildings	0 sq. ft.
Parking Area	0 sq. ft.
Sidewalks	0 sq. ft.
Roadway	0 sq. ft.
Other	0 sq. ft.
<b>Total</b>	<b>0 sq. ft. = 0.00 acres</b>

#### Added Impervious Area:

Impervious Area	10,000 sq. ft.
Parking Area	0 sq. ft.
Sidewalks	0 sq. ft.
Roadway	0 sq. ft.
Other	0 sq. ft.
<b>Total</b>	<b>10,000 sq. ft. = 0.23 acres</b>

#### Total Impervious Area:

**10,000 sq. ft. = 0.23 acres**

### Design Flow Rate.

Design Storm: 0.36 inches of rain falling in 4 hours.

Calculate the volume to be treated.

$$\text{Vol.} = (0.36) \times (1 \text{ ft.}/12 \text{ in.}) \times \frac{10,000 \text{ sq. ft.}}{\text{imp. area}} = \frac{300 \text{ cu. ft.}}{\text{volume}}$$

Calculate the design flow rate over the 4 hour storm period.

$$\text{Flow rate} = \frac{300 \text{ cu. ft.}/(4 \text{ hrs.} \times 3600 \text{ sec.})}{\text{volume}} = \underline{0.021 \text{ cfs}}$$

### Trapezoidal Swale Characteristics.

Design Flow Rate:	<u>0.021 cfs</u>
Manning's "n" value:	<u>0.24</u>
Bottom Width:	<u>2 ft.</u>
Side Slopes (both sides)	<u>3 H:1V</u>
Channel Slope:	<u>0.005 ft./ft.</u>
Flow Depth	<u>0.10 ft.</u>
Flow Top Width	<u>2.62 ft.</u>
<b>Flow Velocity:</b>	<b><u>0.088 ft./sec</u></b>

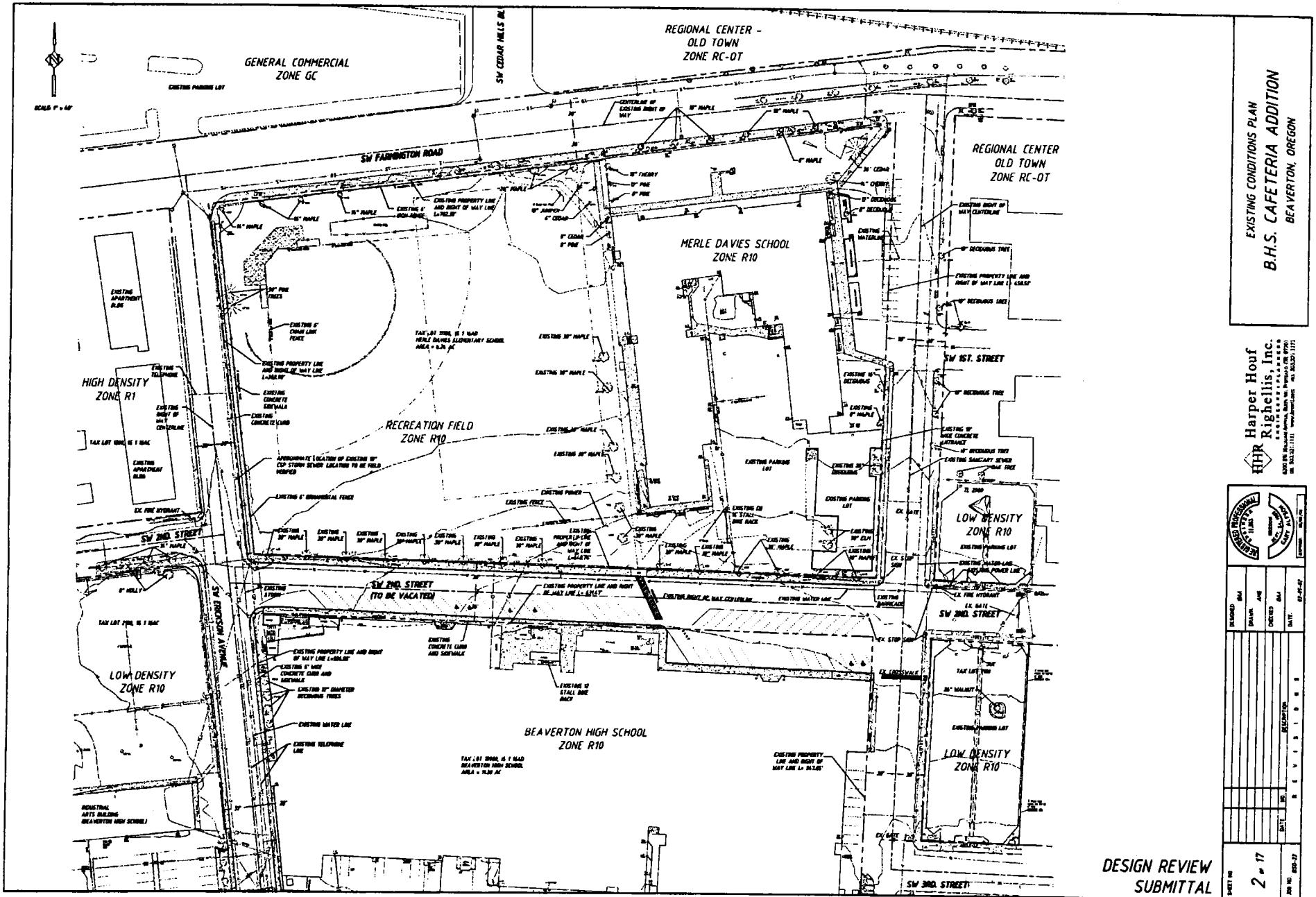
**Required Swale Length.** Calculate the swale length needed for the 9 minute residence time.

$$(9 \text{ min.}) \times (60 \text{ sec./min.}) \times \frac{0.088 \text{ ft./sec.}}{\text{velocity}} =$$

**47.43 foot minimum**  
Use a 100 foot bioswale







SCALE 1" = 40'

EXISTING CONDITIONS PLAN  
**B.H.S. CAFETERIA ADDITION**  
 BEAVERTON, OREGON

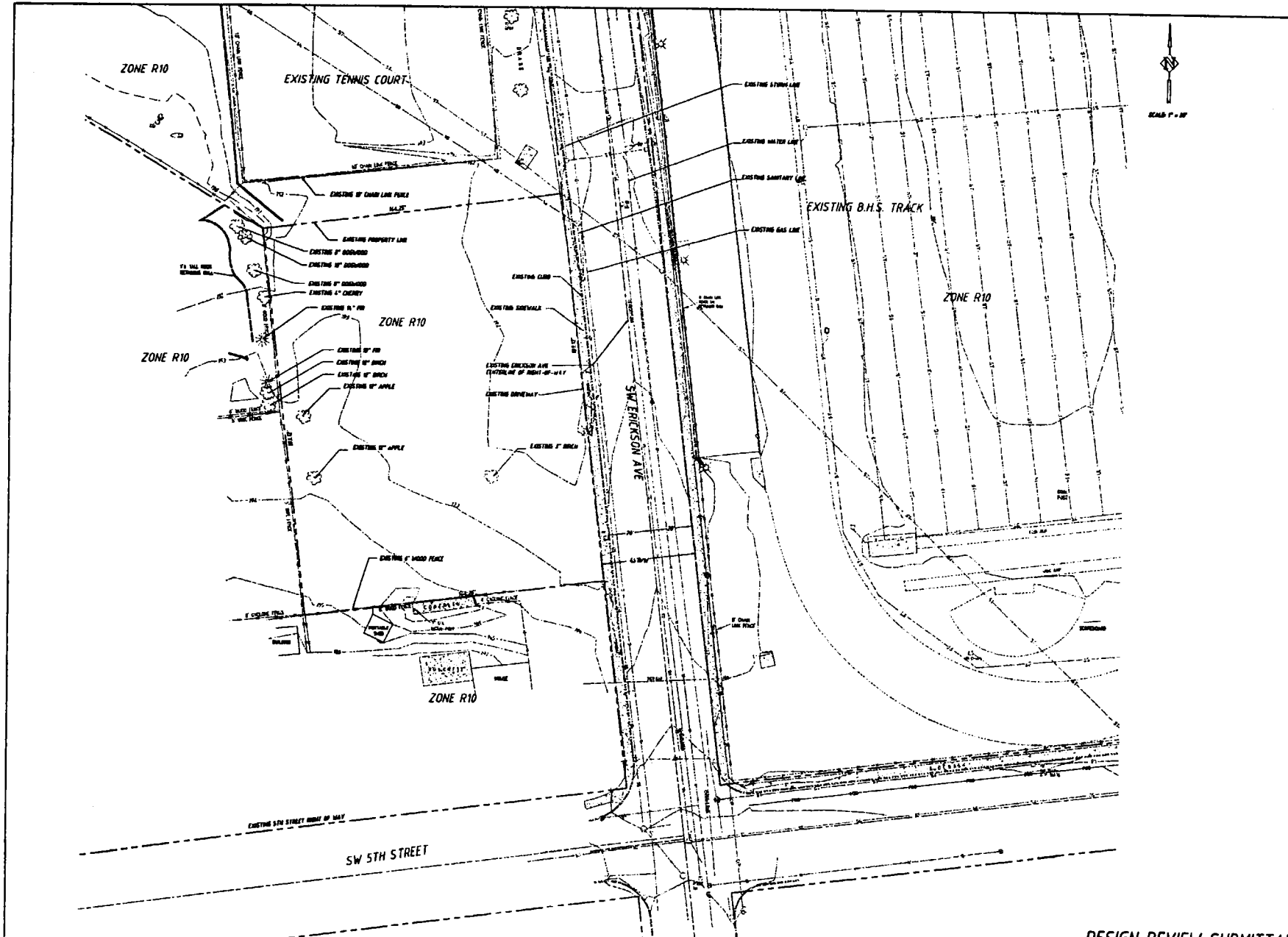
**Harper Houf**  
**RHR Righellis, Inc.**  
 4300 SE Beaverton Avenue, Suite 100, Beaverton, OR 97005  
 503.638.1171



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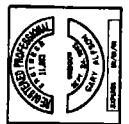
DESIGN REVIEW  
 SUBMITTAL

SHEET NO. **2** OF 17  
 REV. NO. 000-07



EXISTING CONDITIONS PLAN  
**B.H.S. FUTURE PARKING ADDITION**  
 BEAVERTON, OREGON

**Harper Houf**  
**Righellis, Inc.**  
 8500 NE 28th Avenue, Suite 200  
 Beaverton, Oregon 97005  
 Phone: 503.231.1111 Fax: 503.231.1171



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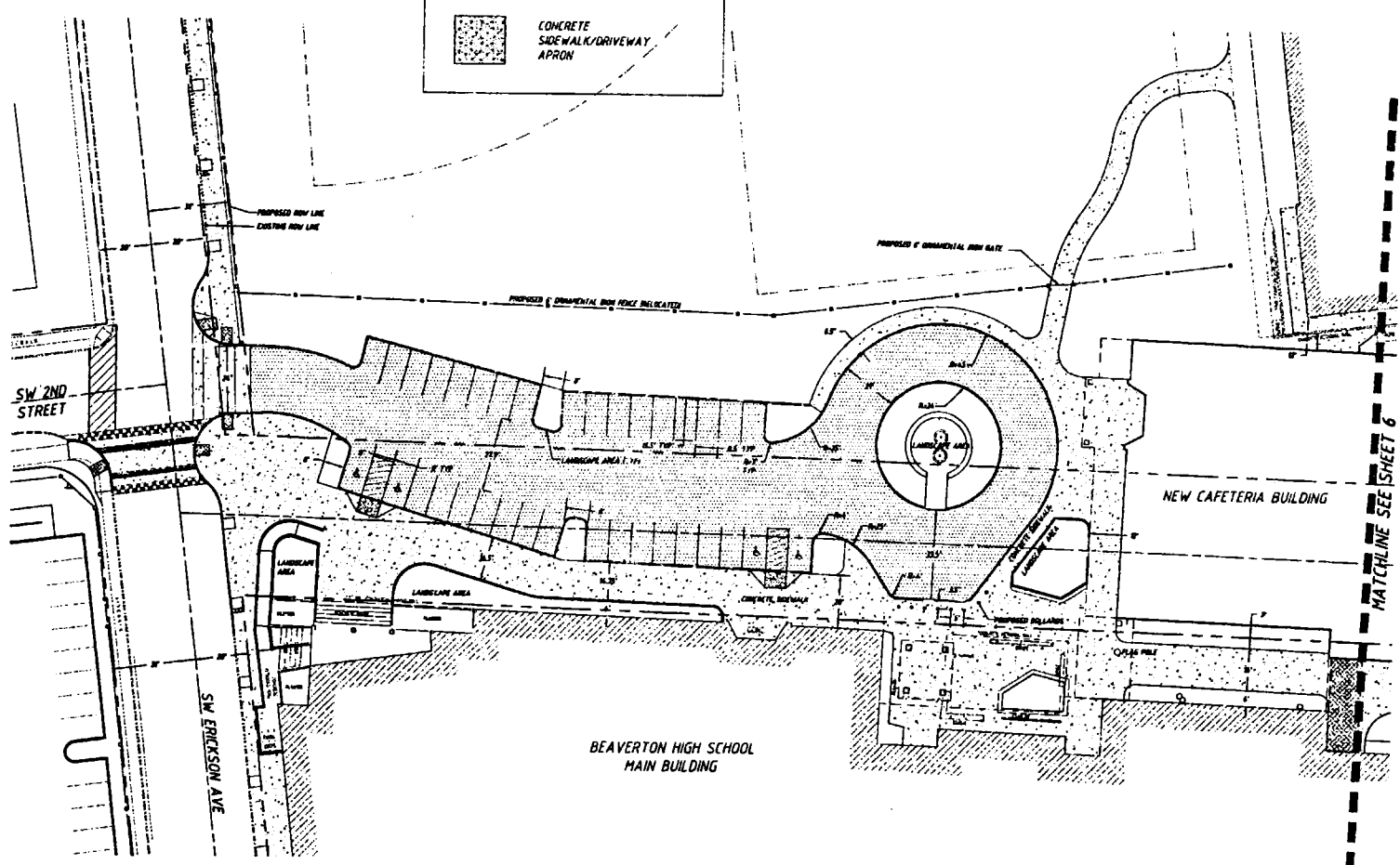
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 CHECKED BY: \_\_\_\_\_  
 APP'D. BY: \_\_\_\_\_





**PROPOSED LEGEND**

	ASPHALT PAVEMENT
	CONCRETE SIDEWALK/DRIVEWAY APRON



DIMENSION PLAN PRIVATE IMPROVEMENTS (WEST)  
**B.H.S. CAFETERIA ADDITION**  
 BEAVERTON, OREGON

**Harper Houf Righellis, Inc.**  
 ENGINEERS ARCHITECTS  
 5305 SW MacLean Avenue, Beaverton, Oregon 97005  
 TEL: 503.231.1311 FAX: 503.231.1170  
 WWW.HARPERHOUF.COM

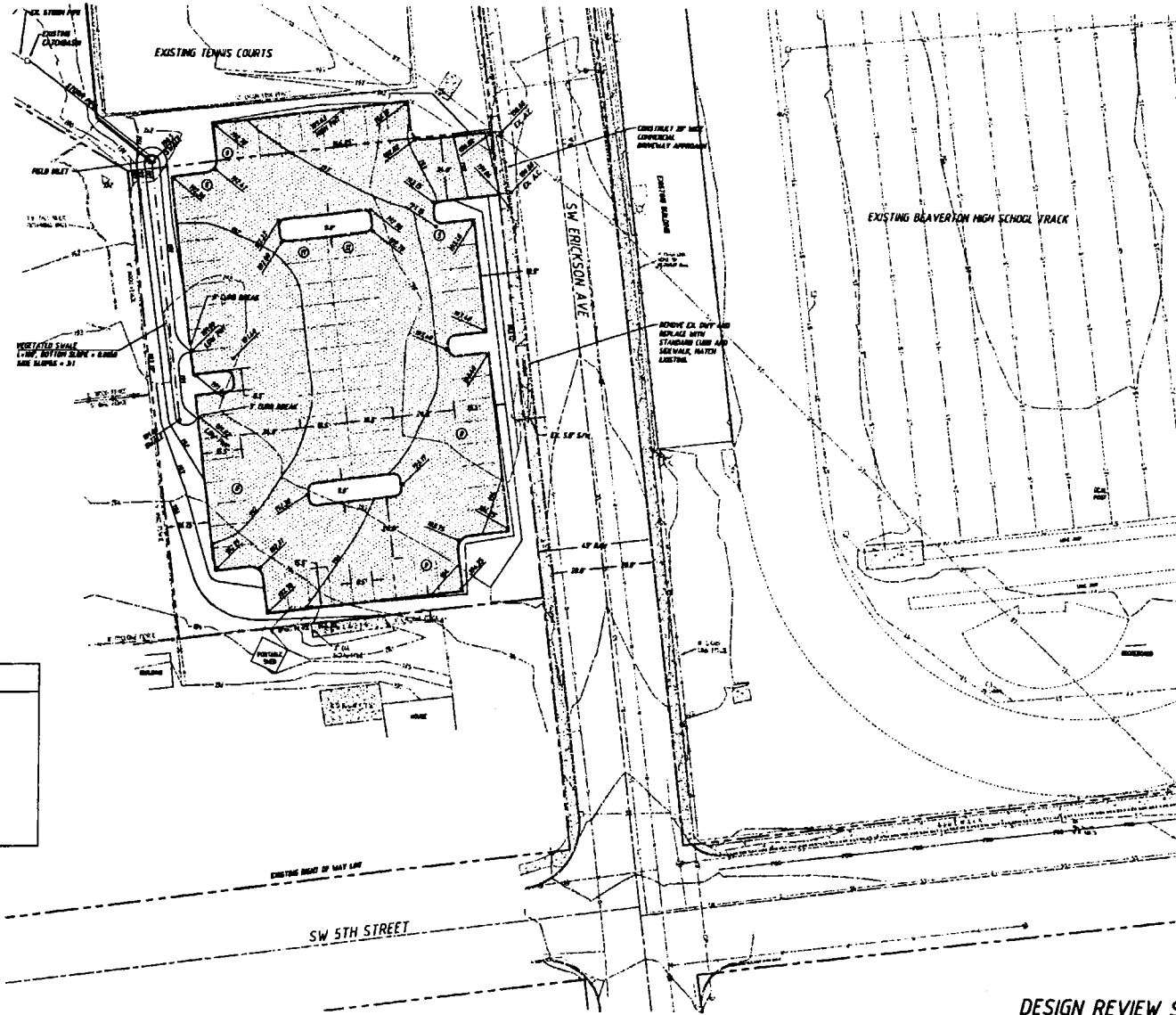
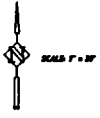


DATE: 11-20-23	BY: JHR	NO. OF SHEETS: 17	SHEET NO.: 5
PROJECT: B.H.S. CAFETERIA ADDITION	SCALE: AS SHOWN	DATE: 11-20-23	BY: JHR
DESIGNED BY: JHR	CHECKED BY: JHR	DATE: 11-20-23	BY: JHR
DRAWN BY: JHR	DATE: 11-20-23	BY: JHR	
PROJECT NO.: 23-001	DATE: 11-20-23	BY: JHR	
PROJECT NAME: B.H.S. CAFETERIA ADDITION	DATE: 11-20-23	BY: JHR	
PROJECT ADDRESS: 5305 SW MacLean Avenue, Beaverton, Oregon 97005	DATE: 11-20-23	BY: JHR	
PROJECT CONTACT: HARPER HOUF RIGHELLIS, INC.	DATE: 11-20-23	BY: JHR	
PROJECT PHONE: 503.231.1311	DATE: 11-20-23	BY: JHR	
PROJECT FAX: 503.231.1170	DATE: 11-20-23	BY: JHR	
PROJECT WEBSITE: WWW.HARPERHOUF.COM	DATE: 11-20-23	BY: JHR	

DESIGN REVIEW SUBMITTAL

5 of 17





PROPOSED LEGEND	
	ASPHALT PAVEMENT
	EXISTING CONCRETE SIDEWALK/DRIVEWAY APRON

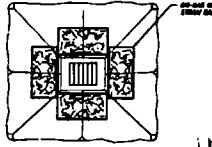
DIMENSION, GRADING AND UTILITY PLAN  
**B.H.S. FUTURE PARKING LOT**  
 BEAVERTON, OREGON

**Harper Houf  
 Righellis, Inc.**  
2209 SW Madison Avenue, Suite 200, Beaverton, OR 97005  
 TEL: 503.221.1151 FAX: 503.221.1171

SHEET NO.				
	DATE	SCALE	DATE	SCALE
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PREPARED BY: J. J. RIGHELLI DRAWN BY: J. J. RIGHELLI CHECKED BY: J. J. RIGHELLI DATE: 06-29-97				
B E A V E R T O N				
JUN 29 1997				

DESIGN REVIEW SUBMITTAL





**BOFILTER BAGS/STRAW  
BALES-TEMPORARY CATCH  
BASIN PROTECTION**

**LEGEND**

	ASPHALT OR PORTLAND CEMENT FINISH GRADE ELEVATION AND 4" FOR TOP OF CURB
	EXISTING GRADE
	NEW ASPHALT PAVEMENT
	CONCRETE
	FILTER BAG AND STRAW PROTECTION
	PROPOSED CENTERLINE
	EXISTING CENTERLINE

**EROSION CONTROL**

**TEMPORARY EROSION CONTROL:**  
THE CONSTRUCTION NOTES AND DETAILS SPECIFY RECOMMENDED PROCEDURES AS ADDRESSSED IN THE EROSION CONTROL PLANS TECHNICAL BARRIAGE DRAWINGS AS ADOPTED BY THE CITY OF BEAVERTON. IT IS THE INTENT OF THESE PLANS TO SHOW POSSIBLE APPLICATIONS OF RECOMMENDED PROCEDURES IN THE EVENT THAT EROSION CONTROL IS NEEDED. ALL RECOMMENDED PROCEDURES ARE DEPENDENT ON CONSTRUCTION METHODS, STAGING AND CONDITIONS, WEATHER AND SCHEDULING. THE PROCEDURES SHOWN ON THE PLANS ARE NOT INTENDED TO BE EXCLUSIVE OF ALL THE PROTECTION REQUIRED ON AN AREA AT A PARTICULAR INSTANT. THEY SHOULD BE USED AS A GUIDELINE ONLY.

THE CONTRACTOR IS RESPONSIBLE FOR CONTROL OF SEDIMENT TRANSPORT WITHIN PROJECT LIMITS DURING CONSTRUCTION. IF AN INSTALLED EROSION CONTROL SYSTEM DOES NOT ADEQUATELY CONTROL SEDIMENT ON SITE, THEN THE EROSION CONTROL MEASURES MUST BE FIELD ADJUSTED BY THE CONTRACTOR AS NECESSARY FOR CONTROLLED STORM EVENTS TO INSURE THAT SEDIMENT LAUNCH MATTER DOES NOT LEAVE THE SITE.

TEMPORARY EROSION CONTROL METHODS MUST REMAIN AND BE MAINTAINED UNTIL PERMANENT EROSION CONTROL METHODS ARE IN PLACE AND OPERATIONAL. THESE METHODS SHALL BE REMOVED ONLY WHEN STABILIZATION HAS BEEN ACHIEVED.

ADDITIONAL EROSION MEASURES WILL INCLUDE, AT A MINIMUM, INSTALLATION OF 1/4" DIA. SEDIMENT BARRIERS ON 2:1 SLOPES.

PERMANENT EROSION CONTROL - VEGETATION ON ALL EMBANKMENTS AND DISTURBED AREAS SHOULD BE REESTABLISHED AS SOON AS CONSTRUCTION IS COMPLETED.

THE EROSION CONTROL MEASURES SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.

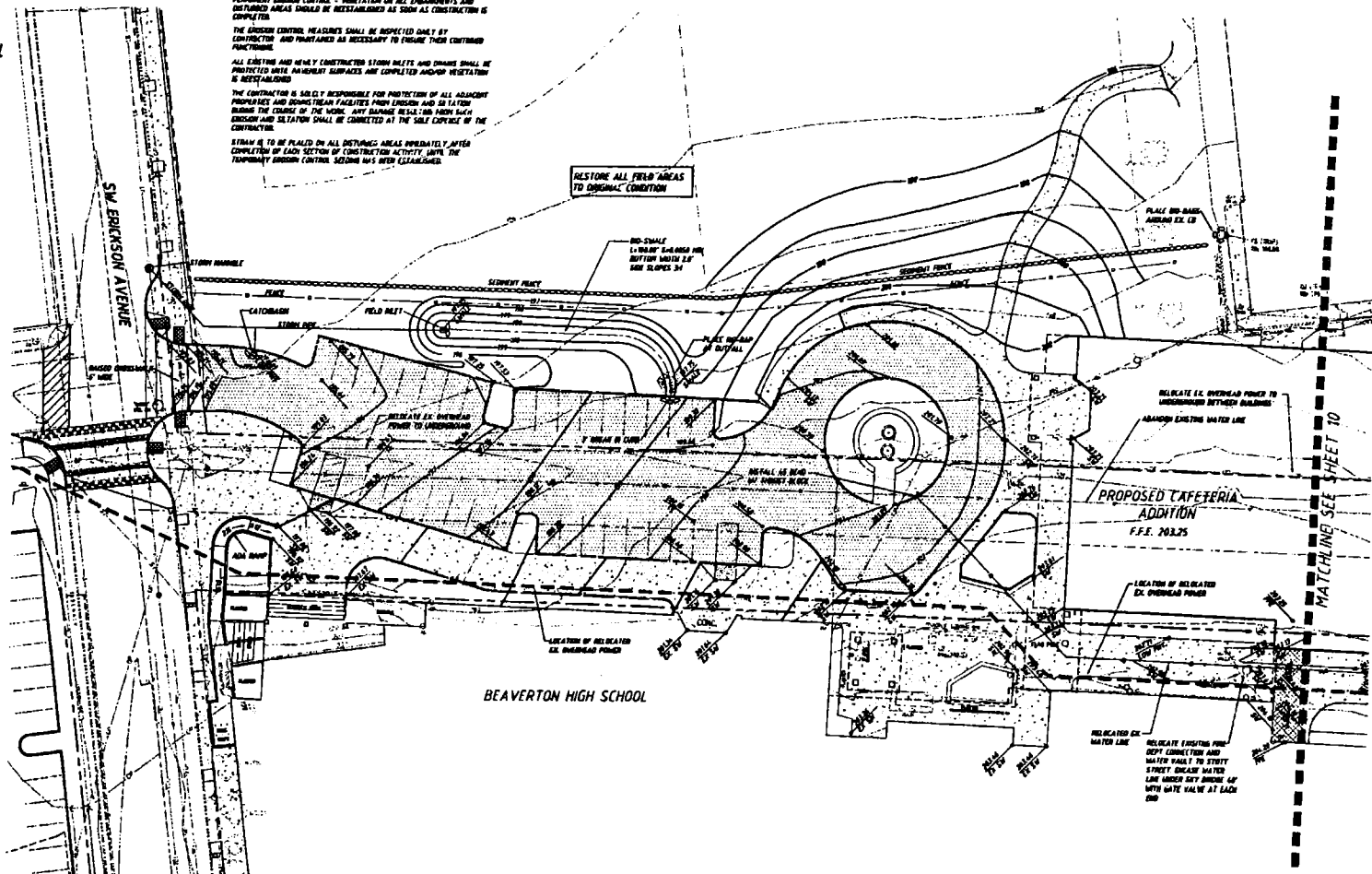
ALL EXISTING AND NEWLY CONSTRUCTED STORM DRAINS AND CHANES SHALL BE PROTECTED UNTIL PERMANENT SURFACES ARE COMPLETED AND/OR VEGETATION IS REESTABLISHED.

THE CONTRACTOR IS SOLELY RESPONSIBLE FOR PROTECTION OF ALL ADJACENT PROPERTIES AND ADJACENT STREAM FACILITIES FROM EROSION AND IS LIABLE BEYOND THE COURSE OF THE WORK. ANY DAMAGE RESULTING FROM SUCH EROSION AND SETTLEMENT SHALL BE CORRECTED AT THE SOLE EXPENSE OF THE CONTRACTOR.

STRAW IS TO BE PLACED ON ALL DISTURBED AREAS IMMEDIATELY AFTER COMPLETION OF EACH SECTION OF CONSTRUCTION ACTIVITY. UNTIL THE TEMPORARY EROSION CONTROL SYSTEM HAS BEEN CLEANED.

**STORM DRAINAGE**

STORM DRAIN PIPE, BENDS, AND FITTINGS SHALL BE PVC (SDR 35) OR EQUIV. OTHER HIGH DENSITY POLYETHYLENE CORRUGATED PIPE AS PRODUCED AND SPECIFIED BY AIAI. PRODUCTS HAVE WRITING OR EQUIVALENT.

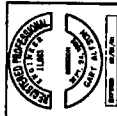


**PRIVATE IMPROVEMENTS (WEST)**

**DESIGN REVIEW SUBMITTAL**

PRIVATE GRADING, UTILITIES, AND EROSION CONTROL (WEST)  
**B.H.S. CAFETERIA ADDITION**  
BEAVERTON, OREGON

**HHR Harper Houff, Inc.**  
**HRR Righellis, Inc.**  
REGISTERED PROFESSIONAL ENGINEERS  
REGISTERED PROFESSIONAL LANDSCAPE ARCHITECTS  
REGISTERED PROFESSIONAL SURVEYORS  
REGISTERED PROFESSIONAL PLANNERS  
REGISTERED PROFESSIONAL ARCHITECTS  
REGISTERED PROFESSIONAL CIVIL ENGINEERS  
REGISTERED PROFESSIONAL ELECTRICAL ENGINEERS  
REGISTERED PROFESSIONAL MECHANICAL ENGINEERS  
REGISTERED PROFESSIONAL CHEMICAL ENGINEERS  
REGISTERED PROFESSIONAL INDUSTRIAL ENGINEERS  
REGISTERED PROFESSIONAL METALLURGICAL ENGINEERS  
REGISTERED PROFESSIONAL NUCLEAR ENGINEERS  
REGISTERED PROFESSIONAL AERONAUTICAL ENGINEERS  
REGISTERED PROFESSIONAL AGRICULTURAL ENGINEERS  
REGISTERED PROFESSIONAL MARINE ENGINEERS  
REGISTERED PROFESSIONAL MINING ENGINEERS  
REGISTERED PROFESSIONAL PETROLEUM ENGINEERS  
REGISTERED PROFESSIONAL TRANSPORTATION ENGINEERS  
REGISTERED PROFESSIONAL ENVIRONMENTAL ENGINEERS  
REGISTERED PROFESSIONAL SANITARY ENGINEERS  
REGISTERED PROFESSIONAL CHEMISTS  
REGISTERED PROFESSIONAL BIOLOGISTS  
REGISTERED PROFESSIONAL GEOLOGISTS  
REGISTERED PROFESSIONAL HISTORIC PRESERVATION ARCHITECTS  
REGISTERED PROFESSIONAL INTERIOR DESIGNERS  
REGISTERED PROFESSIONAL JEWELERS  
REGISTERED PROFESSIONAL LANDSCAPE ARCHITECTS  
REGISTERED PROFESSIONAL PLANNERS  
REGISTERED PROFESSIONAL ARCHITECTS  
REGISTERED PROFESSIONAL CIVIL ENGINEERS  
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REGISTERED PROFESSIONAL SANITARY ENGINEERS  
REGISTERED PROFESSIONAL CHEMISTS  
REGISTERED PROFESSIONAL BIOLOGISTS  
REGISTERED PROFESSIONAL GEOLOGISTS



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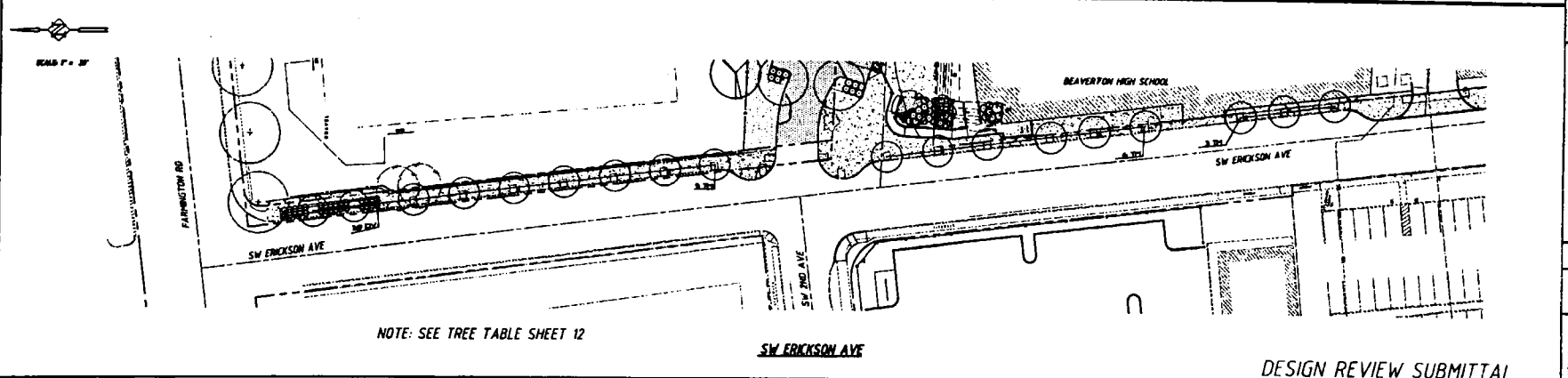
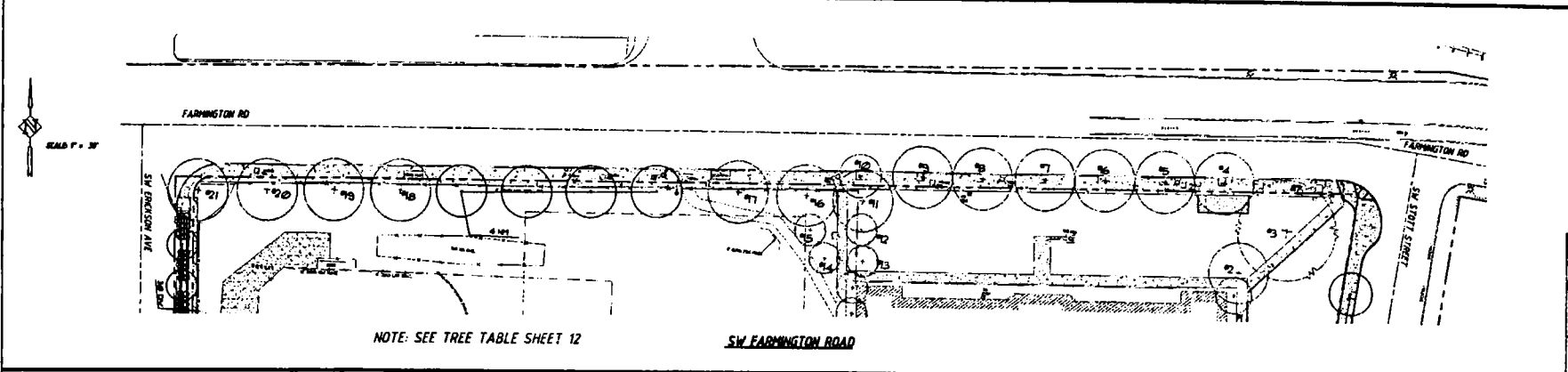
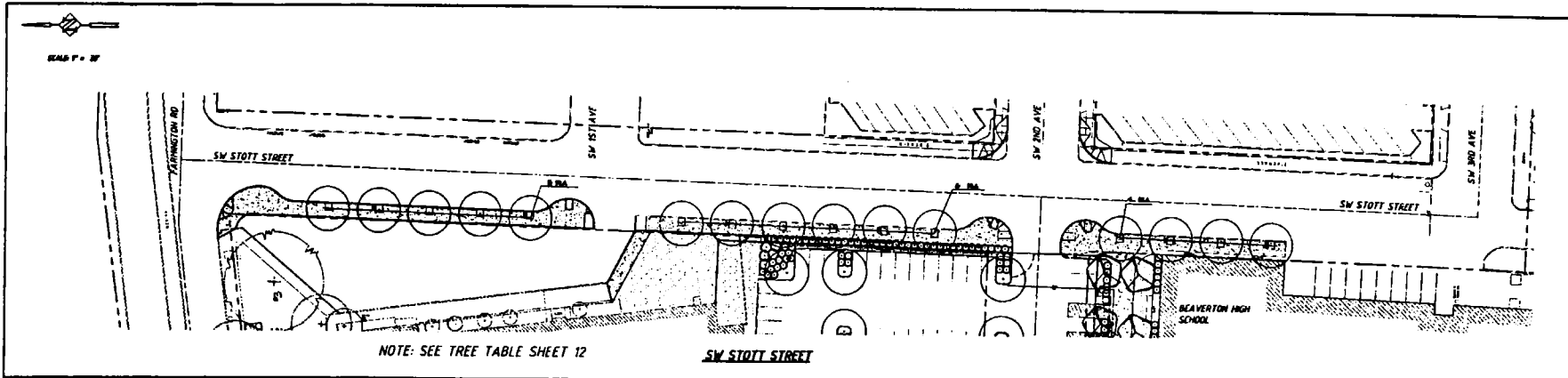
SHEET NO. **9 of 17**  
DATE: 08/27/2013  
TIME: 10:00 AM







NO.	DESCRIPTION	DATE	BY	CHKD BY
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3	CHECKED			
4	DATE			
5	BY			
6	DESCRIPTION			
7	DATE			
8	BY			
9	DESCRIPTION			
10	DATE			
11	BY			
12	DESCRIPTION			
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14	BY			
15	DESCRIPTION			
16	DATE			
17	BY			
18	DESCRIPTION			
19	DATE			
20	BY			



DESIGN REVIEW SUBMITTAL



**LANDSCAPE NOTES**

1. SHADE TREES WITH UTILITY LINES ABOVE ARE SELECTED FOR APPROPRIATE HEIGHT.
2. PROTECTION OF EXISTING TREES: STAKE AND TAPE AREA BY OUTLINE OF DISPLAY OF EACH TREE, ALSO NO COMPACTION OF SOIL, SHALL PROTECTED ZONE, PROTECT FROM HEAVY EQUIPMENT.
3. SEE CIVIL PLAN SHEET FOR 20-20-20 PLANTING PLAN.

**IRRIGATION NOTES**

ALL LANDSCAPE AREAS SHALL BE FULLY IRRIGATED BY AN AUTOMATIC SYSTEM TO BE DETERMINED BY THE IRRIGATION ENGINEER.

**LEGEND**



**PLANT SCHEDULE**

SYM	BOTANICAL NAME	COMMON NAME	QTY	SIZE	SPACING
01	YUCCA	YUCCA	1	12" CAL	30' OC
02	YUCCA	YUCCA	1	12" CAL	30' OC
03	YUCCA	YUCCA	1	12" CAL	30' OC
04	YUCCA	YUCCA	1	12" CAL	30' OC
05	YUCCA	YUCCA	1	12" CAL	30' OC
06	YUCCA	YUCCA	1	12" CAL	30' OC
07	YUCCA	YUCCA	1	12" CAL	30' OC
08	YUCCA	YUCCA	1	12" CAL	30' OC
09	YUCCA	YUCCA	1	12" CAL	30' OC
10	YUCCA	YUCCA	1	12" CAL	30' OC
11	YUCCA	YUCCA	1	12" CAL	30' OC
12	YUCCA	YUCCA	1	12" CAL	30' OC
13	YUCCA	YUCCA	1	12" CAL	30' OC
14	YUCCA	YUCCA	1	12" CAL	30' OC
15	YUCCA	YUCCA	1	12" CAL	30' OC
16	YUCCA	YUCCA	1	12" CAL	30' OC
17	YUCCA	YUCCA	1	12" CAL	30' OC
18	YUCCA	YUCCA	1	12" CAL	30' OC
19	YUCCA	YUCCA	1	12" CAL	30' OC
20	YUCCA	YUCCA	1	12" CAL	30' OC

**BIOMIMIC PLANT SCHEDULE**

SYM	BOTANICAL NAME	COMMON NAME	QTY	SIZE	SPACING
01	YUCCA	YUCCA	1	12" CAL	30' OC
02	YUCCA	YUCCA	1	12" CAL	30' OC
03	YUCCA	YUCCA	1	12" CAL	30' OC
04	YUCCA	YUCCA	1	12" CAL	30' OC
05	YUCCA	YUCCA	1	12" CAL	30' OC
06	YUCCA	YUCCA	1	12" CAL	30' OC
07	YUCCA	YUCCA	1	12" CAL	30' OC
08	YUCCA	YUCCA	1	12" CAL	30' OC
09	YUCCA	YUCCA	1	12" CAL	30' OC
10	YUCCA	YUCCA	1	12" CAL	30' OC
11	YUCCA	YUCCA	1	12" CAL	30' OC
12	YUCCA	YUCCA	1	12" CAL	30' OC
13	YUCCA	YUCCA	1	12" CAL	30' OC
14	YUCCA	YUCCA	1	12" CAL	30' OC
15	YUCCA	YUCCA	1	12" CAL	30' OC
16	YUCCA	YUCCA	1	12" CAL	30' OC
17	YUCCA	YUCCA	1	12" CAL	30' OC
18	YUCCA	YUCCA	1	12" CAL	30' OC
19	YUCCA	YUCCA	1	12" CAL	30' OC
20	YUCCA	YUCCA	1	12" CAL	30' OC

**BIOMIMIC PLANTING NOTES**

1. BIOMIMIC PLANTING SHALL BE PERFORMED AS SHOWN ON THIS PLAN.
2. BIOMIMIC PLANTING SHALL BE PERFORMED AS SHOWN ON THIS PLAN.

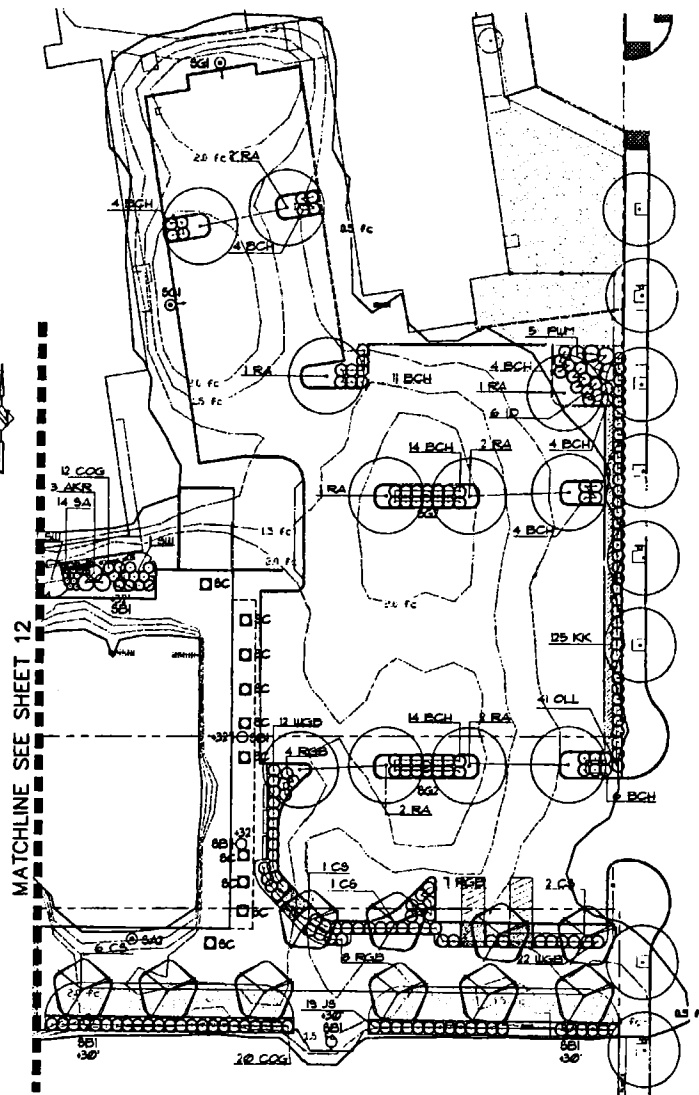
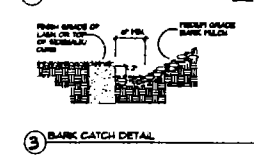
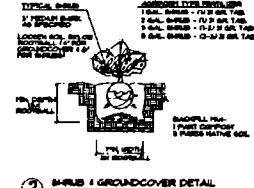
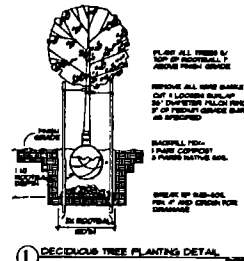
**EXISTING TREE INFORMATION**

**TREES TO BE PRESERVED**

SYM	SPACES	SIZE (DBH)
1	ACER PAPER (ACER SACCHARINUM)	12"
2	ACER PAPER	12"
3	ACER PAPER	12"
4	ACER PAPER	12"
5	ACER PAPER	12"
6	ACER PAPER	12"
7	ACER PAPER	12"
8	ACER PAPER	12"
9	ACER PAPER	12"
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15	ACER PAPER	12"
16	ACER PAPER	12"
17	ACER PAPER	12"
18	ACER PAPER	12"
19	ACER PAPER	12"
20	ACER PAPER	12"

**LUMINAIRE SCHEDULE**

01. EXISTING POLE TOP ACORN LUMINAIRE, 7' H. CASE ALUMINUM FINISH, 1.5' DIA. BOWL, CLEAR GLASS REACTION TYPE B. DIMENSIONS: 1.5' DIA. BOWL, 1.5' DIA. REACTION, 1.5' DIA. BOWL, 1.5' DIA. REACTION, 1.5' DIA. BOWL, 1.5' DIA. REACTION.
02. EXISTING POLE TOP ACORN LUMINAIRE, 7' H. CASE ALUMINUM FINISH, 1.5' DIA. BOWL, CLEAR GLASS REACTION TYPE B. DIMENSIONS: 1.5' DIA. BOWL, 1.5' DIA. REACTION, 1.5' DIA. BOWL, 1.5' DIA. REACTION.
03. EXISTING POLE TOP ACORN LUMINAIRE, 7' H. CASE ALUMINUM FINISH, 1.5' DIA. BOWL, CLEAR GLASS REACTION TYPE B. DIMENSIONS: 1.5' DIA. BOWL, 1.5' DIA. REACTION, 1.5' DIA. BOWL, 1.5' DIA. REACTION.
04. EXISTING POLE TOP ACORN LUMINAIRE, 7' H. CASE ALUMINUM FINISH, 1.5' DIA. BOWL, CLEAR GLASS REACTION TYPE B. DIMENSIONS: 1.5' DIA. BOWL, 1.5' DIA. REACTION, 1.5' DIA. BOWL, 1.5' DIA. REACTION.
05. EXISTING POLE TOP ACORN LUMINAIRE, 7' H. CASE ALUMINUM FINISH, 1.5' DIA. BOWL, CLEAR GLASS REACTION TYPE B. DIMENSIONS: 1.5' DIA. BOWL, 1.5' DIA. REACTION, 1.5' DIA. BOWL, 1.5' DIA. REACTION.
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10. EXISTING POLE TOP ACORN LUMINAIRE, 7' H. CASE ALUMINUM FINISH, 1.5' DIA. BOWL, CLEAR GLASS REACTION TYPE B. DIMENSIONS: 1.5' DIA. BOWL, 1.5' DIA. REACTION, 1.5' DIA. BOWL, 1.5' DIA. REACTION.



MATELA ASSOCIATES INC  
LANDSCAPE ARCHITECTURE  
10000 NE 28TH AVE, SUITE 100  
BEAVERTON, OREGON 97005  
503.768.1111

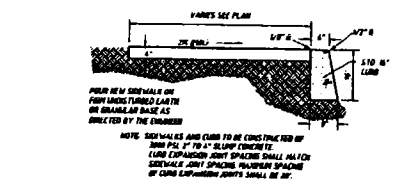
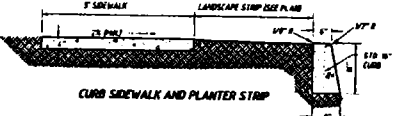
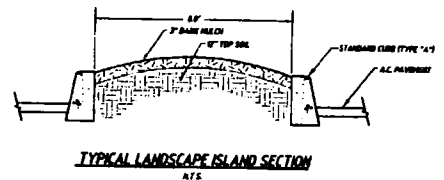
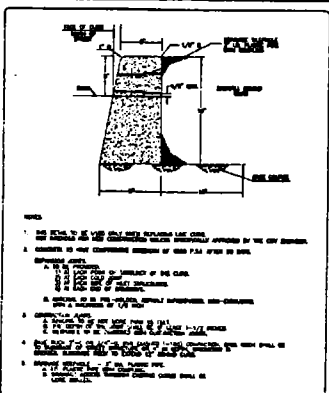
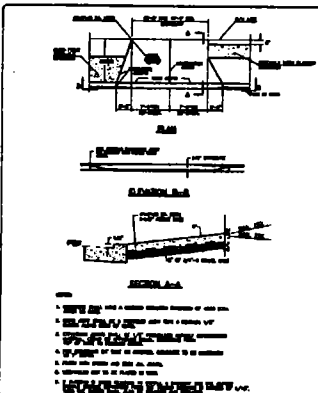
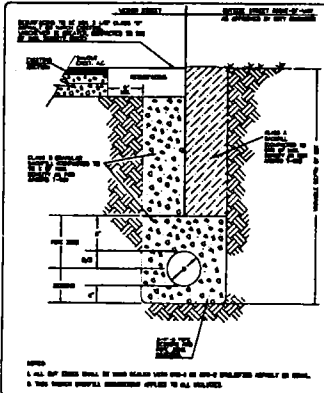
PRIVATE LANDSCAPING AND LIGHTING PLAN (EAST)  
B.H.S. CAFETERIA ADDITION  
BEAVERTON, OREGON



NO.	DATE	BY	FOR
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2	11-21-17	AL	REVISION OF CITY COMMENTS
3	11-21-17	AL	REVISION OF CITY COMMENTS
4	11-21-17	AL	REVISION OF CITY COMMENTS
5	11-21-17	AL	REVISION OF CITY COMMENTS
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18	11-21-17	AL	REVISION OF CITY COMMENTS
19	11-21-17	AL	REVISION OF CITY COMMENTS
20	11-21-17	AL	REVISION OF CITY COMMENTS

DESIGN REVIEW SUBMITTAL

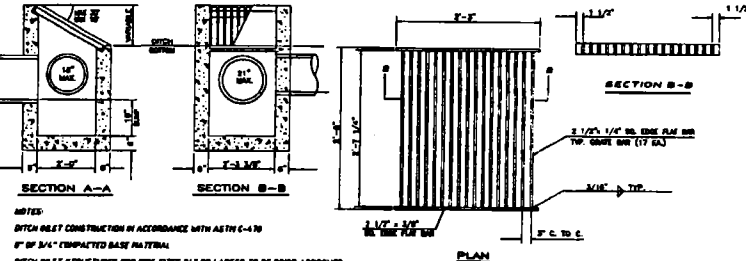
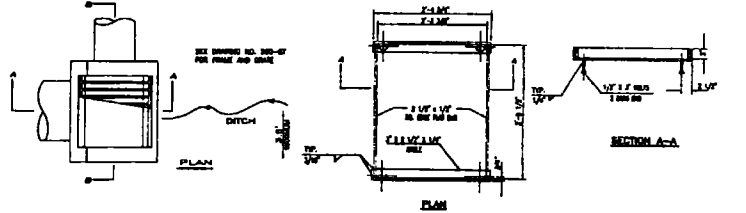




COMMUNITY DEVELOPMENT DEPARTMENT  
 CITY ENGINEER  
 CITY OF SEASIDE, CALIFORNIA

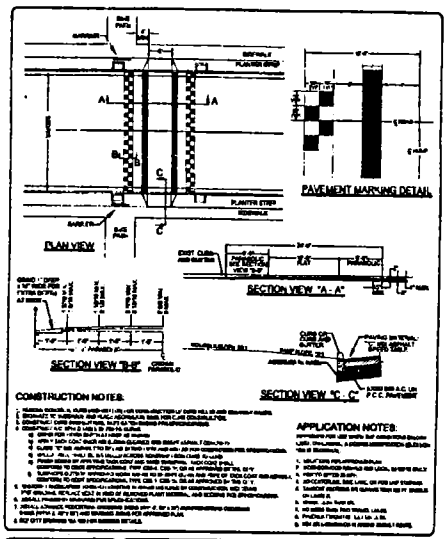
COMMUNITY DEVELOPMENT DEPARTMENT  
 CITY ENGINEER  
 CITY OF SEASIDE, CALIFORNIA

COMMUNITY DEVELOPMENT DEPARTMENT  
 CITY ENGINEER  
 CITY OF SEASIDE, CALIFORNIA



NOTES:  
 DITCH INLET CONSTRUCTION IN ACCORDANCE WITH ASTM C-470  
 8" OF 3/4" COMPACTED BASE MATERIAL  
 DITCH INLET STRUCTURES FOR PIPE SIZES 24" OR LARGER TO BE PRIOR APPROVED  
 PRECAST CONCRETE WALL THICKNESS TO BE 6"  
 REINFORCEMENT TO BE INTERMEDIATE ASTH AAS GRADE OR WELDED WIRE REINFORCEMENT ASHY  
 NOTE:  
 FRAME AND GRATE TO BE PLAT BAR STEEL OR APPROVED EQUAL

**DITCH INLET, FRAME AND GRATE**  
 REFER TO STD. USA DWG. NO. 286-57  
 N.T.S.



CONSTRUCTION NOTES:  
 1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SEASIDE SPECIFICATIONS FOR CONSTRUCTION OF SIDEWALKS AND CROSSWALKS.  
 2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SEASIDE SPECIFICATIONS FOR CONSTRUCTION OF SIDEWALKS AND CROSSWALKS.  
 3. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SEASIDE SPECIFICATIONS FOR CONSTRUCTION OF SIDEWALKS AND CROSSWALKS.  
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 8. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SEASIDE SPECIFICATIONS FOR CONSTRUCTION OF SIDEWALKS AND CROSSWALKS.  
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 10. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SEASIDE SPECIFICATIONS FOR CONSTRUCTION OF SIDEWALKS AND CROSSWALKS.

APPLICATION NOTES:  
 1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SEASIDE SPECIFICATIONS FOR CONSTRUCTION OF SIDEWALKS AND CROSSWALKS.  
 2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SEASIDE SPECIFICATIONS FOR CONSTRUCTION OF SIDEWALKS AND CROSSWALKS.  
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ENGINEERING DEPARTMENT  
 CITY ENGINEER  
 CITY OF SEASIDE, CALIFORNIA

DETAIL SHEET  
**B.H.S. CAFETERIA ADDITION**  
 BEAVERTON, OREGON

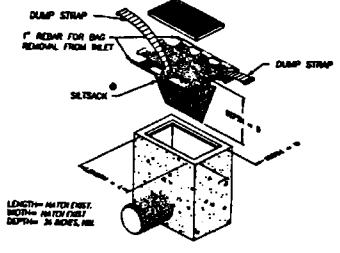
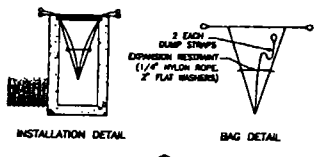
Harper Houff  
 Rhigellis, Inc.  
 6722 SW Macmillan Parkway, Beaverton, OR 97008  
 TEL: 503.231.1131 FAX: 503.231.1131



NO.	DATE	REVISIONS
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2		
3		
4		
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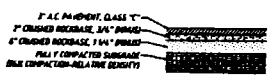
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 SHEET NO: 15 OF 17  
 PLAN NO: 286-57

DESIGN REVIEW SUBMITTAL

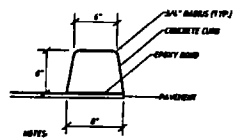


NOTE:  
 INSTALL OF TRAP AND ACCESSORIES AS SUPPLIED BY  
 ACF, WEST, INC. PER INSTRUCTIONS OR APPROVED EQUAL.

**FILTER BAG INLET PROTECTION**  
 R1.2

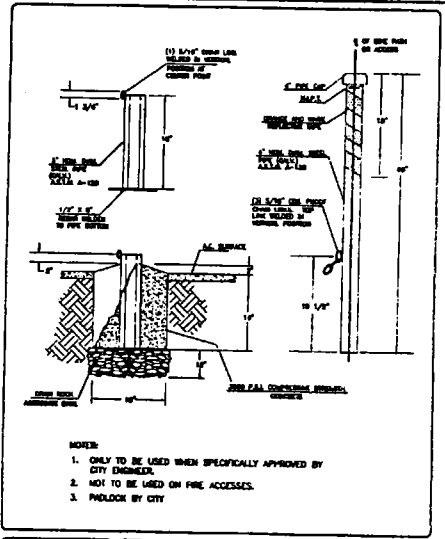


**A.C. PAVEMENT SECTION**  
 R1.1



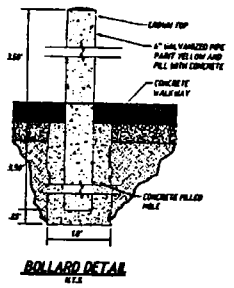
- NOTES:  
 1. CURB SHALL BE FINISHED EXTERIOR.  
 2. CONCRETE TO BE LINED PIP AFTER 28 DAYS.  
 3. EXPANSION JOINT EVERY 30 FEET.  
 4. CURB SHALL BE BUILT TO A.C. PAVEMENT.

**EXTRUDED CURB**  
 R1.2

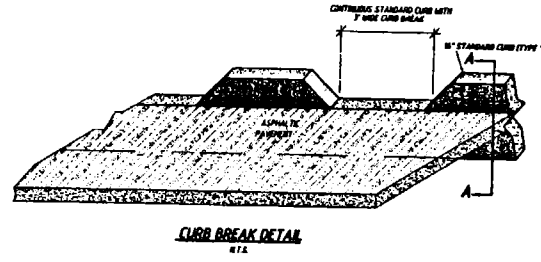


- NOTES:  
 1. ONLY TO BE USED WHEN SPECIFICALLY APPROVED BY CITY ENGINEER.  
 2. NOT TO BE USED ON FIRE ACCESSES.  
 3. HOLDLOCK BY CITY.

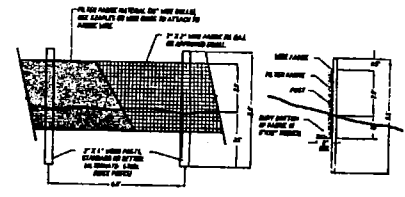
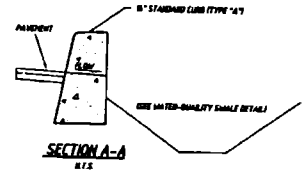
City of Beaverton	COMPLIANT DEVELOPMENT DEPARTMENT		REMOVABLE VEHICLE BARRELS POST	
	CITY ENGINEER	DATE	DESIGNER	DATE
	John Strubbe, P.E.	10-25-04	B-750	09/08/04



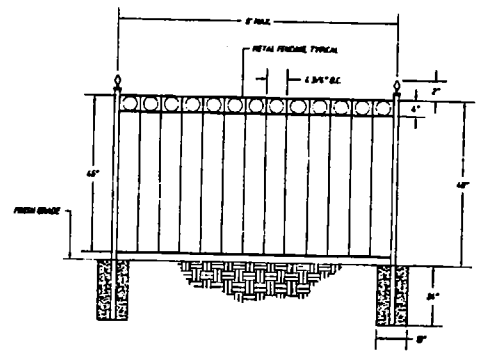
**BOLLARD DETAIL**  
 R1.2



**CURB BREAK DETAIL**  
 R1.2



**SEDIMENT CONTROL FENCE**  
 R1.2



**ORNAMENTAL IRON FENCE AND GATE SECTION**  
 R1.2

- FENCING MATERIAL NOTES:**  
 1. PICKETS SHALL BE 2" IN DIA HOLLOW SQUARE TUBING.  
 2. GATE PICKETS SHALL BE 3-1/2" BLISS HOLLOW SQUARE TUBING.  
 3. BAILS SHALL BE 1-1/2" IN DIA HOLLOW SQUARE TUBING.  
 4. PICKETS SHALL BE 3/4" IN DIA HOLLOW SQUARE TUBING.  
 5. CIRCLES SHALL BE 1/2" BLISS HOLLOW SQUARE TUBING.  
 6. FLANGE PICKETS SHALL BE BLACK POWDER COAT BY "TOMBARD COAT SYSTEM 100" OR EQUIVALENT.  
 7. BARS AND SLIDING GATES TO INCLUDE LOCKS SHALL BE FINISH SHARP DRAWINGS TO ENGINEER FOR REVIEW AND APPROVAL. FINISH LINE SHALL MEET FIRE DEPARTMENT REQUIREMENTS OF CODES-BASED OR HOUSING-LIKE. FIRE DEPARTMENTS SHALL REQUIRE APPROVAL OF LOCATION AND TYPE OF SETBACK PRIOR TO INSTALLATION.  
 8. NEW FINISHING SHALL MATCH EXISTING BLACK FINISH AT NEAR SCHOOLS.

DESIGN REVIEW SUBMITTAL

DETAIL SHEET  
 B.H.S. CAFETERIA ADDITION  
 BEAVERTON, OREGON

Harper Houff  
 Rightellis, Inc.  
 1000 NE 10TH AVE, SUITE 100  
 BEAVERTON, OREGON 97004  
 TEL: 503.321.1111 FAX: 503.321.1111

	DESIGNED	DATE	16 of 17 02-08-07
	CHECKED	DATE	

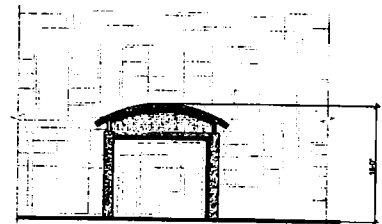




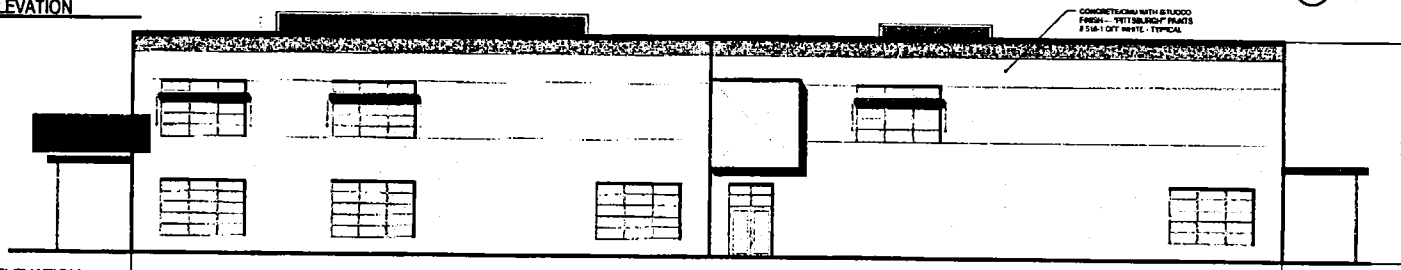
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SCALE 1/8" = 1'-0"



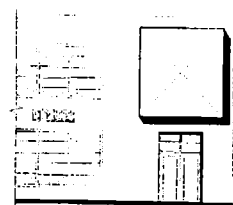
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SCALE 1/8" = 1'-0"



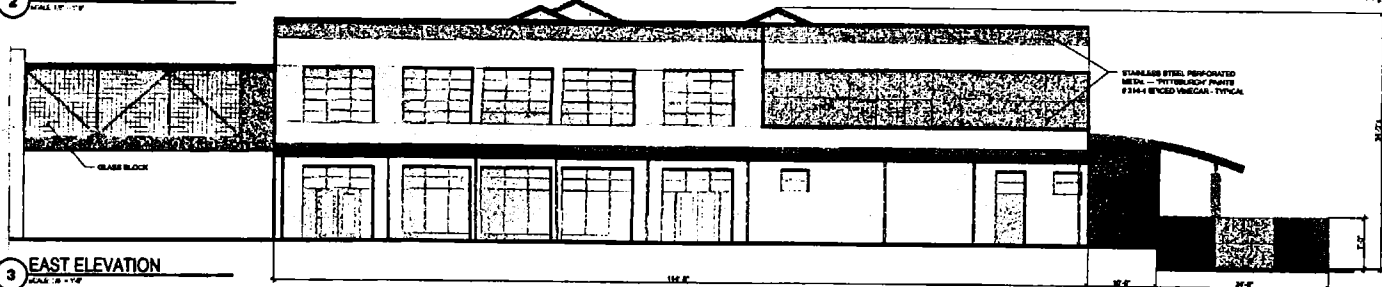
2 SOUTH ELEVATION  
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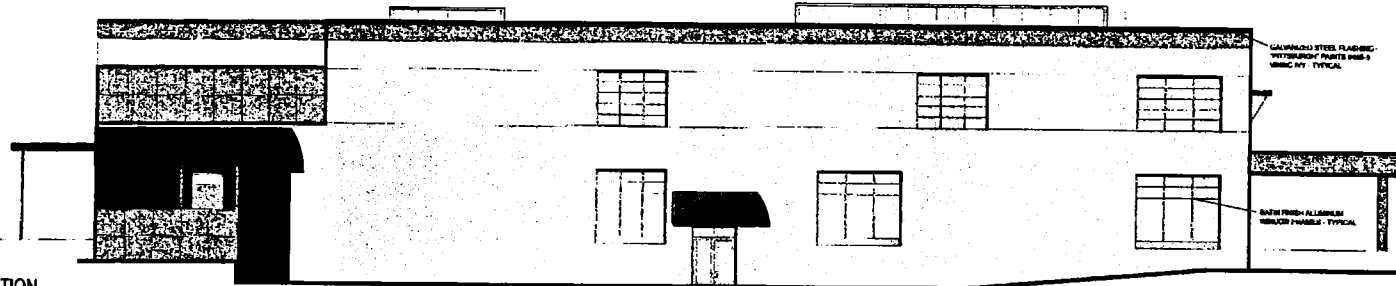
6 PARTIAL NORTH ELEVATION  
SCALE 1/8" = 1'-0"



3 EAST ELEVATION  
SCALE 1/8" = 1'-0"



4 NORTH ELEVATION  
SCALE 1/8" = 1'-0"



RECEIVED  
MILLER  
MARCH 21  
2002

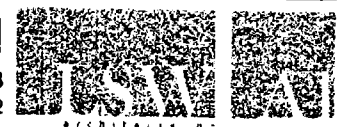
PAINT LEGEND

- "PITTSBURGH" PAINTS  
# 314-4 SPICED VINEGAR - TYPICAL
- "PITTSBURGH" PAINTS  
# 322-4 BURNING SAND - TYPICAL
- "PITTSBURGH" PAINTS  
# 455-8 VINOSE IVY - TYPICAL
- "PITTSBURGH" PAINTS  
# 516-1 OFF WHITE - TYPICAL

# CONCEPTUAL ELEVATIONS | B.H.S. CAFETERIA ADDITION

BEAVERTON SCHOOL DISTRICT # 48

REVISED MARCH 6, 2002

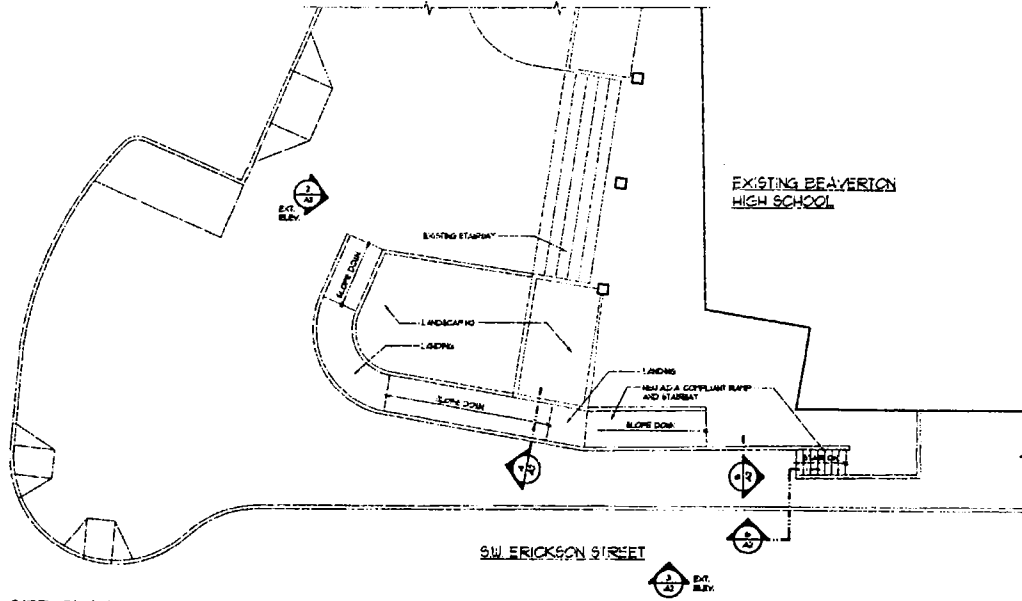




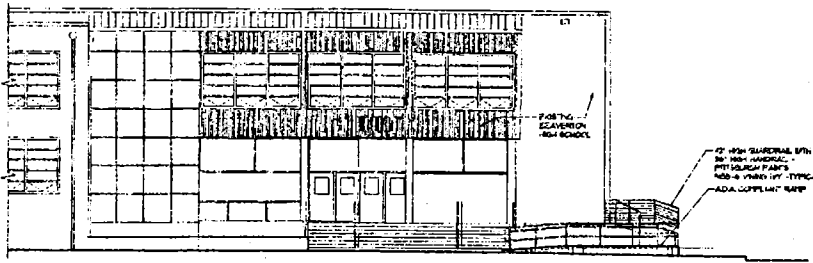
ARCHITECTURE  
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TEL (503) 274-5433  
FAX (503) 274-6055

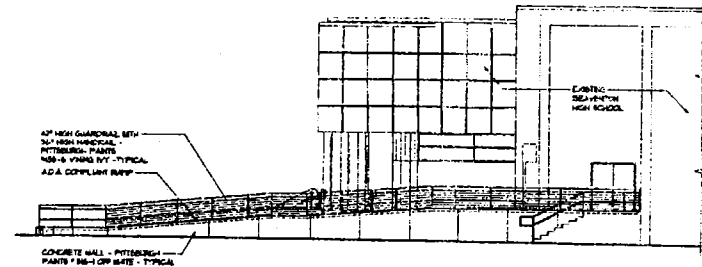
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Portland, OR 97209



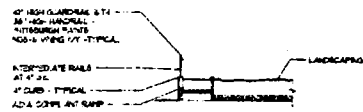
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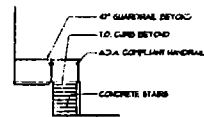
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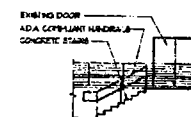
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4 SECTION AT PLANTER  
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5 SECTION AT STAIRWAY  
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6 SECTION AT STAIRWAY  
SCALE: 1/4" = 1'-0"

BEAVERTON HIGH SCHOOL  
BEAVERTON SCHOOL DISTRICT #48  
CAFETERIA ADDITION  
13000 S.W. SECOND ST. BEAVERTON, OREGON 97005

Sheet:  
Architect:  
Date:  
Job No.:

Date: 3-26-07  
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Checked by: JGW  
Drawing Title: CONCEPT ELEVATIONS AND SECTIONS

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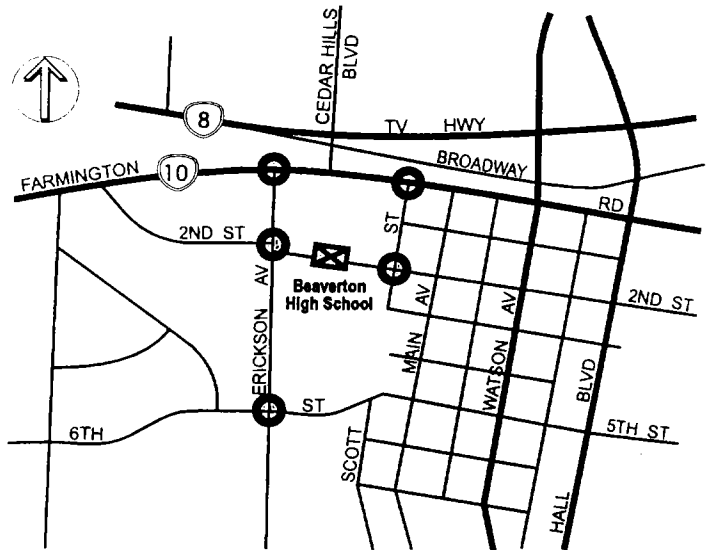
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Report for



**City of  
Beaverton**

# Beaverton High School 2nd Street Vacation Traffic Impact Study



Prepared by

**DKS Associates**

December 2001

**RECEIVED**  
DEC 07 2001

City of Beaverton  
Development Services

# DKS Associates

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1400 SW 5<sup>th</sup> Avenue, Suite 500  
Portland, OR 97201  
Phone: (503) 243-3500  
Fax: (503) 243-1934

December 5, 2001

Gary Alfson  
Harper Houf Righellis  
5200 SW Macadam Avenue #580  
Portland, OR 97201

**Subject: Beaverton High School Street Vacation Traffic Impact Study**

P01294x0

Dear Gary:

DKS Associates is pleased to submit this Traffic Impact Study for the proposed Beaverton High School street vacation, located on SW 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue, in the City of Beaverton. This report meets the guidelines defined by the City of Beaverton. We have enclosed one bound copy and one loose copy of the report for your use.

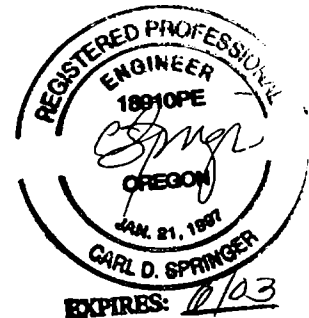
Please call Scott Mansur or me with any questions regarding this report.

Sincerely,

**DKS Associates**  
A Corporation



Carl D. Springer, P.E.  
Senior Project Manager



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**CHAPTER**

**1**

**INTRODUCTION AND SUMMARY**

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**INTRODUCTION**

This report evaluates the transportation impacts associated with the proposed vacation of 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue by the Beaverton School District to accommodate the new Beaverton High School expansion. Currently 2<sup>nd</sup> Street provides one directional travel for eastbound traffic between Erickson Avenue and Stott Avenue. This segment is closed via gates between 6:00 AM and 2:30 PM on weekday school days. With the proposed project, 2<sup>nd</sup> Street will be a cul-de-sac between Stott Avenue and Erickson Avenue. The west connection to Erickson Avenue will provide bi-direction traffic to 32 parking stalls and a turn around adjacent to the high school front door. The east connection will provide bi-directional traffic to new developed parking lots along the western portion of the vacated area.

The expansion will consist of adding a new cafeteria and will renovate the interior of one existing school building. No additional students will be added with the proposed expansion and remodel. The current school enrollment at Beaverton High School is approximately 1,800 students. This enrollment is 400 students lower than the existing school capacity of 2,200. Traffic and transportation impacts are evaluated for existing, existing with the proposed street vacation, and existing with the street vacation and additional traffic from maximum student capacity (worst case conditions) based on the City of Beaverton requirements.

**STUDY AREA AND INTERSECTIONS**

The study area for this analysis has been defined as Farmington Road, Erickson Avenue, Stott Avenue, 2<sup>nd</sup> Street, and 5<sup>th</sup> Street (see Figure 1) based on a conversation with City of Beaverton staff.<sup>1</sup> The study area is located in Washington County within the City of Beaverton School District. The following intersections have been identified for focused analysis based on their proximity to Beaverton High School:

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<sup>1</sup> Based on phone conversations with Randy Wooley from the City of Beaverton.

- Farmington Road/Erickson Avenue
- Farmington Road/Stott Avenue
- 2<sup>nd</sup> Street/Erickson Avenue
- 2<sup>nd</sup> Street/Stott Avenue
- 5<sup>th</sup> Street/Erickson Avenue

## **FINDINGS**

Existing Conditions -- The subject section of SW 2<sup>nd</sup> Street is primarily used by school activity during the week. The weekend vehicular activity ranges from 400 to 600 vehicles daily. The school activity adds 500 to 900 vehicles daily depending on after school events. Unsignalized study area intersections operate at level of service D or better condition with the exception of Farmington Road/Erickson Avenue. This unsignalized intersection continues to operate at LOS "F" due to the minor street left turning movement for all periods due to heavy through volumes along Farmington Road (see unavoidable impacts on page 15).

Proposed Street Vacation -- The proposed street vacation of SW 2<sup>nd</sup> Street between SW Erickson and SW Stott will be necessary for the Beaverton High School addition to function in a safe and efficient manner. The street vacation will relocate the weekend traffic that uses 2<sup>nd</sup> Street today to parallel facilities on SW Farmington Road and SW 5<sup>th</sup>/6<sup>th</sup> Avenues. The existing street system can adequately service this incremental shift in traffic volumes. Pedestrian and bicycle connections across the campus will be enhanced so that non-auto travel will not be significantly impacted by the street vacation.

With the proposed street vacation, all unsignalized study area intersections will operate at LOS "D" or better during all peak periods with the exception of Farmington Road/Erickson Avenue. This unsignalized intersection continues to operate at LOS "F" for the minor street left turning movement for all periods due to heavy through volumes along Farmington Road.

Possible Long-Term Student Enrollment Increase -- School enrollment may increase at Beaverton High School up to 2,200 students that were enrolled prior to recent new high school construction in the district. The campus has capacity for an additional 400 students based on the current enrollment level of 1,800. It was estimated that the added 400 students would generate approximately 184 vehicle trips during the AM peak hour (7:00 AM to 9:00 AM), 108 vehicle trips during the midday peak hour (2:00 PM to 4:00 PM), and approximately 60 vehicle trips during the PM peak hour (4:00 PM to 6:00 PM).

With additional traffic from the remaining 400 students that could potentially be added at full capacity (worst case), all signalized study area intersections will operate at and acceptable LOS "E" or better during all peak periods with the exception of Farmington Road/Erickson Avenue. This unsignalized intersection continues to operate at LOS "F" for the minor street left turning movement for all periods due to heavy through volumes along Farmington Road.

# *DKS Associates*

---

The intersection of Farmington Road/Erickson Avenue currently operates at LOS "F" as an unsignalized intersection due to delay to the northbound left turn movement created by east/west vehicles along Farmington Road. Under the worst case scenario, the proposed street vacation adds 1 to 2 trips to the critical northbound left turn movement. The deficient operation of this intersection is a pre-existing condition, and is therefore considered an unavoidable impact with or without the proposed street vacation project. This intersection is spaced approximately 250 feet from the existing signalized intersection of Farmington Road and Cedar Hills Boulevard. The current access spacing standards on Farmington Road is 660 feet<sup>2</sup>. A traffic signal at this location would not meet the Beaverton spacing standards and therefore is not recommended.

## **MITIGATION**

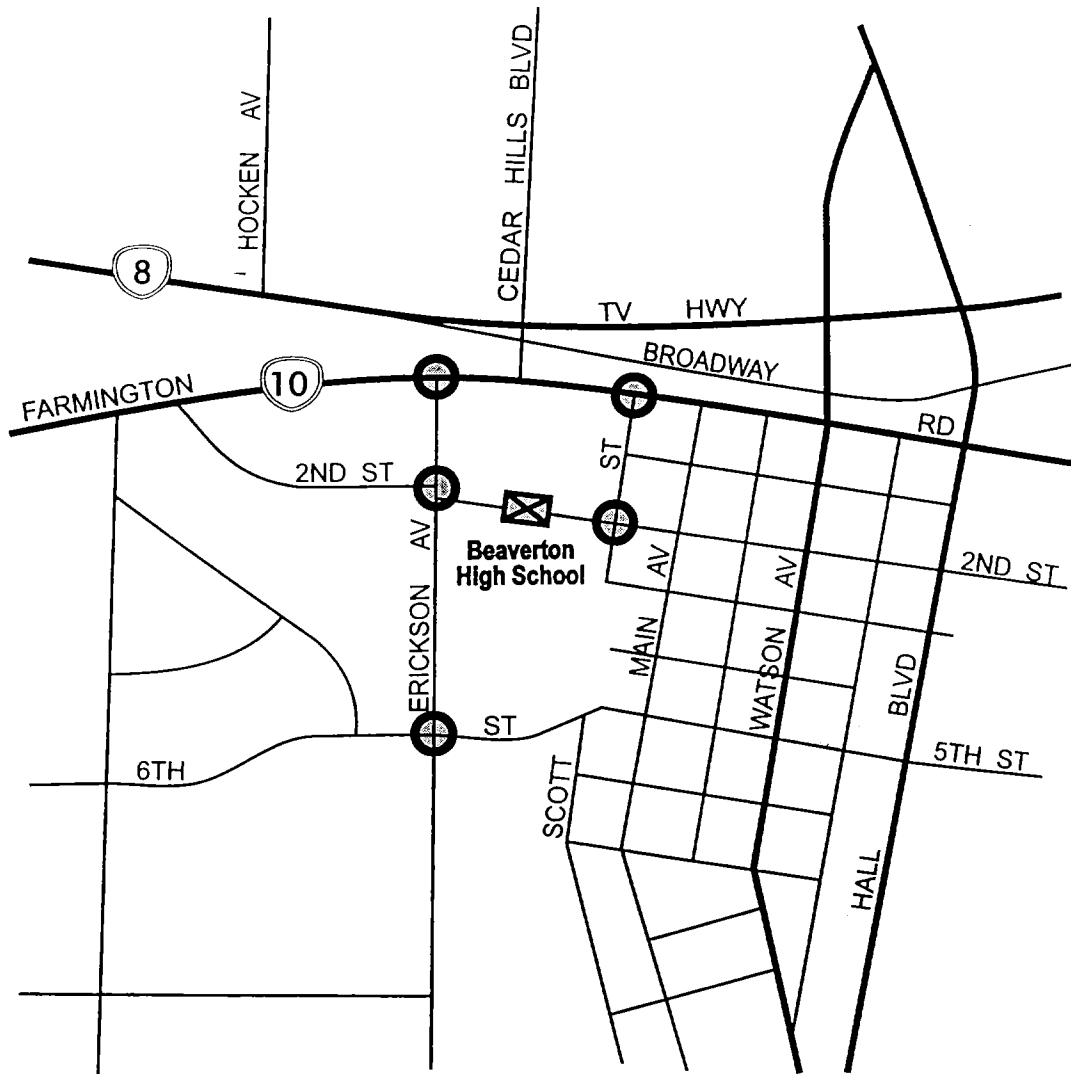
The following mitigation measures would be recommended with the proposed expansion:

- Create pedestrian friendly connections on 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue including the provision of sidewalks and/or off-street pathways.
- Maintain pedestrian and bicycle connectivity between Erickson Avenue and Stott Avenue following the vehicular vacation of 2<sup>nd</sup> Street.

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<sup>2</sup> City of Beaverton Development Code, 60.60.40.6.





**LEGEND**



- Study Intersection



- Proposed Road Vacation

**Figure 1  
STUDY AREA**

**CHAPTER**

**2**

**EXISTING CONDITIONS**

---

This chapter of the report discusses the existing transportation conditions in the vicinity of the proposed street vacation, including roadway geometries, traffic volumes, and pedestrian, transit and bicycle facilities. Existing operating conditions of roadways and key intersections in the study area are also discussed.

The proposed street vacation is located on 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue. Currently 2<sup>nd</sup> Street provides one directional travel for eastbound traffic between Erickson Avenue and Stott Avenue. This segment is closed via gates between 6:00 AM and 2:30 PM on weekday school days. The following intersections were selected for focused analysis in this report based on their proximity and expected impacts to the proposed street vacation.

- Farmington Road/Erickson Avenue
- Farmington Road/Stott Avenue
- 2<sup>nd</sup> Street/Erickson Avenue
- 2<sup>nd</sup> Street/Stott Avenue
- 5<sup>th</sup> Street/Erickson Avenue

**EXISTING NETWORK DESCRIPTION**

The following sections describe the key roadways that would serve the proposed project. The key roadways in the study area are Farmington Road, 2<sup>nd</sup> Street, Erickson Avenue, Stott Avenue, and 5<sup>th</sup> Street.

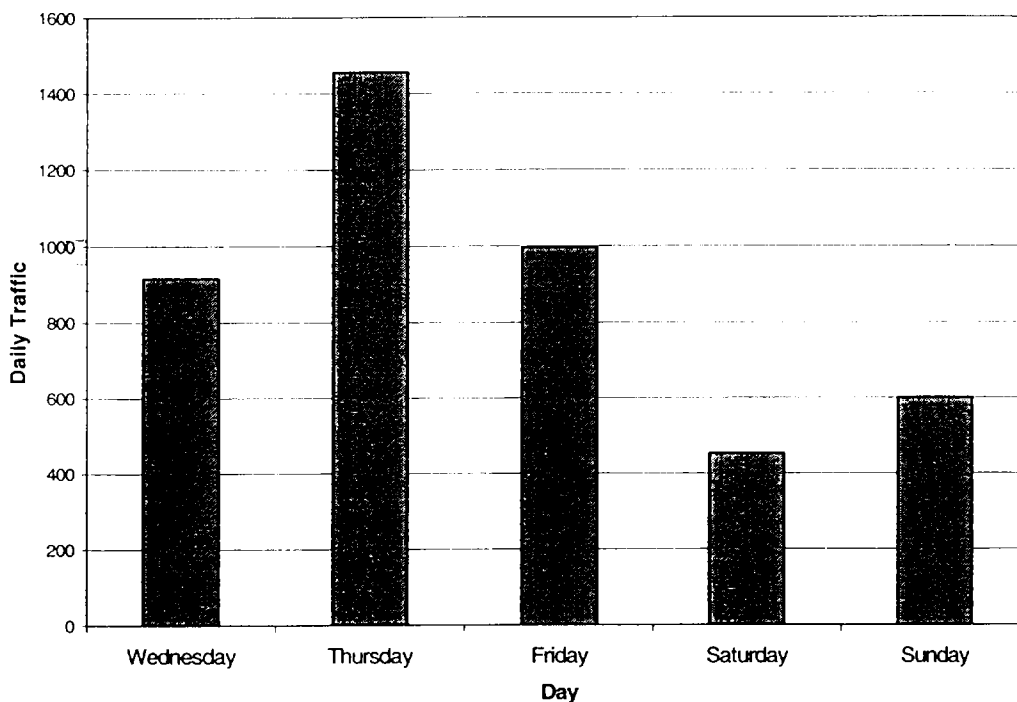
2<sup>nd</sup> Street is classified as a Neighborhood Route within the study area by the City of Beaverton.<sup>3</sup> Typically 2<sup>nd</sup> Street is a two-lane cross-section (including on-street parking in some areas) with sidewalks. It carries approximately 900 to 1,400 weekday daily trips and approximately 450 to 600 weekend daily trips<sup>4</sup>.

---

<sup>3</sup> Based on *Functional Classification System, City of Beaverton Transportation Plan*, September 2001.

<sup>4</sup> Based on 24-hour vehicle tube counts conducted Wednesday October 3<sup>rd</sup>, 2001 through Sunday October 7<sup>th</sup>, 2001 on 2<sup>nd</sup> Street 25 feet east of Erickson Avenue.

## 2nd Avenue Daily Traffic



Note: Based on 24-hour vehicle tube counts conducted Wednesday October 3<sup>rd</sup>, 2001 through Sunday October 7<sup>th</sup>, 2001 on 2<sup>nd</sup> Street 25 feet east of Erickson Avenue.

### PEDESTRIAN AND BICYCLE CONDITIONS

Pedestrian movement counts were performed at study area intersections to help determine the level of pedestrian activity during peak periods of the day. Pedestrian movements during the AM peak, midday peak and PM peak are relatively low (less than ten movements) with the exception of 2<sup>nd</sup> Street/Stott Avenue and 5<sup>th</sup> Street/Erickson Avenue. 2<sup>nd</sup> Street/Stott Avenue is the main entrance to adjacent parking lots to the school and had over 100 pedestrian movements during the AM peak period (6AM to 9AM) and over 170 in the mid-day (2PM to 4PM). 5<sup>th</sup> Street/Erickson Avenue had over 70 pedestrian during the AM peak hour and over 130 during the mid-day peak hour.

All roadways in the study area have sidewalks provided. 5<sup>th</sup> Street is the only study roadway with bike lanes. Farmington Road is classified as a regional on-street bikeway corridor and Stott Avenue has a proposed Tualatin Valley Park and Recreation trail by the bicycle master plan<sup>5</sup>.

Bicycle movement counts were performed at study area intersections to help determine the level of bicycle activity during peak periods of the day. Bicycle movements are moderate (twenty or less) during all peak periods of the day.

<sup>5</sup> Based on *Bicycle Master Plan, City of Beaverton Transportation Plan, September, 2001, Figure 4-8a.*

**Table 1: Roadway Network**

Roadway	Beaverton Classification	Cross Section	On-Street Parking	Pedestrian Master Plan	Bicycle Master Plan
2 <sup>nd</sup> Street	Neighborhood Route	2 Lanes	Yes	No Designation	No Designation
Farmington Road	Arterial	5 Lanes w/CTL	No	Existing Sidewalks	Proposed Bike Lanes
Erickson Avenue	Collector	2 Lanes	Yes	Existing Sidewalks	No Designation
Stott Avenue	Local Street	2 Lanes	Yes	No Designation	No Designation
5 <sup>th</sup> Street	Collector	2 Lanes	Yes	Existing Sidewalks	Existing Bike Lanes

**TRANSIT CONDITIONS**

Farmington Road is classified as a Mixed-Use Transit Corridor route by Metro.<sup>6</sup> Existing transit service to the study area is currently provided by Tri-Met via the route #52 (Farmington-185<sup>th</sup>). Route #52 provides service from the Beaverton transit center along Farmington Road adjacent to the project site to 185<sup>th</sup> Avenue.

The Beaverton School District provides bus service to Beaverton High School students. There are currently 18 buses that serve approximately 40% of the students (720 of the 1800 students).<sup>7</sup>

**EXISTING TRAFFIC OPERATIONS**

Traffic counts at the study area intersections were conducted in October 2001. All count data was collected during regular school operations. Figure 3 summarizes the turning movement data collected.

Intersection turn movements counts were conducted during the AM (6:00 – 9:00 AM), midday (2:00 – 4:00 PM) and PM (4:00 – 6:00 PM) peak periods to determine existing level of service based on the 2000 Highway Capacity Manual methodology for signalized and unsignalized intersections.<sup>8</sup> Table 2 summarizes the transportation operations for study area intersections.

<sup>6</sup> Based on Metro Regional Pedestrian System, December 1, 1997.

<sup>7</sup> Phone conversation with Sue Rushing, Beaverton School District, November 27, 2001.

<sup>8</sup> *Highway Capacity Manual*, Special Report 209, Transportation Research Board, 2000, Chapters 16 and 17.

**Table 2 – Existing Peak Hour Intersection Level of Service**

Intersection	AM Peak			Midday Peak			PM Peak		
	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C
<b>Two-Way Stop Controlled Intersection</b>									
Farmington Rd/ Erickson Ave	>50	F		>50	F		>50	F	
Farmington Rd/ Stott Ave	17.7	C		14.6	B		13.5	B	
2 <sup>nd</sup> Street/Erickson Ave	10.1	B		10.0	B		9.9	A	
<b>All-Way Stop Controlled Intersection</b>									
2 <sup>nd</sup> Street/Stott Ave	10.0	B	0.36	8.2	A	0.18	7.6	A	0.12
5 <sup>th</sup> Street/Erickson Ave	26.7	D	0.87	10.8	B	0.43	12.0	B	0.54

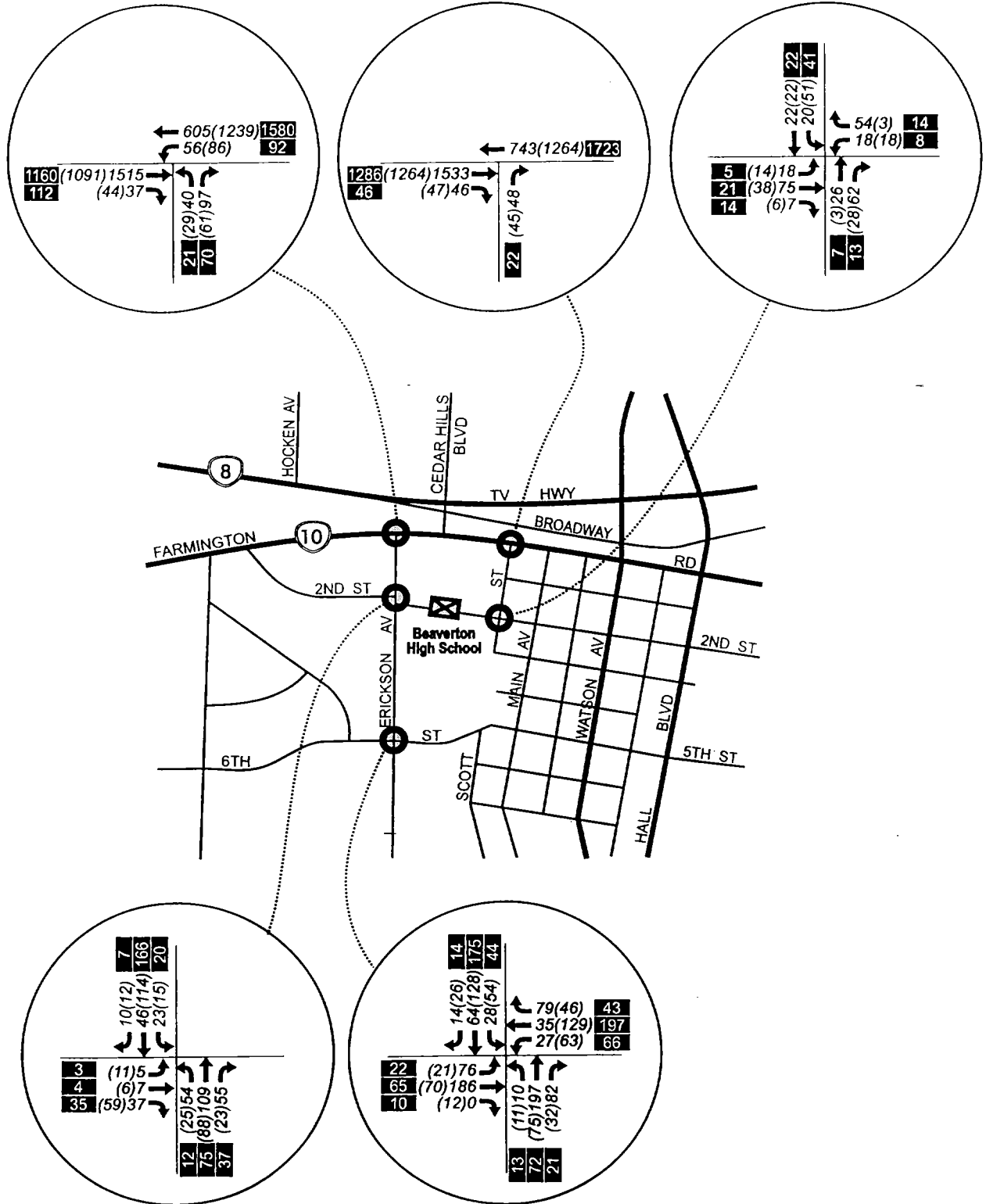
All-way stop intersection:  
 Delay = Average intersection delay  
 LOS = Level of service  
 V/C = Volume-to-capacity ratio

Unsignalized intersection:  
 Delay = Average intersection delay  
 LOS = Minor street LOS

As Table 2 indicates, all signalized study area intersections operate at an acceptable LOS “D” or better during all peak periods with the exception of Farmington Road/Erickson Avenue. This unsignalized intersection operates at LOS “F” for the minor street left turning movement for all periods due to heavy through volumes along Farmington Road.

**SAFETY**

Washington County maintains a listing of all the intersections with three or more accidents or a fatality over a recent three-year period. The intersections that meet either of these criteria are then ranked based on the Safety Priority Index System (SPIS). None of the study area intersections are currently ranked on the County SPIS.



**LEGEND**

- Study Intersection
- AM(Mid)PM - Peak Hour Traffic Volume
- Mid= Mid-day

**Figure 2**  
**EXISTING**  
**PEAK HOUR TRAFFIC VOLUMES**

**CHAPTER**

**3**

**IMPACTS**

---

This chapter reviews the assumptions for transportation analysis, and transportation impacts associated with the proposed road vacation on the study area transportation system. The analysis includes an assessment of the proposed road vacation, trip generation, distribution and assignment, capacity analysis of the study area intersections including traffic from the proposed road vacation as well as background traffic growth from the remaining 400 students that could potentially be added at full capacity. The following two scenarios were developed for analysis for all three-peak hours (AM, midday and PM) of the day.

- Existing with 2<sup>nd</sup> Street road vacation
- Existing with 2<sup>nd</sup> Street road vacation plus full student capacity (Worst Case)

**PROJECT DESCRIPTION**

The proposed project consists of a revision of the current Beaverton High School campus. The revision would include a new cafeteria, renovating the interior of two existing buildings, add two parking lots, and vacating 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue. No additional student capacity would be added with the current campus revisions.

**TRIP GENERATION**

Typically the Institute of Transportation Engineers (ITE) Trip Generation Manual is used to help estimate the number of trips that a new development or expansion would generate. However, the ITE Trip Generation Manual does not include any trip generation rates for High School land use during the midday peak times. This is a critical time for egress from the High School. Existing trip rates were calculated and compared to ITE trip rates based on historical counts at Sunset High School, which is also in the Beaverton School District.

Using historical trip rates for the remaining 400 students that could potentially be added at full capacity, Table 3 compares future trips based on the historical trip generation and the ITE calculated rate.

**Table 3 – Comparison of Existing and ITE Calculated Trip Generation Rates**

	Population	Trips Rate			Trips		
		In	Out	Total	In	Out	Total
<i>Existing Rate</i>							
AM Peak	400	0.17	0.06	0.23	68	24	92
Midday Peak	400	0.03	0.13	0.16	12	52	64
PM Peak	400	0.05	0.04	0.09	20	16	36
				<b>Totals</b>	<b>100</b>	<b>92</b>	<b>192</b>
<i>ITE Calculated Rate</i>							
AM Peak	400	0.32	0.14	0.46	128	56	184
Midday Peak*	400	0.04	0.23	0.27	16	92	108
PM Peak	400	0.06	0.09	0.15	24	36	60
				<b>Totals</b>	<b>168</b>	<b>184</b>	<b>352</b>

\* No ITE data available, count rate= $((\text{Sunset midday rate}/\text{Sunset PM rate}) * (\text{ITE PM rate}))$ .

Comparison of the trip generation in Table 3 shows that the ITE calculated rate is higher than the existing rate. This higher trip rate would indicate a worst-case scenario for evaluating future conditions. The worst-case scenario for additional trips was used for evaluating future conditions to help estimate the full impacts that could be associated with the expansion.

The ITE Manual does not have survey data for the midday peak, so for the purpose of this analysis, a ratio of the historical Sunset High School midday peak and PM peak trip generation rates to the ITE PM peak trip rate will be used.<sup>9</sup>

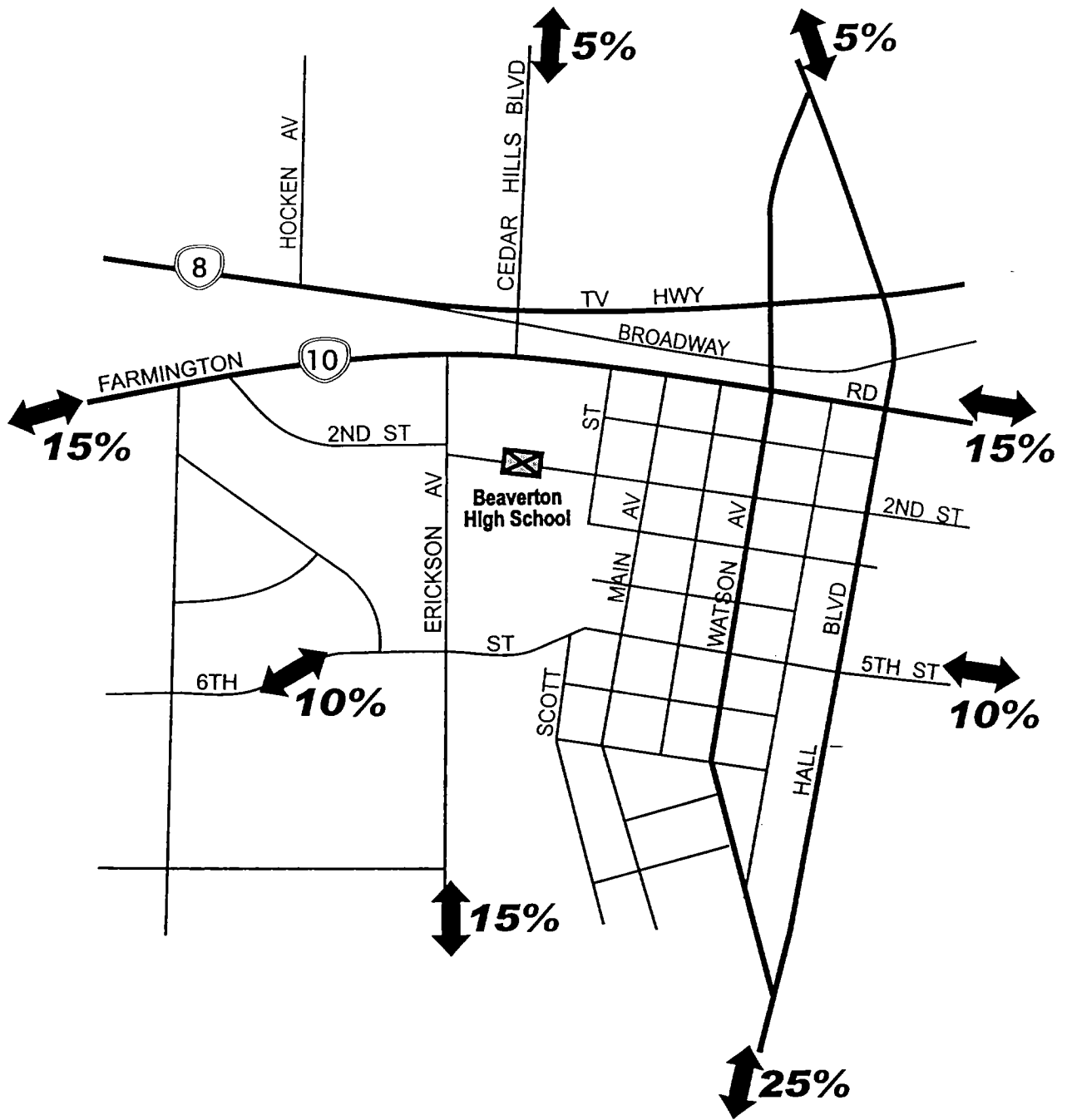
For the worst case scenario with the proposed street vacation, the additional 400 students would generate approximately 184 vehicle trips during the AM peak hour (7:00 AM to 9:00 AM), 108 vehicle trips during the midday peak hour (2:00 PM to 4:00 PM), and approximately 60 vehicle trips during the PM peak hour (4:00 PM to 6:00 PM).

### **TRIP DISTRIBUTION AND ASSIGNMENT**

The existing school traffic volumes observed in the field and the existing school attendance boundary were utilized to help estimate the trip distribution and assignment of additional school traffic onto the street network. Figure 4 summarizes the AM, midday and PM peak hour trip distributions. These trip distributions are then applied to the trips generated by the proposed expansion and assigned to the network.

<sup>9</sup> Midday peak trip rate= $((\text{Sunset HS midday rate}/\text{Sunset HS PM rate}) * (\text{ITE PM rate}))$ .





**LEGEND**

**00%** - Trip Distribution Percentage

- Proposed Road Vacation

**Figure 3**  
**TRIP DISTRIBUTION**  
**With 2nd Street Vacation**

## INTERSECTION CAPACITY

The following sections provide results of intersection capacity analysis for the two scenarios previously listed. Level of service analysis was performed for each of these two scenarios based on the *2000 Highway Capacity Manual* methodology for signalized and unsignalized intersections. A description of each scenario is also included.

### *Existing with 2<sup>nd</sup> Street road vacation*

This scenario redirects existing traffic volumes with the proposed vacation of 2<sup>nd</sup> Street. The roadway network and geometries were assumed to remain the same as those for the existing conditions. Figure 4 summarizes the forecasted volumes for the proposed project for the AM, midday and PM peak hours. Table 4 summarizes the traffic operations under this scenario.

**Table 4 – Future Traffic Operations: Existing with 2<sup>nd</sup> Street Road Vacation**

Intersection	AM Peak			Midday Peak			PM Peak		
	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C
<b><i>Two-Way Stop Controlled Intersection</i></b>									
Farmington Rd/ Erickson Ave	>50	F		>50	F		>50	F	
Farmington Rd/ Stott Ave	19.2	C		14.5	B		13.4	B	
2 <sup>nd</sup> Street/Erickson Ave	12.5	B		11.9	B		11.4	B	
<b><i>All-Way Stop Controlled Intersection</i></b>									
2 <sup>nd</sup> Street/Stott Ave	10.0	B	0.36	8.0	A	0.19	7.7	A	0.13
5 <sup>th</sup> Street/Erickson Ave	34.2	D	0.92	11.1	B	0.44	12.3	B	0.54

All-way stop intersection:

Delay = Average intersection delay

LOS = Level of service

V/C = Volume-to-capacity ratio

Unsignalized intersection:

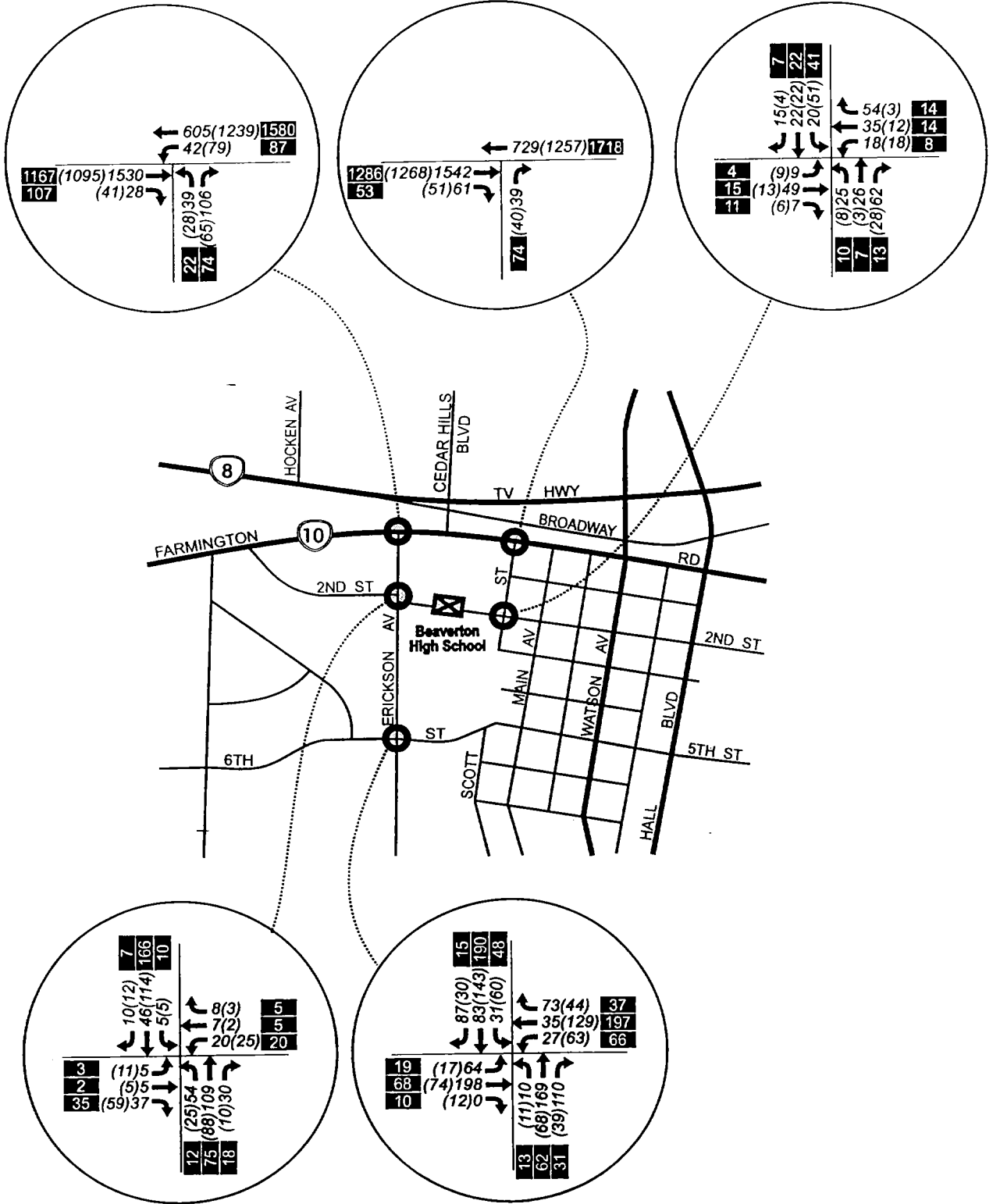
Delay = Average intersection delay

LOS = Minor street LOS

Similar to the existing conditions, all unsignalized study area intersections will continue to operate an acceptable level of service “D” or better during all peak periods with the exception of Farmington Road/Erickson Avenue. This unsignalized intersection continues to operate at LOS “F” for the minor street left turning movement for all periods due to heavy through volumes along Farmington Road.

### *Existing with 2<sup>nd</sup> Street road vacation plus full student capacity (Worst Case)*

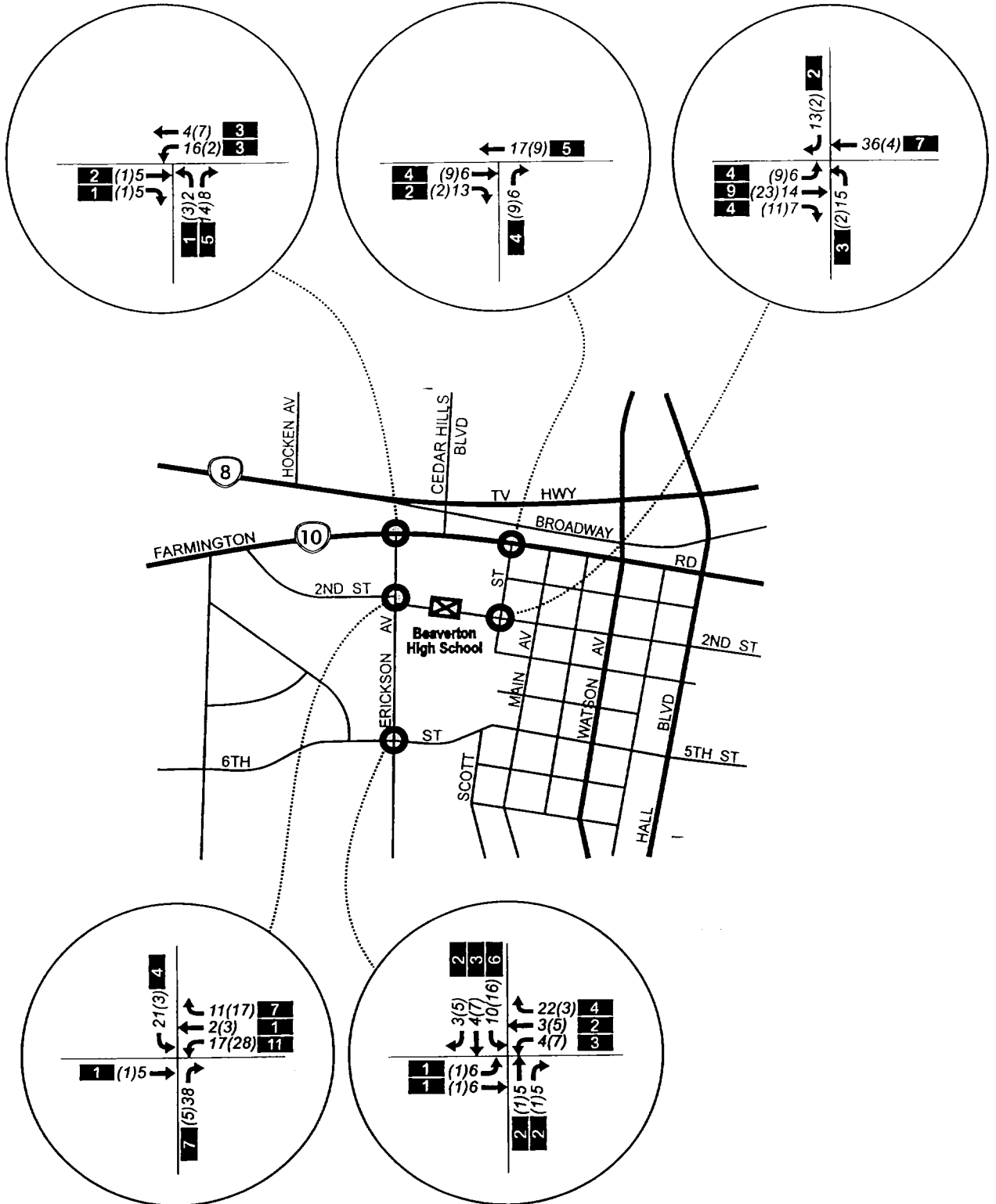
This scenario includes both street vacation and the remaining 400 students that could potentially be added at full capacity. Figure 5 summarizes the forecasted volumes for the future traffic for the remaining 400 students that could potentially be added at full capacity. Figure 6 summarizes the total traffic volumes associated with the worst case scenario. Table 5 summarizes the traffic operations under this scenario.



**LEGEND**

- Study Intersection
- AM(Mid)PM - Peak Hour Traffic Volume
- Mid= Mid-day

**Figure 4**  
**EXISTING WITH STREET VACATION**  
**PEAK HOUR TRAFFIC VOLUMES**



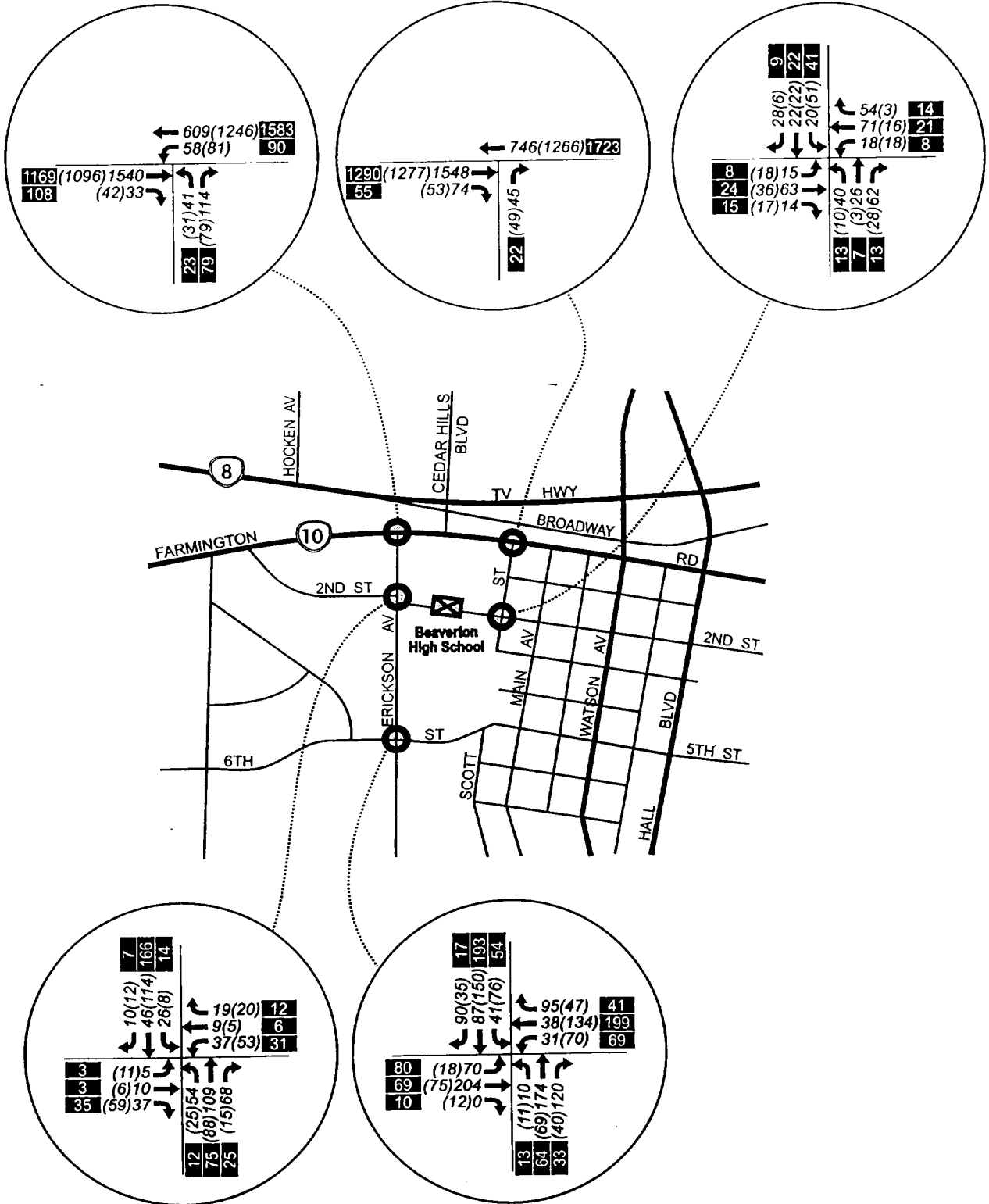
**LEGEND**

- Study Intersection
- AM(Mid)PM - Peak Hour Traffic Volume
- Mid= Mid-day


**Figure 5**  
**ADDED TRAFFIC FOR**  
**MAXIMUM STUDENT CAPACITY**  
**PEAK HOUR TRAFFIC VOLUMES**



NOT TO SCALE



**LEGEND**

-  - Study Intersection
- AM(Mid)PM - Peak Hour Traffic Volume
- Mid= Mid-day

**Figure 6**  
**WORST CASE WITH STREET VACATION AND**  
**MAXIMUM STUDENT CAPACITY**  
**PEAK HOUR TRAFFIC VOLUMES**

**Table 5 – Existing with 2<sup>nd</sup> Street road vacation plus full student capacity (Worst Case)**

Intersection	AM Peak			Midday Peak			PM Peak		
	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C
<b>Two-Way Stop Controlled Intersection</b>									
Farmington Rd/ Erickson Ave	>50	F		>50	F		>50	F	
Farmington Rd/ Stott Ave	19.9	C		16.8	C		14.9	B	
2 <sup>nd</sup> Street/Erickson Ave	14.53	B		12.20	B		11.5	B	
<b>All-Way Stop Controlled Intersection</b>									
2 <sup>nd</sup> Street/Stott Ave	12.3	B	0.36	8.5	A	0.19	7.8	A	0.14
5 <sup>th</sup> Street/Erickson Ave	42.6	E	0.99	11.9	B	0.47	12.7	B	0.55

All-way stop intersection:  
 Delay = Average intersection delay  
 LOS = Level of service  
 V/C = Volume-to-capacity ratio

Unsignalized intersection:  
 Delay = Average intersection delay  
 LOS = Minor street LOS

With additional traffic from the remaining 400 students that could potentially be added at full capacity (worst case), all signalized study area intersections will operate at and acceptable LOS “E” or better during all peak periods with the exception of Farmington Road/Erickson Avenue. This unsignalized intersection continues to operate at LOS “F” for the minor street left turning movement for all periods due to heavy through volumes along Farmington Road.

**PARKING**

Peak parking demand was calculated for the proposed educational facility. Parking demand rates for high schools were taken from surveys at similar high schools in Oregon<sup>10</sup>. The 430 future parking spaces would meet expected demands but would be short of the minimum City code requirement. Parking is summarized in Table 6.

**Table 6: Parking Analysis**

Land Use	Code Required Spaces Per student + staff (Min/Max)	No. of Spaces Required by Code	Peak Expected Demand <sup>11</sup>	Proposed/Existing No. of Spaces
General Retail (2,200 students + 200 staff)	0.2/student + staff to 0.3/student + staff	480 to 720	312	430

KSF- Thousand Square Feet

**ON SITE CIRCULATION ISSUES**

The segment of 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue provides one directional travel for eastbound traffic. This segment is closed via gates between 6:00 AM and 2:30 PM on weekday school days. With the proposed campus revision, 2<sup>nd</sup>

<sup>10</sup> Peak Parking Space Demand Study, Oregon Department of Environmental Quality, JHK and Associates, Table 6.

<sup>11</sup> High Schools have peak parking demand of 0.13 stalls/student+staff.

Street will be a cul-de-sac off Erickson Avenue. The west connection to Erickson Avenue will provide bi-direction traffic to 34 parking stalls and a turn around adjacent to the high school front door. The east connection will provide bi-directional traffic to new developed parking lots along the western portion of the vacated area. Pedestrian and bicycle connections will be maintained along 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue and new wheelchair improvements will include curb and intersection treatments and easier access to the front of the high school building, proposed cafeteria, and auditorium.

School bus loading and unloading is currently accommodated along 2<sup>nd</sup> Street and Stott Avenue. Under the new plan, nine buses could load northbound along Erickson Avenue and 12 buses could load southbound along Stott Avenue. Three special education buses would load in the proposed cul-de-sac on the vacated 2<sup>nd</sup> Street.

Proposed bus circulation and loading along Stott Avenue will be similar to the existing patterns. Buses from the west will enter at Farmington Road and Stott Avenue and buses from the east will use 1<sup>st</sup> Street and 2<sup>nd</sup> Street. Some buses that currently load along 2<sup>nd</sup> Street will be relocated along Erickson Avenue. These buses will head north on Erickson from 5<sup>th</sup> Street rather than east on 2<sup>nd</sup> Street. The high school is currently located on the north enrollment boundary line so buses from 5<sup>th</sup> Street may not increase out of direction traffic substantially.

Emergency access to the site will be provided from the west to the proposed cul-de-sac and from the east via the western parking area.

## **CONNECTIVITY**

City of Beaverton Code states local street connections should be spaced no more than 530 feet in new residential, commercial, and mixed-use development areas. The Beaverton High School is an educational institution within a residential zone. With the proposed vacation of 2<sup>nd</sup> Street for one block, vehicular east-west connectivity between Farmington Road and 5<sup>th</sup> Street will be spaced 1,350 feet. This will cause the existing traffic to divert from 2<sup>nd</sup> Street to Farmington Road and 5<sup>th</sup> Street before and after school hours (since 2<sup>nd</sup> Street is currently closed during school hours). Pedestrian and bicycle connections across the campus will be enhanced so that non-auto travel will not be significantly impacted by the street vacation.

Between 450 and 600 daily trips currently use 2<sup>nd</sup> Street on the weekends<sup>12</sup>. The street vacation will relocate the weekend traffic that uses 2<sup>nd</sup> Street today to parallel facilities on SW Farmington Road and SW 5<sup>th</sup>/6<sup>th</sup> Avenues. The existing street system can adequately service this incremental shift in traffic volumes.

## **UNAVOIDABLE IMPACTS**

The intersection of Farmington Road/Erickson Avenue currently operates at LOS "F" as an unsignalized intersection due to delay to the northbound left turn movement created by

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<sup>12</sup> 24-hour tube counts taken Saturday October 6 and Sunday October 7, 2001.

east/west vehicles along Farmington Road. Under the worst case scenario, the proposed street vacation adds 1 to 2 trips to the critical northbound left turn movement. The deficient operation of this intersection is a pre-existing condition, and is therefore considered an unavoidable impact with or without the proposed street vacation project.

This unsignalized intersection of Farmington Road and Erickson Avenue is spaced approximately 250 feet from the existing signalized intersection of Farmington Road and Cedar Hills Boulevard. The current access spacing standards on Farmington Road is 660 feet<sup>13</sup>. A traffic signal at this location would not meet the Beaverton spacing standards.

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<sup>13</sup> City of Beaverton Development Code, 60.60.40.6.



**CHAPTER**

**4**

**RECOMMENDATIONS**

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This chapter summarizes any recommendations to consider for implementation for the proposed development. These recommendations are based on the previous findings in the transportation impact analysis report and review of the proposed site plan dated March 2001. The following list summarizes any improvements to better facilitate transportation operations and circulation:

- Create pedestrian friendly connections on 2<sup>nd</sup> Street between Erickson Avenue and Stott Avenue including the provision of sidewalks and/or off-street pathways.
- Maintain pedestrian and bicycle connectivity between Erickson Avenue and Stott Avenue following the vehicular vacation of 2<sup>nd</sup> Street.

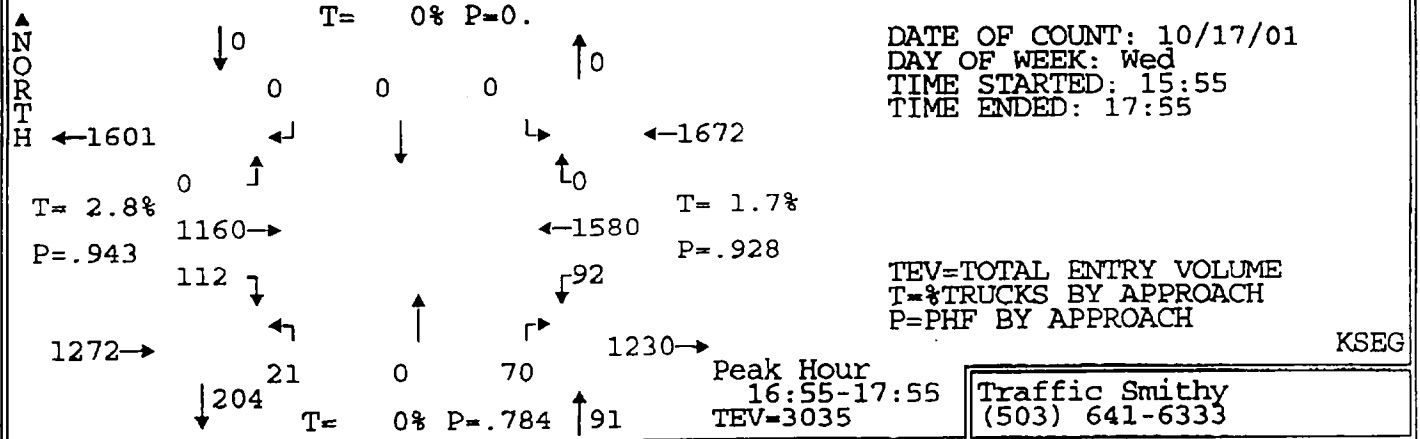
# Appendix

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# Traffic Counts

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INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
FARMINGTON ROAD AT ERICKSON AVENUE



DATE OF COUNT: 10/17/01  
DAY OF WEEK: Wed  
TIME STARTED: 15:55  
TIME ENDED: 17:55

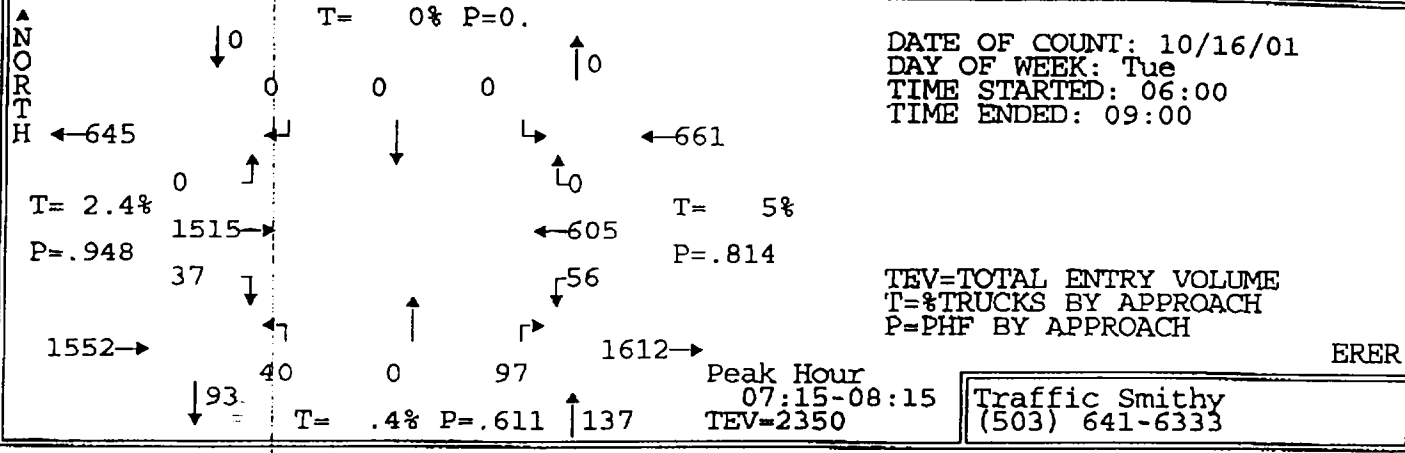
TEV=TOTAL ENTRY VOLUME  
T=%TRUCKS BY APPROACH  
P=PHF BY APPROACH

KSEG

Peak Hour  
16:55-17:55  
TEV=3035  
Traffic Smithy  
(503) 641-6333

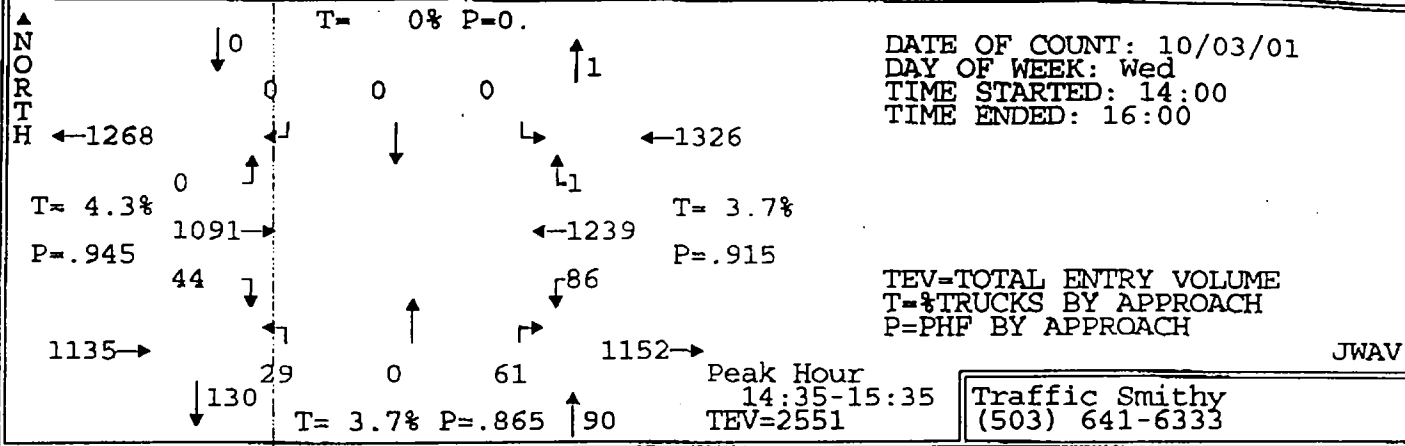
TIME PERIOD FROM - TO	EAST BOUND			SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL
	↓	→	↑	↙	↓	↘	←	↑	↗	↓	←	↑	
15:55-16:00	3	87	0	0	0	0	2	0	3	5	147	0	247
16:00-16:05	4	77	0	0	0	0	3	0	7	6	135	0	232
16:05-16:10	6	116	0	0	0	0	0	0	3	4	111	0	240
16:10-16:15	6	102	0	0	0	0	0	0	5	2	122	1	238
16:15-16:20	5	83	0	0	0	1	3	0	5	3	124	0	224
16:20-16:25	4	96	0	0	0	0	1	0	4	4	134	0	243
16:25-16:30	3	72	0	0	0	0	5	0	2	6	126	0	214
16:30-16:35	10	100	0	0	0	0	1	0	6	4	99	0	220
16:35-16:40	6	97	0	0	0	0	5	0	7	4	94	0	213
16:40-16:45	4	80	0	0	0	0	2	0	2	7	138	0	233
16:45-16:50	5	107	0	0	0	0	1	0	6	5	119	0	243
16:50-16:55	5	86	0	0	0	0	2	0	9	13	118	0	233
16:55-17:00	5	104	0	0	0	0	5	0	5	7	131	0	257
17:00-17:05	8	85	0	0	0	0	1	0	6	9	139	0	248
17:05-17:10	6	102	0	0	0	0	1	0	5	8	148	0	270
17:10-17:15	10	102	0	0	0	0	2	0	8	7	101	0	230
17:15-17:20	14	79	0	0	0	0	0	0	10	10	145	0	258
17:20-17:25	7	89	0	0	0	0	3	0	6	4	156	0	265
17:25-17:30	9	107	0	0	0	0	2	0	2	11	124	0	255
17:30-17:35	16	103	0	0	0	0	2	0	3	6	121	0	251
17:35-17:40	9	93	0	0	0	0	1	0	8	6	134	0	251
17:40-17:45	9	102	0	0	0	0	0	0	5	9	135	0	260
17:45-17:50	15	94	0	0	0	0	1	0	7	9	123	0	249
17:50-17:55	4	100	0	0	0	0	3	0	5	6	123	0	241
<b>Total Survey</b>	<b>173</b>	<b>2263</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>46</b>	<b>0</b>	<b>129</b>	<b>155</b>	<b>3047</b>	<b>1</b>	<b>5815</b>
PHF	.82	.96	0	0	0	0	.75	0	.73	.92	.93	0	.975
% Trucks	2.9	2.7	0	0	0	0	0	0	0	1.3	1.7	0	2.1
Stopped Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Peds	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>Hourly Totals</b>													
15:55-16:55	61	1103	0	0	0	1	25	0	59	63	1467	1	2780
16:10-17:10	67	1114	0	0	0	1	27	0	62	72	1492	1	2836
16:25-17:25	83	1103	0	0	0	0	28	0	72	84	1514	0	2884
16:40-17:40	98	1137	0	0	0	0	22	0	70	93	1574	0	2994
16:55-17:55	112	1160	0	0	0	0	21	0	70	92	1580	0	3035

INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
FARMINGTON ROAD AT ERICKSON AVENUE



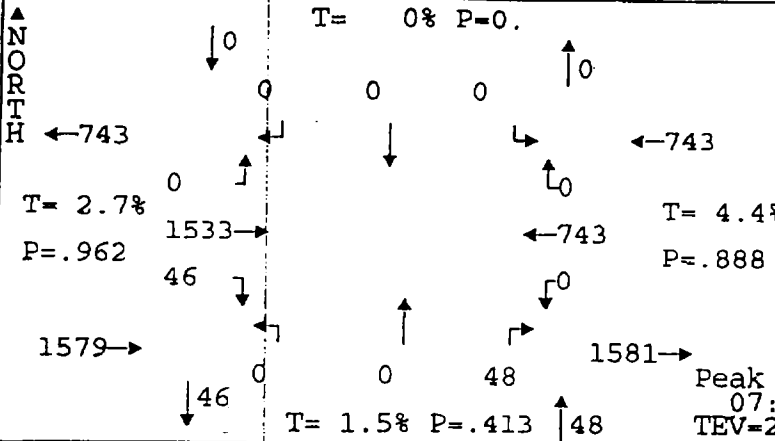
TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL	
	↓	→	↑	←	↓	←	↑	→	↓	←	↑		
06:00-06:05	0	48	0	0	0	0	2	0	0	0	13	0	63
06:05-06:10	1	51	0	0	0	0	1	0	2	0	15	0	70
06:10-06:15	1	93	0	0	0	0	1	0	2	0	15	0	112
06:15-06:20	0	77	0	0	0	0	1	0	1	0	15	0	94
06:20-06:25	1	63	0	0	0	0	2	0	0	1	16	0	83
06:25-06:30	2	90	0	0	0	0	2	0	5	2	16	0	117
06:30-06:35	0	85	0	0	0	0	2	0	7	0	17	0	111
06:35-06:40	0	88	0	0	0	0	4	0	5	1	20	0	118
06:40-06:45	1	131	0	0	0	0	2	0	3	6	19	0	162
06:45-06:50	2	129	0	0	0	0	7	0	3	1	15	0	157
06:50-06:55	0	116	0	0	0	0	1	0	4	5	29	0	155
06:55-07:00	5	104	0	0	0	0	1	0	4	3	18	0	135
07:00-07:05	1	104	0	0	0	0	5	0	4	3	22	0	139
07:05-07:10	0	104	0	0	0	0	5	0	5	3	23	0	140
07:10-07:15	3	120	0	0	0	0	3	0	5	3	9	0	143
07:15-07:20	2	128	0	0	0	0	2	0	5	5	24	0	166
07:20-07:25	4	99	0	0	0	0	4	0	6	7	54	0	174
07:25-07:30	1	131	0	0	0	0	1	0	4	7	53	0	197
07:30-07:35	5	124	0	0	0	0	2	0	14	11	51	0	207
07:35-07:40	3	132	0	0	0	0	6	0	13	3	53	0	210
07:40-07:45	4	141	0	0	0	0	7	0	14	7	41	0	214
07:45-07:50	2	126	0	0	0	0	4	0	12	6	66	0	216
07:50-07:55	3	131	0	0	0	0	6	0	7	2	71	0	220
07:55-08:00	1	126	0	0	0	0	4	0	15	3	55	0	204
08:00-08:05	3	127	0	0	0	0	0	0	3	1	45	0	179
08:05-08:10	5	136	0	0	0	0	1	0	1	0	51	0	194
08:10-08:15	4	114	0	0	0	0	3	0	3	4	41	0	169
08:15-08:20	1	100	0	0	0	0	1	0	5	3	56	0	166
08:20-08:25	1	136	0	0	0	0	3	0	4	3	19	0	166
08:25-08:30	0	122	0	0	0	0	0	0	3	1	53	0	179
08:30-08:35	0	127	0	0	0	0	1	0	3	4	48	0	183
08:35-08:40	3	102	0	0	0	0	5	0	4	4	48	0	166
08:40-08:45	1	113	0	0	0	0	4	0	3	4	47	0	172
08:45-08:50	0	102	0	0	0	0	1	0	7	2	64	0	176
08:50-08:55	0	101	0	0	0	0	2	0	4	2	52	0	161
08:55-09:00	4	106	0	0	0	0	4	0	4	3	41	0	162
Total Survey	64	3927	0	0	0	0	100	0	184	110	1295	0	5680
PHF	.77	.95	0	0	0	0	.59	0	.59	.56	.79	0	.903
% Trucks	4.7	2.3	0	0	0	0	0	0	.5	7.3	4.8	0	2.9
Stopped Buses	0	4	0	0	0	0	0	0	0	0	0	0	0
Peds	0	9	0	0	0	0	0	0	0	0	0	0	0
Hourly Totals													
06:00-07:00	13	1075	0	0	0	0	26	0	36	19	208	0	1377
06:15-07:15	15	1211	0	0	0	0	35	0	46	28	219	0	1554
06:30-07:30	19	1339	0	0	0	0	37	0	55	44	303	0	1797
06:45-07:45	30	1432	0	0	0	0	44	0	81	58	392	0	2037
07:00-08:00	29	1466	0	0	0	0	49	0	104	60	522	0	2230
07:15-08:15	37	1515	0	0	0	0	40	0	97	56	605	0	2350
07:30-08:30	32	1515	0	0	0	0	37	0	94	44	602	0	2324
07:45-08:45	24	1460	0	0	0	0	32	0	63	35	600	0	2214
08:00-09:00	22	1386	0	0	0	0	25	0	44	31	565	0	2073

INTERSEC. N TURN MOVEMENT COUNT SUMMARY REPORT  
 FARMINGTON ROAD AT ERICKSON AVENUE



TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND		ALL			
	→	↑	←	↓	←	↑	→	↓				
14:00-14:05	5	59	0	0	0	4	0	7	4	62	0	141
14:05-14:10	1	87	0	0	0	2	0	5	6	106	0	207
14:10-14:15	4	84	0	0	0	2	0	1	4	72	0	167
14:15-14:20	1	96	0	0	0	3	0	4	9	77	0	190
14:20-14:25	4	86	0	0	0	1	0	4	7	103	0	205
14:25-14:30	2	89	0	0	0	1	0	10	3	50	0	155
14:30-14:35	2	77	0	0	0	5	0	9	3	95	0	191
14:35-14:40	1	105	0	0	0	3	0	9	4	113	0	235
14:40-14:45	4	87	0	0	0	1	0	5	8	85	0	190
14:45-14:50	7	96	0	0	0	0	0	7	12	127	0	249
14:50-14:55	4	84	0	0	0	6	0	2	7	112	0	215
14:55-15:00	1	90	0	0	0	3	0	3	5	77	0	179
15:00-15:05	4	82	0	0	0	4	0	5	11	107	0	213
15:05-15:10	4	96	0	0	0	2	0	5	9	120	0	236
15:10-15:15	4	75	0	0	0	0	0	4	9	106	0	198
15:15-15:20	5	97	0	0	0	4	0	4	7	96	0	213
15:20-15:25	4	84	0	0	0	2	0	6	6	112	1	215
15:25-15:30	4	86	0	0	0	3	0	7	4	97	0	201
15:30-15:35	2	109	0	0	0	1	0	4	4	87	0	207
15:35-15:40	2	102	0	0	0	2	0	4	3	101	0	214
15:40-15:45	2	74	0	0	0	4	0	5	6	110	0	201
15:45-15:50	5	88	0	0	0	4	0	13	4	118	0	232
15:50-15:55	6	80	0	0	0	0	0	5	7	104	0	202
15:55-16:00	7	88	0	0	0	2	0	3	3	110	0	213
<b>Total Survey</b>	<b>85</b>	<b>2101</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>59</b>	<b>0</b>	<b>131</b>	<b>145</b>	<b>2347</b>	<b>1</b>	<b>4869</b>
<b>PHF</b>	<b>.73</b>	<b>.95</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>.56</b>	<b>0</b>	<b>.73</b>	<b>.74</b>	<b>.93</b>	<b>.25</b>	<b>.946</b>
<b>% Trucks</b>	<b>2.4</b>	<b>4.4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>5.1</b>	<b>0</b>	<b>3.1</b>	<b>2.8</b>	<b>3.7</b>	<b>0</b>	<b>4</b>
<b>Stopped Buses</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Peds</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Hourly Totals</b>												
14:00-15:00	36	1040	0	0	0	31	0	66	72	1079	0	2324
14:15-15:15	38	1063	0	0	0	29	0	67	87	1172	0	2456
14:30-15:30	44	1059	0	0	0	33	0	66	85	1247	1	2535
14:45-15:45	43	1075	0	0	0	31	0	56	83	1252	1	2541
15:00-16:00	49	1061	0	0	0	28	0	65	73	1268	1	2545

INTERSEC. ON TURN MOVEMENT COUNT SUMMARY REPORT  
 FARMINGTON ROAD AT STOTT AVENUE



DATE OF COUNT: 10/11/01  
 DAY OF WEEK: Thu  
 TIME STARTED: 06:00  
 TIME ENDED: 09:00

TEV=TOTAL ENTRY VOLUME  
 T=%TRUCKS BY APPROACH  
 P=PHF BY APPROACH

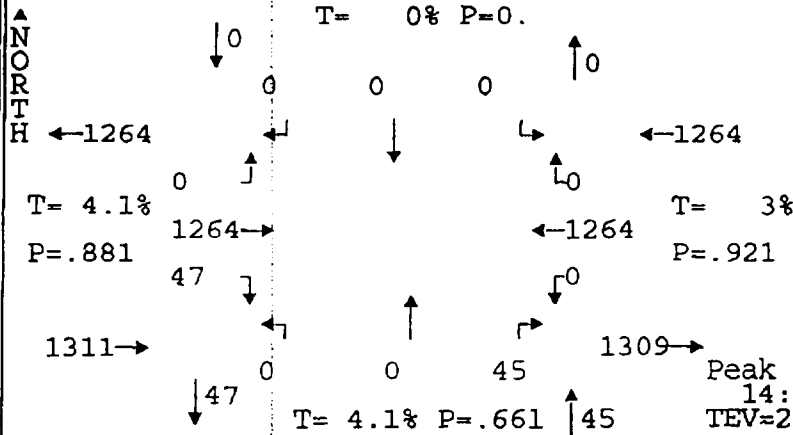
EREO

Peak Hour  
 07:10-08:10  
 TEV=2370

Traffic Smithy  
 (503) 641-6333

TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND		ALL		
	→	↑	↓	←	↑	←	↓	←			
06:00-06:05	1	52	0	0	0	0	0	0	7	0	60
06:05-06:10	0	50	0	0	0	0	0	0	12	0	62
06:10-06:15	2	71	0	0	0	0	0	0	21	0	94
06:15-06:20	2	57	0	0	0	0	0	0	12	0	71
06:20-06:25	1	92	0	0	0	0	0	0	15	0	108
06:25-06:30	1	83	0	0	0	0	0	0	20	0	104
06:30-06:35	0	97	0	0	0	0	0	0	32	0	129
06:35-06:40	0	89	0	0	0	0	0	2	29	0	120
06:40-06:45	0	118	0	0	0	0	0	2	40	0	160
06:45-06:50	0	101	0	0	0	0	0	0	37	0	138
06:50-06:55	2	137	0	0	0	0	0	0	27	0	167
06:55-07:00	4	111	0	0	0	0	0	1	55	0	170
07:00-07:05	0	95	0	0	0	0	0	1	52	0	148
07:05-07:10	5	109	0	0	0	0	0	3	41	0	158
07:10-07:15	3	131	0	0	0	0	0	3	78	0	214
07:15-07:20	1	142	0	0	0	0	0	2	63	0	209
07:20-07:25	6	121	0	0	0	0	0	2	49	0	178
07:25-07:30	3	121	0	0	0	0	0	2	65	0	191
07:30-07:35	11	131	0	0	0	0	0	9	48	0	199
07:35-07:40	6	123	0	0	0	0	0	10	57	0	196
07:40-07:45	1	138	0	0	0	0	0	6	59	0	204
07:45-07:50	7	129	0	0	0	0	0	13	88	0	237
07:50-07:55	1	133	0	0	0	0	0	1	60	0	195
07:55-08:00	3	112	0	0	0	0	0	0	61	0	176
08:00-08:05	2	131	0	0	0	0	0	0	53	0	186
08:05-08:10	2	121	0	0	0	0	0	0	62	0	185
08:10-08:15	0	87	0	0	0	0	0	2	55	0	144
08:15-08:20	1	125	0	0	0	0	0	1	59	0	186
08:20-08:25	2	114	0	0	0	0	0	1	58	0	175
08:25-08:30	1	105	0	0	0	0	0	0	64	0	170
08:30-08:35	0	95	0	0	0	0	0	1	77	0	173
08:35-08:40	0	108	0	0	0	0	0	2	47	0	157
08:40-08:45	1	97	0	0	0	0	0	2	69	0	169
08:45-08:50	0	107	0	0	0	0	0	0	65	0	172
08:50-08:55	3	115	0	0	0	0	0	0	58	0	176
08:55-09:00	4	82	0	0	0	0	0	0	60	0	146
<b>Total Survey</b>	<b>76</b>	<b>3830</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>66</b>	<b>1755</b>	<b>0</b>	<b>5727</b>
PHF	.57	.96	0	0	0	0	0	.41	.89	0	.930
% Trucks	1.3	2.7	0	0	0	0	0	1.5	4.4	0	3.2
Stopped Buses	0	1	0	0	0	0	0	0	0	0	0
Peds.	0	4	0	0	0	0	0	0	0	0	0
<b>Hourly Totals</b>											
06:00-07:00	13	1058	0	0	0	0	0	5	307	0	1383
06:15-07:15	18	1220	0	0	0	0	0	11	438	0	1687
06:30-07:30	24	1372	0	0	0	0	0	18	568	0	1982
06:45-07:45	42	1460	0	0	0	0	0	39	631	0	2172
07:00-08:00	47	1485	0	0	0	0	0	52	721	0	2305
07:15-08:15	43	1489	0	0	0	0	0	48	720	0	2300
07:30-08:30	37	1449	0	0	0	0	0	43	724	0	2253
07:45-08:45	20	1357	0	0	0	0	0	23	753	0	2153
08:00-09:00	16	1287	0	0	0	0	0	9	727	0	2039

INTERSEC. IN TURN MOVEMENT COUNT SUMMARY REPORT  
FARMINGTON ROAD AT STOTT AVENUE



DATE OF COUNT: 10/11/01  
DAY OF WEEK: Thu  
TIME STARTED: 13:50  
TIME ENDED: 15:50

TEV=TOTAL ENTRY VOLUME  
T=%TRUCKS BY APPROACH  
P=PHF BY APPROACH

EREP

Peak Hour  
14:35-15:35  
TEV=2620

Traffic Smithy  
(503) 641-6333

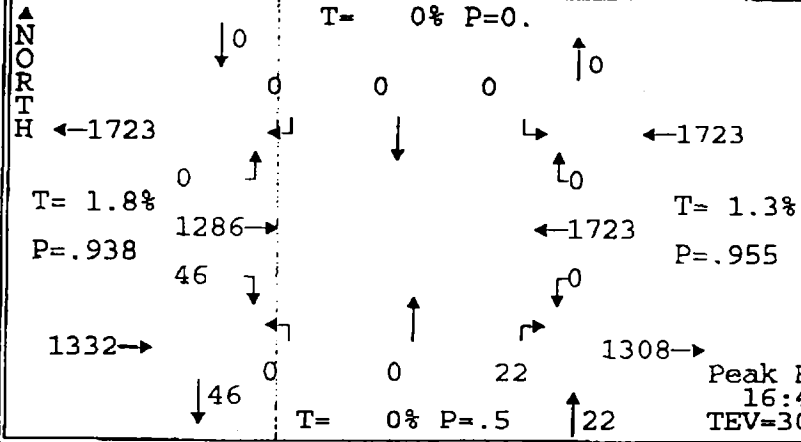
TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND		ALL			
	→	↑	↓	←	↑	←	↓	←				
13:50-13:55	1	85	0	0	0	0	0	2	0	82	0	170
13:55-14:00	1	75	0	0	0	0	0	1	0	99	0	176
14:00-14:05	0	99	0	0	0	0	0	1	0	84	0	184
14:05-14:10	1	94	0	0	0	0	0	1	0	75	0	171
14:10-14:15	0	108	0	0	0	0	0	2	0	87	0	197
14:15-14:20	4	104	0	0	0	0	0	5	0	75	0	188
14:20-14:25	2	98	0	0	0	0	0	2	0	66	0	168
14:25-14:30	3	89	0	0	0	0	0	1	0	41	0	134
14:30-14:35	16	75	0	0	0	0	0	0	0	85	0	176
14:35-14:40	14	122	0	0	0	0	0	4	0	97	0	237
14:40-14:45	3	111	0	0	0	0	0	4	0	108	0	226
14:45-14:50	7	115	0	0	0	0	0	9	0	124	0	255
14:50-14:55	5	100	0	0	0	0	0	2	0	96	0	203
14:55-15:00	2	105	0	0	0	0	0	2	0	95	0	204
15:00-15:05	3	89	0	0	0	0	0	1	0	94	0	187
15:05-15:10	3	90	0	0	0	0	0	3	0	111	0	207
15:10-15:15	2	107	0	0	0	0	0	2	0	121	0	232
15:15-15:20	2	100	0	0	0	0	0	2	0	104	0	208
15:20-15:25	2	106	0	0	0	0	0	2	0	118	0	228
15:25-15:30	3	111	0	0	0	0	0	4	0	119	0	237
15:30-15:35	1	108	0	0	0	0	0	10	0	77	0	196
15:35-15:40	2	99	0	0	0	0	0	5	0	103	0	209
15:40-15:45	3	95	0	0	0	0	0	3	0	136	0	237
15:45-15:50	1	114	0	0	0	0	0	5	0	109	0	229

Total Survey	81	2399	0	0	0	0	0	0	73	0	2306	0	4859
PHF	.49	.91	0	0	0	0	0	0	.66	0	.92	0	.912
% Trucks	6.2	4	0	0	0	0	0	0	4.1	0	3	0	3.6
Stopped Buses	0	3	0	0	0	0	0	0	0	0	0	0	0
Peds	0	3	0	0	0	0	0	0	0	0	0	0	0

Hourly Totals													
13:50-14:50	52	1175	0	0	0	0	0	0	32	0	1023	0	2282
14:05-15:05	60	1210	0	0	0	0	0	0	33	0	1043	0	2346
14:20-15:20	62	1201	0	0	0	0	0	0	32	0	1142	0	2437
14:35-15:35	47	1264	0	0	0	0	0	0	45	0	1264	0	2620
14:50-15:50	29	1224	0	0	0	0	0	0	41	0	1283	0	2577



INTERSEC. IN TURN MOVEMENT COUNT SUMMARY REPORT  
 FARMINGTON ROAD AT STOTT AVENUE



DATE OF COUNT: 10/11/01  
 DAY OF WEEK: Thu  
 TIME STARTED: 16:00  
 TIME ENDED: 18:00

TEV=TOTAL ENTRY VOLUME  
 T=%TRUCKS BY APPROACH  
 P=PHF BY APPROACH

Peak Hour  
 16:40-17:40  
 TEV=3077

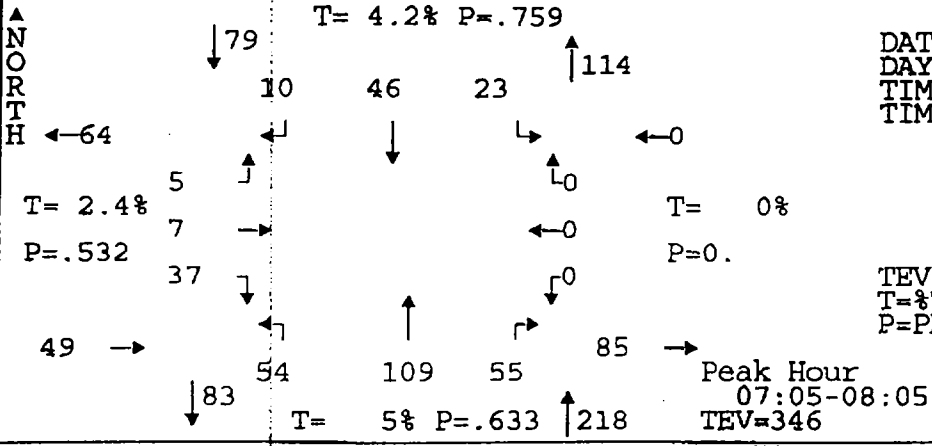
Traffic Smithy  
 (503) 641-6333

EREQ

TIME PERIOD FROM - TO	EAST BOUND			SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL	
	↓	→	↑	←	↓	↘	←	↑	↗	↓	←	↑		
16:00-16:05	3	73	0	0	0	0	0	0	0	3	0	108	0	187
16:05-16:10	3	86	0	0	0	0	0	0	0	4	0	131	0	224
16:10-16:15	1	114	0	0	0	0	0	0	0	2	0	125	0	242
16:15-16:20	0	117	0	0	0	0	0	0	0	5	0	140	0	262
16:20-16:25	6	111	0	0	0	0	0	0	0	2	0	117	0	236
16:25-16:30	4	123	0	0	0	0	0	0	0	1	0	145	0	273
16:30-16:35	1	72	0	0	0	0	0	0	0	3	0	108	0	184
16:35-16:40	0	97	0	0	0	0	0	0	0	4	0	137	0	238
16:40-16:45	2	112	0	0	0	0	0	0	0	1	0	146	0	261
16:45-16:50	3	110	0	0	0	0	0	0	0	1	0	138	0	252
16:50-16:55	3	110	0	0	0	0	0	0	0	3	0	145	0	261
16:55-17:00	0	99	0	0	0	0	0	0	0	3	0	158	0	260
17:00-17:05	2	107	0	0	0	0	0	0	0	5	0	140	0	254
17:05-17:10	4	116	0	0	0	0	0	0	0	0	0	153	0	273
17:10-17:15	4	99	0	0	0	0	0	0	0	3	0	145	0	251
17:15-17:20	3	82	0	0	0	0	0	0	0	2	0	133	0	220
17:20-17:25	2	119	0	0	0	0	0	0	0	1	0	140	0	262
17:25-17:30	7	108	0	0	0	0	0	0	0	1	0	125	0	241
17:30-17:35	6	113	0	0	0	0	0	0	0	0	0	138	0	257
17:35-17:40	10	111	0	0	0	0	0	0	0	2	0	162	0	285
17:40-17:45	2	118	0	0	0	0	0	0	0	3	0	127	0	250
17:45-17:50	7	126	0	0	0	0	0	0	0	3	0	118	0	254
17:50-17:55	9	97	0	0	0	0	0	0	0	2	0	125	0	233
17:55-18:00	7	107	0	0	0	0	0	0	0	1	0	133	0	248
<b>Total Survey</b>	<b>89</b>	<b>2527</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>55</b>	<b>0</b>	<b>3237</b>	<b>0</b>	<b>5908</b>
<b>PHF</b>	<b>.5</b>	<b>.95</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>.5</b>	<b>0</b>	<b>.96</b>	<b>0</b>	<b>.977</b>
<b>% Trucks</b>	<b>3.4</b>	<b>1.7</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1.3</b>	<b>0</b>	<b>1.5</b>
<b>Stopped Buses</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Peds</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Hourly Totals</b>														
16:00-17:00	26	1224	0	0	0	0	0	0	0	32	0	1598	0	2880
16:15-17:15	29	1273	0	0	0	0	0	0	0	31	0	1672	0	3005
16:30-17:30	31	1231	0	0	0	0	0	0	0	27	0	1668	0	2957
16:45-17:45	46	1292	0	0	0	0	0	0	0	24	0	1704	0	3066
17:00-18:00	53	1303	0	0	0	0	0	0	0	23	0	1639	0	3028

INTERSEC. ON TURN MOVEMENT COUNT SUMMARY REPORT  
2ND STREET AT ERICKSON AVENUE

NORTH



DATE OF COUNT: 10/11/01  
DAY OF WEEK: Thu  
TIME STARTED: 06:00  
TIME ENDED: 09:00

TEV=TOTAL ENTRY VOLUME  
T=%TRUCKS BY APPROACH  
P=PHF BY APPROACH

KSED

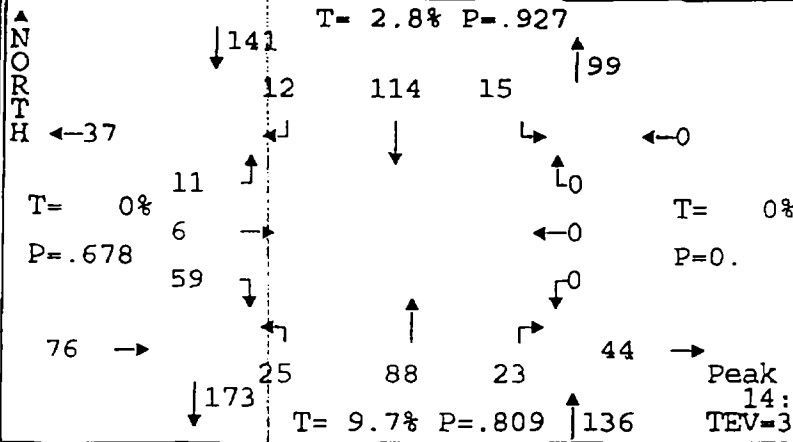
Peak Hour  
07:05-08:05  
TEV=346

Traffic Smithy  
(503) 641-6333

TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND				ALL	
	→	↑	←	↓	←	↑	→	↓	←	↑		
06:00-06:05	1	0	0	0	0	0	2	0	0	0	3	
06:05-06:10	0	0	2	0	0	0	1	0	0	0	3	
06:10-06:15	0	1	1	0	0	0	3	1	0	0	6	
06:15-06:20	1	0	0	0	0	0	2	0	0	0	6	
06:20-06:25	2	0	1	0	0	0	6	1	0	0	10	
06:25-06:30	0	0	0	0	2	1	4	0	0	0	7	
06:30-06:35	0	0	0	0	2	0	3	1	0	0	6	
06:35-06:40	0	0	1	0	0	0	5	3	0	0	9	
06:40-06:45	0	0	0	0	4	1	4	6	0	0	16	
06:45-06:50	3	1	0	0	3	1	5	9	0	0	22	
06:50-06:55	0	1	0	0	2	1	8	12	0	0	24	
06:55-07:00	2	2	0	0	6	1	5	2	1	0	20	
07:00-07:05	2	0	0	0	1	5	3	2	0	0	13	
07:05-07:10	2	0	0	0	5	1	4	5	0	0	18	
07:10-07:15	0	1	0	0	3	3	7	1	0	0	16	
07:15-07:20	3	0	0	1	3	4	8	2	0	0	23	
07:20-07:25	1	2	0	0	7	2	8	10	0	0	30	
07:25-07:30	5	2	1	1	3	4	12	6	0	0	37	
07:30-07:35	0	2	0	0	5	2	10	10	0	0	31	
07:35-07:40	1	0	0	2	6	3	13	11	0	0	40	
07:40-07:45	7	2	1	1	3	12	11	6	0	0	44	
07:45-07:50	6	2	2	1	1	14	12	3	0	0	42	
07:50-07:55	4	0	1	2	3	9	9	0	0	0	28	
07:55-08:00	4	0	0	1	4	2	7	0	0	0	18	
08:00-08:05	4	0	0	1	3	2	8	1	0	0	19	
08:05-08:10	0	0	0	2	0	1	14	0	0	0	17	
08:10-08:15	1	0	0	7	0	1	3	0	0	0	12	
08:15-08:20	2	0	0	1	3	0	8	2	0	0	16	
08:20-08:25	1	0	0	2	0	0	8	1	0	0	12	
08:25-08:30	0	0	0	3	0	1	3	1	0	0	8	
08:30-08:35	1	0	0	2	0	1	5	0	0	0	9	
08:35-08:40	1	1	0	0	0	0	6	0	0	0	8	
08:40-08:45	0	1	1	3	1	1	5	2	0	0	14	
08:45-08:50	1	0	0	4	0	1	4	0	0	0	10	
08:50-08:55	2	0	1	1	0	2	2	0	0	0	8	
08:55-09:00	1	0	0	1	0	1	1	1	0	0	5	
<b>Total Survey</b>	<b>58</b>	<b>13</b>	<b>12</b>	<b>11</b>	<b>95</b>	<b>37</b>	<b>65</b>	<b>219</b>	<b>99</b>	<b>1</b>	<b>0</b>	<b>610</b>
<b>PHF</b>	<b>.54</b>	<b>.44</b>	<b>.31</b>	<b>.63</b>	<b>.77</b>	<b>.57</b>	<b>.39</b>	<b>.76</b>	<b>.51</b>	<b>0</b>	<b>0</b>	<b>.686</b>
<b>% Trucks</b>	<b>3.4</b>	<b>0</b>	<b>0</b>	<b>9.1</b>	<b>2.1</b>	<b>8.1</b>	<b>1.5</b>	<b>1.4</b>	<b>15.2</b>	<b>0</b>	<b>0</b>	<b>4.4</b>
<b>Stopped Buses</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Peds</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Hourly Totals</b>												
06:00-07:00	9	5	5	0	22	5	2	48	35	1	0	132
06:15-07:15	12	5	2	0	31	14	4	56	42	1	0	167
06:30-07:30	18	9	2	0	39	23	9	72	59	1	0	234
06:45-07:45	26	11	2	5	47	28	28	94	76	1	0	318
07:00-08:00	35	7	5	9	44	28	52	104	56	0	0	340
07:15-08:15	36	6	5	10	47	19	54	115	49	0	0	341
07:30-08:30	30	2	4	8	40	12	50	106	35	0	0	287
07:45-08:45	24	1	4	6	31	7	32	88	10	0	0	203
08:00-09:00	14	1	2	2	29	4	11	67	8	0	0	138

INTERSEC ON TURN MOVEMENT COUNT SUMMA REPORT  
2ND STREET AT ERICKSON AVENUE

NORTH



DATE OF COUNT: 10/11/01  
DAY OF WEEK: Thu  
TIME STARTED: 14:00  
TIME ENDED: 16:00

TEV=TOTAL ENTRY VOLUME  
T=%TRUCKS BY APPROACH  
P=PHF BY APPROACH

KSEE

Peak Hour  
14:40-15:40  
TEV=353

Traffic Smithy  
(503) 641-6333

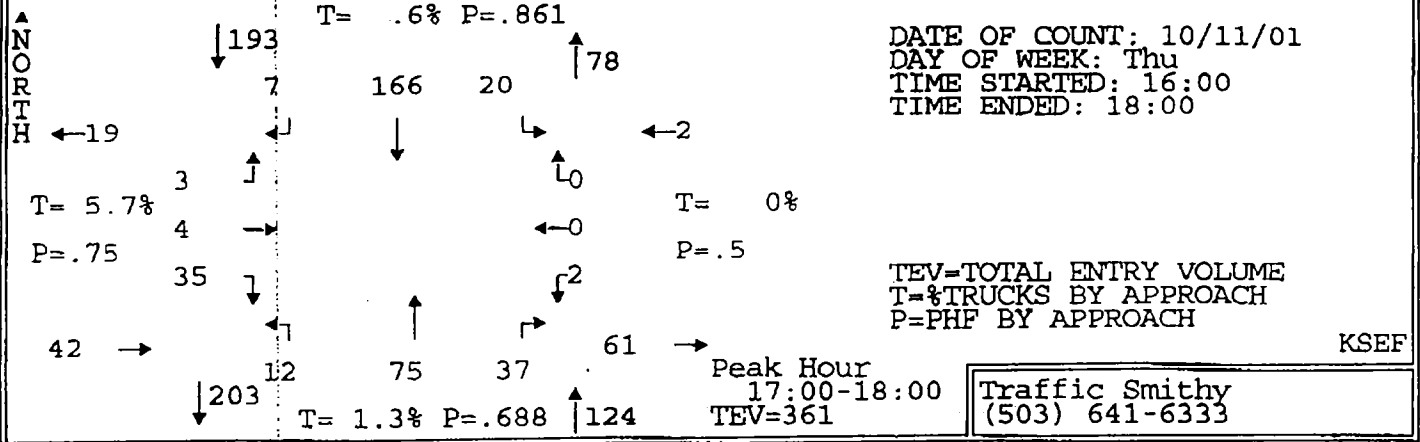
TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND		ALL
	→	↑	←	↓	←	↑	↓	←	

14:00-14:05	4	1	0	0	8	1	1	4	0	0	0	0	19
14:05-14:10	2	0	0	1	5	1	4	7	1	0	0	0	21
14:10-14:15	0	0	0	1	5	1	0	8	0	0	0	0	15
14:15-14:20	1	0	1	1	4	0	1	5	1	0	0	0	14
14:20-14:25	3	0	1	1	8	0	1	2	1	0	0	0	17
14:25-14:30	2	0	0	1	8	1	4	3	4	0	0	0	23
14:30-14:35	6	0	0	1	9	1	2	2	2	0	0	0	23
14:35-14:40	3	0	2	2	6	2	1	10	6	0	0	0	32
14:40-14:45	9	0	0	1	6	0	2	9	2	0	0	0	29
14:45-14:50	13	0	3	3	18	1	4	11	1	0	0	0	54
14:50-14:55	3	0	1	1	3	1	0	7	1	0	0	0	15
14:55-15:00	5	0	3	1	9	1	5	11	2	0	0	0	37
15:00-15:05	1	0	1	2	11	2	2	9	0	0	0	0	28
15:05-15:10	5	3	0	0	10	1	2	5	3	0	0	0	29
15:10-15:15	4	0	0	0	5	1	1	3	1	0	0	0	16
15:15-15:20	2	0	0	2	9	0	0	4	6	0	0	0	23
15:20-15:25	3	1	0	0	14	1	1	2	1	0	0	0	23
15:25-15:30	6	0	1	0	8	0	3	11	1	0	0	0	30
15:30-15:35	4	1	1	0	11	3	3	7	5	0	0	0	35
15:35-15:40	4	1	1	2	10	4	2	9	1	0	0	0	34
15:40-15:45	2	1	1	1	6	1	0	10	2	0	0	0	24
15:45-15:50	4	0	1	0	12	0	3	7	0	0	0	0	27
15:50-15:55	5	1	0	1	6	0	1	7	5	1	0	0	27
15:55-16:00	0	0	0	0	16	1	1	4	1	0	0	0	23

Total Survey	91	9	17	22	207	24	44	157	46	1	0	0	618
PHF	.89	.5	.46	.6	.86	.54	.69	.76	.57	0	0	0	.832
% Trucks	0	0	0	4.5	1.9	8.3	0	4.5	37	0	0	0	5
Stopped Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Peds	0	0	0	0	0	0	0	0	0	0	0	0	0

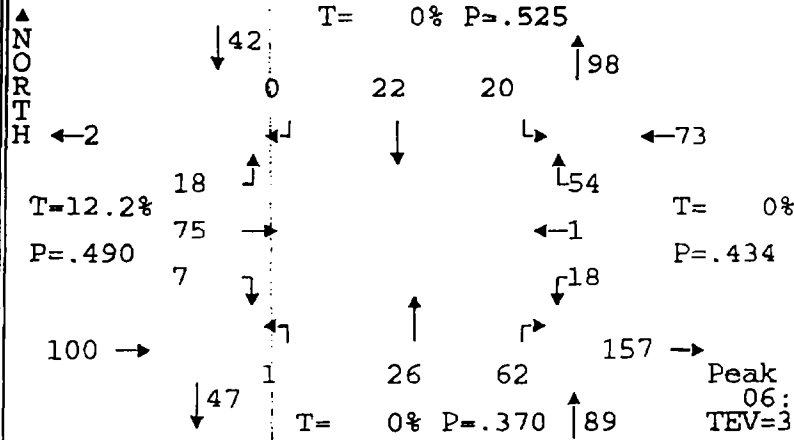
Hourly Totals	→	↑	←	↓	←	↑	↓	←	↑	↓	←	↑	ALL
14:00-15:00	51	1	10	14	89	10	25	79	20	0	0	0	299
14:15-15:15	55	3	12	14	97	11	25	77	23	0	0	0	317
14:30-15:30	60	4	11	13	108	11	23	84	25	0	0	0	339
14:45-15:45	62	7	12	12	114	16	23	89	23	0	0	0	348
15:00-16:00	40	8	7	8	118	14	19	78	26	1	0	0	319

INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
2ND STREET AT ERICKSON AVENUE



TIME PERIOD FROM - TO	EAST BOUND			SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL
	↓	→	↑	←	↓	↘	←	↑	↗	↓	←	↑	
16:00-16:05	5	0	0	2	11	2	1	10	1	0	0	0	32
16:05-16:10	4	0	0	1	10	1	0	7	1	0	0	0	25
16:10-16:15	3	0	0	1	17	0	1	4	0	0	0	0	26
16:15-16:20	3	0	1	0	14	0	3	7	1	0	0	1	30
16:20-16:25	2	0	1	0	10	0	0	12	2	0	0	0	27
16:25-16:30	2	0	1	0	12	2	0	8	1	0	0	0	26
16:30-16:35	3	0	1	0	7	3	1	2	2	0	0	0	19
16:35-16:40	4	0	0	0	9	0	2	6	0	0	0	0	22
16:40-16:45	4	0	0	1	9	0	2	7	1	0	0	0	24
16:45-16:50	5	0	0	0	14	0	0	5	2	0	0	0	22
16:50-16:55	3	1	0	0	9	0	2	3	3	0	0	0	21
16:55-17:00	3	0	1	0	10	2	1	5	3	0	0	0	25
17:00-17:05	3	0	0	1	10	4	1	11	2	0	0	0	32
17:05-17:10	3	1	0	0	13	4	1	7	13	0	0	0	39
17:10-17:15	5	1	1	2	13	2	1	7	2	0	0	0	34
17:15-17:20	3	0	0	0	13	1	2	7	2	0	0	0	28
17:20-17:25	3	1	0	0	17	1	2	6	3	1	0	0	32
17:25-17:30	3	1	0	1	15	1	0	5	1	0	0	0	24
17:30-17:35	5	0	0	0	13	0	2	4	3	0	0	0	27
17:35-17:40	5	0	0	1	16	0	0	4	4	0	0	0	26
17:40-17:45	5	1	0	0	13	3	1	9	1	0	0	0	33
17:45-17:50	2	0	0	2	19	2	0	2	4	0	0	0	31
17:50-17:55	4	0	1	0	10	1	2	5	2	0	0	0	25
17:55-18:00	3	0	1	0	14	1	2	8	0	1	0	0	30
<b>Total Survey</b>	<b>73</b>	<b>5</b>	<b>9</b>	<b>12</b>	<b>298</b>	<b>30</b>	<b>25</b>	<b>151</b>	<b>54</b>	<b>2</b>	<b>0</b>	<b>1</b>	<b>660</b>
<b>PHF</b>	<b>.8</b>	<b>.5</b>	<b>.38</b>	<b>.58</b>	<b>.86</b>	<b>.5</b>	<b>.75</b>	<b>.75</b>	<b>.54</b>	<b>.5</b>	<b>0</b>	<b>0</b>	<b>.859</b>
<b>% Trucks</b>	<b>6.8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>.7</b>	<b>0</b>	<b>4</b>	<b>1.3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1.5</b>
<b>Stopped Buses</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Peds</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Hourly Totals</b>													
16:00-17:00	38	1	6	5	132	10	13	76	17	0	0	1	299
16:15-17:15	34	3	6	4	130	17	14	80	32	0	0	1	321
16:30-17:30	34	4	3	5	139	18	13	71	34	1	0	0	322
16:45-17:45	33	5	2	5	156	18	11	73	39	1	0	0	343
17:00-18:00	35	4	3	7	166	20	12	75	37	2	0	0	361

INTERSEC. ON TURN MOVEMENT COUNT SUMMARY REPORT  
2ND STREET AT STOTT AVENUE



DATE OF COUNT: 10/11/01  
DAY OF WEEK: Thu  
TIME STARTED: 06:00  
TIME ENDED: 09:00

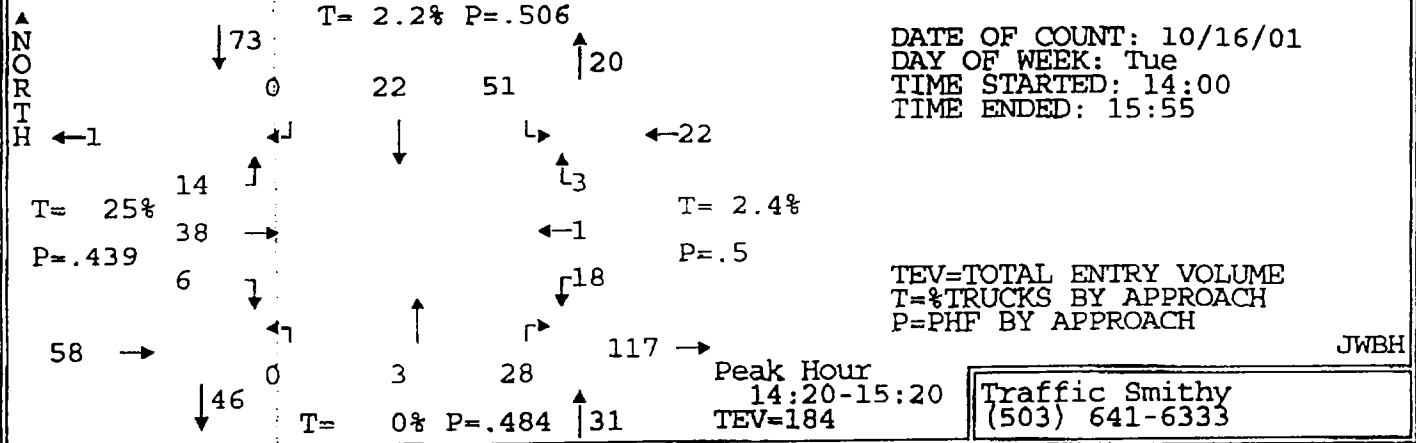
TEV=TOTAL ENTRY VOLUME  
T=%TRUCKS BY APPROACH  
P=PHF BY APPROACH

WCEK

Traffic Smithy  
(503) 641-6333

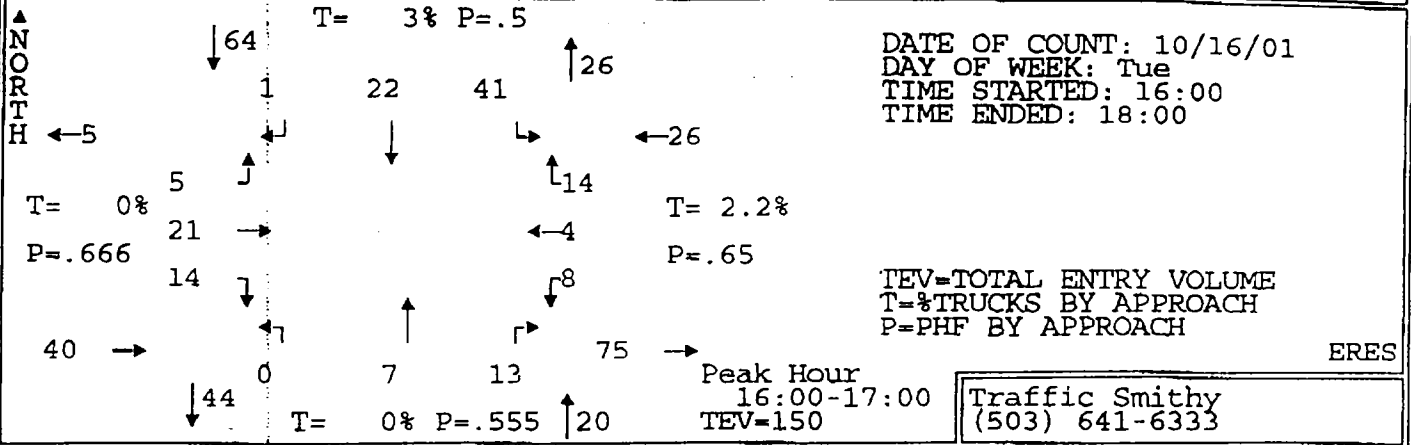
TIME PERIOD FROM - TO	EAST BOUND			SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL
	↓	→	↑	←	↓	↘	←	↑	↗	↓	←	↑	
06:00-06:05	0	0	0	0	0	0	0	0	0	0	0	0	0
06:05-06:10	0	0	0	0	0	0	0	0	0	0	0	0	0
06:10-06:15	0	0	1	0	0	1	0	0	0	0	0	0	2
06:15-06:20	0	0	0	0	0	1	0	0	0	0	0	0	1
06:20-06:25	0	0	0	0	0	1	0	0	0	0	0	0	1
06:25-06:30	1	0	1	0	0	0	0	0	0	0	0	0	2
06:30-06:35	1	1	1	0	0	0	0	0	0	1	0	1	5
06:35-06:40	0	4	1	0	0	1	0	0	2	2	1	1	9
06:40-06:45	1	11	1	1	1	3	0	0	2	4	1	2	25
06:45-06:50	1	8	1	1	1	3	0	0	2	4	1	1	21
06:50-06:55	1	2	0	0	0	1	1	0	0	1	0	0	5
06:55-07:00	1	2	0	0	0	4	0	0	0	1	0	0	8
07:00-07:05	1	1	0	0	0	0	0	0	1	1	0	0	5
07:05-07:10	0	1	2	0	0	2	0	0	0	1	0	3	11
07:10-07:15	0	6	1	0	0	2	1	0	0	3	0	0	13
07:15-07:20	0	4	2	0	0	1	0	0	2	2	1	1	13
07:20-07:25	0	5	3	0	0	0	1	0	1	3	0	0	25
07:25-07:30	0	5	3	0	0	3	0	0	1	4	3	0	25
07:30-07:35	0	13	0	0	0	3	4	0	8	12	2	1	52
07:35-07:40	1	12	3	0	0	6	4	4	16	16	4	1	63
07:40-07:45	3	16	3	0	0	3	6	1	13	13	4	9	56
07:45-07:50	1	9	1	0	0	3	3	3	5	5	2	3	27
07:50-07:55	0	1	0	0	0	0	0	0	3	0	0	2	6
07:55-08:00	0	4	0	0	0	1	1	0	0	0	1	1	7
08:00-08:05	0	0	0	0	0	0	0	0	0	0	0	0	1
08:05-08:10	0	0	0	0	0	1	1	0	0	0	0	0	1
08:10-08:15	0	2	2	0	0	1	1	0	0	0	0	1	6
08:15-08:20	0	0	0	0	0	0	0	0	1	1	0	0	2
08:20-08:25	0	0	0	0	0	1	0	0	0	0	0	0	1
08:25-08:30	0	0	0	0	0	1	0	0	0	1	1	1	4
08:30-08:35	0	0	1	0	0	0	0	0	0	0	0	0	1
08:35-08:40	0	1	1	0	0	0	0	0	0	0	0	0	2
08:40-08:45	0	0	0	0	0	1	0	0	0	0	0	0	1
08:45-08:50	0	0	0	0	0	0	0	0	0	0	0	0	0
08:50-08:55	0	1	0	0	0	0	0	0	0	1	0	0	2
08:55-09:00	0	0	0	0	0	0	0	0	1	1	0	0	2
<b>Total Survey</b>	<b>11</b>	<b>109</b>	<b>28</b>	<b>1</b>	<b>31</b>	<b>29</b>	<b>1</b>	<b>28</b>	<b>70</b>	<b>31</b>	<b>3</b>	<b>63</b>	<b>405</b>
<b>PHF</b>	<b>.35</b>	<b>.46</b>	<b>.64</b>	<b>0</b>	<b>.5</b>	<b>.42</b>	<b>.25</b>	<b>.36</b>	<b>.38</b>	<b>.5</b>	<b>.25</b>	<b>.41</b>	<b>.444</b>
<b>% Trucks</b>	<b>0</b>	<b>15.6</b>	<b>3.6</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>4.4</b>
<b>Stopped Buses</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Peds</b>	<b>0</b>	<b>11</b>	<b>0</b>	<b>0</b>	<b>34</b>	<b>0</b>	<b>0</b>	<b>43</b>	<b>0</b>	<b>0</b>	<b>20</b>	<b>0</b>	<b>0</b>
<b>Hourly Totals</b>													
06:00-07:00	5	28	6	1	11	4	0	2	5	11	1	5	79
06:15-07:15	6	36	9	1	15	8	0	3	10	11	1	8	106
06:30-07:30	5	50	15	1	21	8	0	7	18	17	2	21	165
06:45-07:45	7	75	18	1	26	18	1	23	56	21	1	5	297
07:00-08:00	6	77	18	0	19	21	1	26	62	17	1	5	303
07:15-08:15	5	71	16	0	15	20	1	25	57	18	1	5	282
07:30-08:30	5	57	9	0	8	19	1	21	51	12	1	4	226
07:45-08:45	1	17	5	0	2	8	0	3	10	3	1	9	59
08:00-09:00	0	4	4	0	1	4	0	0	3	3	1	3	23

INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
2ND STREET AT STOTT AVENUE



TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND		WEST BOUND		ALL		
	↓	→	↑	←	↓	←	↑	→	↓	←			
14:00-14:05	0	0	1	0	1	1	0	0	2	0	0	5	
14:05-14:10	0	0	0	0	0	0	0	1	0	0	0	3	
14:10-14:15	0	0	0	0	0	0	0	1	0	0	0	4	
14:15-14:20	0	0	0	0	0	0	0	0	2	0	0	2	
14:20-14:25	0	0	0	0	0	0	0	1	1	4	0	6	
14:25-14:30	0	0	0	0	3	8	1	1	7	1	0	21	
14:30-14:35	0	0	2	0	2	14	2	5	3	0	1	38	
14:35-14:40	1	13	4	0	4	5	0	1	0	0	1	29	
14:40-14:45	1	3	0	0	2	4	0	0	1	1	0	17	
14:45-14:50	1	3	2	0	2	3	0	0	3	1	0	13	
14:50-14:55	1	1	2	0	2	1	0	0	3	0	0	13	
14:55-15:00	1	2	2	0	0	4	0	2	0	0	0	11	
15:00-15:05	1	2	2	0	0	4	0	1	1	0	0	10	
15:05-15:10	1	3	0	0	1	3	0	4	0	0	0	13	
15:10-15:15	1	2	0	0	0	3	0	0	0	0	0	5	
15:15-15:20	1	2	2	0	2	1	0	0	2	0	0	8	
15:20-15:25	1	0	0	0	1	0	0	0	1	0	2	4	
15:25-15:30	2	1	0	0	1	1	0	0	0	0	0	6	
15:30-15:35	2	1	1	0	1	2	0	2	2	0	1	12	
15:35-15:40	1	3	0	0	0	2	0	0	3	0	1	10	
15:40-15:45	0	4	0	0	0	1	0	2	0	0	0	9	
15:45-15:50	1	2	0	0	1	1	0	0	0	0	1	9	
15:50-15:55	0	3	0	0	1	3	0	0	0	0	1	8	
<b>Total Survey</b>	10	54	16	0	30	62	0	5	38	31	1	9	256
<b>PHF</b>	.5	.38	.58	0	.55	.47	0	.25	.54	.56	.25	.25	.522
<b>% Trucks</b>	0	33.3	12.5	0	0	3.2	0	0	0	3.2	0	0	9
<b>Stopped Buses</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>Peds</b>	0	50	0	0	76	0	0	32	0	0	16	0	0
<b>Hourly Totals</b>													
14:00-15:00	4	31	11	0	22	41	0	4	28	17	1	3	162
14:15-15:15	5	38	12	0	20	50	0	3	30	16	1	3	178
14:30-15:30	8	39	14	0	21	43	0	2	21	14	1	4	167
14:45-15:45	7	24	9	0	14	25	0	1	18	11	0	5	114

INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
2ND STREET AT STOTT AVENUE



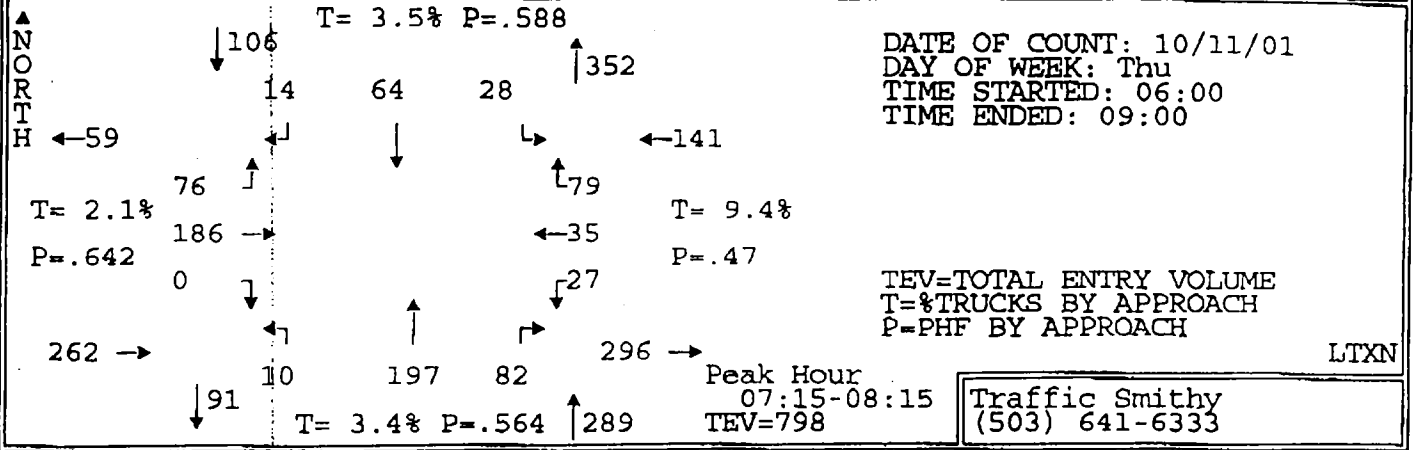
DATE OF COUNT: 10/16/01  
DAY OF WEEK: Tue  
TIME STARTED: 16:00  
TIME ENDED: 18:00

TEV=TOTAL ENTRY VOLUME  
T=%TRUCKS BY APPROACH  
P=PHF BY APPROACH

Traffic Smithy  
(503) 641-6333

TIME PERIOD FROM - TO	EAST BOUND			SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL
	↓	→	↑	←	↓	↘	↙	↑	↗	↖	↑		
16:00-16:05	1	2	1	0	2	2	0	0	2	0	0	3	13
16:05-16:10	1	5	1	0	3	2	0	0	1	0	0	2	15
16:10-16:15	0	3	0	0	3	13	0	1	0	0	0	1	22
16:15-16:20	1	3	0	0	3	7	0	1	3	0	0	4	22
16:20-16:25	1	1	1	0	2	4	0	3	1	0	0	3	16
16:25-16:30	0	1	0	0	1	0	0	0	1	1	2	0	6
16:30-16:35	0	0	0	0	1	1	0	0	2	1	0	0	4
16:35-16:40	4	2	1	1	1	0	0	0	0	1	0	0	10
16:40-16:45	3	0	0	0	3	2	0	0	0	0	0	0	8
16:45-16:50	1	3	1	0	0	4	0	0	3	0	1	1	12
16:50-16:55	1	0	0	0	1	2	0	0	1	1	1	1	8
16:55-17:00	1	1	0	0	2	4	0	2	1	2	1	0	14
17:00-17:05	0	3	0	0	2	0	0	0	1	2	0	0	8
17:05-17:10	1	6	2	0	1	1	0	0	2	2	0	1	16
17:10-17:15	2	3	1	0	1	2	0	1	0	0	0	0	10
17:15-17:20	0	1	1	0	1	2	0	0	1	0	1	2	9
17:20-17:25	1	5	0	0	2	2	1	0	2	1	0	2	16
17:25-17:30	0	3	0	0	3	2	0	0	3	2	0	0	13
17:30-17:35	0	2	0	0	0	1	0	0	1	1	0	1	6
17:35-17:40	2	7	0	0	1	2	0	0	0	1	0	0	13
17:40-17:45	1	5	0	0	3	1	0	1	2	1	0	1	15
17:45-17:50	1	1	0	0	1	2	0	0	3	0	0	0	8
17:50-17:55	3	3	2	0	2	1	0	1	1	1	0	0	14
17:55-18:00	0	2	2	0	3	1	0	0	1	1	0	0	10
<b>Total Survey</b>	<b>25</b>	<b>62</b>	<b>13</b>	<b>1</b>	<b>42</b>	<b>58</b>	<b>1</b>	<b>10</b>	<b>30</b>	<b>20</b>	<b>5</b>	<b>21</b>	<b>288</b>
PHF	.44	.48	.63	.25	.61	.43	0	.35	.65	.5	.5	.44	.625
% Trucks	0	0	0	0	0	5.2	0	0	0	0	0	4.8	1.4
Stopped Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Peds	0	2	0	0	3	0	0	6	0	0	7	0	0
<b>Hourly Totals</b>													
16:00-17:00	14	21	5	1	22	41	0	7	13	8	4	14	150
16:15-17:15	15	23	6	1	18	27	0	7	13	11	4	9	134
16:30-17:30	14	27	6	1	18	22	1	3	14	13	3	6	128
16:45-17:45	10	39	5	0	17	23	1	4	17	13	3	8	140
17:00-18:00	11	41	8	0	20	17	1	3	17	12	1	7	138

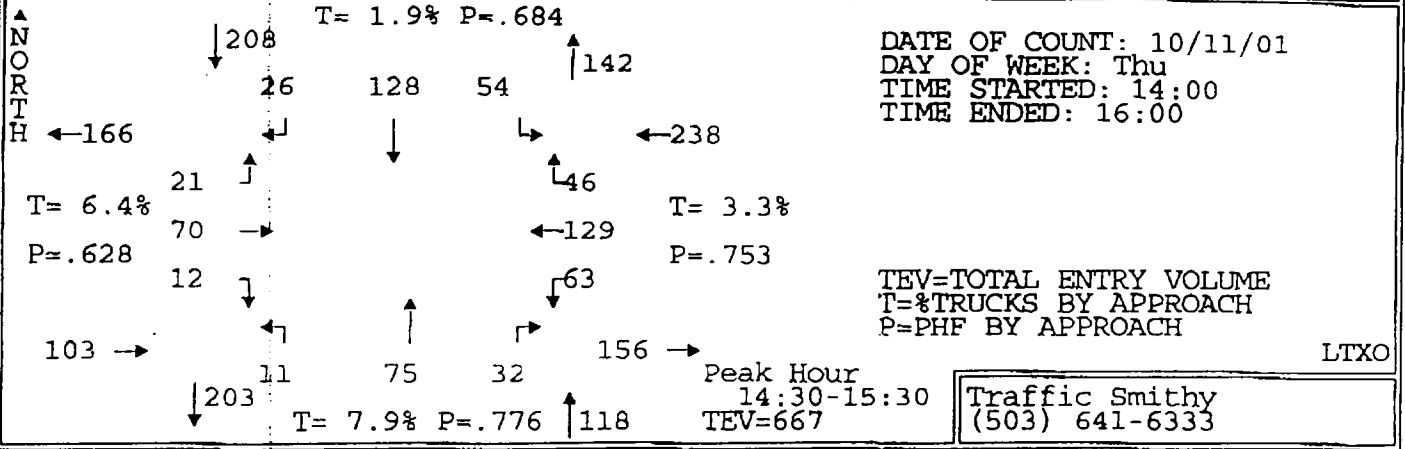
INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
6TH STREET AT ERICKSON AVENUE



TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND		ALL				
	→	↑	←	↓	←	↑	→	↓					
06:00-06:05	0	2	0	0	0	0	1	0	0	3			
06:05-06:10	0	3	0	0	0	0	1	0	0	7			
06:10-06:15	0	3	0	0	2	2	3	0	1	11			
06:15-06:20	0	4	0	0	2	0	6	1	1	15			
06:20-06:25	0	2	0	0	1	0	3	2	0	13			
06:25-06:30	2	1	2	0	1	1	4	1	0	13			
06:30-06:35	0	1	3	0	2	0	8	1	1	19			
06:35-06:40	0	3	0	0	2	0	4	2	2	16			
06:40-06:45	0	5	9	1	2	0	2	1	1	46			
06:45-06:50	1	4	3	0	5	0	9	3	3	37			
06:50-06:55	0	6	1	0	7	1	8	0	2	29			
06:55-07:00	0	10	3	0	2	0	4	0	1	22			
07:00-07:05	0	4	2	0	1	0	8	0	0	24			
07:05-07:10	0	1	2	0	1	1	6	1	3	19			
07:10-07:15	0	8	2	0	2	2	11	0	0	29			
07:15-07:20	0	4	6	0	3	2	12	1	1	34			
07:20-07:25	0	12	7	0	5	1	17	5	1	55			
07:25-07:30	0	9	9	3	8	0	20	5	2	66			
07:30-07:35	0	16	12	1	2	4	19	9	3	79			
07:35-07:40	0	18	17	6	10	5	34	11	5	126			
07:40-07:45	0	23	16	1	7	5	36	16	19	137			
07:45-07:50	0	22	4	3	6	2	15	11	4	83			
07:50-07:55	0	28	1	0	7	2	7	7	1	58			
07:55-08:00	0	19	3	0	3	4	10	10	2	53			
08:00-08:05	0	13	1	0	2	1	15	3	2	39			
08:05-08:10	0	7	0	0	7	1	4	2	1	27			
08:10-08:15	0	15	0	0	4	1	8	2	4	41			
08:15-08:20	1	10	1	0	3	0	5	0	1	23			
08:20-08:25	0	8	0	0	1	0	2	3	1	15			
08:25-08:30	0	10	0	0	2	0	6	1	1	24			
08:30-08:35	0	11	0	0	2	0	6	2	3	26			
08:35-08:40	0	6	2	0	0	0	4	3	0	17			
08:40-08:45	2	5	0	0	3	0	6	3	1	23			
08:45-08:50	0	4	0	1	4	0	2	2	1	18			
08:50-08:55	1	10	1	0	3	1	2	4	5	28			
08:55-09:00	0	8	0	0	3	0	1	1	1	27			
<b>Total Survey</b>	<b>7</b>	<b>315</b>	<b>107</b>	<b>16</b>	<b>115</b>	<b>40</b>	<b>19</b>	<b>337</b>	<b>112</b>	<b>41</b>	<b>68</b>	<b>125</b>	<b>1302</b>
<b>PHF</b>	<b>0</b>	<b>.64</b>	<b>.42</b>	<b>.35</b>	<b>.7</b>	<b>.5</b>	<b>.63</b>	<b>.55</b>	<b>.54</b>	<b>.42</b>	<b>.51</b>	<b>.45</b>	<b>.576</b>
<b>% Trucks</b>	<b>0</b>	<b>1.6</b>	<b>3.7</b>	<b>12.5</b>	<b>2.6</b>	<b>2.5</b>	<b>0</b>	<b>4.2</b>	<b>1.8</b>	<b>9.8</b>	<b>17.6</b>	<b>4.8</b>	<b>4.1</b>
<b>Stopped Buses</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Peds</b>	<b>0</b>	<b>7</b>	<b>0</b>	<b>0</b>	<b>11</b>	<b>0</b>	<b>0</b>	<b>40</b>	<b>0</b>	<b>0</b>	<b>13</b>	<b>0</b>	<b>0</b>
<b>Hourly Totals</b>													
06:00-07:00	3	44	21	1	26	5	3	72	10	8	14	24	231
06:15-07:15	3	49	27	1	28	7	5	92	11	9	17	33	282
06:30-07:30	1	67	47	4	40	10	5	128	18	14	17	45	396
06:45-07:45	1	115	80	11	53	22	7	184	51	26	27	80	657
07:00-08:00	0	164	81	14	55	29	10	195	76	26	30	83	763
07:15-08:15	0	186	76	14	64	28	10	197	82	27	35	79	798
07:30-08:30	1	189	55	11	54	27	8	161	75	23	34	67	705
07:45-08:45	3	154	12	3	40	13	7	88	47	10	25	27	429
08:00-09:00	4	107	5	1	34	6	6	70	26	7	24	18	308

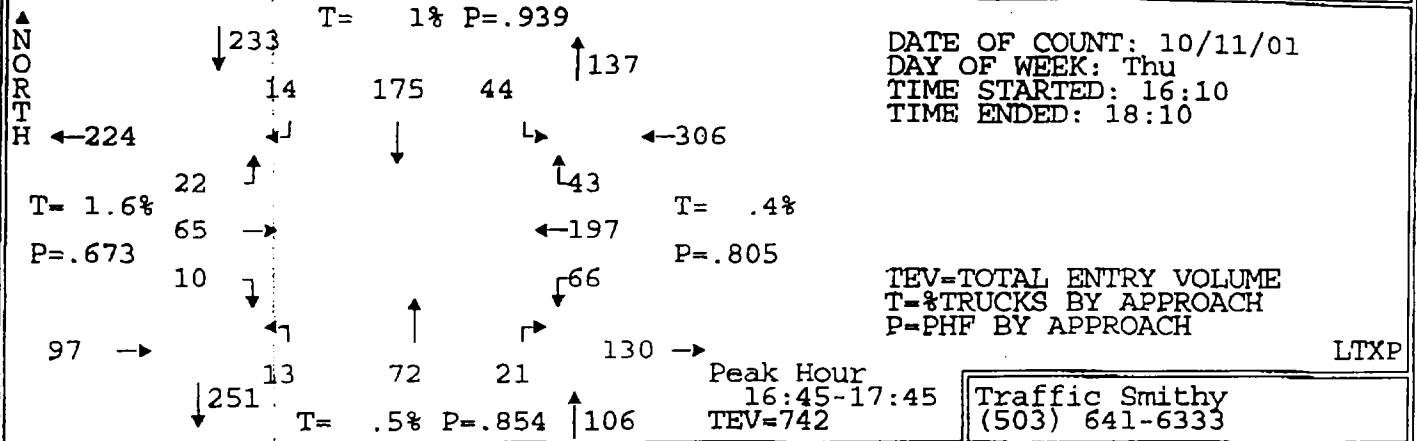


INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
6TH STREET AT ERICKSON AVENUE



TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND		NORTH BOUND		WEST BOUND		ALL				
	↓	→	↑	←	↓	←	↑	→					
14:00-14:05	0	0	0	0	3	1	1	7	0	0	3	2	17
14:05-14:10	0	0	1	1	6	1	0	6	3	1	4	2	30
14:10-14:15	2	2	2	0	4	3	0	3	2	8	2	1	29
14:15-14:20	0	6	3	0	5	0	1	10	1	1	8	3	38
14:20-14:25	0	7	2	1	7	8	0	4	5	3	9	4	50
14:25-14:30	1	6	1	0	8	4	1	13	0	3	2	6	51
14:30-14:35	2	6	5	0	8	5	0	5	3	3	7	2	46
14:35-14:40	0	5	4	11	18	12	4	4	3	11	11	7	90
14:40-14:45	1	4	0	7	6	4	0	7	2	8	14	2	55
14:45-14:50	0	5	0	2	11	5	0	7	2	7	13	6	58
14:50-14:55	2	13	0	1	11	4	0	7	3	5	16	6	68
14:55-15:00	1	8	4	2	9	0	0	5	3	5	13	2	52
15:00-15:05	2	9	2	0	7	5	1	4	2	5	11	1	49
15:05-15:10	2	3	1	0	8	1	1	5	4	1	12	5	43
15:10-15:15	1	3	0	0	15	5	2	2	4	3	7	3	45
15:15-15:20	1	8	1	0	10	4	2	10	2	4	6	2	50
15:20-15:25	0	3	3	1	10	5	1	11	3	4	12	5	58
15:25-15:30	0	3	1	1	15	4	0	8	1	7	7	5	53
15:30-15:35	0	7	0	3	12	1	1	7	5	4	5	1	46
15:35-15:40	0	4	0	2	16	0	1	9	0	2	9	4	47
15:40-15:45	0	8	1	1	11	1	2	8	3	3	8	5	51
15:45-15:50	1	6	0	4	8	2	5	3	1	1	10	2	43
15:50-15:55	0	8	5	0	19	0	1	9	1	6	17	2	68
15:55-16:00	0	5	2	1	14	1	0	7	3	5	8	1	47
<b>Total Survey</b>	<b>16</b>	<b>134</b>	<b>38</b>	<b>45</b>	<b>241</b>	<b>76</b>	<b>24</b>	<b>161</b>	<b>56</b>	<b>100</b>	<b>214</b>	<b>79</b>	<b>1184</b>
<b>PHF</b>	<b>.6</b>	<b>.58</b>	<b>.58</b>	<b>.32</b>	<b>.91</b>	<b>.64</b>	<b>.55</b>	<b>.65</b>	<b>.8</b>	<b>.61</b>	<b>.75</b>	<b>.77</b>	<b>.821</b>
<b>% Trucks</b>	<b>0</b>	<b>7.5</b>	<b>5.3</b>	<b>0</b>	<b>1.7</b>	<b>3.9</b>	<b>0</b>	<b>11.2</b>	<b>1.8</b>	<b>1</b>	<b>2.8</b>	<b>7.6</b>	<b>4.3</b>
<b>Stopped Buses</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Peds</b>	<b>0</b>	<b>9</b>	<b>0</b>	<b>0</b>	<b>22</b>	<b>0</b>	<b>0</b>	<b>84</b>	<b>0</b>	<b>0</b>	<b>16</b>	<b>0</b>	<b>0</b>
<b>Hourly Totals</b>													
14:00-15:00	9	67	22	31	96	47	7	78	27	55	102	43	584
14:15-15:15	12	75	22	30	113	53	10	73	32	55	123	47	645
14:30-15:30	12	70	21	26	128	54	11	75	32	63	129	46	667
14:45-15:45	9	74	13	14	135	35	11	83	32	50	119	45	620
15:00-16:00	7	67	16	14	145	29	17	83	29	45	112	36	600

INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT  
6TH STREET AT ERICKSON AVENUE



TIME PERIOD FROM - TO	EAST BOUND			SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL
	↓	→	↑	←	↓	↘	←	↑	↗	↓	←	↑	
16:10-16:15	0	5	1	1	14	1	0	7	1	2	15	4	51
16:15-16:20	0	6	0	0	14	1	0	9	0	1	12	2	45
16:20-16:25	1	6	1	1	7	2	0	4	4	5	7	1	39
16:25-16:30	2	8	0	2	7	3	0	4	1	3	7	2	39
16:30-16:35	0	6	0	0	11	1	1	13	1	5	16	4	58
16:35-16:40	1	6	0	1	9	1	0	7	1	4	13	0	43
16:40-16:45	1	9	1	2	12	3	2	3	2	2	13	2	52
16:45-16:50	1	7	2	2	13	6	1	4	1	4	11	6	58
16:50-16:55	0	8	2	2	10	4	2	7	2	2	6	5	50
16:55-17:00	0	4	2	1	12	3	0	10	1	6	16	4	59
17:00-17:05	2	9	6	0	16	5	2	5	1	7	15	6	74
17:05-17:10	0	8	4	3	14	7	0	7	5	4	24	1	77
17:10-17:15	0	6	1	2	15	0	0	5	1	9	26	3	68
17:15-17:20	0	3	2	0	17	0	0	3	1	4	21	1	52
17:20-17:25	1	2	0	2	16	8	2	8	3	4	15	2	63
17:25-17:30	3	6	2	1	10	5	1	6	2	9	17	2	64
17:30-17:35	1	4	1	1	15	3	0	5	2	4	16	4	56
17:35-17:40	1	4	0	0	22	3	3	4	2	7	15	4	65
17:40-17:45	1	4	0	0	15	0	2	8	0	6	15	5	56
17:45-17:50	0	6	1	3	6	3	3	6	2	4	11	2	47
17:50-17:55	1	4	2	0	12	5	1	8	2	3	11	3	52
17:55-18:00	1	5	2	2	21	2	1	4	1	6	11	1	55
18:00-18:05	1	5	2	2	9	3	0	6	2	11	10	4	55
18:05-18:10	1	3	1	3	11	0	0	4	5	4	11	0	43
Total Survey	19	134	33	29	308	69	21	147	43	116	334	68	1321
PHF	.5	.71	.46	.7	.84	.69	.65	.82	.75	.82	.69	.72	.847
% Trucks	0	2.2	0	0	1	1.4	0	.7	0	0	.3	1.5	.8
Stopped Buses	0	0	0	0	0	0	0	0	0	0	0	0	0
Peds	0	6	0	0	10	0	0	32	0	0	9	0	0
Hourly Totals													
16:10-17:10	8	82	19	15	139	37	8	80	20	45	155	37	645
16:25-17:25	8	76	20	17	152	41	10	76	20	54	183	36	693
16:40-17:40	10	70	23	16	172	47	13	67	23	62	195	40	738
16:55-17:55	10	60	21	13	170	42	14	75	22	67	202	37	733
17:10-18:10	11	52	14	14	169	32	13	67	23	71	179	31	676

ROADWAY TRAFFIC SURVEY

Roadway: SW 2ND AVENUE  
 Location: EAST OF ERICKSON  
 Direction: EAST BOUND  
 Date: 10/3/1  
 Day of Week: WEDNESDAY  
 Axles per Vehicles: 2

Traffic Smithy  
 Traffic Survey Service

Hour of Day	:00- :05	:05- :10	:10- :15	:15- :20	:20- :25	:25- :30	:30- :35	:35- :40	:40- :45	:45- :50	:50- :55	:55- :00	Hour Tot.
00-01	0	0	0	0	0	0	0	0	0	0	0	0	0
01-02	0	0	0	0	0	0	0	0	0	0	0	0	0
02-03	0	0	0	0	0	0	0	0	0	0	0	0	0
03-04	0	0	0	3	0	0	0	0	0	0	0	0	3
04-05	0	0	0	0	0	0	0	0	2	0	0	0	2
05-06	1	0	0	0	2	1	2	0	2	3	3	2	16
06-07	4	0	0	0	0	1	5	19	24	12	13	11	89
07-08	13	11	11	6	25	37	27	40	33	14	2	0	219
08-09	0	2	0	0	4	1	1	4	0	0	0	0	12
09-10	0	0	0	0	0	0	0	0	0	0	0	0	0
10-11	0	0	0	0	0	0	0	0	0	0	1	0	1
11-12	0	0	0	0	0	0	0	0	0	0	0	0	0
12-13	0	0	0	0	0	3	0	0	0	0	0	0	3
13-14	2	0	0	0	0	2	2	0	3	0	2	4	15
14-15	5	12	8	9	22	14	1	5	11	6	16	2	111
15-16	7	8	12	10	7	5	8	6	10	6	24	9	112
16-17	13	8	7	5	10	7	5	3	13	11	4	3	89
17-18	15	8	8	10	6	6	7	4	7	2	2	4	79
18-19	8	7	9	8	4	3	4	0	7	10	24	24	108
19-20	13	10	2	0	0	0	0	0	0	0	8	1	34
20-21	0	0	1	1	4	5	0	0	0	0	0	0	11
21-22	1	0	1	0	1	1	0	0	0	0	0	0	4
22-23	0	0	0	0	0	0	2	0	1	0	0	0	3
23-24	2	0	0	2	0	0	1	0	0	0	0	0	5

Daily Total: 916  
 AM Peak Hour (06:50-07:50) 241                      26.31% of Daily Total  
 PM Peak Hour (15:10-16:10) 118                      12.88% of Daily Total  
 4th Highest Hour (18:00-19:00 ) 108                      11.79% of Daily Total  
 8th Highest Hour (19:00-20:00 ) 34                      3.71 % of Daily Total

## ROADWAY TRAFFIC SURVEY

Roadway: SW 2ND AVENUE  
 Location: EAST OF ERICKSON  
 Direction: EAST BOUND  
 Date: 10/4/1  
 Day of Week: THURSDAY  
 Axles per Vehicles: 2

Traffic Smithy  
 Traffic Survey Service

Hour of Day	:00- :05	:05- :10	:10- :15	:15- :20	:20- :25	:25- :30	:30- :35	:35- :40	:40- :45	:45- :50	:50- :55	:55- :00	Hour Tot.
00-01	0	0	0	0	0	0	0	0	0	0	0	0	0
01-02	1	0	0	0	0	0	0	0	0	0	0	0	1
02-03	0	0	0	0	0	0	0	0	0	0	0	0	0
03-04	0	0	0	2	0	0	0	0	0	0	0	0	2
04-05	0	0	0	0	0	0	0	0	0	2	0	0	2
05-06	0	0	0	0	2	1	0	0	0	2	0	0	5
06-07	1	0	0	2	3	5	3	14	16	13	19	6	82
07-08	6	6	19	14	19	40	43	49	48	11	7	0	262
08-09	3	1	0	1	3	0	1	4	2	0	0	0	15
09-10	0	0	0	0	0	0	0	0	1	0	0	0	1
10-11	0	0	0	0	0	0	0	0	0	0	0	1	1
11-12	1	0	0	0	0	2	0	0	0	0	0	0	3
12-13	0	0	0	0	0	1	0	0	0	0	0	0	1
13-14	0	1	0	0	0	0	1	0	0	0	0	0	2
14-15	0	0	19	5	18	25	2	5	17	5	6	8	110
15-16	14	7	14	3	9	3	17	3	7	9	4	2	92
16-17	16	13	4	14	6	5	9	7	7	18	18	27	144
17-18	19	10	12	8	13	16	13	12	10	13	18	13	157
18-19	15	21	46	36	45	28	44	46	28	23	18	24	374
19-20	13	8	13	9	5	5	5	7	0	5	0	1	71
20-21	4	11	4	5	4	18	3	3	3	10	12	4	81
21-22	7	14	5	5	2	2	2	0	0	0	0	0	37
22-23	0	2	2	0	0	1	0	0	0	2	2	2	11
23-24	0	0	1	0	0	0	1	0	0	0	0	0	2

Daily Total:	1456	
AM Peak Hour (06:45-07:45)	282	19.37% of Daily Total
PM Peak Hour (18:00-19:00)	374	25.69% of Daily Total
4th Highest Hour (16:00-17:00)	144	9.89 % of Daily Total
8th Highest Hour (20:00-21:00)	81	5.56 % of Daily Total

## ROADWAY TRAFFIC SURVEY

Roadway: SW 2ND AVENUE  
 Location: EAST OF ERICKSON  
 Direction: EAST BOUND  
 Date: 10/5/1  
 Day of Week: FRIDAY  
 Axles per Vehicles: 2

Traffic Smithy  
Traffic Survey Service

Hour of Day	:00-:05	:05-:10	:10-:15	:15-:20	:20-:25	:25-:30	:30-:35	:35-:40	:40-:45	:45-:50	:50-:55	:55-:00	Hour Tot.
00-01	0	0	0	0	0	0	0	0	1	0	0	0	1
01-02	0	0	0	0	0	0	0	0	0	0	0	0	0
02-03	0	0	0	1	0	0	0	0	0	0	0	0	1
03-04	0	0	0	2	0	0	0	0	0	0	0	0	2
04-05	0	0	0	0	0	0	0	0	0	1	1	0	2
05-06	0	0	0	0	0	0	0	0	3	2	0	2	7
06-07	3	0	0	0	0	2	3	9	25	16	3	9	70
07-08	12	21	10	13	21	28	27	36	36	26	4	2	236
08-09	5	4	0	4	2	0	0	5	6	3	0	2	31
09-10	0	0	0	8	4	1	0	0	0	1	1	0	15
10-11	0	0	0	0	0	0	0	0	0	0	0	0	0
11-12	3	0	0	0	0	0	0	0	0	0	0	0	3
12-13	0	0	0	0	0	0	0	0	0	0	0	0	0
13-14	0	0	0	0	0	3	0	0	0	4	2	6	15
14-15	2	4	8	18	11	13	8	4	10	13	16	14	121
15-16	5	18	4	3	6	5	4	5	7	10	12	8	87
16-17	14	7	7	8	5	8	12	6	9	9	14	10	109
17-18	11	13	9	21	15	13	8	6	4	10	6	10	126
18-19	5	4	6	6	16	16	20	7	6	4	4	6	100
19-20	2	0	0	2	0	0	3	0	0	3	0	0	10
20-21	0	0	0	3	0	0	0	0	0	1	0	1	5
21-22	0	3	4	0	1	2	0	2	2	3	0	0	17
22-23	0	0	0	0	3	3	0	1	2	7	2	3	21
23-24	3	3	0	0	0	2	4	0	1	1	3	0	17

Daily Total: 996	
AM Peak Hour (06:55-07:55) 243	24.4 % of Daily Total
PM Peak Hour (16:30-17:30) 142	14.26% of Daily Total
4th Highest Hour (16:00-17:00) 109	10.94% of Daily Total
8th Highest Hour (08:00-09:00) 31	3.11 % of Daily Total

## ROADWAY TRAFFIC SURVEY

Roadway: SW 2ND AVENUE  
 Location: EAST OF ERICKSON  
 Direction: EAST BOUND  
 Date: 10/6/1  
 Day of Week: SATURDAY  
 Axles per Vehicles: 2

Traffic Smithy  
Traffic Survey Service

Hour of Day	:00-:05	:05-:10	:10-:15	:15-:20	:20-:25	:25-:30	:30-:35	:35-:40	:40-:45	:45-:50	:50-:55	:55-:00	Hour Tot.
00-01	1	0	0	0	2	0	0	1	2	0	1	0	7
01-02	0	0	0	0	0	0	2	0	0	0	0	0	2
02-03	0	0	0	0	0	0	0	0	0	0	0	0	0
03-04	0	0	0	0	0	0	0	0	0	0	0	0	0
04-05	0	0	0	2	0	0	0	0	0	0	0	0	2
05-06	0	0	0	1	2	0	1	0	0	3	0	0	7
06-07	0	0	0	5	2	4	5	5	4	6	3	2	36
07-08	0	0	4	2	0	3	0	0	2	3	2	0	16
08-09	0	1	3	0	0	3	3	5	2	3	3	2	25
09-10	1	9	5	5	0	2	2	7	2	4	2	2	41
10-11	3	0	4	3	4	0	1	2	6	3	0	2	28
11-12	2	4	1	2	2	2	7	1	5	3	6	3	38
12-13	0	4	1	2	0	3	4	2	6	2	1	5	30
13-14	3	0	2	2	1	2	2	2	2	2	3	4	25
14-15	3	0	2	2	1	2	2	2	2	2	3	4	25
15-16	2	3	4	0	1	8	1	0	3	0	0	2	24
16-17	2	3	0	1	4	0	1	0	6	5	0	0	22
17-18	2	2	3	2	0	0	5	1	2	4	1	3	25
18-19	2	2	3	2	0	0	0	5	1	2	4	1	33
19-20	1	2	1	5	2	7	6	4	3	2	0	0	33
20-21	6	3	0	0	5	2	3	3	1	1	1	9	34
21-22	6	3	0	0	5	2	3	3	1	1	1	9	34
22-23	0	2	3	0	0	0	0	0	1	2	0	0	8
23-24	0	2	3	0	0	0	0	0	0	1	0	0	3
24-01	0	0	0	1	1	0	0	0	0	1	0	0	3
01-02	0	0	0	1	1	0	0	0	0	1	0	0	3
02-03	0	2	2	0	0	0	2	0	0	3	4	0	13
03-04	0	2	2	0	0	0	2	0	0	3	4	0	13
04-05	0	0	0	0	0	1	0	6	12	0	2	2	23
05-06	0	0	0	2	0	0	2	0	1	0	1	0	10
06-07	3	1	0	2	0	0	2	0	1	0	1	0	10

Daily Total:	452	
AM Peak Hour (09:05-01:05)	43	9.51 % of Daily Total
PM Peak Hour (17:10-18:10)	39	8.63 % of Daily Total
4th Highest Hour (18:00-19:00)	34	7.52 % of Daily Total
8th Highest Hour (08:00-09:00)	25	5.53 % of Daily Total

ROADWAY TRAFFIC SURVEY

Roadway: SW 2ND AVENUE  
 Location: EAST OF ERICKSON  
 Direction: EAST BOUND  
 Date: 10/7/1  
 Day of Week: SUNDAY  
 Axles per Vehicles: 2

Traffic Smithy  
 Traffic Survey Service

Hour of Day	:00- :05	:05- :10	:10- :15	:15- :20	:20- :25	:25- :30	:30- :35	:35- :40	:40- :45	:45- :50	:50- :55	:55- :00	Hour Tot.
00-01	0	0	0	0	0	0	0	0	0	0	0	0	0
01-02	0	0	0	0	0	0	0	0	0	0	0	0	0
02-03	0	0	0	0	0	0	0	0	0	0	0	0	0
03-04	0	0	0	0	0	0	0	0	0	0	0	3	3
04-05	0	2	0	0	0	0	0	0	0	0	0	0	2
05-06	0	1	0	0	0	0	0	0	0	0	0	0	1
06-07	0	0	0	0	0	0	0	4	0	0	0	4	8
07-08	7	1	0	9	0	1	6	10	2	2	4	5	47
08-09	3	4	4	0	0	2	0	2	4	0	2	5	26
09-10	6	1	4	0	0	3	2	3	2	2	4	1	28
10-11	0	5	1	4	4	2	1	2	3	0	2	2	26
11-12	0	10	8	4	0	1	6	6	19	19	8	8	89
12-13	5	0	3	4	3	0	1	9	4	12	2	8	51
13-14	12	20	6	10	6	12	4	2	2	3	8	18	103
14-15	4	3	6	1	2	0	0	3	5	10	2	4	40
15-16	4	1	7	2	4	4	1	7	19	11	8	10	78
16-17	5	3	0	5	8	0	5	5	2	1	5	5	44
17-18	3	0	0	0	3	0	4	4	3	0	3	0	20
18-19	0	2	2	0	0	0	0	0	0	0	2	0	6
19-20	2	0	0	2	0	0	0	4	2	1	0	1	12
20-21	0	0	0	0	0	2	0	2	3	1	0	0	8
21-22	0	0	0	1	2	0	0	0	0	0	0	0	3
22-23	0	0	0	1	0	0	0	0	0	0	0	0	1
23-24	0	0	0	0	0	0	2	2	0	0	0	0	4

Daily Total: 600

AM Peak Hour (11:00-12:00) 89

14.83% of Daily Total

PM Peak Hour (12:35-13:35) 105

17.5 % of Daily Total

4th Highest Hour (12:00-13:00) 51

8.5 % of Daily Total

8th Highest Hour (09:00-01:00) 28

4.67 % of Daily Total

## **Level of Service Description**

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## TRAFFIC LEVELS OF SERVICE

Analysis of traffic volumes is useful in understanding the general nature of traffic in an area, but by itself indicates neither the ability of the street network to carry additional traffic nor the quality of service afforded by the street facilities. For this, the concept of *level of service* has been developed to subjectively describe traffic performance. Level of service can be measured at intersections and along key roadway segments.

Level of service categories are similar to report card ratings for traffic performance. Intersections are typically the controlling bottlenecks of traffic flow and the ability of a roadway system to carry traffic efficiently is generally diminished in their vicinities. Levels of Service A, B and C indicate conditions where traffic moves without significant delays over periods of peak travel demand. Level of service D and E are progressively worse peak hour operating conditions and F conditions represent where demand exceeds the capacity of an intersection. Most urban communities set level of service D as the minimum acceptable level of service for peak hour operation and plan for level of service C or better for all other times of the day. The *Highway Capacity Manual* provides level of service calculation methodology for both intersections and arterials.<sup>1</sup> The following three sections provide interpretations of the analysis approaches.

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<sup>1</sup> 2000 *Highway Capacity Manual*, Transportation Research Board, Washington D.C., 2000, Chapters 16 and 17.

## UNSIGNALIZED INTERSECTIONS (Two-Way Stop Controlled)

Unsignalized intersection level of service is reported for the major street and minor street (generally, left turn movements). The method assesses available and critical gaps in the traffic stream which make it possible for side street traffic to enter the main street flow. The *2000 Highway Capacity Manual* describes the detailed methodology. It is not unusual for an intersection to experience level of service E or F conditions for the minor street left turn movement. It should be understood that, often, a poor level of service is experienced by only a few vehicles and the intersection as a whole operates acceptably.

Unsignalized intersection levels of service are described in the following table.

Level of Service	Expected Delay	(Sec/Veh)
A	Little or no delay	0-10.0
B	Short traffic delay	>10.1-15.0
C	Average traffic delays	>15.1-25.0
D	Long traffic delays	>25.1-35.0
E	Very long traffic delays	>35.1-50.0
F	Extreme delays potentially affecting other traffic movements in the intersection	> 50

Source: *2000 Highway Capacity Manual*, Transportation Research Board Washington, D.C.

## ALL-WAY STOP CONTROLLED INTERSECTIONS

Unsignalized intersections and all-way stop controlled intersections are each subject to a separate capacity analysis methodology. All-way stop controlled intersection operations are reported by leg of the intersection.

This method calculates a delay value for each approach to the intersection. The *2000 Highway Capacity Manual* describes the detailed methodology. The following table describes the amount of delay associated with each level of service.

Delay (Seconds)	Level of Service
0 - 10	A
10 - 15	B
15 - 25	C
25 - 35	D
35 - 50	E
> 50	F

Source: *2000 Highway Capacity Manual*, Transportation Research Board, Washington, D.C.

# Level of Service Calculations

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Existing AM Peak  
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Scenario Report

Scenario: Existing AM  
 Command: Existing AM  
 Volume: Existing AM  
 Geometry: Existing  
 Impact Fee: Default Impact Fee  
 Trip Generation: AM  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

-----  
Existing AM Peak  
-----

Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	Del/ LOS	V/ Veh C	Del/ LOS	V/ Veh C	
# 1 Farmington/Erickson	F 145.5	0.000	F 145.5	0.000	+ 0.000 V/C
# 2 Farmington/Stott	C 16.7	0.000	C 16.7	0.000	+ 0.000 V/C
# 3 Erickson/2nd	B 10.1	0.000	B 10.1	0.000	+ 0.000 V/C
# 4 Stott/2nd	B 10.0	0.360	B 10.0	0.360	+ 0.000 V/C
# 5 Erickson/6th	D 26.7	0.870	D 26.7	0.870	+ 0.000 V/C

Existing AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 145.5 Worst Case Level Of Service: F

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled		
Rights:	Include			Include			Include			Include		
Lanes:	1	0	0	0	0	0	0	0	1	1	0	0

Volume Module:

Base Vol:	40	0	97	0	0	0	0	1515	37	56	605	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	40	0	97	0	0	0	0	1515	37	56	605	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
PHF Volume:	44	0	108	0	0	0	0	1683	41	62	672	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	44	0	108	0	0	0	0	1683	41	62	672	0

Critical Gap Module:

Critical Gp:	6.9	xxxx	7.0	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	4.2	xxxx	xxxxxx
FollowUpTim:	3.5	xxxx	3.3	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	2.2	xxxx	xxxxxx

Capacity Module:

Cnflct Vol:	2164	xxxx	862	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	1724	xxxx	xxxxxx
Potent Cap.:	39	xxxx	294	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	354	xxxx	xxxxxx
Move Cap.:	34	xxxx	294	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	354	xxxx	xxxxxx

Level Of Service Module:

Stopped Del:	439.9	xxxx	24.1	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	17.3	xxxx	xxxxxx			
LOS by Move:	F	*	C	*	*	*	*	*	*	C	*	*			
Movement:	LT	-	LTR	-	RT	LT	-	LTR	-	RT	LT	-	LTR	-	RT
Shared Cap.:	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx
Shrd StpDel:	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	145.5		xxxxxx			xxxxxx			xxxxxx			xxxxxx			xxxxxx
ApproachLOS:	F		*			*			*			*			*

Existing AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 16.7 Worst Case Level Of Service: C

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled		
Rights:	Include			Include			Include			Include		
Lanes:	0	0	0	0	0	0	0	0	1	0	0	0

Volume Module:

Base Vol:	0	0	48	0	0	0	0	1533	46	0	743	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	0	0	48	0	0	0	0	1533	46	0	743	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
PHF Volume:	0	0	52	0	0	0	0	1648	49	0	799	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	0	0	52	0	0	0	0	1648	49	0	799	0

Critical Gap Module:

Critical Gp:	xxxxxx	xxxx	6.2	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx
FollowUpTim:	xxxxxx	xxxx	3.3	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx

Capacity Module:

Cnflct Vol:	xxxx	xxxx	849	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx
Potent Cap.:	xxxx	xxxx	358	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx
Move Cap.:	xxxx	xxxx	358	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx

Level Of Service Module:

Stopped Del:	xxxxxx	xxxx	16.7	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx			
LOS by Move:	*	*	C	*	*	*	*	*	*	*	*	*			
Movement:	LT	-	LTR	-	RT	LT	-	LTR	-	RT	LT	-	LTR	-	RT
Shared Cap.:	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx	xxxx	xxxx	xxxxxx
Shrd StpDel:	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	16.7		xxxxxx			xxxxxx			xxxxxx			xxxxxx			xxxxxx
ApproachLOS:	C		*			*			*			*			*

Existing AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 10.1 Worst Case Level Of Service: B

Approach:	North Bound	South Bound	East Bound	West Bound
Movement:	L - T - R	L - T - R	L - T - R	L - T - R
Control:	Uncontrolled	Uncontrolled	Stop Sign	Stop Sign
Rights:	Include	Include	Include	Include
Lanes:	0 0 1 0 0	0 0 1 0 0	0 0 1 0 0	0 0 0 0 0

Volume Module:

	North Bound			South Bound			East Bound			West Bound		
Base Vol:	54	109	55	23	46	10	5	7	37	0	0	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	54	109	55	23	46	10	5	7	37	0	0	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70
PHF Volume:	77	156	79	33	66	14	7	10	53	0	0	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	77	156	79	33	66	14	7	10	53	0	0	0

Critical Gap Module:

Critical Gp:	North Bound			South Bound			East Bound			West Bound		
FollowUpTim:	4.2	xxxx	xxxxx	4.1	xxxx	xxxxx	6.4	6.5	6.2	xxxxx	xxxx	xxxxx
	2.3	xxxx	xxxxx	2.2	xxxx	xxxxx	3.5	4.0	3.3	xxxxx	xxxx	xxxxx

Capacity Module:

	North Bound			South Bound			East Bound			West Bound		
Cnflct Vol:	80	xxxx	xxxxx	234	xxxx	xxxxx	488	527	73	xxxx	xxxx	xxxxx
Potent Cap.:	1469	xxxx	xxxxx	1327	xxxx	xxxxx	543	459	995	xxxx	xxxx	xxxxx
Move Cap.:	1469	xxxx	xxxxx	1327	xxxx	xxxxx	510	423	995	xxxx	xxxx	xxxxx

Level Of Service Module:

Stopped Del:	North Bound			South Bound			East Bound			West Bound		
LOS by Move:	A	*	*	A	*	*	A	*	*	A	*	*
Movement:	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT
Shared Cap.:	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	xxxx	771	xxxxx	xxxx	xxxx	xxxxx
Shrd StpDel:	xxxxx	xxxx	xxxxx	xxxxx	xxxx	xxxxx	xxxxx	10.1	xxxxx	xxxxx	xxxx	xxxxx
Shared LOS:	*	*	*	*	*	*	*	B	*	*	*	*
ApproachDel:	xxxxx	xxxxxx					10.1		xxxxxx			
ApproachLOS:	*	*					B		*			

Existing AM Peak

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.360  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 10.0  
Optimal Cycle: 0 Level Of Service: B

Approach:	North Bound	South Bound	East Bound	West Bound
Movement:	L - T - R	L - T - R	L - T - R	L - T - R
Control:	Stop Sign	Stop Sign	Stop Sign	Stop Sign
Rights:	Include	Include	Include	Include
Min. Green:	0 0 0	0 0 0	0 0 0	0 0 0
Lanes:	0 0 0 1 0	0 1 0 0 0	0 0 1 0 0	0 0 1 0 0

Volume Module:

	North Bound			South Bound			East Bound			West Bound		
Base Vol:	0	26	62	51	22	0	18	75	7	18	0	54
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	0	26	62	51	22	0	18	75	7	18	0	54
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44
PHF Volume:	0	59	141	116	50	0	41	170	16	41	0	123
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	0	59	141	116	50	0	41	170	16	41	0	123
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	0	59	141	116	50	0	41	170	16	41	0	123

Saturation Flow Module:

Adjustment:	North Bound			South Bound			East Bound			West Bound		
Lanes:	0.00	0.29	0.71	0.70	0.30	0.00	0.18	0.75	0.07	0.25	0.00	0.75
Final Sat.:	0	210	502	442	191	0	114	472	44	175	0	525

Capacity Analysis Module:

Vol/Sat:	North Bound			South Bound			East Bound			West Bound		
Crit Moves:	xxxx	0.28	0.28	0.26	0.26	xxxx	0.36	0.36	0.36	0.23	xxxx	0.23
Delay/Veh:	0.0	9.4	9.4	10.0	10.0	0.0	11.2	11.2	11.2	9.1	0.0	9.1
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	0.0	9.4	9.4	10.0	10.0	0.0	11.2	11.2	11.2	9.1	0.0	9.1
LOS by Move:	A	A	A	B	B	*	B	B	B	A	*	A
ApproachDel:	9.4			10.0			11.2			9.1		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	9.4			10.0			11.2			9.1		
LOS by Appr:	A			B			B			A		

## Existing AM Peak

## Level Of Service Computation Report

2000 HCM 4-Way Stop Method (Base Volume Alternative)

## Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.870  
 Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 26.7  
 Optimal Cycle: 0 Level Of Service: D

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1	0	0	1	0	1	0	0	0	1

## Volume Module:

Base Vol:	10	147	82	27	35	79	76	186	0	27	35	79
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	10	147	82	27	35	79	76	186	0	27	35	79
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.58	0.58	0.58	0.58	0.58	0.58	0.58	0.58	0.58	0.58	0.58	0.58
PHF Volume:	17	253	141	47	60	136	131	321	0	47	60	136
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	17	253	141	47	60	136	131	321	0	47	60	136
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	17	253	141	47	60	136	131	321	0	47	60	136

## Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.04	0.62	0.34	0.19	0.25	0.56	0.29	0.71	0.00	0.19	0.25	0.56
Final Sat.:	21	318	177	89	114	259	151	369	0	90	114	259

## Capacity Analysis Module:

Vol/Sat:	0.80	0.80	0.80	0.53	0.53	0.53	0.87	0.87	xxxx	0.52	0.52	0.52
Crit Moves:	****			****			****			****		
Delay/Veh:	28.2	28.2	28.2	15.9	15.9	15.9	37.0	37.0	0.0	15.8	15.8	15.8
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	28.2	28.2	28.2	15.9	15.9	15.9	37.0	37.0	0.0	15.8	15.8	15.8
LOS by Move:	D	D	D	C	C	C	E	E	*	C	C	C
ApproachDel:	28.2			15.9			37.0			15.8		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	28.2			15.9			37.0			15.8		
LOS by Appr:	D			C			E			C		



Existing Midday

Scenario Report

Scenario: Existing Midday  
 Command: Existing Midday  
 Volume: Existing Midday  
 Geometry: Existing  
 Impact Fee: Default Impact Fee  
 Trip Generation: Midday  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

Existing Midday

Impact Analysis Report  
 Level Of Service

Intersection	Base		Future		Change in
	Del/ LOS	V/ Veh	Del/ LOS	V/ Veh	
# 1 Farmington/Erickson	F 70.9	0.000	F 70.9	0.000	+ 0.000 V/C
# 2 Farmington/Stott	B 14.6	0.000	B 14.6	0.000	+ 0.000 V/C
# 3 Erickson/2nd	B 10.0	0.000	B 10.0	0.000	+ 0.000 V/C
# 4 Stott/2nd	A 8.2	0.180	A 8.2	0.180	+ 0.000 V/C
# 5 Erickson/6th	B 10.8	0.432	B 10.8	0.432	+ 0.000 V/C

Existing Midday

Level of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 70.9 Worst Case Level Of Service: F

Table with columns: Approach, Movement, Control, Rights, Lanes. Rows for North, South, East, West bounds.

Volume Module: Base Vol, Growth Adj, Initial Bse, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol.

Critical Gap Module: Critical Gp, FollowUpTim.

Capacity Module: Conflict Vol, Potent Cap., Move Cap.

Level Of Service Module: Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS.

Existing Midday

Level of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 14.6 Worst Case Level Of Service: B

Table with columns: Approach, Movement, Control, Rights, Lanes. Rows for North, South, East, West bounds.

Volume Module: Base Vol, Growth Adj, Initial Bse, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol.

Critical Gap Module: Critical Gp, FollowUpTim.

Capacity Module: Conflict Vol, Potent Cap., Move Cap.

Level Of Service Module: Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS.

Existing Midday

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 10.0 Worst Case Level Of Service: B

Approach:	North Bound	South Bound	East Bound	West Bound
Movement:	L - T - R	L - T - R	L - T - R	L - T - R
Control:	Uncontrolled	Uncontrolled	Stop Sign	Stop Sign
Rights:	Include	Include	Include	Include
Lanes:	0 0 1 0 0	0 0 1 0 0	0 0 1 0 0	0 0 0 0 0

Volume Module:

Base Vol:	25 88 23	15 114 12	11 6 59	0 0 0
Growth Adj:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00
Initial Bse:	25 88 23	15 114 12	11 6 59	0 0 0
User Adj:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00
PHF Adj:	0.83 0.83 0.83	0.83 0.83 0.83	0.83 0.83 0.83	0.83 0.83 0.83
PHF Volume:	30 106 28	18 137 14	13 7 71	0 0 0
Reduct Vol:	0 0 0	0 0 0	0 0 0	0 0 0
Final Vol.:	30 106 28	18 137 14	13 7 71	0 0 0

Critical Gap Module:

Critical Gp:	4.2 xxx	4.1 xxx	6.4 6.5 6.2	xxxx
FollowUpTim:	2.3 xxx	2.2 xxx	3.5 4.0 3.3	xxxx

Capacity Module:

Cnflct Vol:	152 xxx	134 xxx	361 375 145	xxxx
Potent Cap.:	1381 xxx	1445 xxx	642 559 908	xxxx
Move Cap.:	1381 xxx	1445 xxx	625 540 908	xxxx

Level Of Service Module:

Stopped Del:	7.6 xxx	7.5 xxx	xxxx	xxxx
LOS by Move:	A	A	B	B
Movement:	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT	LT - LTR - RT
Shared Cap.:	xxxx	xxxx	811	xxxx
Shrd StpDel:	xxxx	xxxx	10.0	xxxx
Shared LOS:	*	*	B	*
ApproachDel:	xxxx	xxxx	10.0	xxxx
ApproachLOS:	*	*	B	*

Existing Midday

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.180  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 8.2  
Optimal Cycle: 0 Level Of Service: A

Approach:	North Bound	South Bound	East Bound	West Bound
Movement:	L - T - R	L - T - R	L - T - R	L - T - R
Control:	Stop Sign	Stop Sign	Stop Sign	Stop Sign
Rights:	Include	Include	Include	Include
Min. Green:	0 0 0	0 0 0	0 0 0	0 0 0
Lanes:	0 0 0 1 0	0 1 0 0 0	0 0 1 0 0	0 0 1 0 0

Volume Module:

Base Vol:	0 3 28	51 22 0	14 38 6	18 0 3
Growth Adj:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00
Initial Bse:	0 3 28	51 22 0	14 38 6	18 0 3
User Adj:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00
PHF Adj:	0.52 0.52 0.52	0.52 0.52 0.52	0.52 0.52 0.52	0.52 0.52 0.52
PHF Volume:	0 6 54	98 42 0	27 73 12	35 0 6
Reduct Vol:	0 0 0	0 0 0	0 0 0	0 0 0
Reduced Vol:	0 6 54	98 42 0	27 73 12	35 0 6
PCE Adj:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00
MLF Adj:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00
Final Vol.:	0 6 54	98 42 0	27 73 12	35 0 6

Saturation Flow Module:

Adjustment:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00
Lanes:	0.00 0.10 0.90	0.70 0.30 0.00	0.24 0.65 0.11	0.85 0.00 0.15
Final Sat.:	0 89 801	544 233 0	175 473 78	641 0 110

Capacity Analysis Module:

Vol/Sat:	xxxx 0.07 0.07	0.18 0.18	xxxx 0.15 0.15	0.15 0.15	0.05 xxxx 0.05
Crit Moves:	***	***	***	***	***
Delay/Veh:	0.0 7.1 7.1	8.4 8.4 0.0	8.6 8.6 8.6	7.8 0.0 7.8	
Delay Adj:	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	1.00 1.00 1.00	
AdjDel/Veh:	0.0 7.1 7.1	8.4 8.4 0.0	8.6 8.6 8.6	7.8 0.0 7.8	
LOS by Move:	A A A	A A A	A A A	A A A	
ApproachDel:	7.1	8.4	8.6	7.8	
Delay Adj:	1.00	1.00	1.00	1.00	
ApprAdjDel:	7.1	8.4	8.6	7.8	
LOS by Appr:	A	A	A	A	

## Existing Midday

## Level Of Service Computation Report

2000 HCM 4-Way Stop Method (Base Volume Alternative)

## Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.432  
 Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 10.8  
 Optimal Cycle: 0 Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1	0	0	1	0	0	1	0	0	1

## Volume Module:

Base Vol:	11	75	32	54	128	26	21	70	12	63	129	46
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	11	75	32	54	128	26	21	70	12	63	129	46
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.82	0.82	0.82	0.82	0.82	0.82	0.82	0.82	0.82	0.82	0.82	0.82
PHF Volume:	13	91	39	66	156	32	26	85	15	77	157	56
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	13	91	39	66	156	32	26	85	15	77	157	56
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	13	91	39	66	156	32	26	85	15	77	157	56

## Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.09	0.64	0.27	0.26	0.61	0.13	0.21	0.67	0.12	0.27	0.54	0.19
Final Sat.:	57	397	170	171	403	83	127	414	73	178	363	130

## Capacity Analysis Module:

Vol/Sat:	0.23	0.23	0.23	0.39	0.39	0.39	0.21	0.21	0.21	0.43	0.43	0.43
Crit Moves:	****			****			****			****		
Delay/Veh:	9.7	9.7	9.7	11.1	11.1	11.1	9.7	9.7	9.7	11.6	11.6	11.6
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	9.7	9.7	9.7	11.1	11.1	11.1	9.7	9.7	9.7	11.6	11.6	11.6
LOS by Move:	A	A	A	B	B	B	A	A	A	B	B	B
ApproachDel:	9.7			11.1			9.7			11.6		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	9.7			11.1			9.7			11.6		
LOS by Appr:	A			B			A			B		

## Existing PM Peak

## Scenario Report

Scenario: Existing PM  
 Command: Existing PM  
 Volume: Existing PM  
 Geometry: Existing  
 Impact Fee: Default Impact Fee  
 Trip Generation: PM  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

## Existing PM Peak

Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	Del/ LOS	V/ Veh C	Del/ LOS	V/ Veh C	
# 1 Farmington/Erickson	F	73.0 0.000	F	73.0 0.000	+ 0.000 V/C
# 2 Farmington/Stott	B	13.5 0.000	B	13.5 0.000	+ 0.000 V/C
# 3 Erickson/2nd	A	9.9 0.000	A	9.9 0.000	+ 0.000 V/C
# 4 Stott/2nd	A	7.6 0.122	A	7.6 0.122	+ 0.000 V/C
# 5 Erickson/6th	B	12.0 0.537	B	12.0 0.537	+ 0.000 V/C

Existing PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 73.0 Worst Case Level Of Service: F

Table with 4 columns: Approach (North Bound, South Bound, East Bound, West Bound) and Movement (L-T-R). Rows include Control (Stop Sign, Uncontrolled), Rights (Include), and Lanes.

Volume Module table with columns for Base Vol, Growth Adj, Initial Bse, User Adj, PHF Adj, PHF Volume, Reduct Vol, and Final Vol across four approaches.

Critical Gap Module table with columns for Critical Gp, FollowUpTim, and Capacity Module.

Capacity Module table with columns for Conflict Vol, Potent Cap., and Move Cap.

Level Of Service Module table with columns for Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, and ApproachLOS.

Existing PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 13.5 Worst Case Level Of Service: B

Table with 4 columns: Approach (North Bound, South Bound, East Bound, West Bound) and Movement (L-T-R). Rows include Control (Stop Sign, Uncontrolled), Rights (Include), and Lanes.

Volume Module table with columns for Base Vol, Growth Adj, Initial Bse, User Adj, PHF Adj, PHF Volume, Reduct Vol, and Final Vol across four approaches.

Critical Gap Module table with columns for Critical Gp, FollowUpTim, and Capacity Module.

Capacity Module table with columns for Conflict Vol, Potent Cap., and Move Cap.

Level Of Service Module table with columns for Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, and ApproachLOS.

Existing PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 9.9 Worst Case Level Of Service: A

Approach:	North Bound			South Bound			East Bound			West Bound								
Movement:	L	T	R	L	T	R	L	T	R	L	T	R						
Control:	Uncontrolled			Uncontrolled			Stop Sign			Stop Sign								
Rights:	Include			Include			Include			Include								
Lanes:	0	0	1	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0

Volume Module:

Base Vol:	12	75	37	20	166	7	3	4	35	0	0	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	12	75	37	20	166	7	3	4	35	0	0	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
PHF Volume:	14	87	43	23	193	8	3	5	41	0	0	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	14	87	43	23	193	8	3	5	41	0	0	0

Critical Gap Module:

Critical Gp:	4.2	xxxx	xxxxx	4.1	xxxx	xxxxx	6.4	6.5	6.2	xxxxx	xxxx	xxxxx
FollowUpTim:	2.3	xxxx	xxxxx	2.2	xxxx	xxxxx	3.5	4.0	3.3	xxxxx	xxxx	xxxxx

Capacity Module:

Cnflct Vol:	201	xxxx	xxxxx	130	xxxx	xxxxx	380	402	197	xxxx	xxxx	xxxxx
Potent Cap.:	1324	xxxx	xxxxx	1449	xxxx	xxxxx	626	540	849	xxxx	xxxx	xxxxx
Move Cap.:	1324	xxxx	xxxxx	1449	xxxx	xxxxx	613	526	849	xxxx	xxxx	xxxxx

Level Of Service Module:

Stopped Del:	7.7	xxxx	xxxxx	7.5	xxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxx	xxxxx
LOS by Move:	A	*	*	A	*	*	*	*	*	*	*	*
Movement:	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT
Shared Cap.:	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	xxxx	782	xxxxx	xxxxx	xxxx	xxxxx
Shrd StpDel:	xxxxx	xxxx	xxxxx	xxxxx	xxxx	xxxxx	xxxxx	9.9	xxxxx	xxxxx	xxxx	xxxxx
Shared LOS:	*	*	*	*	*	*	*	A	*	*	*	*
ApproachDel:	xxxxxx			xxxxxx			9.9			xxxxxx		
ApproachLOS:	*			*			A			*		

Existing PM Peak

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.122

Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 7.6

Optimal Cycle: 0 Level Of Service: A

Approach:	North Bound			South Bound			East Bound			West Bound							
Movement:	L	T	R	L	T	R	L	T	R	L	T	R					
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign							
Rights:	Include			Include			Include			Include							
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0					
Lanes:	0	0	0	1	0	0	0	1	0	0	0	0	0	0	1	0	0

Volume Module:

Base Vol:	0	7	13	41	22	0	5	21	14	8	0	14
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	0	7	13	41	22	0	5	21	14	8	0	14
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63
PHF Volume:	0	11	21	65	35	0	8	33	22	13	0	22
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	0	11	21	65	35	0	8	33	22	13	0	22
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	0	11	21	65	35	0	8	33	22	13	0	22

Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.00	0.34	0.66	0.65	0.35	0.00	0.13	0.52	0.35	0.37	0.00	0.63
Final Sat.:	0	315	601	532	287	0	100	412	275	325	0	551

Capacity Analysis Module:

Vol/Sat:	xxxx	0.03	0.03	0.12	0.12	xxxx	0.08	0.08	0.08	0.04	xxxx	0.04
Crit Moves:	****											
Delay/Veh:	0.0	7.0	7.0	7.9	7.9	0.0	7.8	7.8	7.8	7.1	0.0	7.1
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	0.0	7.0	7.0	7.9	7.9	0.0	7.8	7.8	7.8	7.1	0.0	7.1
LOS by Move:	*	A	*	A	A	*	A	A	A	A	*	A
ApproachDel:		7.0			7.9			7.8			7.1	
Delay Adj:		1.00			1.00			1.00			1.00	
ApprAdjDel:		7.0			7.9			7.8			7.1	
LOS by Appr:		A			A			A			A	

## Existing PM Peak

 Level Of Service Computation Report  
 2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.537  
 Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 12.0  
 Optimal Cycle: 0 Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1:0	0	0	1:0	0	0	1:0	0	0	1:0

## Volume Module:

Base Vol:	13	72	21	44	175	14	22	65	10	66	197	43
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	13	72	21	44	175	14	22	65	10	66	197	43
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
PHF Volume:	15	85	25	52	206	16	26	76	12	78	232	51
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	15	85	25	52	206	16	26	76	12	78	232	51
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	15	85	25	52	206	16	26	76	12	78	232	51

## Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.12	0.68	0.20	0.19	0.75	0.06	0.23	0.67	0.10	0.22	0.64	0.14
Final Sat.:	71	400	118	120	477	37	135	394	62	145	432	95

## Capacity Analysis Module:

Vol/Sat:	0.21	0.21	0.21	0.43	0.43	0.43	0.19	0.19	0.19	0.54	0.54	0.54
Crit Moves:	****			****			****			****		
Delay/Veh:	9.9	9.9	9.9	12.0	12.0	12.0	9.8	9.8	9.8	13.5	13.5	13.5
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	9.9	9.9	9.9	12.0	12.0	12.0	9.8	9.8	9.8	13.5	13.5	13.5
LOS by Move:	A	A	A	B	B	B	A	A	A	B	B	B
ApproachDel:	9.9			12.0			9.8			13.5		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	9.9			12.0			9.8			13.5		
LOS by Appr:	A			B			A			B		



Proposed AM Peak

Scenario Report

Scenario: Proposed AM

Command: Proposed AM

Volume: Proposed AM

Geometry: Proposed

Impact Fee: Default Impact Fee

Trip Generation: AM

Trip Distribution: Default Trip Distribution

Paths: Default Paths

Routes: Default Routes

Configuration: Default Configuration

Proposed AM Peak

Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	LOS	Veh C	LOS	Veh C	
# 1 Farmington/Erickson	F	121.1 0.000	F	121.1 0.000	+ 0.000 V/C
# 2 Farmington/Stott	C	16.6 0.000	C	16.6 0.000	+ 0.000 V/C
# 3 Erickson/2nd	B	12.5 0.000	B	12.5 0.000	+ 0.000 V/C
# 4 Stott/2nd	B	10.0 0.362	B	10.0 0.362	+ 0.000 V/C
# 5 Erickson/6th	B	12.1 0.494	B	12.1 0.494	+ 0.000 V/C

## Proposed AM Peak

## Scenario Report

Scenario: Proposed AM  
 Command: Proposed AM  
 Volume: Proposed AM  
 Geometry: Proposed  
 Impact Fee: Default Impact Fee  
 Trip Generation: AM  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

## Proposed AM Peak

Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	Del/ LOS	V/ Veh C	Del/ LOS	V/ Veh C	
# 1 Farmington/Erickson	F 121.1	0.000	F 121.1	0.000	+ 0.000 V/C
# 2 Farmington/Stott	C 19.2	0.000	C 19.2	0.000	+ 0.000 V/C
# 3 Erickson/2nd	B 12.5	0.000	B 12.5	0.000	+ 0.000 V/C
# 4 Stott/2nd	B 10.0	0.362	B 10.0	0.362	+ 0.000 V/C
# 5 Erickson/6th	D 34.2	0.917	D 34.2	0.917	+ 0.000 V/C

Proposed AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 121.1 Worst Case Level Of Service: F

Approach:	North Bound	South Bound	East Bound	West Bound
Movement:	L - T - R	L - T - R	L - T - R	L - T - R
Control:	Stop Sign	Stop Sign	Uncontrolled	Uncontrolled
Rights:	Include	Include	Include	Include
Lanes:	1 0 0 0 1	0 0 0 0 0	0 0 1 1 0	1 0 2 0 0

Volume Module:

Base Vol:	39	0	106	0	0	0	0	1530	28	42	605	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	39	0	106	0	0	0	0	1530	28	42	605	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
PHF Volume:	43	0	118	0	0	0	0	1700	31	47	672	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	43	0	118	0	0	0	0	1700	31	47	672	0

Critical Gap Module:

Critical Gp:	6.9	xxxx	7.0	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	4.2	xxxx	xxxx
FollowUpTim:	3.5	xxxx	3.3	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	2.2	xxxx	xxxx

Capacity Module:

Cnflct Vol:	2145	xxxx	866	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	1731	xxxx	xxxx
Potent Cap.:	40	xxxx	293	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	352	xxxx	xxxx
Move Cap.:	36	xxxx	293	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	352	xxxx	xxxx

Level Of Service Module:

Stopped Del:	381.5	xxxx	25.3	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	16.8	xxxx	xxxx
LOS by Move:	F	*	D	*	*	*	*	*	*	C	*	*
Movement:	LT - LTR - RT		LT - LTR - RT		LT - LTR - RT		LT - LTR - RT		LT - LTR - RT		LT - LTR - RT	
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	121.1		xxxxxx		xxxxxx		xxxxxx		xxxxxx		xxxxxx	
ApproachLOS:	F		*		*		*		*		*	

Proposed AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 19.2 Worst Case Level Of Service: C

Approach:	North Bound	South Bound	East Bound	West Bound
Movement:	L - T - R	L - T - R	L - T - R	L - T - R
Control:	Stop Sign	Stop Sign	Uncontrolled	Uncontrolled
Rights:	Include	Include	Include	Include
Lanes:	0 0 0 0 1	0 0 0 0 0	0 0 1 1 0	0 0 2 0 0

Volume Module:

Base Vol:	0	0	39	0	0	0	0	1542	61	0	729	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	0	0	39	0	0	0	0	1542	61	0	729	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
PHF Volume:	0	0	42	0	0	0	0	1658	66	0	784	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	0	0	42	0	0	0	0	1658	66	0	784	0

Critical Gap Module:

Critical Gp:	xxxx	xxxx	7.0	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
FollowUpTim:	xxxx	xxxx	3.3	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx

Capacity Module:

Cnflct Vol:	xxxx	xxxx	862	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Potent Cap.:	xxxx	xxxx	294	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Move Cap.:	xxxx	xxxx	294	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx

Level Of Service Module:

Stopped Del:	xxxx	xxxx	19.2	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
LOS by Move:	*	*	C	*	*	*	*	*	*	*	*	*
Movement:	LT - LTR - RT		LT - LTR - RT		LT - LTR - RT		LT - LTR - RT		LT - LTR - RT		LT - LTR - RT	
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:			19.2		xxxxxx		xxxxxx		xxxxxx		xxxxxx	
ApproachLOS:			C		*		*		*		*	

Proposed AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 12.5 Worst Case Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Uncontrolled			Uncontrolled			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Lanes:	0	0	1:0	0	0	1:0	0	0	1:0	0	0	1:0

Volume Module:

Base Vol:	54	109	30	5	46	10	5	5	37	20	7	8
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	54	109	30	5	46	10	5	5	37	20	7	8
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70
PHF Volume:	77	156	43	7	66	14	7	7	53	29	10	11
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	77	156	43	7	66	14	7	7	53	29	10	11

Critical Gap Module:

Critical Gp:	4.2	xxxx	xxxxx	4.1	xxxx	xxxxx	7.1	6.5	6.2	7.1	6.5	6.2
FollowUpTim:	2.3	xxxx	xxxxx	2.2	xxxx	xxxxx	3.5	4.0	3.3	3.5	4.0	3.3

Capacity Module:

Cnflct Vol:	80	xxxx	xxxxx	199	xxxx	xxxxx	429	440	73	449	426	177
Potent Cap.:	1469	xxxx	xxxxx	1368	xxxx	xxxxx	540	514	995	524	524	871
Move Cap.:	1469	xxxx	xxxxx	1368	xxxx	xxxxx	500	483	995	468	492	871

Level Of Service Module:

Stopped Del:	7.5	xxxx	xxxxx	7.6	xxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxxx
LOS by Move:	A	*	*	A	*	*	*	*	*	*	*	*
Movement:	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT
Shared Cap.:	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	817	xxxxx	xxxx	529	xxxxx	xxxxx
Shrd StpDel:	xxxxx	xxxx	xxxxx	xxxxx	xxxx	xxxxx	9.8	xxxxx	xxxxx	12.5	xxxxx	xxxxx
Shared LOS:	*	*	*	*	*	*	A	*	*	B	*	*
ApproachDel:	xxxxxx			xxxxxxx			9.8			12.5		
ApproachLOS:	*			*			A			B		

Proposed AM Peak

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.362  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 10.0  
Optimal Cycle: 0 Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1:0	0	0	1:0	0	0	1:0	0	0	1:0

Volume Module:

Base Vol:	25	26	62	20	22	15	9	49	7	18	35	54
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	25	26	62	20	22	15	9	49	7	18	35	54
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44	0.44
PHF Volume:	57	59	141	45	50	34	20	111	16	41	80	123
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	57	59	141	45	50	34	20	111	16	41	80	123
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	57	59	141	45	50	34	20	111	16	41	80	123

Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.22	0.23	0.55	0.35	0.39	0.26	0.14	0.75	0.11	0.17	0.33	0.50
Final Sat.:	158	163	390	225	250	170	83	458	66	118	231	354

Capacity Analysis Module:

Vol/Sat:	0.36	0.36	0.36	0.20	0.20	0.20	0.24	0.24	0.24	0.35	0.35	0.35
Crit Moves:	****			****			****			****		
Delay/Veh:	10.2	10.2	10.2	9.3	9.3	9.3	10.2	10.2	10.2	10.1	10.1	10.1
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	10.2	10.2	10.2	9.3	9.3	9.3	10.2	10.2	10.2	10.1	10.1	10.1
LOS by Move:	B	B	B	A	A	A	B	B	B	B	B	B
ApproachDel:	10.2			9.3			10.2			10.1		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	10.2			9.3			10.2			10.1		
LOS by Appr:	B			A			B			B		

Proposed AM Peak

Level of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.917  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 34.2  
Optimal Cycle: 0 Level of Service: D

Approach: North Bound South Bound East Bound West Bound  
Movement: L - T - R L - T - R L - T - R L - T - R  
Control: Stop Sign Stop Sign Stop Sign Stop Sign  
Rights: Include Include Include Include  
Min. Green: 0 0 0 0 0 0 0 0 0 0 0 0  
Lanes: 0 0 1 0 0 0 0 1 0 0 0 0

Volume Module:  
Base Vol: 10 169 110 31 83 87 64 198 0 27 35 73  
Growth Adj: 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00  
Initial Bse: 10 169 110 31 83 87 64 198 0 27 35 73  
User Adj: 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00  
PHF Adj: 0.62 0.62 0.62 0.62 0.62 0.62 0.62 0.62 0.62 0.62 0.62 0.62  
PHF Volume: 16 273 177 50 134 140 103 319 0 44 56 118  
Reduct Vol: 0 0 0 0 0 0 0 0 0 0 0 0  
Reduced Vol: 16 273 177 50 134 140 103 319 0 44 56 118  
PCE Adj: 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00  
MLF Adj: 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00  
Final Vol.: 16 273 177 50 134 140 103 319 0 44 56 118

Saturation Flow Module:  
Adjustment: 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00  
Lanes: 0.03 0.59 0.38 0.15 0.41 0.44 0.24 0.76 0.00 0.20 0.26 0.54  
Final Sat.: 17 298 193 71 191 200 117 363 0 86 110 231

Capacity Analysis Module:  
Vol/Sat: 0.92 0.92 0.92 0.70 0.70 0.70 0.88 0.88 xxx 0.51 0.51 0.51  
Crit Moves: \*\*\*\* \*\*\*\* \*\*\*\*  
Delay/Veh: 45.0 45.0 45.0 22.7 22.7 22.7 39.9 39.9 0.0 16.9 16.9 16.9  
Delay Adj: 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.00  
AdjDel/Veh: 45.0 45.0 45.0 22.7 22.7 22.7 39.9 39.9 0.0 16.9 16.9 16.9  
LOS by Move: E E E C C E E \* C C C  
ApproachDel: 45.0 22.7 39.9 16.9  
Delay Adj: 1.00 1.00 1.00  
ApprAdjDel: 45.0 22.7 39.9 16.9  
LOS by Appr: E C E C

-----  
Proposed Midday  
-----

## Scenario Report

Scenario: Proposed Midday  
 Command: Proposed Midday  
 Volume: Proposed Midday  
 Geometry: Proposed  
 Impact Fee: Default Impact Fee  
 Trip Generation: Midday  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

-----  
Proposed Midday  
-----Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	Del/	V/	Del/	V/	
	LOS	Veh	LOS	Veh	
# 1 Farmington/Erickson	F	62.9 0.000	F	62.9 0.000	+ 0.000 V/C
# 2 Farmington/Stott	B	14.5 0.000	B	14.5 0.000	+ 0.000 V/C
# 3 Erickson/2nd	B	11.9 0.000	B	11.9 0.000	+ 0.000 V/C
# 4 Stott/2nd	A	8.0 0.185	A	8.0 0.185	+ 0.000 V/C
# 5 Erickson/6th	B	11.1 0.438	B	11.1 0.438	+ 0.000 V/C

Proposed Midday

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 62.9 Worst Case Level Of Service: F

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled		
Rights:	Include			Include			Include			Include		
Lanes:	1	0	0	0	0	0	0	0	1	1	0	2

Volume Module:

Base Vol:	28	0	65	0	0	0	0	1095	41	79	1239	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	28	0	65	0	0	0	0	1095	41	79	1239	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
PHF Volume:	29	0	68	0	0	0	0	1153	43	83	1304	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	29	0	68	0	0	0	0	1153	43	83	1304	0

Critical Gap Module:

Critical Gp:	6.9	xxxx	7.0	xxxx	xxxx	xxxx	xxxx	xxxx	4.2	xxxx	xxxx
FollowUpTim:	3.5	xxxx	3.3	xxxx	xxxx	xxxx	xxxx	xxxx	2.2	xxxx	xxxx

Capacity Module:

Cnflct Vol:	1993	xxxx	598	xxxx	xxxx	xxxx	xxxx	xxxx	1196	xxxx	xxxx
Potent Cap.:	51	xxxx	441	xxxx	xxxx	xxxx	xxxx	xxxx	568	xxxx	xxxx
Move Cap.:	46	xxxx	441	xxxx	xxxx	xxxx	xxxx	xxxx	568	xxxx	xxxx

Level Of Service Module:

Stopped Del:	174.9	xxxx	14.7	xxxx	xxxx	xxxx	xxxx	xxxx	12.4	xxxx	xxxx	
LOS by Move:	F	*	B	*	*	*	*	*	B	*	*	
Movement:	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	62.9		xxxxxx			xxxxxxx			xxxxxxx			
ApproachLOS:	F		*			*			*			

Proposed Midday

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 14.5 Worst Case Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled		
Rights:	Include			Include			Include			Include		
Lanes:	0	0	0	0	0	0	0	0	1	0	0	1

Volume Module:

Base Vol:	0	0	40	0	0	0	0	1268	51	0	1257	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	0	0	40	0	0	0	0	1268	51	0	1257	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
PHF Volume:	0	0	44	0	0	0	0	1393	56	0	1381	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	0	0	44	0	0	0	0	1393	56	0	1381	0

Critical Gap Module:

Critical Gp:	xxxxx	xxxx	6.2	xxxxx	xxxx	xxxxx	xxxx	xxxxx	xxxxx	xxxx	xxxxx
FollowUpTim:	xxxxxx	xxxx	3.3	xxxxxx	xxxx	xxxxxx	xxxx	xxxxxx	xxxxxx	xxxx	xxxxxx

Capacity Module:

Cnflct Vol:	xxxx	xxxx	725	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Potent Cap.:	xxxx	xxxx	422	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Move Cap.:	xxxx	xxxx	422	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx

Level Of Service Module:

Stopped Del:	xxxxx	xxxx	14.5	xxxxx	xxxx	xxxxx	xxxx	xxxxx	xxxxx	xxxx	xxxxx	
LOS by Move:	*	*	B	*	*	*	*	*	*	*	*	
Movement:	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	14.5		xxxxxxx			xxxxxxx			xxxxxxx			
ApproachLOS:	B		*			*			*			

Proposed Midday

Level Of Service Computation Report

2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 11.9 Worst Case Level Of Service: B

Table with columns: Approach, Movement, Control, Rights, Lanes. Rows: North Bound, South Bound, East Bound, West Bound.

Volume Module:

Table with columns: Base Vol, Growth Adj, Initial Bse, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol. Rows: North Bound, South Bound, East Bound, West Bound.

Critical Gap Module:

Table with columns: Critical Gp, FollowUpTim. Rows: North Bound, South Bound, East Bound, West Bound.

Capacity Module:

Table with columns: Cnflct Vol, Potent Cap., Move Cap. Rows: North Bound, South Bound, East Bound, West Bound.

Level Of Service Module:

Table with columns: Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS. Rows: North Bound, South Bound, East Bound, West Bound.

Proposed Midday

Level Of Service Computation Report

2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.185

Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 8.0

Optimal Cycle: 0 Level Of Service: A

Table with columns: Approach, Movement, Control, Rights, Min. Green, Lanes. Rows: North Bound, South Bound, East Bound, West Bound.

Volume Module:

Table with columns: Base Vol, Growth Adj, Initial Bse, User Adj, PHF Adj, PHF Volume, Reduct Vol, Reduced Vol, PCE Adj, MLF Adj, Final Vol. Rows: North Bound, South Bound, East Bound, West Bound.

Saturation Flow Module:

Table with columns: Adjustment, Lanes, Final Sat. Rows: North Bound, South Bound, East Bound, West Bound.

Capacity Analysis Module:

Table with columns: Vol/Sat, Crit Moves, Delay/Veh, AdjDel/Veh, LOS by Move, ApproachDel, Delay Adj, ApprAdjDel, LOS by Appr. Rows: North Bound, South Bound, East Bound, West Bound.



Proposed Midday

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.438  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 11.1  
Optimal Cycle: 0 Level Of Service: B

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Movement (L-T-R), Control (Stop Sign), Rights (Include), Min. Green, and Lanes.

Volume Module: Table with 12 columns representing different traffic movements. Rows include Base Vol, Growth Adj, Initial Bse, User Adj, PHF Adj, PHF Volume, Reduct Vol, Reduced Vol, PCE Adj, MLF Adj, and Final Vol.

Saturation Flow Module: Table with 12 columns. Rows include Adjustment, Lanes, and Final Sat.

Capacity Analysis Module: Table with 12 columns. Rows include Vol/Sat, Crit Moves, Delay/Veh, Delay Adj, AdjDel/Veh, LOS by Move, ApproachDel, Delay Adj, ApprAdjDel, and LOS by Appr.

-----  
Proposed PM Peak  
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## Scenario Report

Scenario: Proposed PM

Command: Proposed PM  
 Volume: Proposed PM  
 Geometry: Proposed  
 Impact Fee: Default Impact Fee  
 Trip Generation: PM  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

-----  
Proposed PM Peak  
-----Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	Del/ LOS Veh	V/ C	Del/ LOS Veh	V/ C	
# 1 Farmington/Erickson	F 73.3	0.000	F 73.3	0.000	+ 0.000 V/C
# 2 Farmington/Stott	B 13.4	0.000	B 13.4	0.000	+ 0.000 V/C
# 3 Erickson/2nd	B 11.4	0.000	B 11.4	0.000	+ 0.000 V/C
# 4 Stott/2nd	A 7.7	0.134	A 7.7	0.134	+ 0.000 V/C
# 5 Erickson/6th	B 12.3	0.536	B 12.3	0.536	+ 0.000 V/C

Proposed PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 73.3 Worst Case Level Of Service: F

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled		
Rights:	Include			Include			Include			Include		
Lanes:	1	0	0	0	0	0	0	0	1	1	0	2

Volume Module:	North Bound			South Bound			East Bound			West Bound		
Base Vol:	22	0	74	0	0	0	0	1167	107	87	1580	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	22	0	74	0	0	0	0	1167	107	87	1580	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
PHF Volume:	22	0	76	0	0	0	0	1191	109	89	1612	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	22	0	76	0	0	0	0	1191	109	89	1612	0

Critical Gap Module:	North Bound			South Bound			East Bound			West Bound		
Critical Gp:	6.9	xxxx	7.0	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	4.2	xxxx	xxxx
FollowUpTim:	3.5	xxxx	3.3	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	2.2	xxxx	xxxx

Capacity Module:	North Bound			South Bound			East Bound			West Bound		
Cnflct Vol:	2229	xxxx	650	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	1300	xxxx	xxxx
Potent Cap.:	35	xxxx	407	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	518	xxxx	xxxx
Move Cap.:	31	xxxx	407	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	518	xxxx	xxxx

Level Of Service Module:	North Bound			South Bound			East Bound			West Bound		
Stopped Del:	266.3	xxxx	15.8	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	13.4	xxxx	xxxx
LOS by Move:	F	*	C	*	*	*	*	*	*	B	*	*
Movement:	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	73.3	*	xxxxxx	xxxxxx	*	*	xxxxxx	*	*	xxxxxx	*	*
ApproachLOS:	F	*	*	*	*	*	*	*	*	*	*	*

Proposed PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 13.4 Worst Case Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled		
Rights:	Include			Include			Include			Include		
Lanes:	0	0	0	0	0	0	0	0	1	1	0	0

Volume Module:	North Bound			South Bound			East Bound			West Bound		
Base Vol:	0	0	18	0	0	0	0	1286	53	0	1718	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	0	0	18	0	0	0	0	1286	53	0	1718	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
PHF Volume:	0	0	18	0	0	0	0	1312	54	0	1753	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	0	0	18	0	0	0	0	1312	54	0	1753	0

Critical Gap Module:	North Bound			South Bound			East Bound			West Bound		
Critical Gp:	xxxxx	xxxx	6.2	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx
FollowUpTim:	xxxxx	xxxx	3.3	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx

Capacity Module:	North Bound			South Bound			East Bound			West Bound		
Cnflct Vol:	xxxx	xxxx	683	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Potent Cap.:	xxxx	xxxx	446	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Move Cap.:	xxxx	xxxx	446	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx

Level Of Service Module:	North Bound			South Bound			East Bound			West Bound		
Stopped Del:	xxxxx	xxxx	13.4	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	xxxx	xxxx
LOS by Move:	*	*	B	*	*	*	*	*	*	*	*	*
Movement:	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	13.4	*	xxxxxxx	xxxxxxx	*	*	xxxxxxx	*	*	xxxxxxx	*	*
ApproachLOS:	B	*	*	*	*	*	*	*	*	*	*	*

Proposed PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Base Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 11.4 Worst Case Level of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Uncontrolled			Uncontrolled			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Lanes:	0	0	1:0	0	0	1:0	0	0	1:0	0	0	1:0

Volume Module:

Base Vol:	12	75	18	10	166	7	3	2	35	20	5	5
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	12	75	18	10	166	7	3	2	35	20	5	5
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
PHF Volume:	14	87	21	12	193	8	3	2	41	23	6	6
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	14	87	21	12	193	8	3	2	41	23	6	6

Critical Gap Module:

Critical Gp:	4.2	xxxx	xxxxx	4.1	xxxx	xxxxx	7.1	6.5	6.2	7.1	6.5	6.2
FollowUpTim:	2.3	xxxx	xxxxx	2.2	xxxx	xxxxx	3.5	4.0	3.3	3.5	4.0	3.3

Capacity Module:

Cnflct Vol:	201	xxxx	xxxxx	108	xxxx	xxxxx	352	356	197	367	350	98
Potent Cap.:	1324	xxxx	xxxxx	1476	xxxx	xxxxx	607	573	849	593	577	964
Move Cap.:	1324	xxxx	xxxxx	1476	xxxx	xxxxx	590	562	849	555	567	964

Level Of Service Module:

Stopped Del:	7.7	xxxx	xxxxx	7.4	xxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxxx	xxxxx
LOS by Move:	A	*	*	A	*	*	*	*	*	*	*	*
Movement:	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT	LT	LTR	RT
Shared Cap.:	xxxx	xxxx	xxxxx	xxxx	xxxx	xxxxx	802	xxxxx	xxxxx	599	xxxxx	xxxxx
Shrd StpDel:	xxxxx	xxxx	xxxxx	xxxxx	xxxx	xxxxx	9.8	xxxxx	xxxxx	11.4	xxxxx	xxxxx
Shared LOS:	*	*	*	*	*	*	A	*	*	B	*	*
ApproachDel:	xxxxxx			xxxxxx			9.8			11.4		
ApproachLOS:	*			*			A			B		

Proposed PM Peak

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.134  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 7.7  
Optimal Cycle: 0 Level of Service: A

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1:0	0	0	1:0	0	0	1:0	0	0	1:0

Volume Module:

Base Vol:	10	7	13	41	22	7	4	15	11	8	14	14
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	10	7	13	41	22	7	4	15	11	8	14	14
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63	0.63
PHF Volume:	16	11	21	65	35	11	6	24	17	13	22	22
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	16	11	21	65	35	11	6	24	17	13	22	22
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	16	11	21	65	35	11	6	24	17	13	22	22

Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.33	0.23	0.44	0.59	0.31	0.10	0.13	0.51	0.36	0.23	0.38	0.39
Final Sat.:	289	199	379	484	260	82	98	393	278	192	324	324

Capacity Analysis Module:

Vol/Sat:	0.06	0.06	0.06	0.13	0.13	0.13	0.06	0.06	0.06	0.07	0.07	0.07
Crit Moves:	****			****			****			****		
Delay/Veh:	7.3	7.3	7.3	7.9	7.9	7.9	7.8	7.8	7.8	7.4	7.4	7.4
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	7.3	7.3	7.3	7.9	7.9	7.9	7.8	7.8	7.8	7.4	7.4	7.4
LOS by Move:	A	A	A	A	A	A	A	A	A	A	A	A
ApproachDel:	7.3			7.9			7.8			7.4		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	7.3			7.9			7.8			7.4		
LOS by Appr:	A			A			A			A		

Proposed PM Peak

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Base Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.536  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 12.3  
Optimal Cycle: 0 Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1	0	0	1	0	0	1	0	0	1

Volume Module:

Base Vol:	13	62	31	48	190	15	19	68	10	66	197	37
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	13	62	31	48	190	15	19	68	10	66	197	37
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
PHF Volume:	15	73	36	56	224	18	22	80	12	78	232	44
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	15	73	36	56	224	18	22	80	12	78	232	44
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	15	73	36	56	224	18	22	80	12	78	232	44

Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.12	0.59	0.29	0.19	0.75	0.06	0.19	0.70	0.11	0.22	0.66	0.12
Final Sat.:	71	346	171	120	479	38	113	410	61	146	433	82

Capacity Analysis Module:

Vol/Sat:	0.21	0.21	0.21	0.47	0.47	0.47	0.20	0.20	0.20	0.54	0.54	0.54
Crit Moves:	****			****			****			****		
Delay/Veh:	9.9	9.9	9.9	12.5	12.5	12.5	9.9	9.9	9.9	13.6	13.6	13.6
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	9.9	9.9	9.9	12.5	12.5	12.5	9.9	9.9	9.9	13.6	13.6	13.6
LOS by Move:	A	A	A	B	B	B	A	A	A	B	B	B
ApproachDel:	9.9			12.5			9.9			13.6		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	9.9			12.5			9.9			13.6		
LOS by Appr:	A			B			A			B		

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Worst Case AM Peak  
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## Scenario Report

Scenario: Worst Case AM

Command: Worst Case AM  
 Volume: Proposed AM  
 Geometry: Proposed  
 Impact Fee: Default Impact Fee  
 Trip Generation: AM  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

-----  
Worst Case AM Peak  
-----Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	LOS	Veh C	LOS	Veh C	
# 1 Farmington/Erickson	F	121.1 0.000	F	151.6 0.000	+ 0.000 V/C
# 2 Farmington/Stott	C	19.2 0.000	C	19.9 0.000	+ 0.000 V/C
# 3 Erickson/2nd	B	12.5 0.000	B	14.5 0.000	+ 0.000 V/C
# 4 Stott/2nd	B	10.0 0.362	B	12.3 0.506	+ 0.144 V/C
# 5 Erickson/6th	D	26.4 0.843	E	42.6 0.972	+ 0.129 V/C

Worst Case AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 151.6 Worst Case Level Of Service: F

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Control, Rights, Lanes, and Volume Module data.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Critical Gap Module and FollowUpTim.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Capacity Module and Move Cap.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Level Of Service Module, Stopped Del, LOS by Move, Movement, Shared Cap, Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS.

Worst Case AM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 19.9 Worst Case Level Of Service: C

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Control, Rights, Lanes, and Volume Module data.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Critical Gap Module and FollowUpTim.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Capacity Module and Move Cap.

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Level Of Service Module, Stopped Del, LOS by Move, Movement, Shared Cap, Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS.

Worst Case AM Peak

Level of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 14.5 Worst Case Level Of Service: B

Table with columns: Approach, Movement, Control, Rights, Lanes. Rows for North, South, East, West bounds.

Volume Module:

Table with columns: Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol.

Critical Gap Module:

Table with columns: Critical Gp, FollowUpTim.

Capacity Module:

Table with columns: Conflict Vol, Potent Cap., Move Cap.

Level of Service Module:

Table with columns: Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS.

Worst Case AM Peak

Level of Service Computation Report  
2000 HCM 4-Way Stop Method (Future Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.506  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 12.3  
Optimal Cycle: 0 Level Of Service: B

Table with columns: Approach, Movement, Control, Rights, Min. Green, Lanes. Rows for North, South, East, West bounds.

Volume Module:

Table with columns: Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Reduced Vol, PCE Adj, MLF Adj, Final Vol.

Saturation Flow Module:

Table with columns: Adjustment, Lanes, Final Sat.

Capacity Analysis Module:

Table with columns: Vol/Sat, Crit Moves, Delay/Veh, Delay Adj, AdjDel/Veh, LOS by Move, ApproachDel, Delay Adj, ApprAdjDel, LOS by Appr.



Worst Case AM Peak

Level of Service Computation Report

2000 HCM 4-Way Stop Method (Future Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.972  
 Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 42.6  
 Optimal Cycle: 0 Level of Service: E

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1 0 0	0	0	1 0 0	0	1	0 0 0	0	0	1 0 0

Volume Module:

Base Vol:	10	169	110	31	83	87	64	198	0	27	35	73
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	10	169	110	31	83	87	64	198	0	27	35	73
Added Vol:	0	10	10	10	4	3	6	6	0	4	3	22
PasserByVol:	0	0	0	0	0	0	0	0	0	0	0	0
Initial Fut:	10	179	120	41	87	90	70	204	0	31	38	95
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65
PHF Volume:	15	275	185	63	134	138	108	314	0	48	58	146
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	15	275	185	63	134	138	108	314	0	48	58	146
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	15	275	185	63	134	138	108	314	0	48	58	146

Saturation Flow Module:

Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.03	0.58	0.39	0.19	0.40	0.41	0.26	0.74	0.00	0.19	0.23	0.58
Final Sat.:	15	283	190	84	179	184	117	341	0	81	98	246

Capacity Analysis Module:

Vol/Sat:	0.97	0.97	0.97	0.75	0.75	0.75	0.92	0.92	xxxx	0.59	0.59	0.59
Crit Moves:			****	****			****			****		
Delay/Veh:	59.6	59.6	59.6	27.5	27.5	27.5	48.8	48.8	0.0	20.4	20.4	20.4
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	59.6	59.6	59.6	27.5	27.5	27.5	48.8	48.8	0.0	20.4	20.4	20.4
LOS by Move:	F	F	F	D	D	D	E	E	*	C	C	C
ApproachDel:		59.6			27.5			48.8			20.4	
Delay Adj:		1.00			1.00			1.00			1.00	
ApprAdjDel:		59.6			27.5			48.8			20.4	
LOS by Appr:		F			D			E			C	

-----  
Worst Case Midday  
-----

## Scenario Report

Scenario: Worst Case Midday

Command: Worst Case Midday  
 Volume: Proposed Midday  
 Geometry: Proposed  
 Impact Fee: Default Impact Fee  
 Trip Generation: Midday  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

-----  
Worst Case Midday  
-----Impact Analysis Report  
Level Of Service

Intersection	Base		Future		Change in
	LOS	Veh C	LOS	Veh C	
# 1 Farmington/Erickson	F	62.9 0.000	F	66.4 0.000	+ 0.000 V/C
# 2 Farmington/Stott	C	16.3 0.000	C	16.8 0.000	+ 0.000 V/C
# 3 Erickson/2nd	B	11.9 0.000	B	12.2 0.000	+ 0.000 V/C
# 4 Stott/2nd	A	8.0 0.185	A	8.5 0.202	+ 0.017 V/C
# 5 Erickson/6th	B	11.1 0.438	B	11.9 0.492	+ 0.054 V/C

Worst Case Midday

Level of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 66.4 Worst Case Level Of Service: F

Table with columns: Approach, Movement, Control, Rights, Lanes. Rows for North, South, East, West Bound movements and their respective controls and lane configurations.

Volume Module:

Table with columns: Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol. Rows for each movement direction.

Critical Gap Module:

Table with columns: Critical Gp, FollowUpTim. Rows for each movement direction.

Capacity Module:

Table with columns: Cnflct Vol, Potent Cap, Move Cap. Rows for each movement direction.

Level of Service Module:

Table with columns: Stopped Del, LOS by Move, Movement, Shared Cap, Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS. Rows for each movement direction.

Worst Case Midday

Level of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 16.8 Worst Case Level Of Service: C

Table with columns: Approach, Movement, Control, Rights, Lanes. Rows for North, South, East, West Bound movements and their respective controls and lane configurations.

Volume Module:

Table with columns: Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol. Rows for each movement direction.

Critical Gap Module:

Table with columns: Critical Gp, FollowUpTim. Rows for each movement direction.

Capacity Module:

Table with columns: Cnflct Vol, Potent Cap, Move Cap. Rows for each movement direction.

Level of Service Module:

Table with columns: Stopped Del, LOS by Move, Movement, Shared Cap, Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS. Rows for each movement direction.

Worst Case Midday

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 12.2 Worst Case Level Of Service: B

Table with columns for Approach (North, South, East, West Bound) and Movement (L, T, R). Rows include Control (Uncontrolled, Stop Sign), Rights (Include), and Lanes (0, 1).

Volume Module table with columns for Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol.

Critical Gap Module table with columns for Critical Gp, FollowUpTim.

Capacity Module table with columns for Cnflct Vol, Potent Cap., Move Cap.

Level Of Service Module table with columns for Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS.

Worst Case Midday

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Future Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.202  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 8.5  
Optimal Cycle: 0 Level Of Service: A

Table with columns for Approach (North, South, East, West Bound) and Movement (L, T, R). Rows include Control (Stop Sign), Rights (Include), Min. Green, and Lanes (0, 1).

Volume Module table with columns for Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Reduced Vol, PCE Adj, MLF Adj, Final Vol.

Saturation Flow Module table with columns for Adjustment, Lanes, Final Sat.

Capacity Analysis Module table with columns for Vol/Sat, Crit Moves, Delay/Veh, Delay Adj, AdjDel/Veh, LOS by Move, ApproachDel, Delay Adj, ApprAdjDel, LOS by Appr.

Worst Case Midday

Level of Service Computation Report  
2000 HCM 4-Way Stop Method (Future Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.492  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 11.9  
Optimal Cycle: 0 Level of Service: B

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Movement (L, T, R), Control (Stop Sign), Rights (Include), Min. Green, and Lanes.

Volume Module:

Table with 13 columns representing different traffic movements. Rows include Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Reduced Vol, PCE Adj, MLF Adj, and Final Vol.

Saturation Flow Module:

Table with 13 columns. Rows include Adjustment, Lanes, and Final Sat.

Capacity Analysis Module:

Table with 13 columns. Rows include Vol/Sat, Crit Moves, Delay/Veh, Delay Adj, AdjDel/Veh, LOS by Move, ApproachDel, Delay Adj, ApprAdjDel, and LOS by Appr.

-----  
 Worst Case PM Peak  
 -----

Scenario Report

Scenario: Worst Case PM

Command: Worst Case PM  
 Volume: Proposed PM  
 Geometry: Proposed  
 Impact Fee: Default Impact Fee  
 Trip Generation: PM  
 Trip Distribution: Default Trip Distribution  
 Paths: Default Paths  
 Routes: Default Routes  
 Configuration: Default Configuration

-----  
 Worst Case PM Peak  
 -----

Impact Analysis Report  
 Level Of Service

Intersection	Base		Future		Change in
	LOS	Veh C	LOS	Veh C	
# 1 Farmington/Erickson	F	73.3 0.000	F	76.9 0.000	+ 0.000 V/C
# 2 Farmington/Stott	B	14.8 0.000	B	14.9 0.000	+ 0.000 V/C
# 3 Erickson/2nd	B	11.4 0.000	B	11.5 0.000	+ 0.000 V/C
# 4 Stott/2nd	A	7.7 0.134	A	7.8 0.142	+ 0.007 V/C
# 5 Erickson/6th	B	12.3 0.536	B	12.7 0.556	+ 0.020 V/C

Worst Case PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #1 Farmington/Erickson

Average Delay (sec/veh): 76.9 Worst Case Level Of Service: F

Approach:	North Bound			South Bound			East Bound			West Bound					
Movement:	L	T	R	L	T	R	L	T	R	L	T	R			
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled					
Rights:	Include			Include			Include			Include					
Lanes:	1	0	0	0	1	0	0	1	1	0	1	0	2	0	0

Volume Module:															
Base Vol:	22	0	74	0	0	0	0	1167	107	87	1580	0	0	0	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	22	0	74	0	0	0	0	1167	107	87	1580	0	0	0	0
Added Vol:	1	0	5	0	0	0	0	2	1	3	3	0	0	0	0
PasserByVol:	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Initial Fut:	23	0	79	0	0	0	0	1169	108	90	1583	0	0	0	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
PHF Volume:	23	0	81	0	0	0	0	1193	110	92	1615	0	0	0	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	23	0	81	0	0	0	0	1193	110	92	1615	0	0	0	0

Critical Gap Module:															
Critical Gp:	6.9	xxxx	7.0	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	4.2	xxxx	xxxx	xxxx	xxxx	xxxx
FollowUpTim:	3.5	xxxx	3.3	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	2.2	xxxx	xxxx	xxxx	xxxx	xxxx

Capacity Module:															
Cnflct Vol:	2239	xxxx	652	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	1303	xxxx	xxxx	xxxx	xxxx	xxxx
Potent Cap.:	35	xxxx	406	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	517	xxxx	xxxx	xxxx	xxxx	xxxx
Move Cap.:	30	xxxx	406	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	517	xxxx	xxxx	xxxx	xxxx	xxxx

Level Of Service Module:															
Stopped Del:	285.7	xxxx	16.0	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	13.5	xxxx	xxxx	xxxx	xxxx	xxxx
LOS by Move:	F	*	C	*	*	*	*	*	*	B	*	*	*	*	*
Movement:	LT	-	LTR	-	RT	LT	-	LTR	-	RT	LT	-	LTR	-	RT
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	76.9		xxxxxx				xxxxxx			xxxxxx					
ApproachLOS:	F		*				*			*					

Worst Case PM Peak

Level Of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #2 Farmington/Stott

Average Delay (sec/veh): 14.9 Worst Case Level Of Service: B

Approach:	North Bound			South Bound			East Bound			West Bound												
Movement:	L	T	R	L	T	R	L	T	R	L	T	R										
Control:	Stop Sign			Stop Sign			Uncontrolled			Uncontrolled												
Rights:	Include			Include			Include			Include												
Lanes:	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	1	0	0	0	2	0	0

Volume Module:															
Base Vol:	0	0	18	0	0	0	0	1286	53	0	1718	0	0	0	0
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	0	0	18	0	0	0	0	1286	53	0	1718	0	0	0	0
Added Vol:	0	0	4	0	0	0	0	4	2	0	5	0	0	0	0
PasserByVol:	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Initial Fut:	0	0	22	0	0	0	0	1290	55	0	1723	0	0	0	0
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
PHF Volume:	0	0	22	0	0	0	0	1316	56	0	1758	0	0	0	0
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Final Vol.:	0	0	22	0	0	0	0	1316	56	0	1758	0	0	0	0

Critical Gap Module:															
Critical Gp:	xxxx	xxxx	7.0	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
FollowUpTim:	xxxx	xxxx	3.3	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx

Capacity Module:															
Cnflct Vol:	xxxx	xxxx	686	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Potent Cap.:	xxxx	xxxx	385	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Move Cap.:	xxxx	xxxx	385	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx

Level Of Service Module:															
Stopped Del:	xxxx	xxxx	14.9	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
LOS by Move:	*	*	B	*	*	*	*	*	*	*	*	*	*	*	*
Movement:	LT	-	LTR	-	RT	LT	-	LTR	-	RT	LT	-	LTR	-	RT
Shared Cap.:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shrd StpDel:	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx	xxxx
Shared LOS:	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
ApproachDel:	14.9		xxxxxx				xxxxxx			xxxxxx			xxxxxx		
ApproachLOS:	B		*				*			*			*		

Worst Case PM Peak

Level of Service Computation Report  
2000 HCM Unsignalized Method (Future Volume Alternative)

Intersection #3 Erickson/2nd

Average Delay (sec/veh): 11.5 Worst Case Level Of Service: B

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Approach, Movement, Control, Rights, Lanes.

Volume Module:

Table with 12 columns for traffic volumes. Rows include Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Final Vol.

Critical Gap Module:

Table with 12 columns for critical gap values. Rows include Critical Gp, FollowUpTim.

Capacity Module:

Table with 12 columns for capacity values. Rows include Cnflct Vol, Potent Cap., Move Cap.

Level Of Service Module:

Table with 12 columns for level of service metrics. Rows include Stopped Del, LOS by Move, Movement, Shared Cap., Shrd StpDel, Shared LOS, ApproachDel, ApproachLOS.

Worst Case PM Peak

Level of Service Computation Report  
2000 HCM 4-Way Stop Method (Future Volume Alternative)

Intersection #4 Stott/2nd

Cycle (sec): 100 Critical Vol./Cap. (X): 0.142

Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 7.8

Optimal Cycle: 0 Level Of Service: A

Table with 4 columns: North Bound, South Bound, East Bound, West Bound. Rows include Approach, Movement, Control, Rights, Min. Green, Lanes.

Volume Module:

Table with 12 columns for traffic volumes. Rows include Base Vol, Growth Adj, Initial Bse, Added Vol, PasserByVol, Initial Fut, User Adj, PHF Adj, PHF Volume, Reduct Vol, Reduced Vol, PCE Adj, MLF Adj, Final Vol.

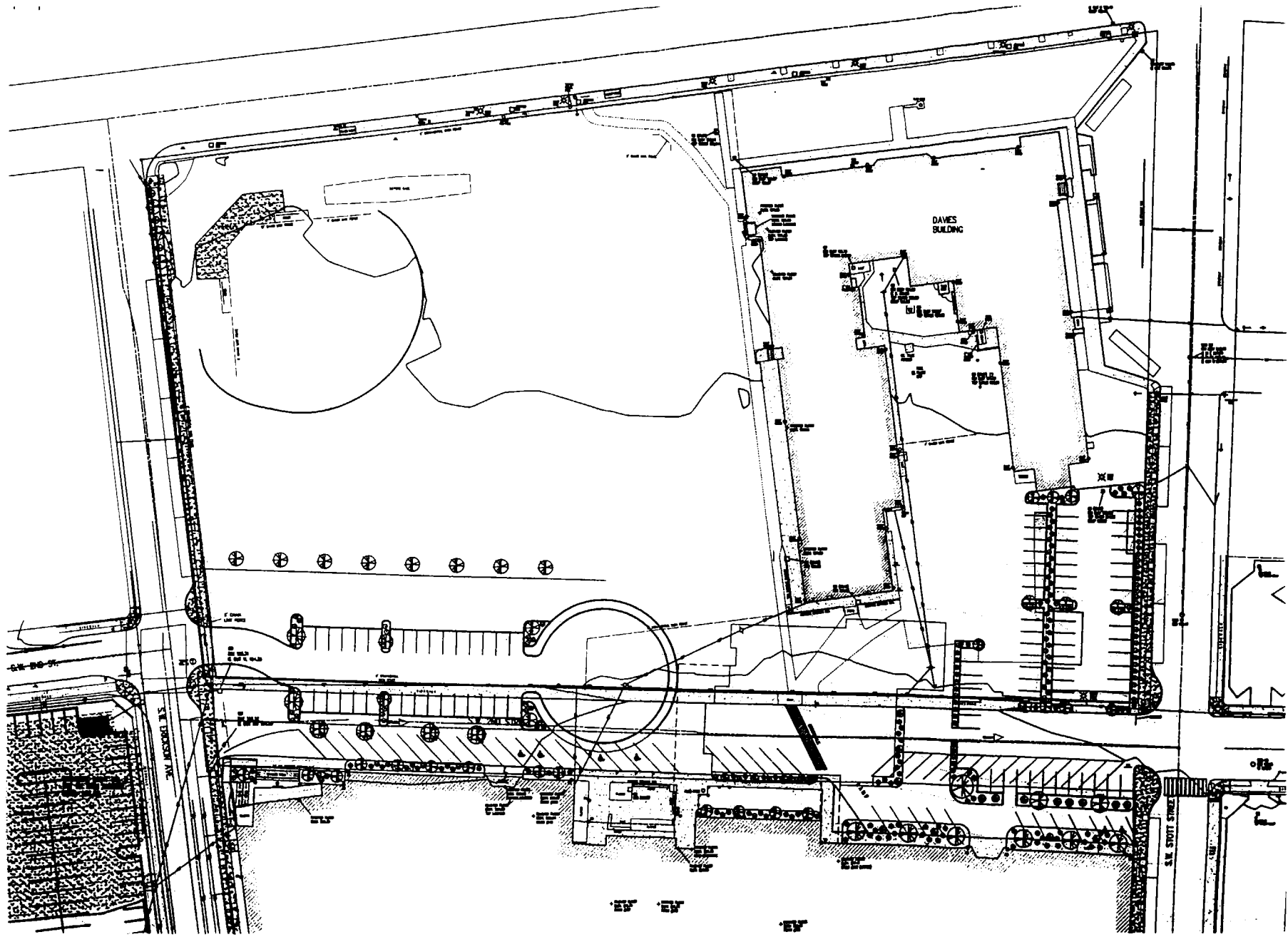
Saturation Flow Module:

Table with 12 columns for saturation flow values. Rows include Adjustment, Lanes, Final Sat.

Capacity Analysis Module:

Table with 12 columns for capacity analysis metrics. Rows include Vol/Sat, Crit Moves, Delay/Veh, Delay Adj, AdjDel/Veh, LOS by Move, ApproachDel, Delay Adj, ApprAdjDel, LOS by Appr.



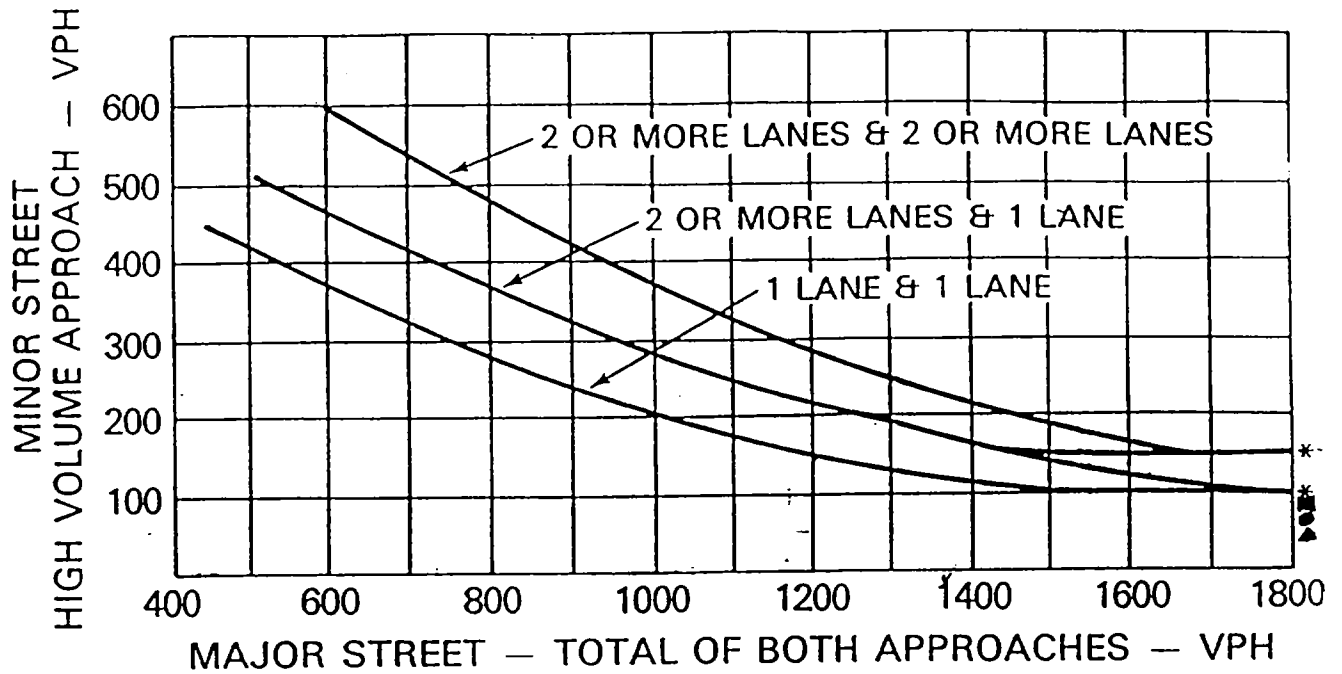


# Signal Warrants

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# Signal Warrant Analysis

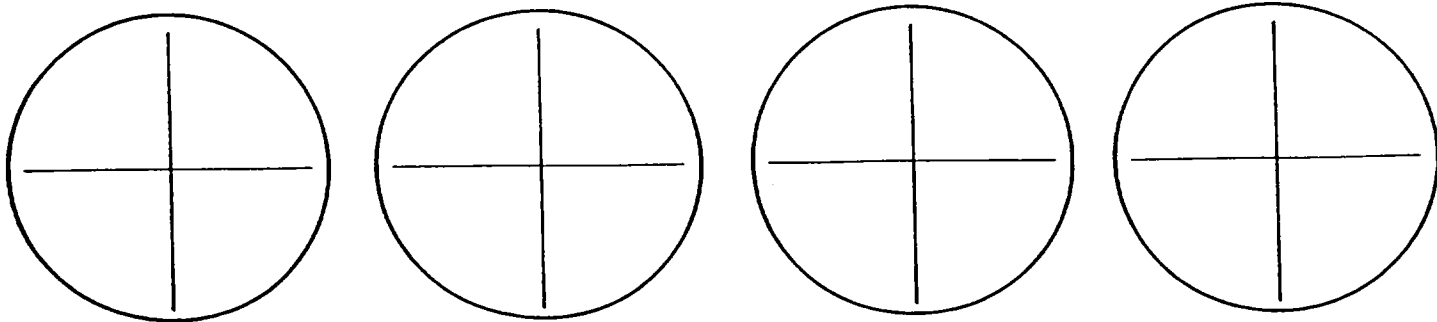
## FIGURE 4-5. PEAK HOUR VOLUME WARRANT



\*NOTE: 150 VPH APPLIES AS THE LOWER THRESHOLD VOLUME FOR A MINOR STREET APPROACH WITH TWO OR MORE LANES AND 100 VPH APPLIES AS THE LOWER THRESHOLD VOLUME FOR A MINOR STREET APPROACHING WITH ONE LANE.

Symbol	Intersection	Major Volume	Minor * Volume	Warrant Met
■	Farmington / Erickson (Am)	2,240	98	No
●	" " (Mid)	2,465	71	No
▲	" " (Pm)	2,950	63	No
⊙				

### Existing Geometries



### Notes:

\* Minor volume includes a 50% reduction for right turns.

1400 SW 5<sup>th</sup> Avenue, Suite 500  
Portland, OR 97201  
Phone: (503) 243-3500  
Fax: (503) 243-1934

## MEMORANDUM

**TO:** Frank Angelo, Angelo Eaton

**FROM:** Carl S. Springer, PE  
Scott Mansur

**DATE:** January 25, 2002

**SUBJECT:** 5<sup>th</sup> Street/Erickson Avenue Level of Service Summary

P01294

In the Beaverton High School 2<sup>nd</sup> Street Vacation traffic impact study<sup>1</sup> the intersection of 5<sup>th</sup> Street and Erickson Avenue would go from a level of service "D" to a level of service "E" in the AM peak hour during the worst case scenario. It has come to our attention that level of service "E" would not meet the City's level of service standards for unsignalized intersections (Beaverton Development Code 60.60.25.). This memorandum provides further discussion on the impacts to the 5<sup>th</sup> Street/Erickson Avenue intersection during the AM peak period.

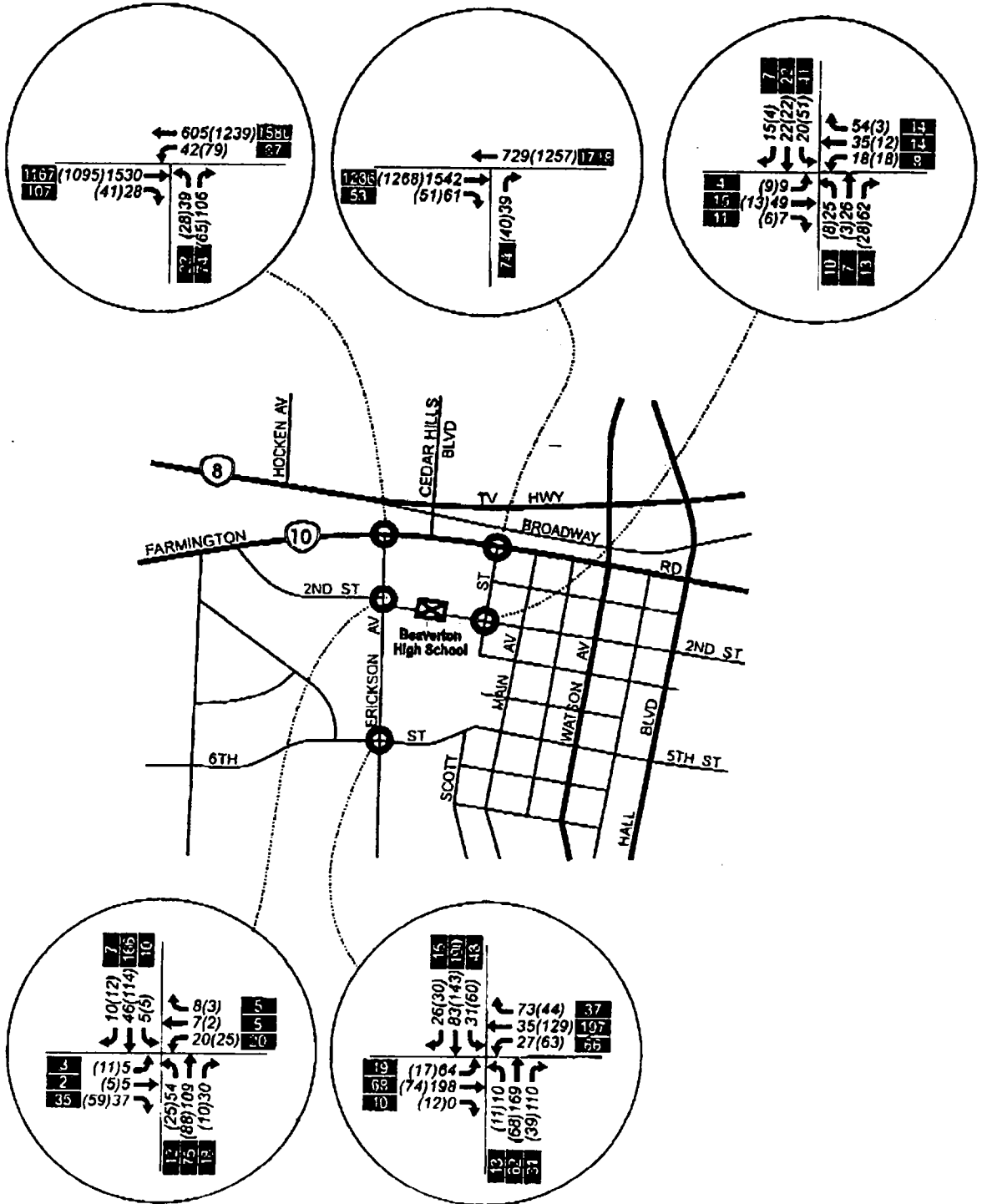
After evaluating the level of service analysis for the subject intersection in the report, we noticed a mistake in our intersection volumes at this intersection. The existing counts at this intersection showed a southbound right turn volume of 14 in the AM peak hour. If 2<sup>nd</sup> Street were vacated, redistributed traffic would add 8 additional AM peak trips to this movement. Our analysis added 73 additional trips rather than 8. These added trips showed a worst level of service for the 2<sup>nd</sup> Street vacation and the worst case future scenarios. Updated intersection analysis is shown in table 1. Updated figures 4 and 6 and intersection capacity analysis from the traffic study and are attached.

**Table 1: Revised Level of Service Results for 5<sup>th</sup>-6<sup>th</sup> Street/Erickson Avenue**

Intersection	AM Peak		
	Delay	LOS	V/C
Existing with 2 <sup>nd</sup> Street Vacation			
5 <sup>th</sup> -6 <sup>th</sup> Street/Erickson Ave	22.1	C	0.79
Existing with 2 <sup>nd</sup> Street Vacation with full student capacity (Worst Case)			
5 <sup>th</sup> -6 <sup>th</sup> Street/Erickson Ave	26.7	D	0.87

The 5<sup>th</sup>-6<sup>th</sup> Street/Erickson Avenue would meet the City's level of service "D" standard for all scenarios based on the revised intersection volumes. Please call if you have any questions.

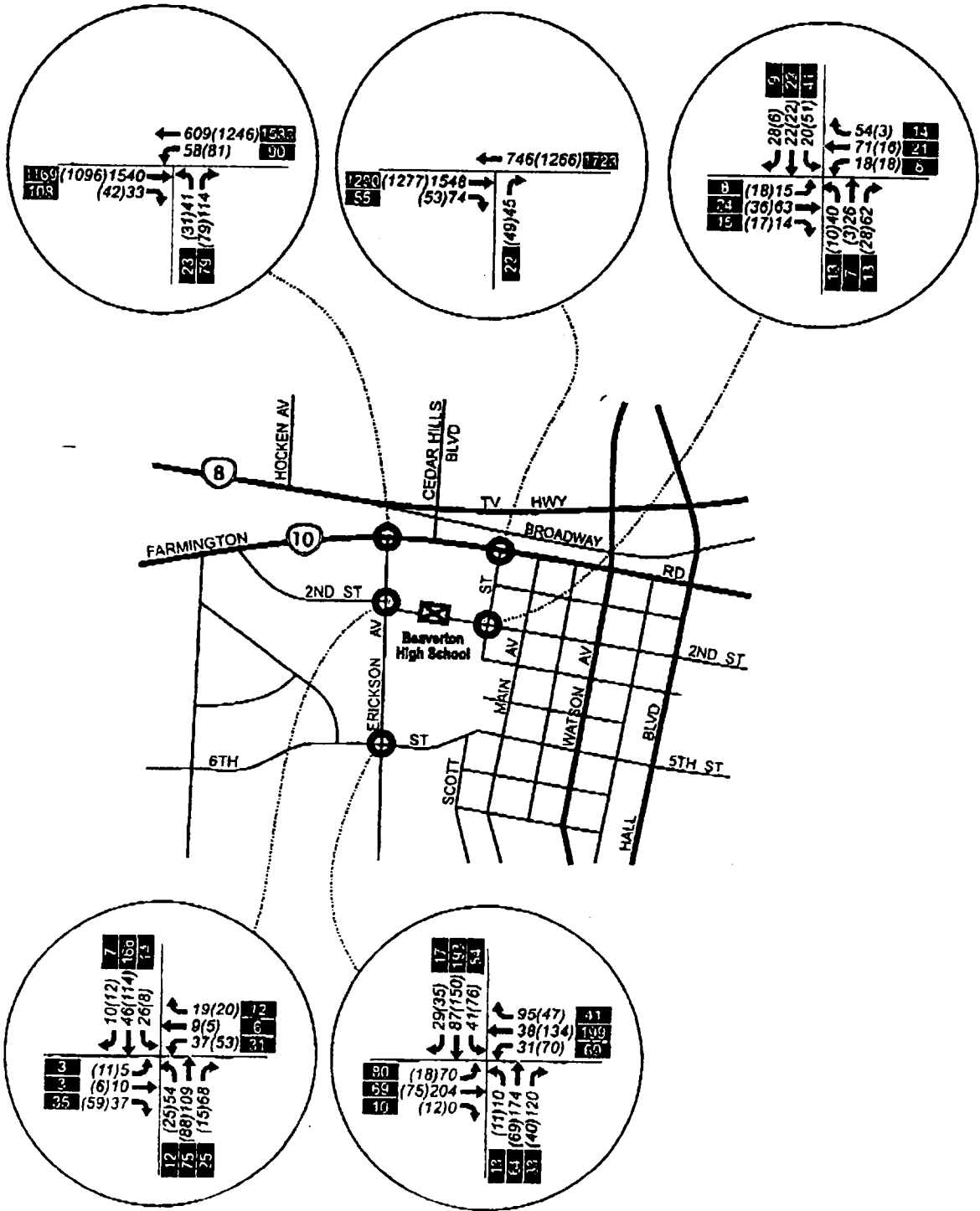
<sup>1</sup> Beaverton High School 2<sup>nd</sup> Street Vacation Traffic Impact Study, DKS Associates, December 2001.



**LEGEND**

- Study Intersection
- AM | Mid | PM - Peak Hour Traffic Volume
- Mid = Mid-day

**Figure 4**  
**EXISTING WITH STREET VACATION**  
**PEAK HOUR TRAFFIC VOLUMES**



**LEGEND**

- Study Intersection
- AM(Mid) PM - Peak Hour Traffic Volume
- Mld= Mid-day

**Figure 6**  
**WORST CASE WITH STREET VACATION AND**  
**MAXIMUM STUDENT CAPACITY**  
**PEAK HOUR TRAFFIC VOLUMES**

Worst Case AM Peak

Level Of Service Computation Report  
2000 HCM 4-Way Stop Method (Future Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.907  
Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 31.7  
Optimal Cycle: 0 Level Of Service: D

Approach:	North Bound			South Bound			East Bound			West Bound		
Movement:	L	T	R	L	T	R	L	T	R	L	T	R
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1	0	0	1	0	1	0	0	0	1

Volume Module:	North Bound			South Bound			East Bound			West Bound		
Base Vol:	10	169	110	31	83	26	64	198	0	27	35	73
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	10	169	110	31	83	26	64	198	0	27	35	73
Added Vol:	0	10	10	10	4	3	6	6	0	4	3	22
PasserByVol:	0	0	0	0	0	0	0	0	0	0	0	0
Initial Fut:	10	179	120	41	87	29	70	204	0	31	38	95
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65
PHF Volume:	15	275	185	63	134	45	108	314	0	48	58	146
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	15	275	185	63	134	45	108	314	0	48	58	146
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	15	275	185	63	134	45	108	314	0	48	58	146

Saturation Flow Module:	North Bound			South Bound			East Bound			West Bound		
Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.03	0.58	0.39	0.26	0.55	0.19	0.26	0.74	0.00	0.19	0.23	0.58
Final Sat.:	17	303	204	114	243	82	126	366	0	86	104	263

Capacity Analysis Module:	North Bound			South Bound			East Bound			West Bound		
Vol/Sat:	0.91	0.91	0.91	0.55	0.55	0.55	0.86	0.86	XXXX	0.56	0.56	0.56
Crit Moves:	****			****			****			****		
Delay/Veh:	42.5	42.5	42.5	17.6	17.6	17.6	36.2	36.2	0.0	17.3	17.3	17.3
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	42.5	42.5	42.5	17.6	17.6	17.6	36.2	36.2	0.0	17.3	17.3	17.3
LOS by Move:	E	E	E	C	C	C	E	E	*	C	C	C
ApproachDel:	42.5			17.6			36.2			17.3		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	42.5			17.6			36.2			17.3		
LOS by Appr:	E			C			E			C		

Proposed AM Peak

Level Of Service Computation Report  
 2000 HCM 4-Way Stop Method (Future Volume Alternative)

Intersection #5 Erickson/6th

Cycle (sec): 100 Critical Vol./Cap. (X): 0.794  
 Loss Time (sec): 0 (Y+R = 4 sec) Average Delay (sec/veh): 22.1  
 Optimal Cycle: 0 Level Of Service: C

Approach:	North Bound			South Bound			East Bound			West Bound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement:												
Control:	Stop Sign			Stop Sign			Stop Sign			Stop Sign		
Rights:	Include			Include			Include			Include		
Min. Green:	0	0	0	0	0	0	0	0	0	0	0	0
Lanes:	0	0	1	0	0	1	0	1	0	0	0	1

Volume Module:												
Base Vol:	10	169	110	31	83	26	64	198	0	27	35	73
Growth Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Initial Bse:	10	169	110	31	83	26	64	198	0	27	35	73
Added Vol:	0	0	0	0	0	0	0	0	0	0	0	0
PasserByVol:	0	0	0	0	0	0	0	0	0	0	0	0
Initial Fut:	10	169	110	31	83	26	64	198	0	27	35	73
User Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
PHF Adj:	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65	0.65
PHF Volume:	15	260	169	48	128	40	98	305	0	42	54	112
Reduct Vol:	0	0	0	0	0	0	0	0	0	0	0	0
Reduced Vol:	15	260	169	48	128	40	98	305	0	42	54	112
PCE Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
MLF Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Final Vol.:	15	260	169	48	128	40	98	305	0	42	54	112

Saturation Flow Module:												
Adjustment:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Lanes:	0.03	0.59	0.38	0.22	0.59	0.19	0.24	0.76	0.00	0.20	0.26	0.54
Final Sat.:	19	328	213	105	280	87	129	400	0	96	123	256

Capacity Analysis Module:												
Vol/Sat:	0.79	0.79	0.79	0.46	0.46	0.46	0.76	0.76	xxxx	0.44	0.44	0.44
Crit Moves:	****			****			****			****		
Delay/Veh:	26.8	26.8	26.8	14.3	14.3	14.3	25.3	25.3	0.0	13.7	13.7	13.7
Delay Adj:	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
AdjDel/Veh:	26.8	26.8	26.8	14.3	14.3	14.3	25.3	25.3	0.0	13.7	13.7	13.7
LOS by Move:	D	D	D	B	B	B	D	D	*	B	B	B
ApproachDel:	26.8			14.3			25.3			13.7		
Delay Adj:	1.00			1.00			1.00			1.00		
ApprAdjDel:	26.8			14.3			25.3			13.7		
LOS by Appr:	D			B			D			B		





TRI-COUNTY METROPOLITAN  
TRANSPORTATION DISTRICT OF OREGON

CAPITAL PROJECTS & FACILITIES DIVISION  
710 N.E. HOLLADAY STREET  
PORTLAND, OREGON 97232

EXHIBIT 6

DocC #CP012682

February 15, 2002

Sambo Kirkman  
City of Beaverton  
PO Box 4755  
Beaverton, OR 97076

RECEIVED  
FEB 15 2002  
COMMUNITY DEVELOP DEPT.

**RE: Beaverton HS library and parking lot improvements**

Dear Ms. Kirkman:

Tri-Met welcomes this opportunity to comment on the proposed improvements to Beaverton HS on Farmington Road. Tri-Met line 52 serves the area with bus stops located near the site on Farmington between Erickson and Stott.

The purpose of our recommendations is to minimize the traffic impacts of new development by encouraging development patterns that are transit, bicycle, and pedestrian supportive. Improved pedestrian access and connection promotes walking and reduces local dependence on automobiles.

TRI-MET STAFF RECOMMENDATIONS

Amenity Requirements

**Provide a transit plaza.** Frank Angelo is working with Tri-Met to design bus stop facilities and a connecting pedestrian infrastructure that best serves the high school, its students, and the immediate community. Transit amenities and bus stop sponsorship opportunities are also being discussed and Tri-Met is confident that the resulting improvements will be beneficial to local transit patrons and a positive contribution to the streetscape.

Thank you for the opportunity to comment on this proposal. If you have any questions, please contact me at 962-2140.

Sincerely,

*Ben Baldwin*  
cc: Ben Baldwin  
Planner/Bus Stops Group

cc: Jillian Detweiler



620 SW Main, Suite 201  
Portland, Oregon 97205

Tel: 503 . 224 . 6974  
Fax: 503 . 227 . 3679

March 21, 2002

Sambo Kirkman  
City of Beaverton  
Planning Department  
4755 SW Griffith Drive  
P.O. Box 4755  
Beaverton, Oregon 97076

RE: Beaverton High School Tree Preservation Plan (TPP 2001-0008)  
Beaverton High School Board of Design Review (BDR 2001-0213)  
Beaverton High School Conditional Use Permit (CUP 2001-0031)

As the representative for the Beaverton School District, I request that the public hearings on the above applications be continued as noted below. We will waive the 120-day limit on all three applications to a date certain - June 18, 2002. According to staff, this will provide sufficient time for the respective hearing bodies to conduct the hearings, render a decision and cover any potential appeals.

Beaverton High School Conditional Use Permit (CUP 2001-0031)

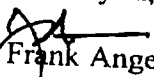
Based on our request at the March 20, 2002 Planning Commission meeting, this hearing has been continued to April 24, 2002.

Beaverton High School Tree Preservation Plan (TPP 2001-0008)

We are requesting a continuance of the March 21, 2002 hearing to April 30, 2002. You indicated that the next meeting of the Historic Review Committee is scheduled for April 30, 2002. It is my understanding that TPP 2001-0008 will be scheduled on that date.

Beaverton High School Board of Design Review (BDR 2001-0213)

We are requesting a continuance of the March 28, 2002 hearing to May 9, 2002. You indicated that there is room on the May 9<sup>th</sup> agenda for this application.

Thank you,  
  
Frank Angelo

cc. Mike Maloney, Beaverton School District

- Frank Angelo
- Chris Eaton, AICP
- Catherine Corliss, AICP
- Ivan D'Agostino, AICP
- Karen Siegel



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## APPLICATION COMPLETENESS OPTION

TO: City of Beaverton

FROM: CITY OF BEAVERTON, DEVELOPMENT SERVICES DIVISION

RE: CASE FILE #: BDR #20010213; TPP 2001-0008; CUP 2001-0031

The Facilities Review Committee finished its completeness review of the referenced application(s) on \_\_\_\_\_, and determined that the application is incomplete, as identified in the letter from \_\_\_\_\_ dated \_\_\_\_\_. Per Oregon Revised Statutes (ORS) 227.178, the following two options are available to proceed with this application.

Please check the appropriate box, sign and date this form and return this completed form to the City of Beaverton, Development Services Division, no later than \_\_\_\_\_. The form may be hand delivered, mailed or faxed.

### CITY OF BEAVERTON

**By Mail:**  
Development Services Division  
Information Counter  
PO Box 4755  
Beaverton, OR 97076

**By Hand Delivery:**  
Development Services Division  
Information Counter  
4755 SW Griffith Drive  
Beaverton, OR 97076

**By Fax:** 503-526-3720

**TEL:** 503-526-2420

**Option 1:**

I, as the applicant or with authority to act on behalf of the applicant(s), hereby request to have the above application processed "as is"; I refuse to submit the remaining required information.

**Option 2:**

I, as the applicant or with authority to act on behalf of the applicant(s), hereby state that some or all of the remaining information required to make the above application complete will be submitted. I understand that the required information must be submitted within 180 days of the date the application was first submitted or the application will be retired without a decision.

Dated January 30, 2002

Signature [Handwritten Signature]

Name (Please Print) Frank Angelo



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

January 18, 2002

Frank Angelo  
Angelo, Eaton and Associates  
620 SW Main  
Suite 201  
Portland, OR 97205

RE: CUP2001-0031 (Beaverton High School Cafeteria & Parking Lot)

Dear Mr. Angelo:

Thank you for submitting the Beaverton High School Conditional Use Permit, on behalf of the Beaverton School District. The Facilities Review Committee finished its completeness review on January 9, 2002 and determined your application deemed incomplete for the following reasons:

**COMMUNITY DEVELOPMENT DEPT. (CDD):  
Conditional Use Permit - Public Hearing Application**

The Conditional Use Permit Checklist requires submission of elements that were not included with your application submittal. The following items must be addressed and submitted in order for this application to be deemed complete:

1. **Existing Conditions Plan:** Please indicate which trees are classified as Significant or Historic, as established by the City of Beaverton's inventory of significant or historical trees.
2. **Dimensioned Site Plan:** Please indicate which trees are classified as Significant or Historic, as established by the City of Beaverton's inventory of significant or historical trees. Also, label existing trees, larger than 6" dbh, proposed to be saved (include genus, species, and size).
3. **Architectural Elevations:** Please provide information that identifies the general character by indicating dimensions, material, color (including manufacture's designation), and proposed textures.
4. **Hours of Operation:** Please indicate the high school hours of operation. Also include all extra curricular school and community activities and their hours of operation.

5. **Approval Criteria Findings:** Please describe how the Conditional Use Permit complies with the applicable provisions of City of Beaverton Ordinance 2050. Address how the proposed project will comply with each related section and sub-section of the Comprehensive Plan.
6. **Bicycle Parking:** No bicycle parking has been indicated on the plan. Provide a summary of the number of existing and proposed bicycle parking spaces based on the current and projected building square footages and number of students. The Development Code requires secure bicycle parking to accommodate long-term users (Development Code Section 60.60.65.4 and 60.20.10). Required long-term space bicycle parking for a high school is a minimum of 1 space per 18 students.
7. **Parking Standards:** The parking demand was calculated based on the existing student population of 1800 students or 390 parking spaces. The narrative indicates that the campus facilities are being expanded to accommodate 2200 students, therefore needing 480 spaces. The plan needs to indicate how and where the 480 spaces are being accommodated and if not what mitigation measures are being proposed. With the present parking shortage at the campus, the staff believes this is an important completeness issue. Alternatively a Parking Variance will be required
8. **Traffic Impact Analysis:** The proposed construction of building additions and additional car parking spaces will generate traffic (vehicle trips per day) in excess of the minimum threshold requirement for a Traffic Impact Analysis, therefore a Traffic Impact Analysis will be required to be submitted (Development Code 60.60.10.7). The analysis should address the impact of the additional student trips and additional school bus trips, and how these new trips and the existing trips will access the site and circulate within the site. The study must be signed by a professional engineer, submitted and accepted by City staff before the application will be considered complete. The traffic analysis received from the School District for the SW 2<sup>nd</sup> Street vacation and CPA is not sufficient for this plan review. It did not indicate the number of existing and proposed school buses and how the bus circulation was to be handled.

When you are ready to re-submit your application, please provide three (3) copies each of the written narrative, reports, and folded complete plan sets bound. The materials provided should be collated into three submittal packages. Additional copies will be required at a later time when your project has been scheduled for final review and processing. Additionally, please provide one (1) letter/memorandum addressing the specific items requested in this letter so that we might better understand the materials that you submit.

One set of the original application materials is kept on file at the Development Services Division. At the time of a future application, we can provide the information on file to assist you in preparing your materials. For information about application requirements, forms, fees and schedules, please contact the Development Services Counter at 503-526-2420.

If you have any questions regarding this letter or any other aspect of our process, please don't hesitate to call. I am including a list of the primary members of the Facilities Review Committee who were involved in the completeness review.

LAND USE & DESIGN: Jeff Caines – 503-526-2419

TRANSPORTATION PLANNING: Don Gustafson – 503-350-4057

SITE ENGINEERING: Jim Duggan – 503-526-2442

Thank you and we look forward to working with you to complete your application.

Sincerely,



Jeff Caines  
Assistant Planner

cc: (5): Lampa; Caines (2); Counter; Dept. file

\*\*\*\*\*  
\*\*\* TX REPORT \*\*\*  
\*\*\*\*\*

TRANSMISSION OK

TX/RX NO 4000  
CONNECTION TEL 95032273679  
SUBADDRESS  
CONNECTION ID  
ST. TIME 01/18 10:25  
USAGE T 03'12  
PGS. SENT 10  
RESULT OK



**CITY OF BEAVERTON**  
**COMMUNITY DEVELOPMENT DEPARTMENT**  
**P.O. BOX 4755**  
**BEAVERTON, OR 97076-4755**

**FAX TRANSMITTAL FORM**

TO: Robin Scholetzky

FAX: 503-227-3679

DATE: January 18, 2002

#PGS: 10 (INCLUDING COVER SHEET)

FROM: Jeff Caines / Development Services Division

PHONE: (503) 526-2419 FAX: (503) 526-3720

RE: Beaverton High School

COMMENTS:

C. U. ... Have a good weekend



# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

October 8, 2001

Frank Angelo  
620 SW Main Street, Suite 201  
Portland, OR. 97205

RE: File: PA 2001-0060 – Beaverton High School Expansion  
Pre-Application Conference on August 21, 2001  
Tax Map: 1S1-16AD; Tax Lots: 10900 and 11100  
Zoned: Single Family Urban Density R-10

Dear Mr. Angelo:

Thank you for participating in a pre-application conference with City staff. We hope the information presented will be useful as you prepare your development plans. This letter addresses the issues that were covered in your pre-application conference of August 21, 2001. Please be aware that the comments have been prepared without benefit of a site visit; therefore, particular site conditions or constraints may exist that staff is not aware of. If you have questions, please direct them to the person identified within particular comments in this letter.

## A. COMMUNITY DEVELOPMENT DEPT. (CDD):

Prepared by Colin Cooper - (503) 526-2425 or [ccooper@ci.beaverton.or.us](mailto:ccooper@ci.beaverton.or.us)

### PROPOSAL:

The applicant is considering a proposal to expand the Beaverton High School in two phases. The first phase consists of an approximately 30,935 square foot cafeteria building to the north of the existing building. Phase 2 would include the development of a two story approximately 59,032 square foot proposed classroom building. In order to locate the proposed buildings to the north of the existing building the School District is proposing to vacate 2<sup>nd</sup> Street.

### IDENTIFIED ISSUES:

1. Street Vacation. There are a number of important issues that will require review and analysis for the proposed vacation of SW 2<sup>nd</sup> Street. As described in



Comprehensive Plan Section 6.2, Transportation Goals and Policies, and Development Code Section 60.60.35, the City has specific multi-modal connectivity standards. The vacation of the 2<sup>nd</sup> Street right-of-way will remove the only public right-of-way between SW Farmington Road and 5<sup>th</sup> Street. Your application for a street vacation will need to demonstrate that the vacation is consistent with these goals and policies.

2. **Historic Structure revisions and demolition.** The proposed expansion of the High School includes both revisions and ultimately the demolition of the Merle Davies school building. As discussed at the pre-application conference this structure and surrounding trees are identified as historic. Any revision of the building or removal of the trees must be justified in a manner that illustrates that there are no feasible alternatives.
3. **Timing of Land Use Review.** As discussed in greater detail later in this report the City is willing to process the Street Vacation concurrently with the other identified land use applications. Staff will direct that the first set of public hearing will be with the Planning Commission and City Council for the Street Vacation. This action will be followed by the Historic Resource Review Committee, then the Planning Commission and Board of Design Review.
4. **Building Design.** Based on the proposed revisions and removal of the Merle Davies school building staff strongly recommend that the architect for the District incorporate architectural elements and materials into the design of the new structure. Specifically, by replicate the style and rhythm of the windows on the new building the visual impact for the loss of the historic building will be reduced.
5. **Bus Circulation.** Several possible bus pick-up and drop-off scenario's where discussed during the pre-application conference. Staff suggest that a system that provides for as little conflict as possible with parking and public circulation be designed. It appears that a bus loading facility on the south side of the building may provide the least impact. It is critical to include a complete analysis of the bus circulation design in the application.
6. **Building Programming.** The District shall describe the full projected enrollment of the high school and all school and community related activities. For example, the use of the high school by the Portland Community College, community theatre groups and any other community groups. Please include hours of operation and overall use figures.

The Transportation issues discussed at the pre-application conference are identified later on within this report under Transportation Division section.

The Site Development issues discussed at the pre-application conference are identified later on within this report under Site Development Engineering section.

## **CLEAN WATER SERVICES**

The Clean Water Services (CWS) is the agency that regulates sanitary and storm water within Washington County and the City of Beaverton. CWS has adopted a Resolution & Order (R&O 00-007) that requires an applicant who is proposing development to obtain a site assessment "SERVICE PROVIDER LETTER" from CWS. Pursuant to Development Code Section 50.10 the applicant is required to submit the CWS service provider letter in order for their application to be deemed complete. For more information regarding "Service Provider Letters" contact Ms. Heidi Berg, Site Assessment Coordinator, at (503) 846-3613 or [bergh@cws-cleanwater.org](mailto:bergh@cws-cleanwater.org).

Please note that Facilities Review Committee could not make a positive recommendation towards this proposal without this letter.

## **ZONING:**

The site is within the Urban Low Density "R-10" Zoning District. The proposed expansion of Beaverton High School is permitted or requires a Conditional Use, per Section 20.05.10.2.B.5. Therefore, this application is subject to a public hearing in front of the Planning Commission.

The City's Inventory of Significant Trees identifies Significant Tree #34 located near the corner of 2<sup>nd</sup> and Stott Avenue. In addition, the City's Inventory of Historic Trees identifies all the trees located between Farmington and 5<sup>th</sup> Avenue and Erickson and Stott as Historic Trees.

The City's Inventory of Historic Resources identifies the Merle Davis building as a historic structure. Therefore, any alteration or proposed demolition of this building would require review by the City's Historic Resource Review Committee.

New structures on a site require a Design Review Type 3 application. The material presented to staff during the course of the pre-application conference indicates that your application would be processed as a Design Review Type 3.

The site requirements in the Urban Low Density R-10 zoning district are as follows:

### **MINIMUM LOT DIMENSIONS:**

#### **Width:**

Corner Lots	90-feet
Interior Lots	80-feet

#### **Depth:**

Corner Lots	110-feet
Interior Lots	120-feet

<u>SETBACKS:</u>	<u>Standard</u>	<u>With Flexible Setback</u>
Front Yard:		
Dwelling	25-feet	10-feet
Garage	25-feet	20-feet
Side Yard:		
Dwelling	9-feet	5-feet
Garage	20-feet	20-feet
Rear Yard:		
Dwelling	25-feet	5-feet
Garage	22-feet	20-feet

Maximum Building Height: 30-feet  
(without a CUP)

Landscape Area required: 15% of lot

Parking:

The parking requirements can be found in Section 60.20.10.05 of the development code. Please note that the City has adopted minimum and maximum parking requirements. The maximum parking for your site is based on Zone A. The minimum and maximum parking requirement is as follows:

<u>USE</u>	<u>Minimum Spaces</u>	<u>Maximum Spaces</u>
Educational Institutions: High School (Spaces / number of FTE students and staff)	0.2	0.3

Please be aware of **Section 60.20.15**, which regulates the construction and dimensions of parking lots. Specifically, the requirement that all-parking areas are at least 6-feet from the property line. In addition to these design standards, please note the Board of Design Review has adopted a standard that requires a parking lot landscape island, which has a width and length of a parking stall be located for every 12 spaces, and be planted with a canopy tree.

In addition to vehicular parking, the proposal will need to address bicycle parking. Bicycle parking includes both short term and long-term parking.

<u>USE</u>	<u>Short Term Spaces</u>	<u>Long Term Spaces</u>
Educational Institutions:  High School (Spaces staff)	None	1 for every 18 students

### **NEIGHBORHOOD REVIEW MEETING:**

A Neighborhood Review Meeting (NRM) is required prior to application submittal by the Development Code Section 50.10, for all projects that appear before the Board of Design Review, Planning Commission, or City Council. Your application will not be deemed complete until satisfactory evidence that the meeting was conducted is presented to the City. Specific information with regard to this requirement was given to you at the Pre-Application Conference. The site is within the Central Beaverton Neighborhood Association Committee (NAC). Therefore, please coordinate with them for your Neighborhood Review Meeting. It should be noted that the NRM is only valid for 6 months.

### **APPLICATIONS:**

Your proposed development will require three separate application process prior to construction; 1) Land Use Permit(s); 2) Site Development Permit; and, 3) Building Permit(s). All three of these permits may be applied for concurrently with the provision that the Site Development Permit and Building Permit(s) will not be issued until all Land Use applications have been approved.

#### Land Use Applications

1. Street Vacation
2. Design Review Type 3
3. Historic Review
4. Conditional Use Permit (CUP)

The Street Vacation requires public hearings before both the Planning Commission and City Council. Please be aware that the Street Vacation requires two readings of The CUP requires a public hearing before the Planning Commission, the Design Review Type 3 requires a public hearing before the Board of Design Review, and the Historic Review requires a public hearing before the Historic Review Committee.

Normally, the City requires that a Street Vacation decision be rendered and complete prior to accepting any application that is predicated on the use of a public right-of-way. However, in this case the City is willing to accept and process all the applications simultaneously based on several conditions. First, the City requires that the applicant waive the 120-day rule for the remaining applications. Secondly, the applicant shall provide in writing a hold harmless agreement and assume all risks arising out of processing these applications simultaneously. Third, that the applicant understand that City Attorney will not take an active role in defending this decision should it be appealed to LUBA on the grounds that the application processing was not handled in the proper sequential manner.

Fees:

Street Vacation	\$830
CUP-Hearing	\$1,199
Design Review Type 3	\$1,662
Historic Review	\$286
Tree Preservation Plan	\$1,986
Historic Tree Removal	\$1,986

Fees are valid until June 30, 2002.

**PROCESS:**

Please note that each application is separate and distinct and can not be combined in one submittal.

Street Vacation

Except as otherwise specifically provided for in Beaverton Code Section 9.03.010-.060, provisions applicable to vacation set forth in ORS 271.080-.230 shall apply to any vacation by the City. Any alternative procedures allowed by State law may be followed.

When the city recorder has checked the petition for sufficiency as provided by ORS 271.090 the city recorder shall refer the petition to the planning commission, the city engineer or the police chief for reports and recommendations on the proposed vacation.

On receiving the reports and recommendations, the city recorder shall file the petition for vacation and forward the petition, reports and recommendation to the Council for preliminary consideration as provided by ORS 271.100.

Note that each franchised utility authorized to do work in the City rights-of-way must concur with any vacation request. A letter from each franchises utility is required prior to street vacation approval.

When the Council is petitioned to vacate a street, public place or plat in which water mains, fire hydrants, police or fire alarm system, gas mains, steam heating mains, conduits, sewer mains or laterals, manhole structures, poles, cables, wires or other utility or public service facilities are located, and the proposed vacation will require the removal of the utility or public service facilities or a portion of them, or if curbs or sidewalks are required to be extended or relocated, or if grading or additional paving is required, the ordinance vacating the street or part of it may provide that the vacation shall not be effective unless the petitioner files with the city recorder the petitioner's acceptance of the terms and provisions of the ordinance together with a surety bond or cash deposit. The amount of the surety bond or cash deposit shall be fixed by the Council.

The surety bond or cash deposit shall provide that if the vacation is granted, petitioner will, within ninety days or such other time fixed by the Council, remove or have removed by the owner, all or any part of the utility or public service facilities as required by the vacation ordinance and reconstruct and relay the facilities or have them reconstructed and relaid by

the owner in the places required by the city engineer, and obtain other work as required by the ordinance in the manner directed by the city engineer at the expense of the petitioner.

No vacation shall become effective until a certified copy of the vacation ordinance and any map, plat or other record relating to the vacation that may be required or provided for by law have been filed with the county department of records and elections. The cost of preparing, the person petitioning for the vacation shall pay filing and recording the ordinance and map.

Pursuant to ORS 271.080 the applicant's petition for vacation must include all the consent of all abutting owners and no less than two-thirds in area the real property affected by the proposed vacation. The real property affected by the proposed vacation is defined by ORS as the land lying on either side of the street proposed for vacation by 200 feet and to either end by 400 feet.

### CUP – Public Hearing

To apply for a CUP you must submit 3 copies of complete plans and narratives along with a completed application form. (Please note that an application form was given to you at the Pre-Application Conference. If you would like additional you can request that the application form be sent by mail, or you may pick up the application form at the Development Services Information Counter, call (503) 526-2420). In addition, you will need to supply the information required as a result of your pre-application meeting. After your application has been deemed complete and the fees have been paid, the Facilities Review Committee will review your application and in approximately 5 weeks, at a scheduled meeting, review with you the recommended conditions of approval. After the Facilities Review Committee Meeting a staff report will be written and will be available 7 days prior to your scheduled hearing before the Planning Commission. Within two weeks following the hearing a Land Use Order (LUO) will be prepared, signed and mailed to the applicant, property owner, the NAC, and parties of record. A 10-day appeal period follows the signing and mailing of the LUO. If no appeal than the CUP will appear on the City Council's consent agenda. The entire process will take approximately 14 weeks from submission of a complete application to the end of the appeal period.

### Design Review Type 3

To apply for a Design Review Type 3 submit 3 copies of complete plans and narratives along with a completed application form. (Please note that an application form was given to you at the Pre-Application Conference. If you would like additional you can request that the application form be sent by mail, or you may pick up the application form at the Development Services Information Counter, call (503) 526-2420). In addition, you will need to supply any additional information identified during your pre-application meeting as allowed for under Section 40.10.15.3.B.11 in the Development Code. After your application has been deemed complete and the fees have been paid, the Facilities Review Committee will review your application and in approximately 5 weeks, at a scheduled Facilities Review Meeting, review with you any recommended conditions of approval. These conditions of approval are with regard to technical standards, Section 40.10.15.3.C.1 (a-k). After the Facilities Review Committee Meeting

a staff report will be written and will be available 7 days prior to your scheduled hearing before the Board of Design Review. Within two weeks following the hearing a Land Use Order (LUO) will be prepared, signed and mailed to the applicant, property owner, the NAC, and parties of record. A 10-day appeal period follows the signing and mailing of the LUO. The entire process will take approximately 12 weeks from submission of a complete application to the end of the appeal period.

#### Historic Resource Review

To apply for a Historic Resource Review for either alternation or demolition you must submit 3 copies of complete plans and narratives along with a completed application form. After your application has been deemed complete and the fees have been paid, the Facilities Review Committee will review your application and in approximately 5 weeks, at a scheduled Facilities Review Meeting, review with you any comments or conditions of approval. After the Facilities Review Committee Meeting your application is scheduled before the Historic Resource Review Committee for a public hearing in approximately 2 to 3 weeks.

Applications for revisions or alternations to a historic structure are referred to the HRRC, who, after notice and public hearing held pursuant to Section 50.30.1.B. of the Development Code, shall approve issuance, approve issuance with conditions or disapprove issuance of the Permit Approval for exterior alterations. The decision of the HRRC shall include findings of fact.

The HRRC shall have final approval unless the proposed alteration is within jurisdiction of the Board of Design Review. In those cases the HRRC recommendation shall be transmitted to Board of Design Review for their approval.

If conflicting decisions are made by the HRRC and BDR, final decisions will be made by the City Council.

**Please note that there are several important additional steps that are required to demolish a historic structure.** To begin with an application for a permit to demolish a designated Landmark or any building must be received by the Building Official who shall within seven (7) days transmit to the Planning Director a copy of said application unless the Building Official has ordered the removal or demolition of such building determined to be dangerous to life, health or property. Nothing contained in this ordinance shall be construed as making it unlawful for any person without prior approval of the HRRC, pursuant to this ordinance, to comply with such an order.

If an application for a permit to demolish a designated Landmark or any building within a designated Historic District is received, the Building Official shall within seven (7) days transmit to the Planning Director a copy of said application unless the Building Official has ordered the removal or demolition of such building determined to be dangerous to life, health or property. Nothing contained in this ordinance shall be

construed as making it unlawful for any person without prior approval of the HRRC, pursuant to this ordinance, to comply with such an order.

Prior to the issuance of a permit for the demolition of any other designated landmark, the Planning Director shall first determine to his or her satisfaction that the applicant has met the following conditions:

- a. The applicant has advertised such building for sale and/or removal from the site, with such advertisement to run twice over two consecutive weeks (no less than seven days apart) in a newspaper of general circulation in the Beaverton area.
- b. The applicant has not rejected the highest bona fide offer for sale and removal of the building.
- c. The applicant has posted a sign for a continuous period of 90 days from the date thereof. The sign shall be provided by the city and be posted in a prominent and conspicuous place within ten feet of a public street abutting the premises on which the building is located, and shall contain the legend "THIS HISTORIC BUILDING TO BE DEMOLISHED" in letters at least one foot in height, together with a statement that includes the date, time and place of the Public Hearing of the HRRC before whom any affected party may testify. The applicant is responsible for assuring that the sign is posted for the continuous 90 days.

The Planning Director may notify interested parties or groups where appropriate.

If after 90 days no party interested in purchasing or moving the property has come forward, the HRRC shall hold a public hearing.

The HRRC shall approve or deny the application for a permit to demolish a designated Landmark.

### Tree Preservation Plan

To apply for a Tree Preservation Plan submit 3 copies of complete plans and narratives along with a completed application form. In addition, you will need to supply any additional information identified during your pre-application meeting. After your application has been deemed complete and the fees have been paid, the Facilities Review Committee will review your application and in approximately 5 weeks, at a scheduled Facilities Review Meeting, review with you any recommended conditions of approval. After the Facilities Review Committee Meeting a staff report will be written and will be available 7 days prior to your scheduled hearing before the Board of Design Review. Within two weeks following the hearing a Land Use Order (LUO) will be prepared, signed and mailed to the applicant, property owner, the NAC, and parties of record. A 10-day appeal period follows the signing and mailing of the LUO. The entire



process will take approximately 12 weeks from submission of a complete application to the end of the appeal period.

## **APPLICATION APPROVAL CRITERIA:**

### **HISTORIC REVIEW**

Revisions as proposed by Phase 1 of the Beaverton High School expansion require an application to the Historic Resource Review Committee as noted above. The Approval Criteria for a proposed revision to a historical structure is as follows:

1. For exterior alterations of designated Landmarks, the criteria to be used by the HRRC in reaching its decision on the permit approval shall include the following:
  - a. The distinguishing original qualities or character of a building, structure, or site and its environment shall not be destroyed. The removal or alteration of any historic material or distinctive architectural features should be avoided when possible.
  - b. All buildings, structures, and sites shall be recognized as products of their own time. Alterations that have no historical basis and which seek to create an earlier appearance shall be discouraged.
  - c. Changes which may have taken place in the course of time are evidence of the history and development of a building, structure, or site and its environment. These changes, particularly those affected during the historic period, may have acquired significance in their own right, and this significance shall be recognized and respected.
  - d. Distinctive stylistic features or examples of skilled craftsmanship which characterize a building, structure or site shall be treated with sensitivity.
  - e. Deteriorating architectural features shall be repaired rather than replaced, wherever possible. In the event replacement is necessary, the new material should match the material being replaced in composition, design, color, texture and other visual qualities. Repair or replacement of missing architectural features should be based on accurate duplications of features, substantiated by historic, physical or pictorial evidence rather than on conjectural designs or the availability of different architectural elements from other buildings or structures.
  - f. The surface cleaning of structures shall be undertaken with the gentlest means possible. Cleaning methods that will damage the historic building materials shall not be undertaken.
  - g. Contemporary design for alterations and additions to existing properties shall not be discouraged when such alterations and additions do not destroy significant historical, architectural, or cultural material, and such design is compatible with the size, scale, material, and character of the property, neighborhood or environment.

h. Whenever possible, new additions or alterations to structures shall be done in such a manner that if such additions or alterations were to be removed in the future, the essential form and integrity of the structure would be unimpaired.

i. Every reasonable effort shall be made to protect vegetation which is specifically designated as a Landmark.

j. Every reasonable effort shall be made to protect and preserve significant archeological resources.

2. Other information that may be taken into consideration may include:

a. Economic use and the significance of the landmark and the reasonableness of the proposed alteration and their relationship to the public interest in the building's preservation or renovation,

b. The physical condition of the landmark,

c. The general compatibility of exterior design, arrangement, proportion, detail, scale, texture and materials will be compared with the existing buildings, and

d. The historic or architectural value of the landmark.

In order to approve the demolition of the Merle Davies classroom building the HRRC shall make findings based on but not limited to, the following considerations:

1. The historic or architectural significance of the resource,

2. The physical condition of the building,

3. The economic use of the building and the economic reuse of the property,

4. The proposed new use, and

5. The economic, social, environmental and energy consequences of allowing the demolition as opposed to preserving the historic building.

6. If within a Historic District, its contribution to the district and the subsequent integrity of the District once the structure is demolished.

#### **TREE PRESERVATION PLAN:**

The application form contains a detailed checklist of submittal requirements for a TPP. The following information provides greater detailed of some of the checklist items.

When submitting for the formal application you will need to submit a narrative describing the proposed project. The narrative provides a listing all trees within the

project area, identifying the species, size, and health of the tree. The trees to be identified are those 6 inches in diameter and larger. For trees to be removed, the narrative should include an evaluation, for each tree, based on the removal criteria identified in Section 40.75.15.1.C.3.a (1 – 10) of the Development Code. For trees to remain on the site, the narrative should address how the trees are to be saved and what measure are being proposed to be use. This should also include an Arborist report as to methods of construction, setbacks, and any other applicable information to support you application.

### **CONDITIONAL USE PERMIT**

When submitting for the formal application you will need to submit a narrative describing the proposed project. The narrative needs to address how the project meets the development code criteria for CUP's, Section 40.05.15.2.C. The following is the criteria as found in the code:

#### **C. Approval Criteria.**

In order to grant a Conditional Use Permit, the Planning Director shall make findings of fact to support the following conclusions:

- 1. The proposed conditional use will comply with the purpose of this section and with all of the applicable provisions of this ordinance.*
- 2. The proposed development will comply with the Comprehensive Plan.*
- 3. That the location, size, design, and functional characteristics of the proposed use are such that it can be made reasonably compatible with and have a minimum impact on the livability and appropriate development of other properties in the surrounding neighborhood.*

For your convenience I have listed the Comprehensive Plan Land Use, Transportation, and Natural Resource Objectives and Policies. If you feel that an Objective or Policy is not applicable, then in your narrative you will need to discuss why it is not applicable.

#### **3.3.1 Land Use Standards and Design Objectives**

- a) Community appearance should become a major concern and the subject of a major effort in the area. Street tree planting and landscaping, sign regulation, and building improvement and painting programs all contribute to an improved environment.
- b) Recognize the future development of the Central Business District as the major element in creating an individual identity for Beaverton. The image of the City should protect the new and the old relating to:
- c) The Central Business District as a major regional employment and commercial center; and
- d) The City's past reflected in its older buildings.

- e) Concerted efforts should be made to improve the appearance of the community.
- f) Use the appearance of the community as a principal means of establishing an individual identity in the metropolitan area.
- g) Beaverton should capitalize on its tree-filled environment in establishing a unique urban identity.

### 3.3.2 Land Use Policies

Older sections of the community should be carefully evaluated and efforts made to rehabilitate or redevelop those areas requiring special treatment.

Efforts should be made to obtain ten-foot landscape strips along either side of Canyon Road, the Beaverton-Hillsdale Highway, the Tualatin Valley Highway, Murray Boulevard, Cedar Hills Boulevard, and along other major streets in the planning area, which are bordered by commercial or industrial developments. Landscaped median strips should also be installed in these streets wherever possible.

### 3.4.2 Residential Objectives

- 3.4.2.1. The primary focus of residential development should be towards maintaining or creating maximum livability and promoting quality living areas.
- 3.4.2.2. All residential areas should be provided with services and facilities necessary for safe, healthful, convenient urban living.
- 3.4.2.3. Residential areas should be developed in ways which are consistent with the geographic features so as not to create health or erosion hazards.

### 6.2.2. Goal: A balanced transportation system.

#### **Policies:**

- a) Develop and implement public street standards that recognize the multi-purpose nature of the street right-of-way for utility, pedestrian, bicycle, transit, truck, and auto use, and recognize these streets as important to community identity as well as providing a needed service.

**Action:** Develop and maintain a series of system maps and design standards for motor vehicle, bicycle, pedestrian, transit, and truck facilities in Beaverton.

- b) Provide connectivity to each area of the City for convenient multi-modal access.

*Actions: Require the provision of an adequate local public street system for both residential and non-residential development. Give particular attention to large blocks of commercially developed properties to assure that local circulation has adequate public streets and is not forced to use only private parking and driveway areas or the major street systems to conduct local trips. Develop and maintain appropriate on-site loading, parking, and internal circulation standards for private development based on adopted standards in the City's development code.*

- c) Develop a safe, complete, attractive, and efficient system of pedestrian ways and bicycle ways, including bike lanes, shared roadways, off-street pathways, and sidewalks according to the pedestrian and bicycle system maps.

*Actions: Use the City of Beaverton Engineering Design Manual standards in design of facilities. Conform to the design guidelines set forth in the "Guide for Development of New Bicycle Facilities" (latest edition) as published by the American Association of State Highway and Transportation Officials (AASHTO) and the Oregon Bicycle and Pedestrian Plan adopted by the Oregon Transportation Commission. Coordinate with Washington County, Metro, Beaverton School District, Oregon Department of Transportation (ODOT) and the Tualatin Hills Park and Recreation District (THPRD). Bicycle and pedestrian facilities should be provided and designed to accommodate the unique requirements of various user groups and trip types (including school trips, commuter trips, neighborhood circulation trips, and recreation trips). Pathways should be located to provide the "shortest path" between origins and destinations. Accommodate non-automobile movements specifically by bicyclists and pedestrians within neighborhoods. Sidewalks will continue to be the responsibility of fronting property owners. Maintain the opportunity for resident groups to fund pathway improvements through the local improvement district process. Continue to recognize the importance of walking and bicycling as forms of transportation and recreation. Develop a network of "Major Pedestrian Routes" in areas of the city where development occurs at higher levels of intensity, density, or both. Develop standards for sidewalk designs and standards for development on adjacent properties to be included in the Development Code.*

**6.2.3. Goal: A safe transportation system.**

- e) Designate routes to schools for each school and to and from any new residential project.

*Action: The City should work with the school district and community in developing safe bus, pedestrian, and bicycle routes to schools.*

**6.2.4. Goal: An efficient transportation system that reduces the number of trips and limits congestion.**

**Policies:**

- a) Support trip reduction strategies developed regionally, including employment, tourist, and recreational trip programs.

*Actions: Encourage implementation of travel demand management programs, which reduce the number of single occupant vehicle trips per capita. Shift traffic to off-peak travel hours. Coordinate trip reduction strategies with Washington County, Metro, Westside Transportation Alliance, ODOT, Tri-Met, and Department of Environmental Quality. Seek to raise PM peak average vehicle occupancy (AVO) to 1.3 AVO or more in the evening peak and/or move 50 percent or more of standard evening peak trip generation outside the peak hour. Educate business groups, employees, and residents about trip reduction strategies, and work with business groups, residents, and employees to develop and implement travel demand management programs.*

- b) Limit the provision of parking to meet regional and state standards.

*Actions: Meet Metro Urban Growth Management Functional Plan Title 2 requirements. Establish maximum and minimum parking requirements. Use research conducted by Department of Environmental Quality for guidance in determining demand. Work to reduce parking per capita by ten percent or more relative to prior parking standards in Beaverton, and minimize impacts to neighborhoods.*

- c) Maintain level of service consistent with regional goals. Reduce traffic congestion and enhance traffic flow through such measures as intersection improvements, intelligent transportation systems, signal synchronization, and other similar measures.

*Actions: In adopting standards for the City, level of service D, Highway Capacity Manual, Chapter 11 is recommended to balance provision of capacity with level of service and funding. Continue to monitor Washington County's and Metro's work to develop a regional level of service standard.*

#### **7.2.1 Parks and Recreation Planning Objectives**

- a) Schools, park, and recreation development should be considered as a vital part of the future livability of the area, and efforts should be made to keep these facilities abreast of growth.
- b) Provide for schools, parks, and natural open space throughout the city in locations that are easily accessible to those they are intended to serve.
- c) Help offset higher residential densities and intense commercial developments in the community with parks and open spaces.

#### **7.3.1. Cultural and Historic Resources Objectives**

- a) Protect, where possible, cultural and historic resources in order to enhance and perpetuate landmarks and districts representing or reflecting elements of the city's cultural, social, economic, political and architectural history.
- b) Stabilize and improve property values in historic districts and other designated landmarks.

c) Foster civic pride in the beauty and noble accomplishments of the past.

### **DESIGN REVIEW:**

The application form contains a detailed checklist of submittal requirements for a Design Review Type 3. The following information provides greater detailed of some of the checklist items.

The application submittal will need to include a narrative describing the proposed project. The narrative needs to address **how and why** the project meets development code requirements and the Technical Standards Section 40.10.15.3.C.1 (a-k), and Design Standards Section 40.10.15.3.C.2 (a-h). If a standard is not applicable please state why it is not applicable. A full written text of the criteria is contained within the application checklist given to you at the Pre-Application Conference meeting.

The site plan prepared for Design Review must include the surrounding area for approximately 100 feet. The area will need to include surrounding streets showing right-of-way lines, driveways, parking, building locations, and surrounding zoning. The site plan shall not be prepared using an architect's scale, but shall be prepared using an engineering scale; on a sheet size no greater than 24" x 36", and at 1 inch = 20 feet. The City has 100 foot scale topographic maps showing buildings, driveways, vegetation, etc. that may assist you in preparation of your site plan. Contact Ed Parks at 526 - 2441 for more information about the City's topographic maps.

### **Landscaping:**

A detailed landscape plan is required with the Design Review application. The landscape plan needs to include the location of all plant material, which is keyed to a plant list. The detailed requirements are contained within the application checklist.

As described earlier in this report the removal of significant or historic trees would require significant mitigation. Staff suggest that the landscape plan include Oregon White Oaks and other trees that have the potential to become landmarks for the school and community. Staff also suggest the use of plant material around buildings that will discourage people from being in areas they shouldn't and thereby improve security.

### **Phasing:**

As discussed at the pre-application meeting the Development Code allows for several different types of phasing. Construction phasing can be allowed based on full approval for ultimate build out. However, if designs are not complete the code provides for either General Site Plan or based on an interpretation by the Planning Director a Master Plan approval. Approval of a BDR Type 3 "Master Plan" would allow future portions of the building to be processed as Type 2 BDR's rather than Type 3 BDR's. However, the District will need to provide sufficient architectural massing and detail to enable to Board of Design Review the ability to provide an overall approval.

### Lighting:

A detailed lighting plan is required with the Design Review application. The plan shall illustrate existing and proposed lighting with the following requirements:

- a. Areas of night illumination measured at one-half foot candlepower.  
Please note that the isolumes need to be shown as a light contour line and not a grid of numbers.
- b. Light pole height, type, and number of fixtures per pole.
- c. Light fixture type
- d. Wattage per fixture and lamp type (e.g. sodium, mercury, halide, etc.)
- f. In addition submit a copy of the cut sheets of each fixture being proposed.

Keep in mind that no illumination greater than one-half (1/2) foot-candle should cross property lines nor may direct glare from fixture lamps or bulbs (except diffused light) are visible from the public right-of-way or adjacent properties. An exception to this recommendation would be in instances where crossover access driveways and parking areas will exist and require uniform lighting. This was not submitted with the pre-application material. In addition please be aware of landscape and lighting conflicts.

### Building Design:

Since no architectural elevations were submitted, I cannot identify any specific Building Design Issues. As previously discussed it is vital that the proposed classroom building replicate some of the architectural elements of the Merle Davies School. In addition, the overall design should blend the original building with the new building.

### Time Limit:

Design review approval shall be void after two years from the date of approval unless a building permit has been issued and substantial construction pursuant thereto has taken place. (ORD 3739). To extend a Design Review Type 3 the process is to submit for a Design Review Type 2 subject to Section 40.10.15.2.i, which reads as follows: (i) Resubmittal of Type III plans which have expired pursuant to Section 40.10.15.3.G. and are being resubmitted within one year of expiration and exactly as originally approved.

### **BOARD OF DESIGN REVIEW STANDARD CONDITIONS OF APPROVAL:**

The following are the Board of Design Review standard conditions of approval, which are not identified within the code, but may be adopted by the Board for any proposal:

1. All site development and landscaping shall be carried out in accordance with the approved plan marked "Exhibit A", as approved by the Planning Director. (On file at City Hall).
2. All construction shall be carried out in accordance with the color and materials board marked "Exhibit B", as approved by the Planning Director. (On file at City Hall).



3. All construction shall be carried out in accordance with the elevations and plans marked "Exhibit C", as approved by the Planning Director. (On file at City Hall).
4. All landscaping required and approved by the Board for commercial and industrial projects shall be installed prior to issuance of occupancy permits unless security equal to 110 percent of the cost of the landscaping is filed with the City assuring such installation within six months of occupancy. All security bonds submitted must itemize major items in terms of cost.
5. Height and screening shall be emphasized through the planting of deciduous trees. These trees shall have straight trunks, be fully branched, have a minimum caliper of 1 ¼ inches and a minimum height of 8 feet at the time of planting. Deciduous trees can be supplied bare root provided the roots are protected against damage. Each tree is to be adequately staked.
6. Evergreen trees must be balled and burlapped or in suitable containers in which the tree has grown for one year. The ball of each tree shall be firm and the burlap sound; no loose ball or made ball will be accepted. Each tree shall be a minimum of 6 feet in height, fully branched, and adequately staked at the time of planting.
7. Ground cover plantings shall be planted on a maximum of 30 inches on center and 30 inches between rows. Rows of plants are to be staggered for a more effective covering. Ground cover shall be supplied in a minimum 4-inch size container or a 2 ¼ inch container if planted 18 inches on center.
8. Shrubs shall be supplied in one-gallon containers or 8-inch burlap balls with a minimum spread of 12 to 15 inches.
9. The installation of an approved irrigation system shall be required to ensure the longevity of all landscaping. Further, landscaping shall be maintained by weeding, pruning, and replacing as necessary.
10. All mechanical equipment, vents, and utility meters shall be screened from public view and made an integral part of the structure.
11. The site shall be kept clean at all times and all trash shall be stored within the building or within the exterior opaque enclosures and be gated. The design and materials of the trash enclosures shall be compatible with the subject development and shall be a minimum of 6 feet in height.
12. All exterior lighting shall have cut-off fixtures so no glare is emitted beyond the property line or into the public right-of-way.
13. Landscaping shall be provided within areas designated for parking in accordance with the following provisions:

All off-street parking and maneuvering areas shall be screened from the public right-of-way or surrounding property by evergreen and/or deciduous plant material with a minimum height of 30 to 36 inches.

If landscaped parking areas include landscaped berms, decorative walls, or raised planters which effectively screen the parking lot areas from view, the evergreen and/or deciduous plant materials measuring a minimum of 30 to 36 inches will not be required.

A planter island for approximately every 12 spaces in the parking lot shall be landscaped and be a minimum of 8 feet wide by one parking stall.

14. The installation and location of all mailboxes shall be incorporated and made an integral part of any proposal.
15. No A-frame signs or other incidental signs, price signs, "open" or "closed" signs, flags or banners, or special product advertising shall be erected on a permanent or temporary basis. No window signs in excess of 20 percent of the window area are permitted at any time.
16. In landscape areas to be planted in grass, sod shall be placed from October 1 to May 1. Grass seed shall be an option at other times of the year.

**B. DEVELOPMENT SERVICES/SITE ENGINEERING:**

**Prepared by Jim Duggan - (503) 526-2442 or [jduggan@ci.beaverton.or.us](mailto:jduggan@ci.beaverton.or.us)**

A letter from each franchise utility authorized to work in City right-of-ways will be required to be submitted with any street vacation application. These letters must document the existing facilities or planned facilities for this street right-of-way and any requests from the utilities in regard to the street vacation. The applicable utilities are AT&T Broadband Cable, Northwest Natural Gas, Verizon (General Telephone), and Portland General Electric. There is a City water main in 2<sup>nd</sup> Street; this line will need to be preserved or relocated. In the location of the proposed skybridge and for at least 25 feet in either direction, the water line would need to be placed within a steel casing designed like a bore-placed water line. Other City utility impacts would be evaluated with each phase or application. The southern portion of the site contains an area of 100-year floodplain. The proposed parking lot on Erickson opposite the football field will be within the 100-year floodplain. **The comments below are typical conditions of approval that would apply to the proposed project, as shown in the submittal.**

1. The design and construction of the project shall comply with all applicable requirements of Beaverton Municipal Code, Beaverton Development Code (Ordinance 2050 +rev.), the City of Beaverton Engineering Design Manual and Standard Drawings (Ordinance 4060), and the Unified Sewerage Agency (USA) Design and Construction Standards (February 2000, Ordinance 2000-007; NOTE: USA is now Clean Water Services).
2. Prior to any work on the site governed by Beaverton Municipal Code 9.05.020, the project shall obtain a site development permit from the City Development Services Division. A separate application (with transmittal) shall be made to the Development Services Division for this permit. (Note: All applications and legal form submittals shall be on originals as provided by the City; no facsimiles, copies, or substitutes will be accepted). Site plans shall be submitted on 24 inch x 36 inch size sheets, and to engineer's scale. Each sheet shall be stamped and signed by a registered professional engineer or as otherwise determined by the City Engineer. After the site development permit is issued, all revisions shall be approved by the City Engineer and the Planning Director; any required land use action must be final prior to approval of the engineering revision and work commencing as revised.

*ADVISORY NOTE: Site Development Permit application packets are available in the second-floor, Community Development Department Resource Center. Information concerning existing public utilities may be available via "as-built" drawings. Contact the City As-built Archive at 503.526.2441 for more information. When submitting for site development permit, submit 7 sets of complete plans, a detailed cost estimate, a check for one percent of the cost of public improvements, site grading, private street and common driveway paving to the Engineering Section of the Development Services Division. (Beaverton Code 9.05.020, 9.05.040, Ordinance 4060, and Ordinance 2050 +rev.)*

3. The applicant shall contract with a professional engineer (or professional architect as allowed by the City Engineer) to design and monitor the construction as set forth in the City Standard Applicant/Engineer agreement. The applicant shall submit a completed City Standard Engineer/Applicant agreement prior to issuance of a site development permit. *ADVISORY NOTE: The City standard form is available from the second-floor, Community Development Department Resource Center, and is a part of the Site Development Permit application packet. (Ordinance 4060 and Beaverton Code 9.05.070)*

4. Prior to issuance of any building permits, the owner of the subject property shall install all public improvements, as required by the City Engineer, including but not limited to the following: water lines, water services, fire hydrants, sanitary sewer, storm facilities, street lights, regulatory street signs, street names and markings. Utilities shall be provided underground. Water meters shall be located adjacent to the public streets, or as otherwise determined by the City Engineer. *(Beaverton Code 9.05.055C and Ordinance 4060)*
5. All public improvements, site grading, private streets, and common driveway paving shall be guaranteed at 100 percent of cost. The security shall be approved by City Attorney prior to issuance of a site development permit. The location, design, size, and type of all public improvements shall be approved by the City Engineer. *(Oregon Revised Statutes Chapter 92, Beaverton Code 9.05.020, 9.05.070, and Ordinance 2050 +rev.)*
6. All existing overhead utilities, except high voltage lines (>57kV), within the project and along any existing street frontage shall be placed underground prior to the release of the site development performance security or as otherwise determined by the City Engineer and Planning Director. *(Ordinance 4010 Sections 60.40.25.11.B., 40.10.15.1.C.1.h./2.C.1.h./3.C.1.h., 40.35.15.2.D.9/3.D.9., 40.35.15.2.E.2.d./3.E.2.d)* **NOTE:** Per the requirements of Ordinance 4010, Section 40.10.15.2.C.1h, the existing overhead utility lines onsite and along the street frontages shall be placed underground. The applicant may request that the City consider allowing payment of a fee-in-lieu of utility undergrounding; however, **the design review application submittal must include the information required under Ordinance 4010, Section 60.65.20.** The application narrative needs to include a specific request with justifying information, per the criteria listed in Section 60.65.25, that a fee-in-lieu of undergrounding should be granted. Additionally, the information on the plans must clearly document the number and types of utility lines currently overhead onsite and along the street frontages.

7. All underground piping which is not public shall be installed in accordance with Uniform Plumbing Specialty Code including Oregon amendments (UPC). The private plumbing plan for this project shall be approved by the City Building Division prior to the issuance of the site development permit. All plumbing that serves more than one lot, or crosses onto another lot, shall be considered a public system and shall be constructed to the requirements of the City Engineer as required with a site development permit. *ADVISORY NOTE: Contact the City Building Division at 526-2403 for more information concerning site plumbing permits for private plumbing. (UPC, Beaverton Municipal Code 4.02.070, 4.04.040, 9.05.055C, and Ordinance 4060)*
8. All easements necessary for public facilities (including slope easements, and surface drainage easements) shall appear on the final plans and/or plat and shall be to the standard specified by the City Engineer and Planning Director. *(Ordinance 2050 +rev., and Ordinance 4060)*
9. All public utilities shall be installed and centered within a minimum fifteen foot wide easement. Easements for all public utilities including storm sewer, sanitary sewer, water, and illumination lines shall be of sufficient width to ensure that the foundation of any structure or parallel utility line shall be a minimum of 45 degree vertical angle from the invert elevation of the pipe measured from the trench wall in accordance with Section 315 of the Uniform Plumbing Code. The applicant's engineer or architect of record shall verify all existing and proposed easements are of sufficient width to ensure that structures and parallel utility lines will be outside the trench zone described above. If the existing easement or proposed new easement is insufficient to meet the criteria above, a revised or additional easement shall be provided prior to issuance of the site development permit or a building permit. *(UPC, Beaverton Code 9.05.055C, Ordinance 4060)*
10. No tree (with a mature height greater than 12 feet) shall be allowed within 5 feet of a public utility line or publicly-owned structure. Trees greater than 12 feet in height at maturity that are proposed between the outside of the 5 foot zone and the edge of the required easement can be evaluated by City staff on a case by case basis, and if acceptable, allowed by the City Engineer. *(Beaverton Code 9.05.055C, Ordinance 4060)*
11. All public utilities shall be installed at ultimate grade, size, and location unless otherwise specifically approved by the City Engineer. Public utilities (water, sanitary sewer, and storm drainage) shall be designed and constructed according to the City master plan. They shall also be extended within the site, adjacent to the site, and along public right of way frontages to all neighboring undeveloped and underdeveloped parcels. *(Beaverton Code 9.05.046, 9.05.055C, Ordinance 4060, and Ordinance 2050 +rev.)*

12. A detailed grading plan shall be prepared by a professional engineer or surveyor, showing existing and proposed grading at two-foot contour intervals. Grading plan topography shall extend a minimum of fifty feet onto adjacent properties, or as otherwise approved by the City Engineer. The use of aerial photos, or the City's quarter-section grading plans will be allowed for preliminary design, however final plans shall be from on-site surveys. The applicant's engineer or surveyor shall submit to the City a copy of the field notes documenting the extent of the field survey with the site development permit application. No grading shall occur within 10 feet of a property line unless the requirements of Beaverton City Code sections 9.05.110 and 9.05.115 are met. (*Beaverton Code 9.05 and Ordinance 4060*)
13. All existing wells, tanks, and septic drain fields shall be shown on the site development plans. These items, if proposed by the applicant or required by City Engineer to be abandoned, shall be removed during the site work in a manner approved by the governing authority. (*Ordinance 4060, Oregon Revised Statutes Chapter 92, Beaverton Code 4.04.030, 9.05.055C*)
14. All existing buildings and structures shall be shown on the site development plans. An ALTA-type survey shall be prepared by a professional surveyor or engineer for the project site at least 100 feet surrounding each existing structure to remain. This survey shall be submitted to the City with the site development permit application. (*Ordinance 4060*)
15. The applicant shall submit a geotechnical report, prepared by a professional engineer and to the specifications of the City Engineer, with the site development permit application. This shall be reviewed and approved by the City Engineer prior to issuance of the site development permit. (*Beaverton Code 9.05.035.B11*)
16. The site development plans shall include erosion control measures that are designed to meet the requirements of Oregon Administrative Rule 340-41-455. *ADVISORY NOTE: The current Technical Guidance Handbook from USA is dated December 2000.*
17. The applicant shall submit a completed 1200-C General Permit (Erosion Control Joint Permit) to the City prior to the issuance of the site development permit. *ADVISORY NOTE: The application form is available at the second floor, Community Development Resource Center. This permit is issued through the City for USA and the Department of Environmental Quality (Beaverton Code 9.05.035D, Department of Environmental Quality Mandate, and Intergovernmental Agreement with USA).*

18. The design and construction of private streets and parking lots along with drive access to public streets shall comply with City standards. *(Ordinance 4060 Section 210.17, Ordinance 2050 + rev., and UFC)*
19. Sidewalks which are damaged, deteriorated, or removed by construction shall be replaced to City specifications prior to the Building Division releasing any building for occupancy. *(Beaverton Code 9.05.130)*
20. All public improvements built as a condition of development in the City of Beaverton shall be designed and constructed in such a manner as to be readily accessible to and usable by individuals with disabilities as per the requirements of the Americans With Disabilities Act of 1990. This includes installing curb ramps (per City Standard Details) at intersections with pedestrian crosswalks to allow a smooth transition between street and sidewalk elevations during site development. The sidewalk shall also be installed around the radius of the corner (P.C. to P.T.) during site development. The applicant shall replace any existing ramps adjacent to the site which do not meet the new standard. *(Ordinance 4060)*
21. Street centerline monumentation shall be protected by a City standard monument box at all street intersections. All other street centerline monumentation shall be metallic caps. *(Ordinance 4060)*
22. The site development plans shall show water service, meter size, location and easements. All water services shall be installed and paid for by the applicant. The materials and workmanship shall conform to City standards and service line size shall match meter size, unless specifically approved by both the City Water Section Supervisor and the City Engineer. *(Ordinance 4060)*
23. Prior to issuance of the site development permit, a detailed water analysis shall be provided. This analysis shall consist of an actual flow test and analysis by a professional engineer meeting the standards set by the City Engineer. The analysis shall indicate adequate water service to each meter, and adequate volume and pressure for fire protection service of the proposed buildings. *(Ordinance 4060)*
24. The site may require a Clean Water Services Industrial Sewage Permit. If an Industrial Sewage Permit is required, this shall be obtained and a copy submitted to the City Building Official prior to final building occupancy. *ADVISORY NOTE: Contact Clayton Brown (503.844.8923) at Clean Water Services Source Control for more information. This is a condition required by intergovernmental agreement (IGA) for all non-residential projects; Clean Water Services (formerly USA) must be aware of any unusual chemicals will be used in new buildings that could end up being discharged to the sanitary sewer system. (Beaverton Code 9.05.035D, and Intergovernmental Agreement)*

25. The applicant shall submit a copy of issued permits or other approvals needed from the Clean Water Services for storm system connections prior to the issuance of the site development permit. *ADVISORY NOTE: Contact Lee Walker (503.648.8621) at Clean Water Services for more information (Beaverton Code 9.05.035D USA Resolution and Order 2000-007, and Intergovernmental Agreement).*
26. The applicant's engineer shall provide a detailed drainage analysis of the subject site and prepare 24 x 36-inch sheet identifying all contributing drainage areas and plumbing systems on and adjacent to the site with the site development permit application. The analysis shall also delineate all areas on the site that are inundated during a 100 year storm event in addition to any mapped FEMA flood plains and flood ways. *(Beaverton Code 9.05.135)*
27. Any grading within the 100-year floodplain or work in the flood way will require a public notice to be issued by the City Engineer to all properties within 300 feet of a property with proposed floodplain modifications. The applicant's engineer shall certify in writing that the project as designed will meet the requirements of City Code, Unified Sewerage Agency Resolution and Order 2000-007, and Ordinance 2050 +rev., as they refer to the 100 year floodplain, prior to this notice being sent. If work is proposed in a flood way, the applicant's engineer shall submit a certification on FEMA standard form verifying that the project as proposed will not increase the base flood elevation prior to issuance of the public notice. This notice and a 10 day appeal period shall occur after final approval of the site development permit plans by the City Engineer and Planning Director, but prior to the issuance of a site development permit. *(Beaverton Code 9.05.085)*



28. This project shall provide on-site storm detention if shown to be required by the analysis specified in Section 3.05.2&3 of Unified Sewerage Agency Resolution and Order 2000-007, as determined by the City Engineer upon criteria show in USA 2000-007 Section 3.11.2, and in Beaverton Ordinance 4060, Section 330. Detention systems shall be designed for the 2, 10, and 25-year frequency storms to the technical requirements of the 1999 Engineering Design Manual (Ordinance 4060), with provision for the safe overflow of the 100 year event. The applicant's engineer shall provide design computations with the site development permit application and field verification with submittal of "as-built" drawings that the detention volume and conveyance required by this condition has been provided. Prior to the installation of any asphalt or concrete pavement, a detention facility required by this condition shall be completed and operational. If detention is determined to not be required by the City Engineer, then a system development charge in-lieu of constructing an on-site storm water quantity facility shall be assessed at time of permit issuance. *ADVISORY NOTE: Currently per Ordinance 3787, this is \$291 per each Equivalent Dwelling Unit (EDU) (2,640 square feet of new impervious surface). (Beaverton Code 9.05.135D, Ordinance 4060, Unified Sewerage Agency Resolution and Order 2000-007, and Intergovernmental Agreement)*
29. This project shall construct and maintain an on-site storm water quality facility meeting or exceeding the minimum requirements of the City of Beaverton and the Clean Water Services. Prior to the installation of any asphalt or concrete pavement, the storm water quality facility shall be completed and operational. If there are site constraints that make installation of a storm water quality facility impractical or impossible, then other options should be discussed with City Development Services Staff. With the land-use approval (BDR or subdivision) application, a letter of justification from the applicant's engineer is required for obtaining approval to allow a systems development charge in-lieu-of option. This justification shall be based on the criteria of the Unified Sewerage Agency Resolution and Order 2000-007, Section 3.12.b.1, and will be evaluated by the City Engineer. If allowed, a system development charge in lieu of constructing an on-site storm water quality facility shall be assessed at time of permit issuance. *(Ordinance 4060, Unified Sewerage Agency Resolution and Order 96-44, and Intergovernmental Agreement) ADVISORY NOTE: Currently per Ordinance 3787, this is \$238 per each Equivalent Dwelling Unit (EDU) (2,640 square feet of new impervious surface)*
30. A storm water system development charge shall be assessed at time of building permit issuance. *ADVISORY NOTE: Currently per Ordinance 3910, this is \$610 per each Equivalent Dwelling Unit (EDU) (2,640 square feet of new impervious surface).*

31. The applicant's engineer, architect, or surveyor shall provide to the City a certified impervious surface determination of the proposed project. Preliminary calculations shall be submitted with the site development and building permit applications. The final certification shall be submitted to the City prior to final occupancy. The analysis and calculations shall include square footage of all impervious surfaces as a total. In addition, specific types of impervious area totals, in square feet, shall be given for roofs, parking lots and driveways, sidewalk and pedestrian areas, and any gravel surfaces. Calculations shall also indicate the square footage of pre-existing impervious surfaces, the new impervious surface area created, and total final impervious surfaces areas on the entire site or individual tax lots if applicable. (*Ordinance 4060 and Ordinance 3687*)
32. All wetlands, floodplains, drainage ways, and sensitive areas as defined by the Unified Sewerage Agency Resolution and Order 2000-007, the site assessment determination and conditions set by the storm system connection permit from USA, and City requirements shall be identified and clearly delineated on plans submitted with any land use permit and site development permit application. The applicant shall not rely on general community or FEMA maps for that information, but shall make an independent, professional determination for the project site and vicinity. The limits of such areas shall be determined, surveyed, and certified by a professional engineer, recognized wetland scientist or biologist, and/or professional surveyor as appropriate considering the circumstances associated with the project site. A copy of the delineation report shall be submitted with each application. (*Beaverton Code 9.05.035, Ordinance 4060, and Ordinance 2050*)

### **C. TRANSPORTATION DIVISION:**

**Prepared by Don Gustafson- (503) 350-4057 or [dgustafson@ci.beaverton.or.us](mailto:dgustafson@ci.beaverton.or.us)**

1. With the Type 3 Design Review application, the applicant will be required to provide street right of way dedication to the Beaverton Comprehensive Plan Arterial Street Standard (48 ft. from centerline) on SW Farmington Road, to the Collector Standard (31 ft from centerline) on SW Erickson Ave, and to the Local Street Standard (26 ft from centerline) on SW Stott St, unless already present (Development Code 40.10.15.3.C.1.b&c, 60.60.45.1). Label the centerlines and rights of way and dimension the distance from centerline to property line. Dimension sidewalks and pavement widths.
2. With the Type-3 Design Review application the applicant will be required to improve SW Stott St to the Beaverton Local Street Comprehensive Plan Standard, including planter area, sidewalk, and street trees, for the frontage of the site, if not already present (Development Code 40.10.15.3.C.1.a&b, 60.60.30).

3. Show development within 100-feet around the site. Show driveways, structures, and relevant utilities that may indicate required extensions of facilities from the neighboring properties (Development Code 60.60.15.3).
4. The proposed construction of building additions and additional car parking spaces will generate traffic (vehicle trips per day) in excess of the minimum threshold requirement for a Traffic Impact Analysis, therefore a Traffic Impact Analysis will be required to be submitted (Development Code 60.60.10.7). The analysis should address the impact of the additional student trips and additional school bus trips, and how these new trips and the existing trips will access the site and circulate within the site. The study must be signed by a professional engineer, submitted and accepted by City staff before the application will be considered complete. Please discuss the scope of the study with transportation staff prior to report preparation.
5. Provide a summary of the number of existing and proposed new vehicle and bicycle parking spaces based on the current and projected building square footages and number of students (Development Code 60.20.05 & 60.20.10).
6. Provide parking circulation and dimensions that meet City of Beaverton design requirements (Development Code 60.20.15& 60.20.20). Two-way driveway aisles shall be a minimum of 24 feet wide, with a minimum 30-foot driveway width provided at the intersecting street if curb-tight sidewalks are present (Engineering Manual 210.17).
7. No obstructions shall be placed within the driveway intersection sight vision triangle except as provided by City Ordinance, including but not limited to parking. (Development Code 60.60.50.1 and Engineering Design Manual Sec. 210.5).
8. Where appropriate bumper overhang area is provided, parking stall depth may be reduced by 3 feet for full sized parking spaces, and reduced by 2 feet for compact parking spaces. The regular parking stalls located with such overhang area may be reduced in depth from 18.5 feet to 15.5 feet (Development Code 60.20.15). Compact car stalls may be reduced from 15 feet to 13 feet.
9. Walkways bordering perpendicular parking spaces shall be at least seven feet wide unless concrete wheel stops, bollards, curbing, landscaping, or other similar improvements are provided which prevent parked vehicles from obstructing the walkway (Development Code 60.60.70.2E).
10. Parking spaces along the outer boundaries of a parking area shall be designed to include a continuous curb located a minimum of 6-feet from the property line (Development Code 60.20.20.2C).
11. Provide secure bicycle parking to accommodate long-term users per Development Code Section 60.60.65.4 and 60.20.10. Required long-term space bicycle parking for a high school is a minimum of 1 space per 18 students.

12. Walkways shall be differentiated from parking areas and circulation aisles by grade, different paving material, landscaping, or other similar method (Development Code 60.60.70.2E). Walkways must have a minimum of 4-foot unobstructed width.
13. A Traffic Impact Fee (TIF) will be due at issuance of building permits. The TIF will be for any increase in the number of students at the rate of \$94.00 per student trip (1.39 trips per student) or \$130.66 per student, based on TIF rates valid through June 30, 2002. For more information regarding TIF contact Don Gustafson at 350-4057.
14. Comments from Tri-Met are important to the City's decision and will be considered prior to facilities review approval (Development Code 60.60.60.1). Any required improvements shall be reviewed and approved by staff prior to issuance of site development permit. Contact Ben Baldwin, Tri-Met Project Planner, at 962-2140 to determine whether Tri-Met will require any improvements. Applicant is to provide a letter from Tri-Met as part of application indicating what, if any, improvements Tri-Met requires.
15. Comments from ODOT are important to the City's decision and will be considered prior to facilities review approval. Provide written financial assurance for any improvements conditioned by ODOT prior to issuance of site development permit.

**D. BUILDING DIVISION - Prepared by Brad Roast - (503) 526-2524**

1. The proposed project shall comply with the State of Oregon Building Code in effect as of date of application for building permit. This currently includes: The 1998 edition of the Uniform Building Code and 1999 International Mechanical Code, published by the International Conference of Building Officials, including the State of Oregon Amendments; the 2000 edition of the Uniform Plumbing Code, published by the International Association of Plumbing and Mechanical Officials, including the State of Oregon Amendments, 2000 edition of the National Electrical Code published by the National Fire Protection Association, including the State of Oregon Amendments.
2. A demolition permit is required for the removal of the existing building(s). If a septic tank exists, it shall be pumped out and filled in with sand or gravel, or completely removed. An inspection shall be obtained from the plumbing inspector after the tank is filled or removed. A copy of the receipt from the pumping company shall be provided. If the building is connected to the public sanitary sewer system, the building sewer shall be capped off at the property line and inspected by the plumbing inspector. [SSC Section 106, PSC Section 722]
3. The removal of existing buildings on the property may provide credits towards some system development (SDC) fees, such as water, sanitary sewer, impervious surface, traffic.
4. Applications for plans review must include the information outlined in the "Application Checklist" available at the Building Division Counter. Incomplete applications will not be accepted. [City policy.]

5. Building permits will not be issued until all related plans and permits have been reviewed, approved and issued (i.e., mechanical, plumbing, electrical, fire sprinkler systems, fire alarm systems, ...). [City Policy.]
6. The building code plan review can run concurrent with the BDR and Site development review.
7. A separate plumbing permit is required for installation of private on-site utilities (i.e., sanitary sewer, storm sewer, water service, catch basins, ...). If the applicant desires to install those types of private utilities during the same period as the "Site Development" work, a separate application and three sets of the private utility plans must be submitted to the building division for review and approval.
8. Disabled parking stalls shall be provided in accordance with ORS 447.233.
9. The proposed buildings shall be accessible to persons with disabilities. [SSC Chapter 11]
10. An accessible route shall be provided to persons with disabilities throughout the site. [SSC Section 1106]
11. The applicant should be aware that food service operations are required to be reviewed and approved by the Washington County Health Department for applicable health regulations. For more information contact the Washington County Health Department 693-4402.

**E. FINANCE DEPARTMENT - Prepared by Kathy Gaona - (503) 526-2255**

1. There are no liens.

**F. TUALATIN VALLEY FIRE DISTRICT –  
Prepared by John K. Dalby - (503) 526-2519**

1. No comments submitted.

**G. OPERATIONS DEPARTMENT - Prepared by Leonard Apling - (503) 526-2223**

1. Utilities are available to the site. No additional comments at this time.

**ITEMS GIVEN TO APPLICANT:**

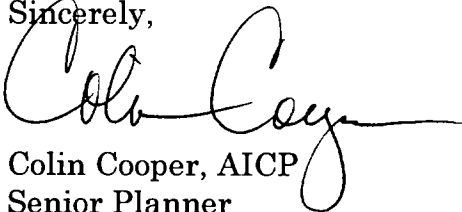
1. BDR Application

The pre-application conference is intended to aid and assist the public in understanding the procedures and regulations of the City of Beaverton and how they might apply to a

particular development given the information provided at the conference. The conference is not intended, however, to approve specific site plans. Accurately detailed plans will have to be submitted to fully determine what will be required to develop this site. Please be aware that various permits are required to build in Beaverton.

If you have any questions, please feel free to contact me at (503) 526-2425.

Sincerely,

A handwritten signature in black ink, appearing to read "Colin Cooper". The signature is fluid and cursive, with a long horizontal stroke extending to the right.

Colin Cooper, AICP  
Senior Planner

Attachments: Attendance Sheet  
c: Development Services Information Counter  
Jim Duggan

## TRAFFIC IMPACT FEE PROGRAM SUMMARY

The following summarizes the Traffic Impact Fee (TIF) in the City of Beaverton. See page 2 for TIF rates for most projects. For more information or for TIF rates for developments not listed on page 2, contact Sean Morrison, TIF Coordinator, at 350-4012.

### Types of Development Subject to TIF

- All new development.
- Changes in land use resulting in increased trip generation. Examples:
  - Existing office/warehouse tenant converting some warehouse area to office area.
  - New leaseholder converting existing office space into video rental.

### TIF Exemptions

- Remodeling of single-family dwellings.
- Remodeling of multi-family dwellings, except if new units are added.
- Public mass transit improvements.

### How TIF is Assessed

- TIF is a tax based on national trip generation averages for each land use.
- Assessed with building permit for construction of building shell.
- Specific procedures in accordance with TIF Ordinance.
- Based on land use information provided by applicant and trip generation for land use type.
- If trip generation rates not available, most similar land use type used.

### Payment Methods

- Cash or check prior to issuance of building permit.
- Can defer payment until occupancy if TIF is greater than \$5,000 and deferral selected on payment option form.
- Must apply in writing no later than time of application for building permit (use payment option form)
- Must select road construction credit or offset at time of deferral.
- Not eligible for Bancroft (installment plan) unless requested at time of deferral via payment option form. Bancrofting allows semi-annual payment of any TIF amount.
  - \$250 non-refundable fee.
  - Interest rate is prime plus two percent.
  - Must apply at time of building permit application using payment option form.
  - Offset not allowed with bancrofting.

### Credit

- Credits allowed for certain capacity and/or safety improvements to collector and arterials.
  - For capacity improvements, must be constructed to ultimate line and grade.
  - For safety improvements, must be a condition of development approval.
  - Must apply in writing within 90 days of acceptance of the improvement by the City.
- Credit form provided by City. Applicant responsible to present form to receive credit.

### Notification

- Applicants and property owners notified in writing of TIF amount.

### Refunds

- Must apply in writing. Refunds allowed under only two specific conditions:
- Clerical error in calculation of the fee.
- Failure to claim credit within 30 days of issuance of building permit or occupancy permit if deferred.

### Appeals

- Appeal fee is \$625. Must be in writing within 14 days after notification mailed.

TABLE 1  
TIF RATES FOR MOST COMMON LAND USES

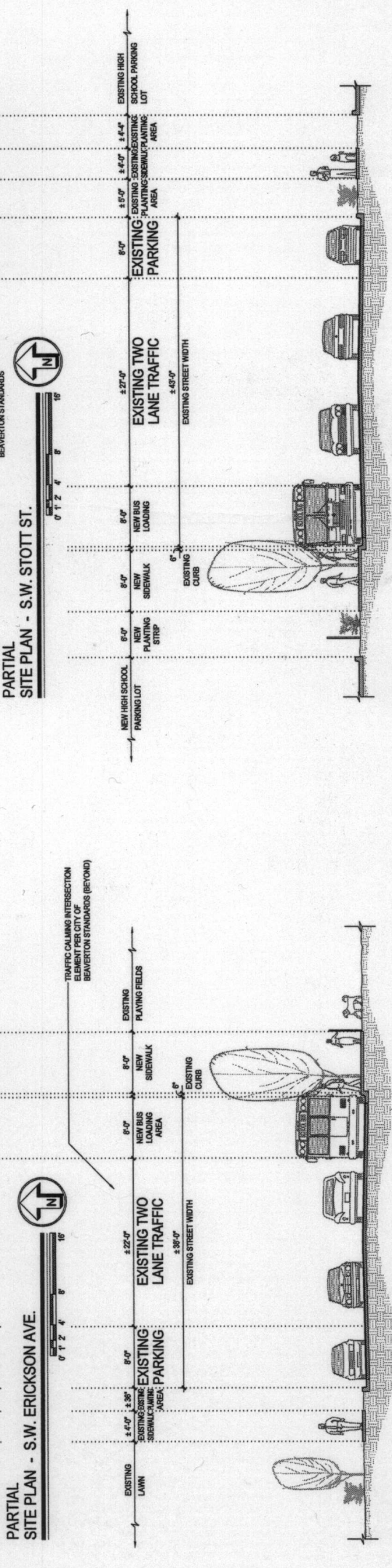
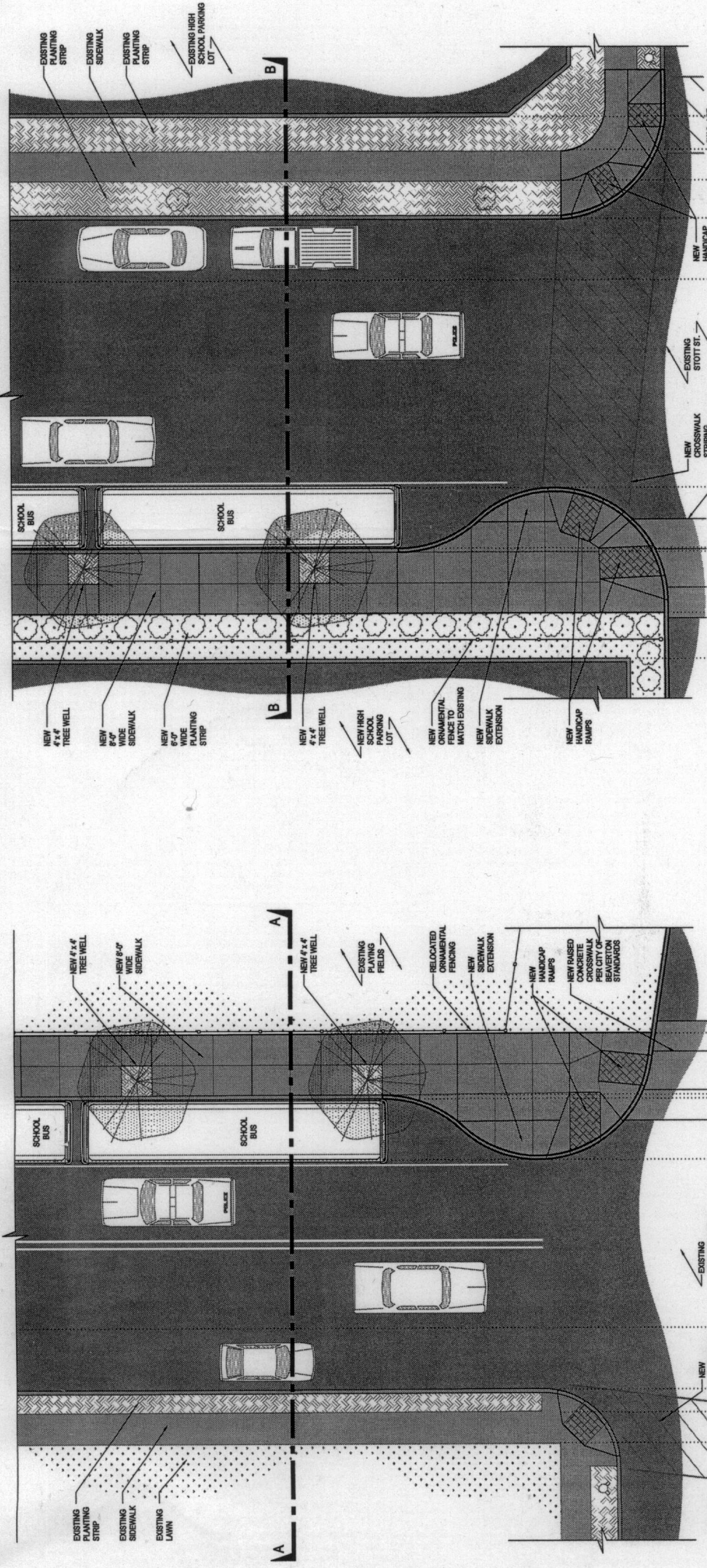
<u>Category</u>	<u>Land Use (Units)</u>	<u>TIF per Unit</u>
<u>Residential</u>	Single-family house (per unit)	\$2,260
	Apartment (per unit)	\$1,379
	Condominium (per unit)	\$1,322
	Retirement Community (per unit)	\$ 746
<u>Institutional</u>	Church (per 1,000 gsf)	\$1,005
	Day Care (per 1,000 gsf)	\$4,663
	Nursing Home (per bed)	\$ 235
	Hospital (per 1,000 gsf)	\$1,424
	Elementary School (per student)	\$ 97
<u>Commercial</u>	Specialty Retail (per 1,000 gsf)	\$2,319
	Discount/Bulk Retail (per 1,000 gsf)	\$3,999
	Shopping Center (per 1,000 gsf)	
	< 50,000 gsf	\$5,398
	50,000 gsf to 99,999 gsf	\$4,817
	100,000 gsf to 199,999 gsf	\$3,797
	Fast Food (per 1,000 gsf)	\$5,700
	Quality Restaurant (per 1,000 gsf)	\$5,450
	Convenience Market (per 1,000 gsf)	\$5,700
	Bank (per 1,000 gsf)	\$5,700
	New Car Sales (per 1,000 gsf)	\$2,709
Supermarket (per 1,000 gsf)	\$5,700	
<u>Office</u>	General Office (< 100,000sf) (per 1,000 gsf)	\$3,376
	Medical Office (per 1,000 gsf)	\$7,073
	Business Park (per 1,000 gsf)	\$2,571
<u>Industrial</u>	Light Industrial (per 1,000 gsf)	\$1,512
	Heavy Industrial (per 1,000 gsf)	\$ 326
	Manufacturing (per 1,000 gsf)	\$ 835
	Warehouse (per 1,000 gsf)	\$1,059
	Mini-warehouse (per 1,000 gsf)	\$ 566

NOTE: TIF rates valid through June 2002.



**LEGEND**

	ASPHALT
	CONCRETE
	EARTH/PLANTED AREA
	HANDICAP RAMP
	CONSTRUCTION LINES
	FENCE LINE



**PARTIAL SECTION 'A-A' - S.W. ERICKSON AVE.**

**PARTIAL SECTION 'B-B' - S.W. STOTT ST.**

# PARTIAL SITE PLAN / STREET SECTIONS | B. H. S. SITE IMPROVEMENTS

## BEAVERTON SCHOOL DISTRICT # 48





# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

February 1, 2002

Ms. Claire Doolittle  
Beaverton School District  
Facilities Department  
16550 SW Merlo Road  
Beaverton, OR 97006

RE: REQUEST FOR PAYMENT

Dear Ms. Doolittle:

On 1/31/2002 we received a **Purchase Order** for the Beaverton School District for **Beaverton High School Cafeteria Phase II, Tree Preservation, Conditional Use and Variance for Bicycle Rack Storage** fees as listed below. The request was accompanied by Purchase Order(s) # F 05997 dated 1/31/2002 for processing payment of the following fees:

\$ 1,662.00	Type 3 Design Review Fee – Cafeteria Phase II – BDR 2001-0213
\$ 1,199.00	Conditional Use Permit Fee – CUP 2001-0031
\$ 883.00	Variance Fee – Bicycle Rack Storage – VAR 2002-0002
\$ 600.00	Tree Preservation Fee – TPP 2001-0008

Please remit a check payable to the City of Beaverton in the amount of **\$4,344.00** representing payment for **PO # F 05997** per above-referenced fees, and refer to the following permit number(s) on the check: **BDR 2001-0213, CUP 2001-0031, VAR 2002-0002, TPP 2001-0008**

If you have any questions, please call me at (503) 526-2429.

Sincerely,

Steven A. Sparks, AICP  
Development Services Manager

Enclosure(s)

cc: file

G:Bonnie/Beaverton School Dist. PO Letter

**BEAVERTON SCHOOL DISTRICT #48**  
 16550 SW Merlo Rd. Beaverton, Oregon 97006-5152  
 503-591-8000 FAX: 503-591-4139

Contact: Vlad Voytilla 503-591-4457 1-31-02  
 Name Phone FAX Date

PURCHASE ORDER NUMBER  
 VALID ONLY WHEN NUMBERED, DATED AND SIGNED

**F05997 JAN 31 02**

**DELIVER TO:**

SCHOOL:  
Facilities Department

STREET ADDRESS:  
16550 SW Merlo Rd.

CITY:  
Beaverton, OR 97006-5152

**BILL TO:**

ACCOUNTS PAYABLE  
 Beaverton School District #48  
 16550 SW Merlo Rd.  
 Beaverton, Oregon 97006-5152  
 Telephone: 503/591-4324

**VENDOR INSTRUCTIONS**

1. Send original invoice and duplicate to the District. A separate invoice for each purchase order is required.
2. Enclose a packing list with all deliveries.
3. Ship prepaid.
4. Include purchase order number on all packages, invoices, shipping notices, and all other correspondence relating to this order.
5. Additional terms and conditions on the reverse are a part of this purchase order.

VENDOR ADDRESS

CITY OF BEAVERTON  
 PO BOX 4755  
 BEAVERTON, OR 97076-4755

RECEIVED

JAN 31 2002

ATTENTION:

PROJECT # 1045

PAYMENT TERMS: NET 30  
 THIS IS A TAX EXEMPT DISTRICT.

QTY.	UNIT	CATALOG NUMBER	DESCRIPTION	UNIT PRICE	AMOUNT
			BEAVERTON HIGH SCHOOL, PHASE II	\$	\$
		BDR 2001-0213	CAFETERIA BDR APPLICATION FEE		1662.00
		TPP 2001-0008	TREE PRESERVATION APPL. FEE		600.00
		CUP 2001-0031	CONDITIONAL USE PERMIT FEE		1199.00
		VAR 2002-0002	VARIANCE FOR BICYCLE RACK STORAGE FEE		883.00
SHIP VIA:			FOB POINT:	SHIPPING/HANDLING:	
FOR EDUCATIONAL PURPOSES ONLY. TAX EXEMPTION NO. A-93-730104K Federal Tax I.D. No. 93-6001065 Acceptance of and filing this order certifies compliance with all Federal and State of Oregon safety and health laws. FAILURE TO INCLUDE OUR PURCHASE ORDER NUMBER ON ALL CORRESPONDENCE COULD RESULT IN DELAY IN PAYMENT OF YOUR INVOICE.				<b>TOTAL</b>	\$ 4,344.00

*C. Wick*  
 Beaverton School District #48

BELOW INFORMATION IS FOR DISTRICT USE ONLY.

FISCAL YEAR 01-02

RETURN VENDOR COPY TO ORIGINATOR.

X 0452	1045	62	3003	\$
410	640	4152	0670	\$
				\$
				\$
				\$
				\$
FUND OR GRANT COST CENTER PROGRAM			OBJECT	TOTAL
Total must match total above.				\$

**PARTIAL PAYMENT RECORD**

Date Partial Order Rec'd.	Received By (Name)	Amount Approved
		\$
		\$
		\$
		\$
		\$
	Completed Order Total	\$

*M. Money*  
 COST CENTER AUTHORIZATION  
 Retain GOLDENROD copy in building. Forward all other copies to Business Office.

PURCHASE ORDER  
 301 080

## Sambo Kirkman

---

**From:** Frank Angelo [fangelo@angelo Eaton.com]  
**Sent:** Friday, March 08, 2002 7:54 AM  
**To:** Sambo Kirkman  
**Subject:** BHS Height

Sambo - the height of Beaverton HS is about 31feet for the majority of the building. The auditorium is 60 feet.  
Frank

# DKS Associates

1400 SW 5<sup>th</sup> Avenue, Suite 500  
Portland, OR 97201  
Phone: (503) 243-3500  
Fax: (503) 243-1934

## Memorandum

**TO:** Frank Angelo, Angelo Eaton  
**FROM:** Carl D. Springer, P.E.  
Scott Mansur  
**DATE:** March 13, 2002  
**SUBJECT:** Response to Planning Commission Comments

P01294

RECEIVED  
COMMUNITY DEVELOPMENT DEPT.

This memorandum responds to transportation related queries made by the City of Beaverton Planning Commission at their March 6, 2002 meeting regarding the 2nd Street Vacation. The following items have been addressed:

- Discussion on the traffic pattern changes due to the proposed street vacation with the attached figure of rerouted trips.
- Impacts to the northbound left turn from Erickson Avenue onto Farmington Road.
- Existing and Proposed bus loading and unloading operations.
- Further clarification to the actual volume changes summarized in the DKS Associates Level of Service memo dated January 25, 2002.

### ***School Traffic Pattern Changes due to the Proposed Street Vacation***

Existing traffic volumes that travel on 2<sup>nd</sup> Street between Erickson and Stott were rerouted to simulate the changes in travel patterns if the street were closed. Factors considered in making these re-assignments included the school service area boundary, the proposed configurations of the new parking lots and student drop-off area, and proposed school bus circulation routes. Figure 3A has been developed to show the proposed rerouting of traffic due to the 2<sup>nd</sup> Street vacation.

The parking lot layout at Beaverton High School after the street vacation will include a 35-stall parking lot with a vehicle turn around accessed via Erickson Avenue and a 78-stall parking lot accessed via Stott Street. This type of school parking would shift some vehicles away from Erickson Avenue to Stott Street. This is partial cause for the reduced volumes turning from Farmington Road onto Erickson Avenue with the proposed street vacation. Looking at Figure 3A attached, a portion of the existing eastbound right hand turns from Farmington Road to Erickson Street were rerouted as through volumes that would now turn right at Stott Street to access the parking lots on the opposite side of where they are today. Similarly, the westbound left hand turns from Farmington Road to Erickson Avenue (coming from the eastern school boundary) were rerouted as left turns from Farmington Road to Main Street or Watson Avenue. No left turns are currently allowed from Farmington Road to Stott Street due to a median on Farmington Road. Traffic destined for the east parking lots would turn left at Main Street or Watson Avenue and head west on 2<sup>nd</sup> Street since the vacation will not allow access to the east parking areas from Erickson Avenue.

Figure 3A also shows that a small amount of school traffic will be added to 5<sup>th</sup> Street between Erickson and Stott for similar reasons noted above for Farmington Road. The net change in peak hour volume is approximately 36 vehicles. This road segment carries about 440 vehicles during peak hours today. With

the street vacation, 5<sup>th</sup> Street would be expected to carry 476 vehicles during the peak hours. The resulting total volume is within the volumes expected on a collector facility. 5<sup>th</sup>/6<sup>th</sup> Street would continue to operate within Beaverton's performance standards.

As previously stated, the traffic impact analysis considered the city's performance standard (cited below) in assessing the significance of traffic changes resulting from the 2<sup>nd</sup> Street vacation. In terms of street operations and performance, this is the only objective criterion available for evaluating the significance of the proposed street vacation. Other issues noted at the hearing (e.g., neighborhood livability) do not have quantifiable parameters.

**Minimum Transportation Performance Standard.** The City of Beaverton has identified LOS E (average vehicle delay below 55.0 seconds) as the minimum performance standard for their signalized intersections during peak hours. Level of service D (average delay below 35.0 seconds) standards was identified for unsignalized intersections.<sup>1</sup>

We continue to find that the 2nd Street Vacation between Erickson and Stott will not cause any of the study area intersections to fall below the City of Beaverton's minimum transportation performance standards. The Erickson Avenue approach to Farmington Road does not comply to the city's performance standard today, and the significance of the street vacation at that location is explained in the following section.

#### ***Impacts to the Northbound Left Turn on Erickson Avenue at Farmington Road***

The northbound left turn from Erickson Avenue to Farmington Road currently operates at level of service "F". All other approaches operate as acceptable service levels. Approximately 20 to 40 vehicles make this movement during peak hours, which is 1% to 2% of the total intersection volume. The proposed street vacation was shown to reduce the northbound left turn by one trip in each of the peak hours. Based on the discussion at the Planning Commission, we reevaluated the distribution at this intersection and determined that this left turn would be expected to gain a maximum 3 trips due to the new parking lot egress on Erickson Avenue. Revisions were made to Figures 3A, 4 and 6 and attached to this memo. A level of service analysis was not redone at this intersection since this movement is already at LOS F and the street vacation would not impact the intersection any further. The additional trips at this intersection resulting from the street vacation would add approximately 1 vehicle to the 95<sup>th</sup> percentile queue of the northbound left turn during the peak hours<sup>2</sup>. The city does not have criterion for the significance of the added one vehicle to the maximum queue, but it is concluded that the relative impact is minimal.

A related issue discussed at the Planning Commission meeting questioned if the left-turning vehicles from northbound Erickson onto 2<sup>nd</sup> Street were avoiding the delays for making left-turns from Erickson onto Farmington noted above. It was speculated that during the a.m. peak hour that up to 54 left-turns were traveling west along 2<sup>nd</sup> Street to make a subsequent left-turn onto westbound Farmington Road. However, traffic counts taken in the fall of 2001 showed that the left-turns at 2<sup>nd</sup> Street and Farmington Road included only 13 northbound left turns. The other 40 vehicles counted stopped at intermediate destinations (e.g., 2<sup>nd</sup> Street driveway from the student parking lot) or made U-turns and returned back to Erickson Avenue. Either way, it does not appear that this is an attractive route to avoid delays at Erickson and Farmington. Furthermore, it does not seem that this activity is related to the 2<sup>nd</sup> Street vacation and, therefore, will have no bearing on those possible impacts.

<sup>1</sup> Beaverton Development Code, Section 60.60.25.

<sup>2</sup> Highway Capacity Manual 2000, Transportation Research Board, Chapter 16, Exhibit 17-19.

***Existing and Proposed Bus Loading and Unloading***

Buses currently load and unload in two parallel rows along 2<sup>nd</sup> Street in front of the main Beaverton High School building. This configuration requires students to walk in front of buses during loading and unloading. With the proposed street vacation, buses will load and unload along Stott Street and Erickson Avenue. Approximately nine buses will load and unload along Erickson Avenue. These buses will face northbound within a curb lane along the east sidewalk on Erickson Avenue. Approximately 15 buses along Stott Street will face southbound within a curb lane along the west sidewalk on Stott Street. Commissioner Maks questioned the impacts to Erickson Avenue and Stott Street due to the flashing red lights that would stop traffic for 10 to 15 minutes while buses are parked along Erickson Avenue and Stott Street. Based on Oregon Law<sup>3</sup> bus drivers are not required to use their flashing red lights when their bus is loading and unloading off of the main traveled portion of the roadway. Along both Erickson Avenue and Stott Street, buses will be off of the travel lanes in a curbside loading and unloading area and therefore would not be required to stop traffic along both streets. This assumption was confirmed in a phone conversation with the Beaverton School District Transportation Manager<sup>4</sup>. Therefore, student loading and unloading along Stott Street and Erickson Avenue will not cause school buses to use their flashing red lights to stop traffic. Traffic on Stott Street and Erickson Avenue will not be interrupted by school bus loading and unloading operations.

***Further clarification to the actual volume changes summarized in the DKS Associates Level of Service memo dated January 25, 2002***

DKS Associates submitted a letter on January 25, 2002 clarifying that due to an error in analysis while reviewing the intersection of 5<sup>th</sup>-6<sup>th</sup> Street/Erickson Avenue the intersection level of service would be LOS D rather than LOS E as stated in the traffic study. Commissioner Maks requested further clarification to the movement and change of volume.

The mistake and clarification can be understood by looking at the AM peak hour southbound right turn volume of 14 vehicles at 5<sup>th</sup>-6<sup>th</sup> Street/Erickson Avenue in Figure 2 in the traffic study. Then turning to the same movement volume during the AM peak hour in Figure 4 (with the proposed street vacation), the right turn volume was increased to 87 trips. This volume should have been increase by only 8 trips (as shown in Figure 3A attached) rather than 73 trips as shown in Figure 4. The same mistake was continued to Figure 6 as explained in the previous memo.

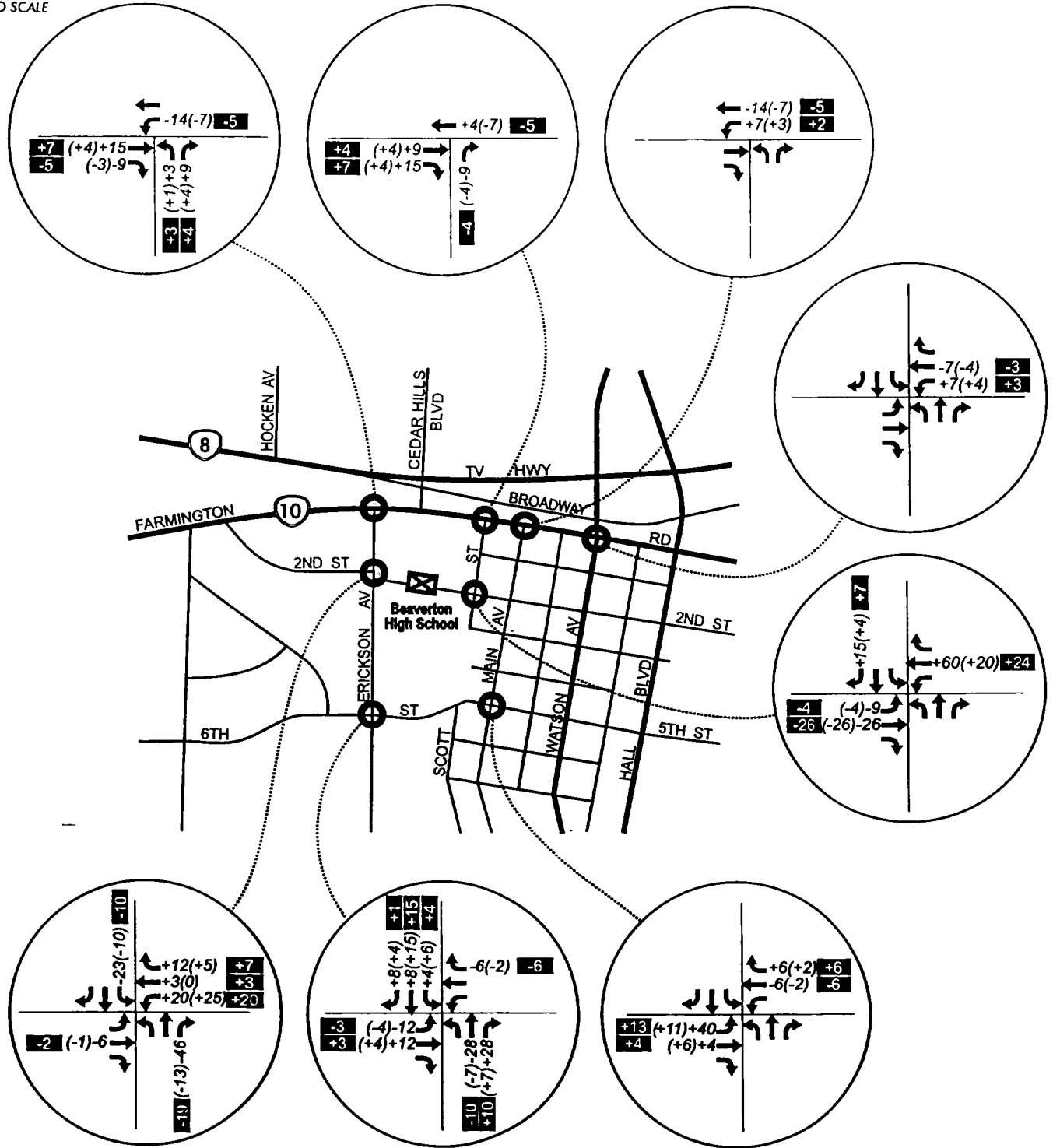
***Conclusion***

Based on the City of Beaverton's minimum transportation standards we have shown no significant impacts to study area roadways due to the proposed street vacation. This additional analysis is consistent with the original findings in the 2<sup>nd</sup> Street Vacation Traffic Report.



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<sup>3</sup> OAR 581-053-0015

<sup>4</sup> Phone Conversation with Dennis Essary, Beaverton School District Transportation Manager, March 13, 2002.

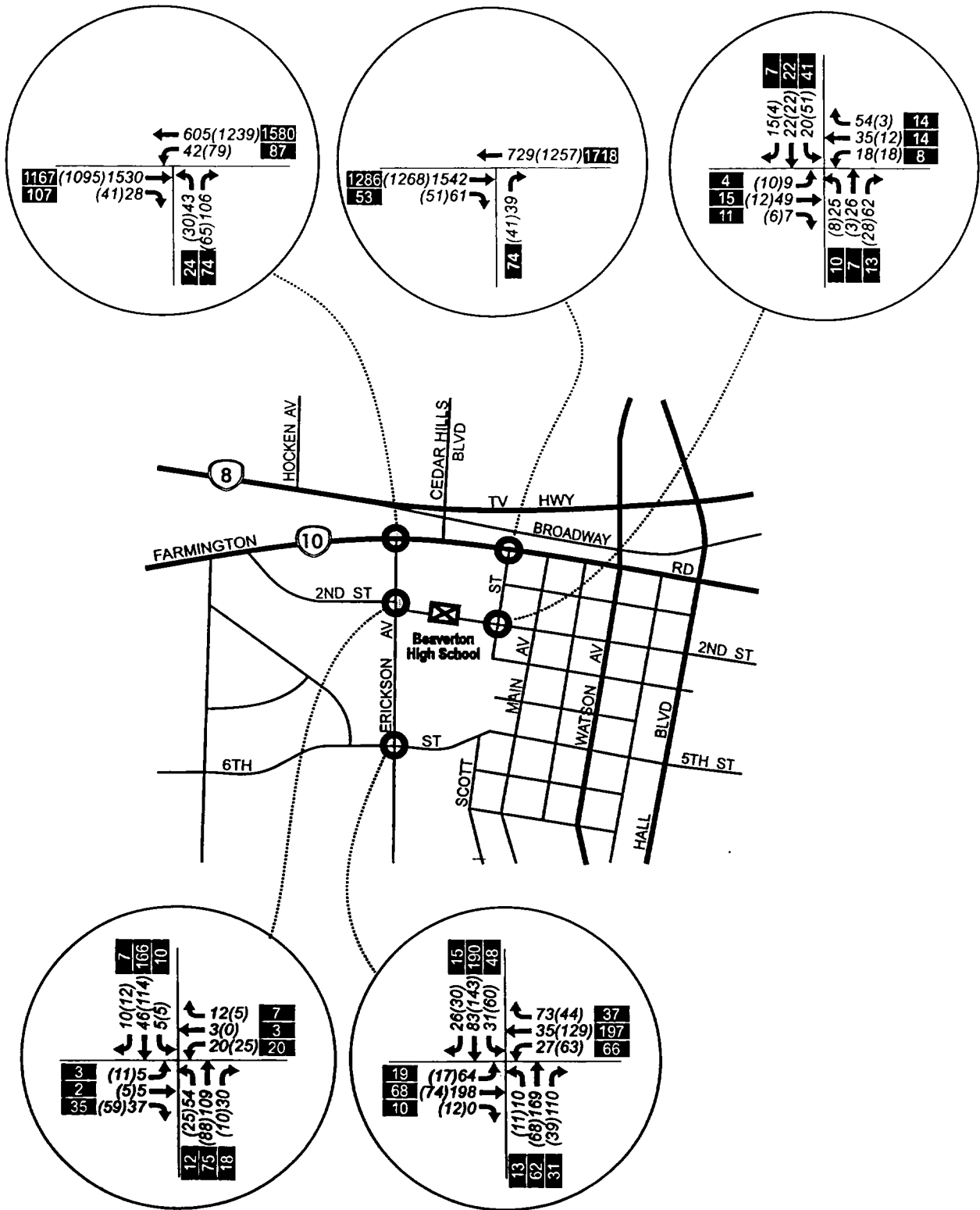


**LEGEND**

-  - Effected Intersection
- AM(Mid)  - Peak Hour Traffic Volume
- Mid= Mid-day

**Figure 3A**  
**REDISTRIBUTED TRAFFIC**  
**WITH STREET VACATION**  
**PEAK HOUR TRAFFIC VOLUMES**





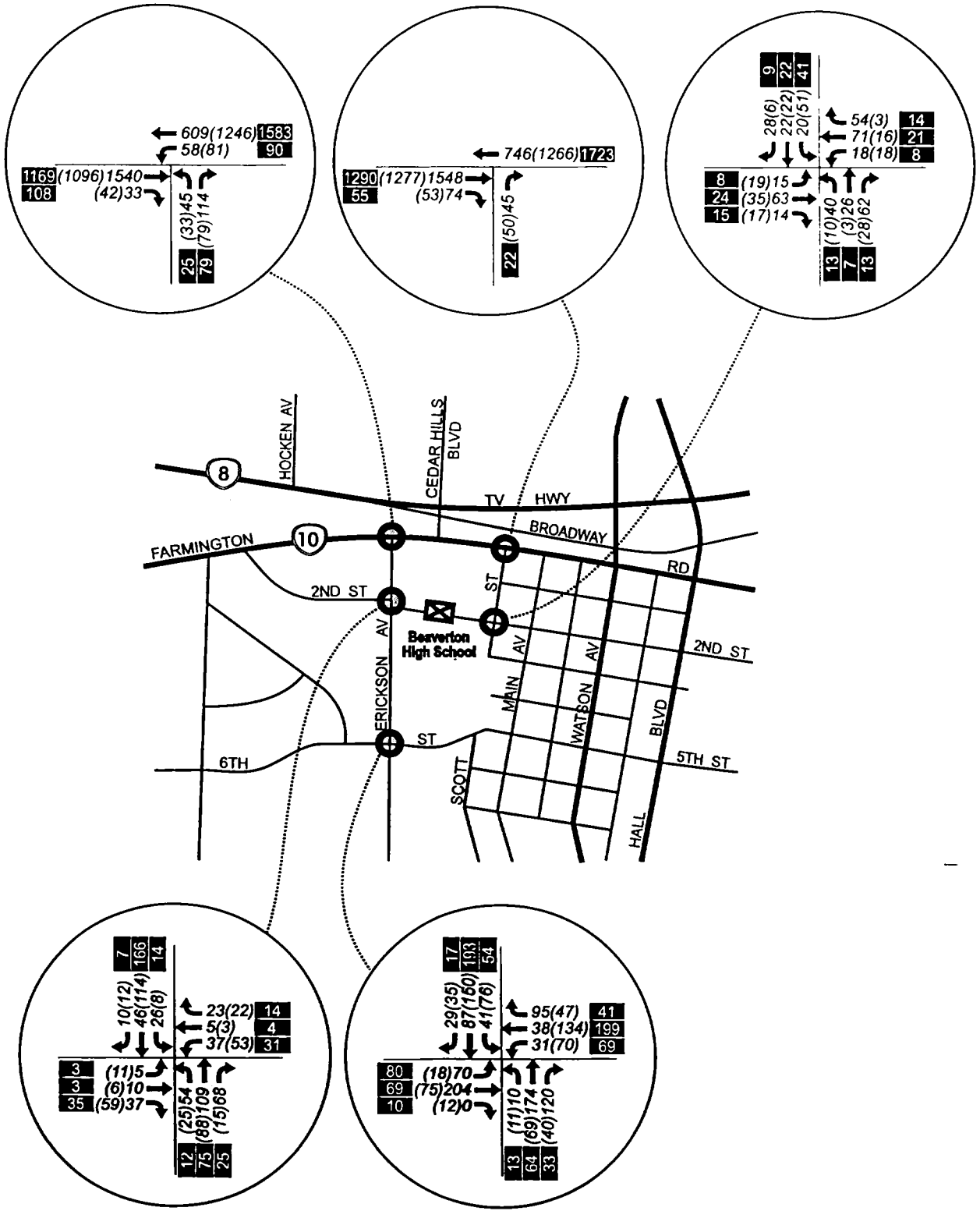
**LEGEND**



- Study Intersection

AM(Mid) PM - Peak Hour Traffic Volume  
Mid= Mid-day

**Figure 4**  
**EXISTING WITH STREET VACATION**  
**PEAK HOUR TRAFFIC VOLUMES**

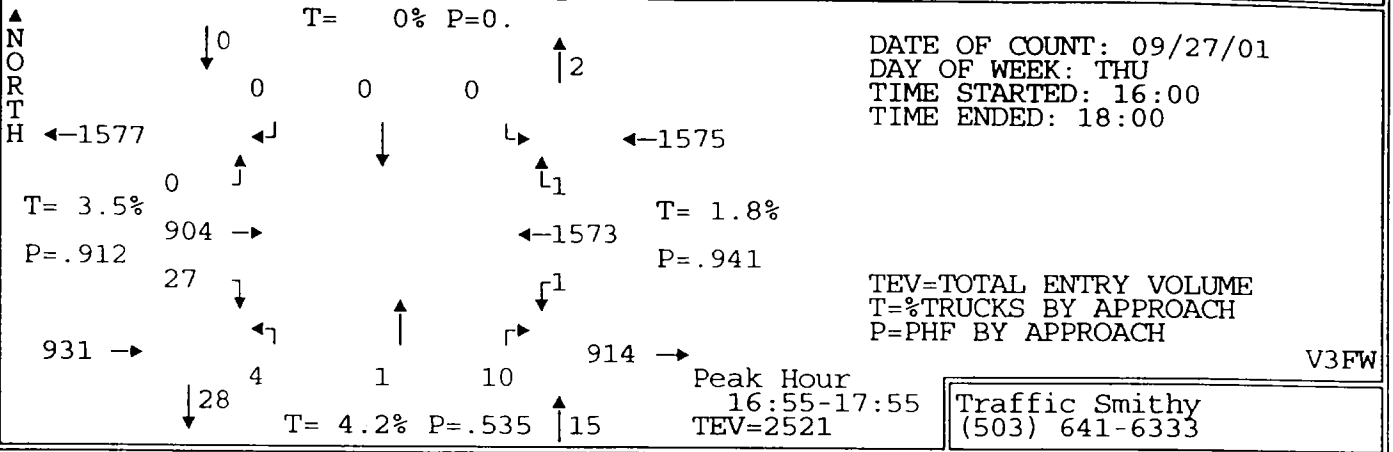


**LEGEND**

- Study Intersection
- AM(Mid)PM - Peak Hour Traffic Volume
- Mid= Mid-day

**Figure 6**  
**WORST CASE WITH STREET VACATION AND**  
**MAXIMUM STUDENT CAPACITY**  
**PEAK HOUR TRAFFIC VOLUMES**

**INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT**  
**FARMINGTON ROAD @ 2ND STREET**

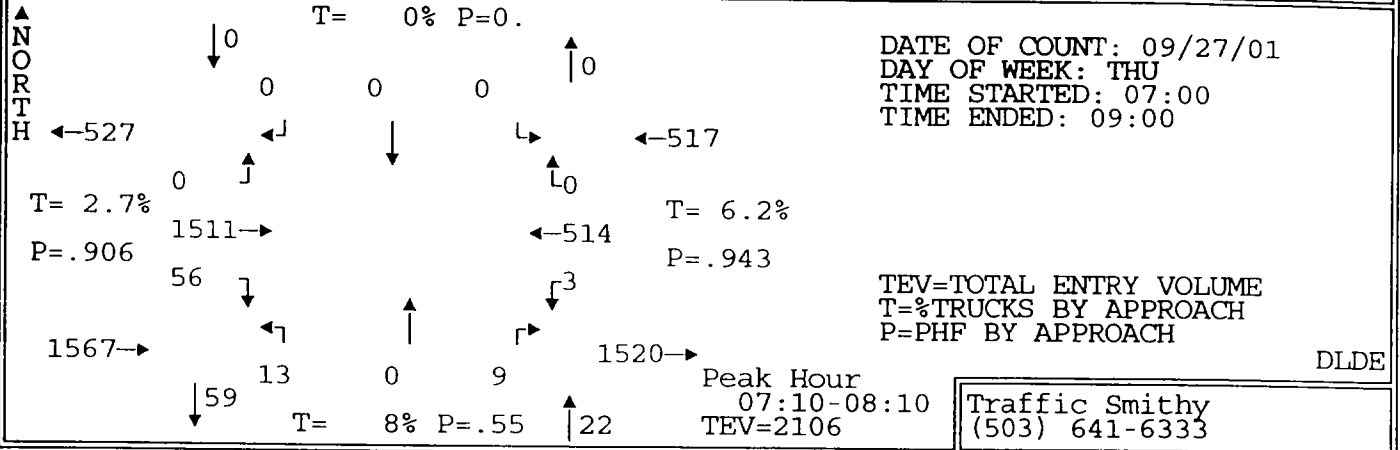


TIME PERIOD FROM - TO	EAST BOUND		SOUTH BOUND			NORTH BOUND		WEST BOUND			ALL		
	↓	→	↑	←	↓	←	↑	→	↓	←			
16:00-16:05	1	76	0	0	0	0	2	0	0	1	111	1	192
16:05-16:10	3	77	0	0	0	0	0	0	0	0	89	0	169
16:10-16:15	2	78	0	0	0	0	1	0	0	1	114	0	196
16:15-16:20	5	88	0	0	0	0	1	0	1	1	121	0	217
16:20-16:25	1	80	0	0	0	0	0	0	0	1	138	0	220
16:25-16:30	1	73	0	0	0	0	0	0	0	1	119	0	194
16:30-16:35	1	76	0	0	0	0	0	0	0	0	136	0	213
16:35-16:40	0	67	0	0	0	0	1	0	1	1	131	0	201
16:40-16:45	4	89	0	0	0	0	0	0	1	0	100	0	194
16:45-16:50	2	92	0	0	0	0	0	0	0	0	125	0	219
16:50-16:55	1	83	0	0	0	0	1	0	0	2	125	0	212
16:55-17:00	2	57	0	0	0	0	1	0	0	0	128	0	188
17:00-17:05	1	83	0	0	0	0	1	0	0	0	135	0	220
17:05-17:10	6	71	0	0	0	0	0	1	4	0	109	0	191
17:10-17:15	1	60	0	0	0	0	0	0	0	0	127	0	188
17:15-17:20	3	94	0	0	0	0	1	0	0	0	134	1	233
17:20-17:25	2	71	0	0	0	0	0	0	2	0	139	0	214
17:25-17:30	0	85	0	0	0	0	0	0	0	0	112	0	197
17:30-17:35	1	75	0	0	0	0	0	0	0	0	142	0	218
17:35-17:40	3	83	0	0	0	0	0	0	2	0	130	0	218
17:40-17:45	5	78	0	0	0	0	0	0	0	0	139	0	222
17:45-17:50	1	67	0	0	0	0	1	0	2	0	136	0	207
17:50-17:55	2	80	0	0	0	0	0	0	0	1	142	0	225
17:55-18:00	3	83	0	0	0	0	0	0	0	1	91	0	178

<b>Total Survey</b>	51	1866	0	0	0	0	10	1	13	10	2973	2	4926
<b>PHF</b>	.68	.9	0	0	0	0	.5	.25	.63	.25	.94	.25	.957
<b>% Trucks</b>	5.9	3.5	0	0	0	0	10	0	0	10	1.7	50	2.5
<b>Stopped Buses</b>	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>Peds</b>	0	0	0	0	0	0	0	0	0	0	0	0	0

<b>Hourly Totals</b>													
16:00-17:00	23	936	0	0	0	0	7	0	3	8	1437	1	2415
16:15-17:15	25	919	0	0	0	0	5	1	7	6	1494	0	2457
16:30-17:30	23	928	0	0	0	0	5	1	8	3	1501	1	2470
16:45-17:45	27	932	0	0	0	0	4	1	8	2	1545	1	2520
17:00-18:00	28	930	0	0	0	0	3	1	10	2	1536	1	2511

**INTERSECTION TURN MOVEMENT COUNT SUMMARY REPORT**  
**FARMINGTON ROAD @ 2ND STREET**



TIME PERIOD FROM - TO	EAST BOUND			SOUTH BOUND			NORTH BOUND			WEST BOUND			ALL	
	↓	→	↑	←	↓	↘	←	↑	↗	↓	←	↑		
07:00-07:05	2	121	0	0	0	0	0	0	0	0	1	24	0	148
07:05-07:10	3	104	0	0	0	0	0	0	0	0	0	37	0	144
07:10-07:15	3	132	0	0	0	0	0	0	1	0	0	33	0	169
07:15-07:20	1	120	0	0	0	0	0	0	1	0	0	37	0	159
07:20-07:25	5	115	0	0	0	0	0	0	0	0	0	51	0	171
07:25-07:30	5	128	0	0	0	0	0	0	2	1	0	40	0	177
07:30-07:35	12	124	0	0	0	0	1	0	0	1	0	39	0	178
07:35-07:40	11	133	0	0	0	0	3	0	0	1	1	50	0	197
07:40-07:45	7	145	0	0	0	0	1	0	1	0	0	36	0	192
07:45-07:50	3	126	0	0	0	0	2	0	2	1	0	39	0	173
07:50-07:55	2	132	0	0	0	0	1	0	0	0	0	52	0	187
07:55-08:00	2	135	0	0	0	0	0	0	0	0	0	38	0	175
08:00-08:05	3	97	0	0	0	0	2	0	0	0	0	47	0	149
08:05-08:10	2	124	0	0	0	0	0	0	1	0	0	52	0	179
08:10-08:15	3	105	0	0	0	0	0	0	0	1	1	30	0	139
08:15-08:20	2	109	0	0	0	0	0	0	0	1	0	36	0	148
08:20-08:25	3	116	0	0	0	0	0	0	0	1	0	43	0	163
08:25-08:30	4	118	0	0	0	0	0	0	0	0	0	31	0	153
08:30-08:35	0	99	0	0	0	0	0	0	1	0	0	38	0	138
08:35-08:40	1	119	0	0	0	0	1	0	0	1	0	52	0	174
08:40-08:45	0	116	0	0	0	0	0	0	0	0	0	36	0	152
08:45-08:50	4	90	0	0	0	0	0	0	1	0	0	43	0	138
08:50-08:55	0	96	0	0	0	0	0	0	0	0	0	31	0	127
08:55-09:00	0	87	0	0	0	0	0	0	0	0	0	22	0	109
<b>Total Survey</b>	<b>78</b>	<b>2791</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>14</b>	<b>0</b>	<b>11</b>	<b>8</b>	<b>937</b>	<b>0</b>	<b>3839</b>	
<b>PHF</b>	<b>.47</b>	<b>.94</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>.46</b>	<b>0</b>	<b>.56</b>	<b>.38</b>	<b>.94</b>	<b>0</b>	<b>.928</b>	
<b>% Trucks</b>	<b>5.1</b>	<b>2.7</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>14.3</b>	<b>0</b>	<b>0</b>	<b>12.5</b>	<b>6.2</b>	<b>0</b>	<b>3.6</b>	
<b>Stopped Buses</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	
<b>Peds</b>	<b>0</b>	<b>8</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	
<b>Hourly Totals</b>														
07:00-08:00	56	1515	0	0	0	0	11	0	8	4	476	0	2070	
07:15-08:15	56	1484	0	0	0	0	13	0	8	4	511	0	2076	
07:30-08:30	54	1464	0	0	0	0	12	0	5	5	493	0	2033	
07:45-08:45	25	1396	0	0	0	0	6	0	4	5	494	0	1930	
08:00-09:00	22	1276	0	0	0	0	3	0	3	4	461	0	1769	



RECEIVED

FEB 20 2002

City of Beaverton  
Development Services

M E M O R A N D U M

**TO:** Sambo Kirkman, City of Beaverton  
**FROM:** Frank Angelo *F.A.*  
**DATE:** February 19, 2002  
**CC:** Project Team  
**FILE #:** 007-008  
**RE:** Beaverton High School

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As a result of our meeting with staff on Wednesday, February 13<sup>th</sup>, we have made revisions to the following applications related to the proposed improvements at Beaverton High School:

- Design Review Application
- Tree Preservation Plan
- Variance Application
- Conditional Use Application

This memo summarizes the revisions and addresses some of the broader questions that came up at our meeting.

1. Bus Transportation / Circulation

State law requires the District to provide bus transportation for students. There are currently 19 school buses serving students at Beaverton High School. These buses presently drop-off and pick-up in front of the school on SW 2<sup>nd</sup>. Because SW 2<sup>nd</sup> is a one-way (eastbound) road, buses enter off of SW Erickson and exit via SW Stott. Bus drop-off and pick-up occur between the hours of 7:00 am to 7:45 am and 2:15 pm to 2:45 pm respectively. Following the site improvements, bus drop-off and pick-up will occur along both SW Erickson (10 buses - northbound) and SW Stott (12 buses - southbound). The increase from 19 to 22 buses assumes student enrollment of 2,200 students. The hours of drop-off and pick-up will remain the same as currently. Special education buses (smaller buses) will use the traffic circle area on the west side of the school - entering off of SW Erickson. This traffic circle area will be open throughout the day and, therefore, will also be available for parents and visitors to the school, unlike the

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620 SW Main, Suite 201  
Ph: 503.224.6974 Fax: 503.227-3679

Land Use Planning Transportation Planning Project Management

- *Memorandum - continued*
- *Page 2*

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current situation where visitors need to use on-street parking because SW 2<sup>nd</sup> is closed during school hours.

The separation of buses (10 on SW Erickson and 12 on SW Stott) will provide a better distribution of bus service to the school. The District's Transportation Services department will be able to distribute the routing patterns of buses based on how the buses will enter and exit the school. Buses using SW Erickson will enter from the south and west and, therefore, these buses will be targeted to serve students in the southern and western portion of Beaverton High School's service boundary. Buses using SW Stott will enter from the north and east and, therefore, these buses will be targeted to serve students in the northern and eastern portion of Beaverton High School's service boundary.

## 2. Student Enrollment

You requested information on when the District would anticipate student enrollment to reach 2,200 students as we have noted in the applications. As the applications note, prior to the opening of Southridge High School, enrollment at Beaverton High School reached 2,200 students. The opening of Southridge High School allowed the District to better balance high school enrollment patterns in the southern portion of the district. The certified enrollment for Beaverton High School (September 2001) is 1,775 students. Projecting when the school will reach 2,200 students in the future is a difficult task and, obviously, dependent upon a number of variables. I have discussed this issue with the District's demographic specialists and, based on their best judgement, the soonest Beaverton High School would be expected to reach 2,200 students would be the 2006/2007 school year. Of course, this is dependent on a variety of factors and, if another comprehensive high school was constructed in the district in the next five years, additional high school capacity would be available and Beaverton High School would likely not reach the estimated 2,200 students until a later date.

## 3. Parking Demand

You requested additional information regarding our request for the 10% Vehicle Parking Reduction for Transit Amenities (Section 60.20.10.A.2).

Regarding our discussions with Tri-Met staff, we have contacted Ben Baldwin, Planner/Bus Stop Group about the design detail of the bus stop and pedestrian plaza that is required as a part of the 10% reduction standard (see attached letter). As his letter notes, we are working with Ben on the transit plaza design requirements. While we don't have any specific design as of this date, the District is committed to provide this type of improvement in conjunction with the reduction in the vehicle parking requirement.

In terms of the adequacy of parking with the reduction of parking in place, we would note that the current parking of 311 spaces has been adequate for the current student and employee levels. The District has not received any complaints from surrounding

- *Memorandum - continued*
- *Page 3*

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property owners regarding the current parking situation at Beaverton High School. Our proposal is to provide 433 total spaces on the campus (an increase of 122 spaces over current parking spaces). The District has the ability to control on-site student parking through the use of parking permits. The District, as a part of its Employee Commute Options (ECO) program, encourages carpooling by students and staff. The District is also mandated to provide school bus service to students living more than one mile from the school. Finally, Tri-Met provides direct service to the campus on Farmington Road with a bus stop (Line 52) on Farmington near Stott.

It is also important to note that, when Beaverton High School had an enrollment of 2,200 students (prior to the opening of Southridge High School), portable classrooms were provided on campus and located in parking lots. Those parking lots were, therefore, not available for parking during that period. Additionally, the existing parking lot in the southeast corner of SW Erickson and SW 2<sup>nd</sup> was recently converted from a gravel lot to a paved lot, providing a much more organized and efficient parking arrangement. Finally, while not a preferred option, on-street parking is legal and available in the area.

The combination of these program and transportation alternatives gives the District the ability to monitor and control the use of parking facilities on campus. We believe that with the additional 122 parking spaces that will be provided in the two new parking lots on top of the existing available parking, the school will be able to meet the vehicle parking demand with the vehicle parking reduction in place.

#### 4. Site Constraints

You requested additional information regarding the site constraints at Beaverton High School, particularly in regards to the location of the driveway/parking area at Erickson. As we have mentioned in our application narratives, it is necessary to place the cafeteria and the supporting improvements as we have proposed because there are few alternatives to expand the campus without disrupting critical educational and community facilities.

The location of the cafeteria was determined in order to take advantage of student circulation patterns and because the existing campus site is constrained in terms of where facility development can occur. Beaverton High School is a closed campus. The proposed improvements will allow the campus to be contained and provide efficient and convenient use for students. The location of the cafeteria places it between the primary educational spaces – the existing high school and Merle Davies. Building expansion of the campus to the east or west would disrupt either SW Erickson or SW Stott. This would be far more disruptive to overall traffic circulation in the area than vacating the short segment of SW 2<sup>nd</sup> as we have proposed.

Expanding high school facilities to the south would eliminate athletic facilities that could not be replaced on campus and would disrupt current physical plant operations. In addition to providing athletic facilities for students, the fields on the south side of

- *Memorandum - continued*
- *Page 4*

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Beaverton High School also provide athletic facilities for Tualatin Hills Park and Recreation (THPRD) events and are used by the neighborhood as community recreational facilities.

Regarding the proposed location of the driveway, parking and turnaround area that enters the school from Erickson on the north side of the school, we have shifted the existing driveway access slightly to the north to eliminate the current offset intersection. The new location will provide a standard intersection. This shift has caused the improvements to require the removal of the existing trees in the current parking area adjacent to SW 2<sup>nd</sup>. You questioned whether we could relocate this driveway further to the north to avoid impacting the trees. Moving the driveway further to the north would recreate the offset intersection (only on the north side of SW 2<sup>nd</sup>) and would severely encroach on athletic fields that are used by students and the community. The encroachment on these athletic fields would be such that the fields would not be able to conduct competitive events because the remaining area would not meet specified athletic standards for field size. The trees that will be removed are identified as Historic in Beaverton's inventory. The arborist's report accompanying the Tree Preservation Plan notes that most of the trees to be removed are in poor or fair condition. These trees will be removed and replaced in other locations.

5. Parking Area on SW Stott at SW Farmington (east side of Merle Davies)

We have removed the six parallel parking spaces that were shown in the bus parking area adjacent to Merle Davies. These spaces were not included in our overall parking count and, therefore, removal will not impact the parking requirement for the high school. Removal of these spaces will improve bus circulation and facilitate safe pedestrian connections to the campus by way of the north – south pedestrian access between Farmington Road and the campus via the sidewalk that is located on the east side of Merle Davies.

6. Flagpoles

Sheet 5 indicates the placement of four (4) flagpoles – three in the traffic circle and one in front of the school. The single flagpole in front of the school currently exists. While the District would like to install three additional flagpoles, we understand that we will need a Flagpole Variance to exceed a total of two flagpoles. We, therefore, will amend our plan to provide a total of two flagpoles.

7. Landscaping within the Traffic Circle Area

You requested information on the landscape treatment within the traffic circle area (Sheet 5). This information is found on Sheet 12. The treatment inside of the traffic circle will include a bench for students and visitors to use. This is located at the base of the new flagpole area.



- *Memorandum - continued*
- *Page 5*

8. Bioswale (Sheet 12)

You asked whether the bioswale (Sheet 12) would be fenced. It will not be fenced.

9. Lighting Plan (Sheet 13)

You identified an area in the center of the parking lot that did not meet the standard for lighting. This plan has been amended to address the deficiency.

10. Parking Lot C

We have added additional landscape islands to address the city's parking lot landscape standard. The parking area that is on the inside of Merle Davies will be improved (restripping) and, therefore, we have provided additional landscaping. As a result of the additional landscaping, 3 parking spaces have been removed from our overall parking count (see comment 11).

11. Revised Parking Count

Due to the addition of parking lot landscaping to meet city requirements, the parking count (existing and proposed) for Beaverton High School has been revised as follows:

**Beaverton High School Vehicle Parking  
Table 1**

<b>Lot</b>	<b>Current Number of Parking Stalls</b>	<b>Proposed Number of Parking Stalls</b>
A	25	25
B	56	56
C	51	79
D*	0	35
E	80	80
F	56	56
G	32	32
H*	0	70
2 <sup>nd</sup> Street	11**	0
<b>TOTAL</b>	<b>311</b>	<b>433</b>

\* These are new lots

\*\* This lot will be removed, pending street vacation along 2<sup>nd</sup> Street (stalls outside right-of-way).

NOTE: Current parking within the 2<sup>nd</sup> Street right-of-way is 61 spaces, which will be removed pending the street vacation along 2<sup>nd</sup> Street.

As noted, we have requested the 10% reduction for Transit Plaza Credit. The required parking per City of Beaverton standards (for 2,200 students and 200 staff) is 480 spaces. Application of the 10% Transit reduction results in a requirement of 432 parking spaces. As the above table indicates, we comply with the minimum number of

- *Memorandum - continued*
- *Page 6*

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required parking spaces. The 433 parking spaces will be adequate to meet the parking demand on campus when one considers the other alternatives that are available for student and staff transportation.

12. Variance Request

Our request for a Variance from the Bicycle Parking standard has been revised to provide additional narrative demonstrating how our request complies with the Variance criteria. The revised narrative is included with this packet.

13. Tree Preservation Plan

Reference to "potentially historic" has been removed in the narrative and the reference is now to historic trees. Sheet 1 of the Tree Preservation Plan will remove the "potentially historic" note. The boundary of the critical root zone is accurate on the Tree Preservation Plan and is based on the size of existing trees.

14. Elevations

The elevation drawings have been revised to show the location of the proposed skybridge on the west elevation connecting the new cafeteria to the existing high school.

15. Sheet 17

The bicycle parking details have been revised.

16. The following plans have been included in this transmittal:

- 3 sets of full size plans
- 3 sets of 11 x 17 plans

17. The following revised narratives have been included in this transmittal:

- Revised Tree Preservation Plan
- Revised Variance for Bicycle Parking

I believe this addresses the comments that were raised by staff on February 13<sup>th</sup>. We look forward to discussing these items with you at the February 27<sup>th</sup> Facility Review Committee meeting. If you have any questions prior to that meeting, please give me a call at 503-227-3664.

cc. Project Team



TRI-COUNTY METROPOLITAN  
TRANSPORTATION DISTRICT OF OREGON

CAPITAL PROJECTS & FACILITIES DIVISION  
710 N.E. HOLLADAY STREET  
PORTLAND, OREGON 97232

DocC #CP012682

February 15, 2002

Sambo Kirkman  
City of Beaverton  
PO Box 4755  
Beaverton, OR 97076

**RE: Beaverton HS library and parking lot improvements**

Dear Ms. Kirkman:

Tri-Met welcomes this opportunity to comment on the proposed improvements to Beaverton HS on Farmington Road. Tri-Met line 52 serves the area with bus stops located near the site on Farmington between Erickson and Stott.

The purpose of our recommendations is to minimize the traffic impacts of new development by encouraging development patterns that are transit, bicycle, and pedestrian supportive. Improved pedestrian access and connection promotes walking and reduces local dependence on automobiles.

TRI-MET STAFF RECOMMENDATIONS

Amenity Requirements

**Provide a transit plaza.** Frank Angelo is working with Tri-Met to design bus stop facilities and a connecting pedestrian infrastructure that best serves the high school, its students, and the immediate community. Transit amenities and bus stop sponsorship opportunities are also being discussed and Tri-Met is confident that the resulting improvements will be beneficial to local transit patrons and a positive contribution to the streetscape.

Thank you for the opportunity to comment on this proposal. If you have any questions, please contact me at 962-2140.

Sincerely,

*Charlotte Craig*  
cc: Ben Baldwin  
Planner/Bus Stops Group

cc: Jillian Detweiler

**Revised Narrative for Variance Application**

**Beaverton High School**

**February 19, 2002**

### **III. Applicable Development Code Variance Approval Criteria**

The Variance – Public Hearing Written Narrative Requirements Submittal Checklist identifies five approval criteria, as listed in Beaverton Development Code Section 40.8015.2.C:

1. *Special conditions exist which are peculiar to the land, structure or building involved and which are not applicable to other lands, buildings or structures in the same district.*
2. *Strict interpretation of the provisions of this ordinance would deprive the applicant of rights commonly enjoyed by other properties in the same district under the terms of this ordinance.*
3. *The special conditions and circumstances do not result from the actions of the applicant and such conditions and circumstances do not merely constitute pecuniary hardship or inconvenience.*
4. *Granting the variance will be in harmony with the objectives of this ordinance, and not injurious to the neighborhood or otherwise detrimental to the public welfare.*
5. *Any variance granted shall be the minimum variance that will make possible a reasonable use of land, building and structures.*

The following responses address the listed approval criteria.

1. *Special conditions exist which are peculiar to the land, structure or building involved and which are not applicable to other lands, buildings or structures in the same district.*

#### **RESPONSE:**

The proposed Beaverton High School campus revisions would provide a new cafeteria building intended to improve the quality of education at Beaverton High School by (1) making the Beaverton High School campus more urban and integrated, and (2) providing a modern cafeteria facility in order to free up additional space in the existing high school building for the expansion and modernization of educational opportunities. The improvements would include:

- A new cafeteria building located north of the existing BHS main building;
- Renovation to the interior of the existing high school to enhance educational facilities;
- Enhanced pedestrian and bicycle facilities;
- Additional parking area; and
- Improvements to the front and entrance to BHS.

Special conditions exist related to the use of the Beaverton High School campus property because it is used for a public high school campus (as a conditional use within an R-10 residential zone). The special conditions include the following:

1. Beaverton High School is a public educational institution. The purpose of this type of land use by its very nature differentiates it from many other lands, buildings, and structures in the area. The primary purpose of Beaverton High School is to facilitate the education of high school students in order to help them become competent, self-sufficient members of society and allow them to learn skills for their future lives. *The mission of the Beaverton School District is: "To meet the educational needs of each student in our schools and to provide a quality education which prepares each student to become a productive member of the community."* For the Beaverton School District, decisions regarding the Beaverton High School campus should keep the students' interest in mind at all times. The Beaverton School District is mandated to dedicate the maximum amount of resources and campus space to educational and education-related facilities rather than non-educational facilities. This variance request stems from the belief that it is more important for space and resources at Beaverton High School be used to fulfill the mission of the Beaverton School District by providing space for educational facilities rather than for bicycle parking that will ultimately be underutilized by students (as discussed in the next paragraph).
2. Based on observations at Beaverton High School and at other high schools in the area, 56 long-term bicycle spaces, as proposed in the site plan, would be more than adequate to accommodate student and staff demand for bicycle parking. A survey conducted November 13-21, 2001 at Sunset High School showed that the maximum number of bicycles (student and staff) parked on campus at any one time was five. According to Sunset High School officials, the number of bicycles observed increases somewhat during the early fall and the spring (ideal bicycling times) and is at most a doubling of bicycles observed during the winter months, which would bring demand to approximately ten parking spaces. Sunset High School was required to add 55 new long-term bicycle spaces in 1999, and the supply far exceeds the demand. Sunset High School is comparable to Beaverton High School in terms of student population. Observers at Beaverton High School have indicated that the current bicycle parking at Beaverton High School (less than 56 spaces) is not fully utilized. Other high schools in Beaverton have applied for bicycle parking variances, indicating that the current requirement in the Beaverton code may be excessive and not indicative of the actual demand for bicycle parking at a public high school.
3. The available space at Beaverton High School is constrained due to both internal campus structure and adjacent land uses. Because of continued growth in student population throughout the Beaverton School District, it has become important to more efficiently use existing school sites. The District does not have alternative locations to provide additional high school student capacity. This is the result of a number of factors, including the lack of available sites for new high schools resulting from the constrained land supply within the Urban Growth Boundary and lack of funding for new high schools. Using existing facilities more efficiently is also consistent with Metro's 2040 Concept that promotes better utilization of land through infill. Devoting space to additional bicycle parking for which there is no demand, particularly on a constrained campus like Beaverton High School, detracts from the efficient use of the property as a public high school.

The Beaverton High School campus is a constrained site located in an urban residential area. The site is bounded to the north by Farmington Road and to the south by SW 5<sup>th</sup> Street. Most of the Beaverton High School campus is located between SW Erickson Avenue and SW Stott Street. There are some parking areas, a tennis court and ancillary buildings located across SW Stott and SW Erickson from the main High School campus. The main high school site currently includes athletic fields and facilities, parking areas, pedestrian and bicycle facilities, the main high school building and the historic Merle Davies annex. The site also includes some historic and significant trees. Non-school related uses adjacent to the high school campus are primarily residential, as the campus is located within an R-10 zoning district. The location of the proposed cafeteria building shown on Figure 1 was determined because it is the site that best functions to integrate the building with the other buildings on campus. Building location and other physical logistics are discussed in greater detail in the Conditional Use Permit application submitted to the City of Beaverton in December 2001.

As shown on the Revised Cover Sheet and the Revised Detail Sheet Bike Rack Detail, Attachments 1 and 17, 56 long-term bicycle parking spaces are proposed for the Beaverton High School campus. This places the bicycle parking in close proximity to the entrance of the school and within an area of high student/staff activity, which is important from a security perspective. Space that would be available for bicycle parking near the school buildings (main high school building, Merle Davies building, and proposed cafeteria) is limited. Farmington Road lies to the north of the Merle Davies building. To the west of the buildings there are a recreation field, pedestrian facilities, school bus drop-off/loading facilities and vehicle parking areas, including parking for handicapped persons. To the south of the building there are a track, athletic fields, loading facilities and parking areas. There are vehicle parking areas (including parking for handicapped persons) and school bus drop-off/loading facilities located to the east of the building. Drop-off/loading facilities for students enrolled in special education are also located directly to the east of the building. The uses directly adjacent to the school buildings are located as shown on the Revised Cover Sheet, Attachment 1, to efficiently accommodate students, staff, and the community.

Although schools are exempted from providing cover for long-term bicycle parking per Beaverton Development Code Section 60.60.65.4.A.2, long-term bicycle parking occupies a more significant amount of space when compared to short-term bicycle parking because long-term parking requires more security accommodations. According to Beaverton Development Code Section 60.60.65.4.A.2, long-term bicycle parking shall be "separated from motor vehicle parking and maneuvering areas by a barrier or sufficient distance to prevent damage to parked bicycles." As shown on the Revised Cover Sheet, Attachment 1, the proposed bicycle parking locations will meet the applicable code standards, particularly the bicycle parking located within the inside corridor of the Merle Davies Building.

The proposed Beaverton High School campus plan is planned to efficiently circulate students, staff, and community members to/from and through campus. Space on the Beaverton High School campus is at a premium, and the plan is intended to maximize land use efficiency. A 10% parking requirement reduction for transit amenities per Beaverton Development Code Section 60.20.10.10.A.2 is included as part of the Revised Design Review application (revised for Design Review completeness according to City

review) submitted to the City of Beaverton concurrently with this Variance-Public Hearing. In addition to providing enhanced transit stop benefits for the Beaverton High School campus and the community at large, the parking requirement reduction seeks to accommodate the parking demand in light of a scarcity of usable space on campus. Therefore, space used for vehicle parking could not instead be dedicated to additional space for bicycle parking.

One of the purposes of the proposed cafeteria building is to allow more space in the main high school building for educational classrooms and facilities. The Merle Davies annex is also at capacity in terms of use for educational facilities, offices, and storage space. It would be difficult to accommodate additional long-term bicycle parking within one of the campus buildings.

2. *Strict interpretation of the provisions of this ordinance would deprive the applicant of rights commonly enjoyed by other properties in the same district under the terms of this ordinance.*

**RESPONSE:**

The majority of other properties in the district (R-10) are not educational facilities. The R-10 zoning district is single-family residential (urban low density) and Beaverton High School is located within the R-10 zone as a conditional use. According to Beaverton Development Code Section 60.20.10.5, there are no minimum short-term or long-term bicycle parking spaces requirements for single-family dwellings, whereas there are minimum bicycle parking requirements for educational institutions. Other properties in the same district (R-10) do not have the same characteristics as the Beaverton High School campus, and therefore, Criterion #2 as it reads cannot be directly applied to this variance request.

If one were to apply Criterion #2 in terms of rights enjoyed by other students attending other high schools in Beaverton, strict interpretation of the bicycle parking requirement would deprive Beaverton High School students from access to opportunities due to the site constraints on the Beaverton High School campus. Other students at other high schools may not face the same space constraints. Those opportunities include (1) the provision of educational and education-related facilities, and (2) the provision of safe, secure easily accessible bicycle parking

- (1) *The provision of educational and education-related facilities.* The Beaverton High School campus is seriously constrained in terms of available space, and the Beaverton Development Code requirement for bicycle parking impedes the efficient use of space for different educational, administrative, and activity facilities. It has been observed at other Beaverton area high schools that bicycle parking at high schools is generally significantly underutilized. At Beaverton High School, scarcity of available space dictates that tough choices must be made regarding the best possible use of space. This variance request reflects the belief that the space to be used for fulfilling the bicycle parking requirement would be better utilized as some other facility that contributes to the well-being of Beaverton High School students and efficiency of the Beaverton High School campus.

The logical and most secure location for bicycle parking is on the northern side of Beaverton High School. The site design has maximized the number of long-term bicycle parking spaces that can fit in this area. Additional spaces would remove areas for other necessary campus elements or amenities, such as building space (education,



administration, and storage), vehicle parking, school bus drop-off/loading facilities, athletic facilities, pedestrian and bicycle pathways, or landscaping and beautification. Therefore, strict interpretation of this provision would impede the District's ability to provide necessary facilities that are provided at other high schools in Beaverton.

- (2) *The provision of safe and secure bicycle parking.* Strict interpretation of the ordinance would require that the Beaverton High School campus accommodate 66 long-term bicycle spaces in addition to the 56 spaces proposed, for a total of 122 spaces. The proposed bicycle parking (56 spaces) maximizes the amount of long-term bicycle parking that can be located north of the main high school building in light of site constraints, such as the location and positioning of existing and proposed buildings, athletic facilities, vehicle parking, school bus circulation, and landscaping. According to Beaverton Development Code Section 60.60.65.4.C, the location of short-term and long-term bicycle parking should ensure that bicycle parking is "secure, visible, and convenient to use." The 56 proposed long-term bicycle parking spaces would fulfill this requirement, as they will be located in areas with high levels of visibility and with easy connections to the bicycle way on SW 2<sup>nd</sup> Street.

The other 66 parking spaces would be pressed to fulfill the requirements of Section 60.60.65.4.C. Due to the layout of the Beaverton High School campus, the only feasible location for an additional 66 long-term bicycle spaces would be behind (south of) the main high school building. The area north of the main high school building is filled with other uses, and the area further south of the high school buildings is primarily devoted to athletic facilities and parking, and would be too far from the high school. The area behind the main high school building is off-the-beaten path, with accessibility problems and low visibility. The campus layout would require that bicyclists navigate through athletic fields and fencing to reach the area.

The lack of "eyes on the street" behind the main high school building would also likely make some bicyclists uncomfortable in terms of both bicycle security and personal safety. Students may not feel comfortable leaving their bicycles, many of which can cost \$300.00 or more, in an area that is not consistently supervised. In addition, students who participate in after-school activities may feel uncomfortable walking behind the building to get their bicycles in the dark. It often takes some time to unlock a bicycle (put the seat back on, pack the lock, put the headlight back on the bicycle, etc.), and students may feel vulnerable behind the main building because it is a low traffic area.

The bicycle parking requirement would in effect require Beaverton High School to provide 66 less-than-desirable bicycle parking spaces, whereas other schools may have the space to provide adequate facilities.

A reduction in the required number of long-term bicycle parking spaces at Beaverton High School would not affect students or community members wishing to bicycle to and from the high school campus. Based on observation of other high schools in the area, 56 long-term bicycle spaces would be more than adequate to accommodate student and staff demand. A survey conducted November 13-21, 2001 at Sunset High School showed that the maximum number of bicycles (student and staff) parked on campus at any one time was five. According to Sunset High School officials, the number of bicycles observed increases somewhat during the early fall and the spring (ideal bicycling times) and is at most a doubling of bicycles observed during the winter months, which would bring demand to approximately ten parking spaces. Sunset High School

was required to add 55 new long-term bicycle spaces in 1999, and the supply far exceeds the demand. Sunset High School is comparable to Beaverton High School in terms of student population.

The majority of Beaverton High School students ride school buses to school, and there is a Tri-Met bus stop on the north side of campus on the corner of SW Stott Street and Farmington Road (see Revised Dimension Plan Public Improvements, Attachment 4), which will be improved via a pedestrian plaza and other multimodal amenities as part of this project. Tri-Met transit service (Route 52) runs every 15 minutes at this transit stop from approximately 6:30am to 6:30pm, in both eastbound and westbound directions on weekdays.

**3. *The special conditions and circumstances do not result from the actions of the applicant and such conditions and circumstances do not merely constitute pecuniary hardship or inconvenience.***

**RESPONSE:**

The special conditions and circumstances discussed in Criterion #1 do not result from the actions of the applicant, and do not constitute pecuniary hardship or inconvenience. The special conditions and circumstances listed in the Response to Criterion #1 that motivated the variance request are summarized and addressed as follows:

*Special Condition #1:* Beaverton High School is a public educational institution. The mission of the Beaverton School District is: "To meet the educational needs of each student in our schools and to provide a quality education which prepares each student to become a productive member of the community." The provision of educational facilities is the highest priority on high school campuses in the Beaverton School District.

*Response:* Beaverton School District is required to devote a maximum amount of space and resources to educational and education-related facilities. The applicant cannot change the mission of the school district, and the space constraints at Beaverton High School have forced the applicant to make tough choices regarding the best use of campus space. Per the Beaverton School District's mandate to put education first, this variance request reflects the belief that the space required for mandated long-term bicycle parking would be better used for other education-related facilities that could be used by a majority of students.

*Special Condition #2:* Based on observations at Beaverton High School and at other high schools in the area, 56 long-term bicycle spaces would be more than adequate to accommodate student and staff bicycle parking demand. Observers at Beaverton High School have indicated that the current bicycle parking at Beaverton High School is not fully utilized.

*Response:* The applicant cannot influence the demand for bicycle parking at high schools in Beaverton. In the case of Beaverton high school students biking to school, the supply of bicycle parking spaces does not drive the demand for bicycle parking. A survey conducted November 13-21, 2001 at Sunset High School showed that the maximum number of bicycles (student and staff) parked on campus at any one time was five, even when a plenitude of high-quality bicycle parking spaces was available. According to Sunset High School officials, the number of bicycles observed on campus increases somewhat during the early fall and the spring (ideal bicycling times) and is at most a doubling of bicycles observed during the winter months, which would bring demand to approximately ten parking spaces. Sunset High School was required to add 55

new long-term bicycle spaces in 1999, and the supply far exceeds the demand. Sunset High School is comparable to Beaverton High School in terms of student population.

It does not appear that Beaverton High School is the only high school in Beaverton finding it difficult to comply with the code requirements for bicycle parking at educational institutions. Other high schools have also applied for bicycle parking variances, indicating that perhaps the code requirements are unusually stringent, particularly in light of the lack of student demand for bicycle parking and the allocation of available space that must occur on urban high school campuses.

*Special Condition #3:* The available space at Beaverton High School is constrained both due to internal campus structure and adjacent land uses. Because of continued growth in student population throughout the Beaverton School District, it has become important to more efficiently use existing school sites. The District does not have alternative locations to provide additional high school student capacity. Devoting space to additional bicycle parking for which there is no demand, particularly on a constrained campus like Beaverton High School, detracts from the efficient use of the property as a public high school.

*Response:* The Beaverton High School campus is a constrained site located in an urban residential area. The applicant cannot affect the external site constraints, and this application is intended to maximize the efficiency of the internal site structure. The site is bounded to the north by Farmington Road and to the south by SW 5<sup>th</sup> Street. Most of the Beaverton High School campus is located between SW Erickson Avenue and SW Stott Street. There are some parking areas, a tennis court and ancillary buildings located across SW Stott and SW Erickson from the main High School campus. The main high school site currently includes athletic fields and facilities, parking areas, pedestrian and bicycle facilities, the main high school building and the historic Merle Davies annex. The site also includes some historic and significant trees. Non-school related uses adjacent to the high school campus are primarily residential, as the campus is located within an R-10 zoning district. Given the internal and external space constraints, there are few ways in which facilities at Beaverton High School can be modernized or expanded; this variance is part of an application that offers a realistic option.

The Beaverton School District faces the challenge to provide effective, modern education while also facing the reality of scarcity of land or space for new or updated facilities. This is the result of a number of factors, including the lack of available sites for new high schools resulting from the constrained land supply within the Urban Growth Boundary and lack of funding for new high schools. Using existing facilities more efficiently is also consistent with Metro's 2040 Concept that promotes better utilization of land through infill.

This variance is not requested merely to avoid financial obligations associated with the construction of bicycle parking facilities. The request stems from the need to efficiently allocate space on the Beaverton campus for the uses that will best enhance the learning environment for students. Other uses of space, such as educational or athletic facilities, or efficient circulation of students and staff, are more important for the majority of students than the devotion of additional space to bicycle parking that few students will use. The proposed number of bicycle parking spaces (56) will be enough to accommodate those students and staff that ride bicycles to school.

- 4. Granting the variance will be in harmony with the objectives of this ordinance, and not injurious to the neighborhood or otherwise detrimental to the public welfare.*

**RESPONSE:**

Granting this variance would not cause harm to the surrounding neighborhood or the community, including high school students and staff. The proposed variance will, in fact, assist the District in providing a needed public service – an improved high school campus and educational capacity to serve the growing demand in student enrollment. There will be adequate bicycle parking to meet the demand of the students and staff as well as community members attending special events, such as athletic events, if the variance request is approved. As discussed in the response to Criteria #1 and #2, the supply of long-term bicycle parking spaces at area high schools far exceeds demand during the school day.

- 5. Any variance granted shall be the minimum variance that will make possible a reasonable use of land, building and structures.*

**RESPONSE:**

The proposed site plan for the Beaverton High School campus is intended to efficiently use land and allocate resources to the school's primary purpose – educational services. This variance request stems from the belief that it is more important that space on the Beaverton High School campus is used to fulfill the mission of the Beaverton School District by providing space for educational or education-related facilities than that the space be used for bicycle parking that will ultimately be underutilized by students. Because most high school students do not bicycle to school (the majority ride school buses), it is unreasonable to dedicate such a large amount of space to bicycle parking when it would not be fully utilized. Based on actual bicycle use observations at another Beaverton School District High School, 56 long-term bicycle parking spaces would be more than enough to accommodate those who wish to bicycle to school. The reduction in the bicycle parking requirement by 66 spaces (56 instead of 122) will not create a more intensive land use and will not change any function of the Beaverton High School campus. In fact, the approval of this variance would allow for greater efficiency on campus grounds.

The logical and most secure location for bicycle parking is on the north side of Beaverton High School. The site design has maximized the number of long-term bicycle parking spaces that can fit in this area. Additional spaces would remove areas for other necessary campus elements or amenities, such as building space (education, administration, and storage), vehicle parking, school bus drop-off/loading facilities, athletic facilities, pedestrian and bicycle pathways, or landscaping and beautification. Therefore, strict interpretation of this provision would impede the District's ability to provide educational and education-related facilities.

**Revised Narrative for Tree Preservation Plan**

**Beaverton High School**

**February 19, 2002**

## II. Introduction

The Beaverton School District (BSD) is seeking revisions to the existing Beaverton High School (BHS) campus (Site Improvement Plan, Figure 1) in order to provide a new cafeteria and to create a more urban high school campus that is integrated with the surrounding Beaverton urban area. As part of the funded improvement program, the improvements will include:

- A new cafeteria building located north of the existing BHS main building;
- Renovation to the interior of the existing high school to enhance educational facilities;
- Additional parking; and
- Improvements to the front and entrance to BHS.

The improvements at BHS are being designed not only to provide a modern educational facility but also to provide the community accessible space through pedestrian plazas and open space. The improvements will include measures to improve compliance with the Americans with Disabilities Act (ADA), such as easily accessible parking near the school entrance and auditorium and pedestrian system enhancements. The development will include the addition of one new building to be used for expanded cafeteria facilities. The new building will be located immediately north of the existing high school structure. In recent years Beaverton High School student population has ranged from 1,800 to 2,200 students (prior to the opening of Southridge High School). The Beaverton School District expects that enrollment will not exceed 2,200 in the future. The proposed improvements will enable the Beaverton School District to effectively accommodate curriculum for this range in student population. The campus improvements are intended to enable the District to enhance the educational facilities at BHS and improve the campus environment for students and the overall community.

The cafeteria addition, renovations to the existing high school, and the new parking lots are funded improvements through a school bond measure approved by district voters. There are no other funded improvements at BHS that will require land use review at this time.

This application seeks approval of a Tree Preservation Plan. This Plan includes a proposal to remove trees designated on the City's Tree Inventory. This removal of trees is part of a larger development/expansion project for BHS as described above. The Revised Landscape and Lighting Plans, Attachments 11-14, provide mitigation for this removal by installing a variety of trees, including Japanese Maples, Ash and White Oak trees.

The following written application is in response to the Tree Preservation Plan (TPP) submittal requirements for a Narrative Statement. As a consequence of the proposed improvements at BHS, a row of existing trees adjacent to the current alignment of SW 2<sup>nd</sup> Street (Tree Preservation Plan, Attachment 18) will be removed to enable new access to the school and pedestrian streetscape improvements to be constructed. Improvements will be made to the front of BHS to enhance the pedestrian environment and to clearly identify the main entrance. A more urban streetscape will be developed along the northern face of BHS. These improvements will emphasize pedestrian and bicycle access to and through the site. A strong pedestrian and bicycle connection between SW Stott and SW Erickson will be promoted through the streetscape improvements.

The City of Beaverton Tree Inventory provided by the City identifies trees as Historic or Significant, using a listing of criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above (indicating a historic tree by either age or condition), see City of Beaverton, Historic Tree Inventory Sheets in original application, Appendix C. The proposed improvements at BHS will require removal of 20 trees which are designated on the City's inventory as historic. No trees designated as Significant Trees in the City's Inventory will be impacted by the proposed development.

The proposed improvements to Beaverton High School also requires submittal of the following applications:

- Street Vacation Permit Application for the vacation of SW 2<sup>nd</sup> between SW Stott and SW Erickson.
- Comprehensive Plan Amendment to remove the Neighborhood Route Designation of SW 2<sup>nd</sup> between SW Stott and SW Erickson from the City's Functional Classification Plan Map.
- Design Review Type 3 Application.
- Conditional Use Application.
- Bicycle Parking Variance-Public Hearing Application

The Street Vacation Permit Application and the Comprehensive Plan Amendment applications were submitted to the City of Beaverton on November 15, 2001. The Design Review and Conditional Use applications have been submitted concurrently with this application.

### **III. Historic Tree Removal Review Criteria**

The City of Beaverton Tree Inventory provided by the City identifies trees as Historic or Significant, using a listing of criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above (indicating a historic tree by either age or condition), see City of Beaverton, Historic Tree Inventory Sheets in original application, Appendix C. The proposed improvements at BHS will require removal of 20 trees which are designated on the City's inventory as historic. No trees designated as Significant Trees in the City's Inventory will be impacted by the proposed development.

The following table illustrates the condition and status of the trees to be retained and the trees to be removed on the site with a cross-reference to the report completed by Pruett Tree and Landscape, a licensed arborist. Most of the trees to be removed are in poor or fair condition as noted by both the City's Inventory and the arborist report.

As noted in the far right column, City Tree Inventory Rating, Table 1 and 2, the majority of the trees to be removed are rated as a "3" for "some historic value" for age and a 2 for "fair condition." None of the trees to be removed were rated as having historical value or significant historic value.

## Proposed Trees to be Preserved

### Table 1

Tree Number on Tree Plan *	Arborist Report #	Species	Tree Diameter (DBH) (in inches)	Conditions/Comments	City Tree Inventory (approximate location)	City Tree Inventory Rating***	
1	n/r**	Beech	6.0	Not inspected	C	3	3
2	n/r**	Cherry	14.0	Not inspected	C	3	3
3	n/r**	Deodar cedar	40.0	Not inspected	C	3	3
4	n/r**	Norway Maple	10.0	Not inspected	C	3	3
5	n/r**	Norway Maple	8.0	Not inspected	C	3	3
6	n/r**	Norway Maple	10.0	Not inspected	C	3	3
7	n/r**	Norway Maple	10.0	Not inspected	C	3	3
8	n/r**	Norway Maple	10.0	Not inspected	C	3	3
9	n/r**	Norway Maple	10.0	Not inspected	C	3	3
10	n/r**	Norway Maple	4.0	Not inspected	C	3	3
11	24	Cherry	18.0	Not inspected	C	3	3
12	n/r**	Pine	12.0	Not inspected	C	3	3
13	n/r**	Pine	8.0	Not inspected	C	3	3
14	n/r**	Cedar	6.0	Not inspected	C	3	3
15	n/r**	Cedar	10.0	Not inspected	C	3	3
16	n/r**	Norway Maple	26.0	Not inspected	C	3	3
17	n/r**	Norway Maple	26.0	Not inspected	C	3	3
18	28	Norway Maple	14.0	Very poor	F	3	2
19	29	Norway Maple	18.8	Poor	F	3	2
20	30	Norway Maple	16.7	Poor	F	3	2
21	31	Norway Maple	17.0	Poor	F	3	2
22	n/r**	Pine	26.0	Not inspected	E	4	2
23	n/r**	Pine	26.0	Not inspected	E	4	2
24	n/r**	Sycamore	29.0	Not inspected	G	2	3
25	n/r**	Sycamore	29.0	Not inspected	G	2	3
26	n/r**	Sycamore	29.0	Not inspected	G	2	3
27	n/r**	Sycamore	29.0	Not inspected	G	2	3

\* Tree Protection Plan, Attachment 15

\*\* n/r = Not Referenced in Arborist Report

\*\*\* City's Tree/Grove Historical Rating Sheet



## Proposed Trees to be Removed

### Table 2

Tree Number on Tree Plan *	Arborist Report #	Species	Tree Diameter (DBH) (in inches)	Conditions/Comments	City Tree Inventory (approximate location)	City Tree Inventory Rating***	
28	22	Sugar Maple	6.8	Fair	I	3	2
29	23	Norway Maple	17.7	Fair	I	3	2
30	1	Norway Maple	16.1	Fair	D	3	2
31	2	Norway Maple	16.6	Poor	D	3	2
32	3	Norway Maple	19.0	Poor	D	3	2
33	4	Norway Maple	20.4	Poor	D	3	2
34	5	Norway Maple	18.3	Fair	D	3	2
35	6	Norway Maple	21.7	Poor	D	3	2
36	7	Norway Maple	15.8	Fair	D	3	2
37	12	Norway Maple	18.1	Poor	D	3	2
38	13	Norway Maple	15.5	Fair	D	3	2
39	14	Norway Maple	20.0	Poor	D	3	2
40	15	Norway Maple	19.3	Fair	D	3	2
41	16	Norway Maple	18.2	Fair	D	3	2
42	17	Sycamore	28.6	Fair	H	3	2
43	18	Sycamore	29.0	Fair	H	3	2
44	19	Elm	20.0	Poor	D	3	2
45	20	Elm	17.9	Fair	D	3	2
46	21	Norway Maple	25.4	Poor	I	3	2
47	n/r**	Red Maple	10.0	Not inspected	C	1	3

\* Tree Protection Plan, Attachment 15

\*\* n/r = Not Referenced in Arborist Report

\*\*\* City of Beaverton Tree Inventory, Tree/Grove Historical Rating Sheet

Section 40.75.15.1.C.2.a (1-5) of the Beaverton Development Code contains the review criteria to be considered in an application for removal of trees designated as Historic Trees by the City. The following provides findings that demonstrate how the proposed development at BHS will meet the applicable criteria for removal.

**Section 40.75.15.1.C.2**

2. *Historic Trees*

a. *The HRRC and Council shall approve the application if it finds the request meets at least one of the following factors:*

(1) *That removal is necessary to accommodate a new improvement, structure or remodeled structure, and no alternative exists for relocation of the improvement on the site, or that variances to setback provisions of the Development Code will not allow the tree to be saved or will cause other undesirable circumstances on the site or adjacent properties.*

**RESPONSE:** The request meets this factor. Removal of the 20 trees is the result of construction of the new cafeteria and driveway entrance at BHS. The new cafeteria building will be located immediately north of and connected to the existing BHS building on a vacated portion of SW 2<sup>nd</sup> Street. It is necessary to place the cafeteria at this location to take advantage of student circulation patterns and because the existing campus site is constrained in terms of where facility expansion can occur. There are no expansion opportunities to the east or west of the existing BHS. Expansion of facilities to the south would eliminate athletic facilities that could not be replaced on campus and would disrupt current physical plant operations.

(2) *That the tree has become a nuisance by virtue of damage to personal property or improvements, either public or private, on the subject site or adjacent sites, or that extraordinary maintenance is required to prevent damage to such improvements or property.*

**RESPONSE:** This criterion is not applicable to this application.

(3) *That the tree has lost its significance in terms of its original designation on the list due to damage from natural or accidental causes, or for some other reason it can be established that it is no longer of historic significance.*

**RESPONSE:** This criterion is not applicable to this application.

(4) *The removal of the tree is necessary to accomplish other public purposes such as installation of public utilities, street widening and similar needs and that no alternative exists to the removal of the tree without significantly increasing public costs or inconvenience or reducing safety.*

**RESPONSE:** Removal of the identified trees is necessary to widen the main entrance street to BHS and to provide sufficient space for pedestrian and bicycle connections through the campus.

As is indicated on Revised Tree Protection Plan, Attachment 18, seven trees are located in what will be the alignment of the revised main entrance to BHS.

- (5) *That the tree is unsafe to the occupants of the property, and adjacent property, or the general public.*

**RESPONSE:** This criterion is not applicable to this application.

- b. *The historic tree shall be preserved unless the applicant proves to the satisfaction of the reviewing body that removal is necessary as a result of:*

- (1) *The necessity to remove trees which pose a safety hazard to pedestrians, property or vehicular traffic or threaten to cause disruption of public service; or which pose a safety hazard to persons or buildings*

**RESPONSE:** This criterion is not applicable to this application.

- (2) *The necessity to remove diseased trees or trees weakened by age, storm, fire or other injury.*

**RESPONSE:** This criterion is not applicable to this application.

- (3) *The necessity to observe good forestry practices according to adopted City standards on the subject.*

**RESPONSE:** This criterion is not applicable to this application.

- (4) *Need for access immediately around the proposed structure for construction equipment.*

**RESPONSE:** The proposed Revised Tree Protection Plan, Attachment 18, has taken the need for access into consideration and has only illustrated the removal of trees necessary for construction. All other trees will remain on-site. Measures will be taken to ensure the existing trees' safety during construction, such as fencing and marking of trees that are near to the construction area. The Revised Tree Preservation Plan, Attachment 18 identifies the limits of disturbance. The line type has been emboldened and identified in the legend for further clarity. The approximate location of material storage during construction of the cafeteria will be the area between the new cafeteria and SW Stott Street. There are no historic or significant trees in this location.

*(5) Need for access to the building site for construction equipment.*

**RESPONSE:** The Revised Tree Preservation Plan, Attachment 18 has taken the need for access into consideration and has only illustrated the removal of trees necessary for construction. All other trees will remain on-site. Measures will be taken to ensure the existing trees safety during construction, such as fencing and marking of trees, which are near to the construction area. The Revised Tree Preservation Plan, Attachment 18 identifies the limits of disturbance. The line type has been emboldened and identified in the legend for further clarity. The approximate location of material storage during construction of the cafeteria will be the area between the new cafeteria and SW Stott Street. There are no historic or significant trees in this location.

*(6) Essential grade changes. Essential grade changes are those grade changes needed to implement standards common to standard engineering or architectural practices.*

**RESPONSE:** As shown on the Revised Grading, Storm, and Erosion Control Plan and the Grading, Utilities and Erosion Control Plans (Attachments 8-10), there are minor grade changes associated with the development of the cafeteria and associated parking. The proposed Revised Tree Preservation Plan, Attachment 18 has taken this need into consideration and has only illustrated the removal of trees necessary for construction. All other trees will remain on-site.

*(7) Surface water drainage and utility installations.*

**RESPONSE:** As shown on the Revised Tree Preservation Plan, Attachment 18, no trees will need to be removed for installation of utilities.

*(8) Location of driveways, buildings, or other permanent improvements.*

**RESPONSE:** The proposed Revised Tree Preservation Plan, Attachment 18 illustrates which trees will need to be removed for access and buildings on the site. The Plan only illustrates the removal of trees necessary for construction. All other trees will remain on-site. Measures will be taken to ensure the existing trees safety during construction, such as fencing and marking of trees that are near to the construction area.

*(9) Compliance with other ordinances or codes.*

**RESPONSE:** This criterion is not applicable to this application.

*(10) Necessity to install solar energy equipment.*

**RESPONSE:** This criterion is not applicable to this application.



#### **IV. Conclusion**

In conclusion, the proposed Revised Tree Preservation Plan, Attachment 18, indicates trees on the Beaverton School District site to be either preserved or removed. The Revised Landscape and Lighting Plan Public Improvements and Landscape and Lighting Plan Private Improvements, Attachments 11-14, indicate that the applicant has proposed mitigation for the removal of trees by planting seven White Oak trees. White Oak trees were designated by the City of Beaverton as a desired species for creating a landmark tree according to the Pre-application notes, dated October 9, 2001.



& Associates

## M E M O R A N D U M

**TO:** Jeff Caines, City of Beaverton  
**FROM:** Frank Angelo, Principal   
Robin Scholetzky, AICP, Planner   
**DATE:** January 30, 2002  
**RE:** Design Review Type 3, Beaverton High School Cafeteria Development  
Conditional Use, Beaverton High School  
Tree Preservation Plan  
Variance—Public Hearing, Bicycle Parking  
**CC:** Vlad Voytilla, Project Manager, Beaverton School District

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The attached plans are in response to your request dated January 18, 2002 for additional material for the following land use applications:

- Design Review Type 3, Beaverton High School Improvements
- Conditional Use, Beaverton High School Improvements
- Tree Preservation Plan, Beaverton High School Improvements

Please note that in addition to these three applications, we are submitting a Variance – Public Hearing request relating to bicycle parking requirements.

We have included revised narratives for each of the three applications. Any related attachments or exhibits that have not been amended are not included in this resubmittal package and should refer to the original application.

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620 SW Main, Suite 201  
Phone: 503.224.6974 Fax: 503.227-3679

Land Use Planning Transportation Planning Project Management

- Application Resubmittal Materials - continued
- Page 2

## Plans

The following revised plans replace plans submitted with the original application:

ORIGINAL PLANS	ARE NOW	REVISED PLANS
1. Cover Sheet	→	1. Revised Cover Sheet
2. Existing Conditions Plan	→	2. Revised Existing Conditions Plan
		3. Revised Existing Conditions Plan
3. Dimension Plan Public Improvements	→	4. Revised Dimension Plan Public Improvements
4. Dimension Plan Private Improvements (west)	→	5. Revised Dimension Plan Private Improvements (west)
5. Dimension Plan Private Improvements (east)	→	6. Revised Dimension Plan Private Improvements (east)
		7. Revised Dimension, Grading Utility Lot H
6. Grading, Storm and Erosion Control Public Improvements	→	8. Revised Grading, Storm and Erosion Control Public Improvements
7. Grading, Utilities and Erosion Control Private Improvements (west)	→	9. Revised Grading, Utilities and Erosion Control Private Improvements (west)
8. Grading, Utilities and Erosion Control Private Improvements (East)	→	10. Revised Grading, Utilities and Erosion Control Private Improvements (east)
9. Landscape and Lighting Plan Public Improvements	→	11. Revised Landscape and Lighting Plan Public Improvements
10. Landscape and Lighting Plan Private Improvements (west)	→	12. Revised Landscape and Lighting Plan Private Improvements (west)
11. Landscape and Lighting Plan Private Improvements (East)	→	13. Revised Landscape and Lighting Plan Private Improvements (East)
		14. Revised Landscape and Lighting Plan Private Improvements (Lot H)
12. Detail Sheet for Water Quality Construction	→	15. Revised Detail Sheet for water quality construction
13. Detail Sheet for Water Quality Construction		16. Revised Detail Sheet
14. Detail Sheet for Water Quality Construction and Bike Rack Detail	→	17. Revised Detail Sheet for Bike Rack Detail
15. Tree Protection Plan	→	18. Revised Tree Preservation Plan
16. Architectural Elevations	→	19. Revised Architectural Elevations
17. Materials Board		NOT INLCUED IN REVISED SET

We have provided six full size set of plans for this application submittal.

### **Design Review Type 3, Beaverton High School Improvements**

In response to the letter dated January 18, 2002 for the Design Review Type 3, Beaverton High School Improvements, please find a revised narrative that addresses the following:

1. The request to indicate trees listed on the Existing Conditions Plan; Dimensioned Site Plan; Grading Plan; Utility Plan and Landscape Plan has been met by the following:

The City of Beaverton Tree Inventory provided by the City does not identify trees as Historic or Significant, it only lists criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above (indicating a potentially historic tree). For more information on this analysis, please find attached the Tree Preservation Plan.

2. Grading Plan. The Revised Grading Plan indicates the dimension from centerline to edge of proposed right-of-way. All existing trees over 6" dbh have been labeled.
3. Utility Plan. The Utility Plan illustrates in greater detail how the proposed utility relocations will work in the narrow corridor. Existing water line and overhead utilities (electrical service, telephone, and cable television) along the section of 2<sup>nd</sup> Street to be vacated, will be relocated underground between the new cafeteria building and the existing High School (See Revised Grading, Utilities and Erosion Control Private Improvements (west) and Revised Grading, Utilities and Erosion Control Private Improvements (east). The corridor between the building is a minimum of 20 feet-wide. Due to the proposed sky bridge between the buildings, the water line will be installed within a pipe casing to eliminate the need for trench excavation under the sky bridge.
4. Landscape Plan. All plants have been identified on the landscape plan. The identification of the plants includes the genus and species. Two symbols have been used to identify trees to be saved and trees to be removed. A notation in each of these two symbols indicates either deciduous trees or evergreen/coniferous trees. The areas for stormwater quality have been revised to meet Clean Water Services standards. The use of seed mix has been minimized.
5. Lighting Plan. The Lighting Plans (Revised Landscape And Lighting Plan Public Improvements; Revised Landscape And Lighting Plan Private Improvements (West); Revised Landscape And Lighting Plan Private Improvements (East) and Revised Landscape And Lighting Plan Private Improvements (Lot H)) better indicate the location and type of lighting proposed. All exterior lighting indicates the area and pattern of illumination measured at ½ foot candlepower. Each isobar has been labeled with its corresponding candlepower.

NOTE: Revised cut sheets (Revised Appendix D) are included with the Design Review Application.

6. Hours of Operation. Beaverton High School is open from 6:30am to 2:30pm for classroom activities. After-school activities can run until 10:00pm, depending on the activity. It is important to note that the school facilities are available for a number of users and are not limited to Beaverton School District programs. Tualatin Hills Parks and Recreation District and community groups conducting classes or events are able to use the facilities at Beaverton High School. These events are scheduled through the school as with all Beaverton School



- *Application Resubmittal Materials - continued*
  - *Page 4*
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District schools, Beaverton High School is a community facility and is made available to groups when scheduling permits.

7. **Architectural Elevations.** A revised set of Architectural Elevations indicates the location and type of materials and colors according to the Materials Board submitted with the initial application. NOTE: One additional Pittsburgh Paint color, #314-4 is shown on the revised Architectural Elevations to be used an exterior siding color. All other colors to remain the same.
8. **Bicycle Parking.** An application for a Variance-Public Hearing for a reduction in the amount of required long-term parking for bicycles is included with this submittal package. The locations of the proposed bicycle parking is illustrated on the following plans, Cover Sheet and Revised Grading, Utilities and Erosion Control Private Improvements (East).
9. **Parking Standards.** The improvements to the Beaverton High School site include development of Lot H. The addition of Lots C, D and H provide 436 spaces which is 46 spaces more than is required based on current enrollment of 1,800 students and 150 staff. Based on a potential enrollment of 2,200 students and increase of staff to 200; the parking minimum required would increase to 480 spaces. This application includes a 10% parking reduction credit for construction of a pedestrian plaza and transit improvements as described in Section 60.20.10.10.A.2 of the Beaverton Development Code. The development of Lots C, D and H along with the pedestrian plaza parking reduction credit will meet the parking requirements for the High School. We have made preliminary contact with Tri-Met representatives regarding the location, size and materials for the plaza. The location of the plaza is shown on the Revised Dimension Plan Public Improvements Plan and the Revised Grading, Storm and Erosion Control Public Improvements Plan.
10. **Traffic Impact Analysis.** During the Facilities Review meeting with the City of Beaverton on January 23, 2002, the City stated that the Traffic Impact Analysis provided with the Comprehensive Plan Amendment and Street Vacation application was adequate.
11. **Four-way Intersections.** All sidewalk ramps at four-way intersections have been designed with double ramp configuration where applicable to meet the City of Beaverton standards.

A new appendix, Appendix H has been included to provide information regarding the construction of two proposed raised crosswalks on SW Erickson. These crosswalks, shown on Revised Dimension Plan Public Improvements, Attachment 4 are part of the proposed street improvements around the High School to improve pedestrian safety and connectivity. The proposed raised crosswalks have been designed to be in compliance with the Neighborhood Traffic Calming Program sponsored by the City of Beaverton's Engineering Department, Transportation Division. Compliance with the Traffic Calming Eligibility Criteria are found in Appendix H.

#### **Conditional Use Beaverton High School**

1. The request to indicate which trees listed on the Existing Conditions Plan; Dimensioned Site Plan; Grading Plan; Utility Plan and Landscape Plan has been met by the following:

The City of Beaverton Tree Inventory provided by the City does not identify trees as Historic or Significant, it only lists criteria with ratings to indicate a level of historic or significant tree. From our analysis of this document, only some of the trees on-site meet a level of 3 or above

- *Application Resubmittal Materials - continued*
  - *Page 5*
- 

(indicating a potentially historic tree). For more information on this analysis, please find attached the revised narrative for the Tree Preservation Plan.

2. **Dimensioned Site Plan.** The Revised Grading Plan indicates the dimension from centerline to edge of proposed right-of-way. All existing trees over 6" dbh have been labeled.
3. **Architectural Elevations.** A revised set of Architectural Elevations indicates the general character of the proposed project with dimensions. Materials and colors have been provided on the Materials and Finishes form and the Materials Board submitted with the initial application.
4. **Hours of Operation.** Beaverton High School is open from 6:30am to 2:30pm for classroom activities. After-school activities can run until 10:00pm, depending on the activity. It is important to note that the school facilities are available for a number of users and are not limited to Beaverton School District programs. Tualatin Hills Parks and Recreation District and community groups conducting classes or events are able to use the facilities at Beaverton High School. These events are scheduled through the school. As are all Beaverton School District schools, Beaverton High School is a community facility and is made available to groups when scheduling permits.
5. **Approval Criteria Findings.** The revised narrative for the Conditional Use application includes a review of how the development meets the Development Code Section 20.05. This includes standards for setbacks, landscaping, vehicle and bicycle parking.
6. **Bicycle Parking.** An application for a Variance-Public Hearing for a reduction in the amount of required long-term parking for bicycles is included with this submittal package. The locations of the proposed bicycle parking is illustrated on the following plans: Cover Sheet and Revised Grading, Utilities and Erosion Control Private Improvements (east).
7. **Parking Standards.** The improvements to the Beaverton High School site include development of Lot H. The addition of Lots C, D and H provide 436 spaces which is 46 spaces more than is required based on current enrollment of 1,800 students and 150 staff. Based on a potential enrollment of 2,200 students and increase of staff to 200; the parking minimum required would increase to 480 spaces. This application includes a 10% parking reduction credit for construction of a pedestrian plaza and transit improvements as described in Section 60.20.10.10.A.2 of the Beaverton Development Code. The development of Lots C, D and H along with the pedestrian plaza parking reduction credit will meet the parking requirements for the High School.
8. **Traffic Impact Analysis.** During the Facilities Review meeting with the City of Beaverton on January 23, 2002, the City stated that the Traffic Impact Analysis provided with the Comprehensive Plan Amendment and Street Vacation application was adequate.

### **Tree Preservation Plan**

In response to the letter dated January 18, 2002 for the Tree Preservation Plan, Beaverton High School Improvements, please find the following:

1. **Specific Tree Species.** The Tree Preservation Plan identifies all existing trees with a numeric key and corresponding list noting common names, and size. The inventory is divided into two lists: trees to be preserved and trees to be removed.
2. **Drip Line of Each Tree or Entire Grove.** The Revised Tree Preservation Plan, Attachment 18 indicates in note 2 that the tree symbols approximate the actual canopy of existing trees (i.e. the drip line). The existing tree symbols on the landscape plan are also approximate to true scale.
3. **Clear Delineation of Construction Disturbance Limits.** The Revised Tree Preservation Plan, Attachment 18 identifies the limits of disturbance. The line type has been emboldened and identified in the legend for further clarity. The approximate location of material storage during construction of the cafeteria will be the area between the new cafeteria and SW Stott Street. There are no historic or significant trees in this location.
4. **Methods of Survival.** The Revised Tree Preservation Plan, Attachment 18 indicates in notes 3 and 4 the following:

NOTE: 3) Preserved trees within the limits of construction shall be protected by approved fencing placed five feet outside of drip line of tree.

NOTE: 4) Where construction occurs within the drip line of any tree to be preserved, existing grades shall not be changed without approval of landscape architect.

### **Bicycle Parking Variance Public Hearing**

An application for a Variance-Public Hearing is being submitted with this resubmittal package to request a reduction in the amount of bicycle parking required at Beaverton High School. According to Beaverton Development Code Section 60.20.10.5, the minimum parking ratio requirements for bicycle parking for an Educational Institution – High School require zero short-term spaces and one long-term space per 18 students (a 1:18 ratio). Based on a potential enrollment of 2,200 students, the long-term bicycle parking requirement for Beaverton High School would be 122 spaces. This Variance asks that the long-term bicycle parking requirement for Beaverton High School be reduced to 56 spaces (or 66 fewer than the requirement). The narrative describes that the demand for long-term bicycle parking can be met with the proposed number of spaces, more importantly, demand at area schools indicates that high school students do not choose to bicycle to school even when facilities are available. There are a number of transportation alternatives for students including frequent bus service provided by the School District and Tri-Met.

- *Application Resubmittal Materials - continued*
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## **Conclusion**

In summary, we believe the following resubmittal package completes the following applications:

- Design Review Type 3, Beaverton High School Improvements
- Conditional Use, Beaverton High School Improvements
- Tree Preservation Plan, Beaverton High School Improvements

We are also submitting a Bicycle Parking Variance Application to modify the required number of long-term bicycle spaces for the High School.

To provide for a timely review of our applications, we are submitting an “Application Completeness Option” form. This Option would apply to the Design Review Type 3; Conditional Use; and Tree Preservation Plan applications.

Please do not hesitate to contact me at 503-227-3664.



December 28, 2001

Gary Alfson  
Harper Houf Righellis, Inc.  
5200 SW Macadam Ave, Suite 580  
Portland, OR 97201

**Re: New cafeteria, addition to Beaverton High School, located on 2<sup>nd</sup> St.  
between Scott and Erickson  
CWS file 1684 (Tax map 1S116AD, Tax lots 10900 and 11100)**

Clean Water Services (formerly USA) has reviewed your proposal for the above referenced activity on your site. Staff has conducted a pre-screen review and requested completion of a Sensitive Areas Certification Form. Following the review it is apparent that sensitive areas are greater than 200' from your project. In light of this result, the above referenced project does not need a Service Provider letter as required by Resolution and Order 00-7, Section 3.02.1. Prior to construction, a Stormwater Connection Permit from Clean Water Services or its designee is required pursuant to Ordinance 27, Section 4.B. All required permits and approvals must be obtained and completed under applicable local, state, and federal law.

This concurrence letter does NOT eliminate the need to protect sensitive areas if they are subsequently identified on your site.

If you have any questions, please feel free to call me at 503-846-3613.

Sincerely,

A handwritten signature in cursive script that reads 'Heidi Berg'.

Heidi Berg  
Site Assessment Coordinator

**U.S. Postal Service**  
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*(Domestic Mail Only; No Insurance Coverage Provided)*

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**Total P** **FRANK ANGELO**  
**Recipient** **APPLICANT**  
**Street, A** **ANGELO EATON & ASSOCIATES**  
**620 SW MAIN SUITE 201**  
**City, State** **PORTLAND OR 97205**

PS Form 3800, February 2000 See Reverse for Instructions

SENDER: COMPLETE THIS SECTION	COMPLETE THIS SECTION ON DELIVERY
<ul style="list-style-type: none"> <li>Complete items 1, 2, and 3. Also complete item 4 if Restricted Delivery is desired.</li> <li>Print your name and address on the reverse so that we can return the card to you.</li> <li>Attach this card to the back of the mailpiece, or on the front if space permits.</li> </ul>	<p>A. Received by <i>(Please Print Clearly)</i>  <b>KAREN Siegel</b></p> <p>B. Date of Delivery  <b>4-29-02</b></p> <p>C. Signature  <b>X Karen Siegel</b>    <input type="checkbox"/> Agent  <input type="checkbox"/> Addressee</p> <p>D. Is delivery address different from item 1?    <input type="checkbox"/> Yes          If YES, enter delivery address below:    <input type="checkbox"/> No</p>
<p>1. Article Addressed to:</p> <p><b>FRANK ANGELO</b>  <b>APPLICANT</b>  <b>ANGELO EATON &amp; ASSOCIATES</b>  <b>620 SW MAIN SUITE 201</b>  <b>PORTLAND OR 97205</b></p>	<p>3. Service Type  <input checked="" type="checkbox"/> Certified Mail    <input type="checkbox"/> Express Mail  <input type="checkbox"/> Registered    <input type="checkbox"/> Return Receipt for Merchandise  <input type="checkbox"/> Insured Mail    <input type="checkbox"/> C.O.D.</p> <p>4. Restricted Delivery? <i>(Extra Fee)</i>    <input type="checkbox"/> Yes</p>
<p>2. Article Number <i>(Copy from service label)</i></p> <p><b>7000 0600 0028 5577 8762</b></p>	

**U.S. Postal Service**  
**CERTIFIED MAIL RECEIPT**  
*(Domestic Mail Only; No Insurance Coverage Provided)*

7000 0600 0028 0224 2339

CUP 2001-0031 (82)

Postage	\$
Certified Fee	
Return Receipt Fee (Endorsement Required)	
Restricted Delivery Fee (Endorsement Required)	

3/26/12  
 Postmark Here

**Total Postage** MIKE MALONEY  
**Recipient's** OWNER  
**Street, Apt.** BEAVERTNO SCHOOL DISTRICT  
**City, State.** 16550 SW MERLO ROAD  
 BEAVERTO OR 97006-5152

PS Form 3800, February 2000

**SENDER: COMPLETE THIS SECTION**

- Complete items 1, 2, and 3. Also complete item 4 if Restricted Delivery is desired.
- Print your name and address on the reverse so that we can return the card to you.
- Attach this card to the back of the mailpiece, or on the front if space permits.

1. Article Addressed to:

**MIKE MALONEY**  
**OWNER**  
**BEAVERTNO SCHOOL DISTRICT**  
**16550 SW MERLO ROAD**  
**BEAVERTO OR 97006-5152**

**COMPLETE THIS SECTION ON DELIVERY**

A. Received by *(Please Print Clearly)* **VERA CLIFT** B. Date of Delivery

C. Signature  
 x *Vera Clift*  Agent  
 Addressee

D. Is delivery address different from item 1?  Yes  
 If YES, enter delivery address below:  No

3. Service Type  
 Certified Mail  Express Mail  
 Registered  Return Receipt for Merchandise  
 Insured Mail  C.O.D.

4. Restricted Delivery? *(Extra Fee)*  Yes

2. Article Number *(Copy from service label)* 7000 0600 0028 0224 2339



SK

# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

## See BDR Packet

Project Number:

BDR 2001-0213 / cup 2001-0031 / VAR 2002-0002

Project Name:

Beaverton High School TPP 2001-0008

Re-notice  
MAP Revision

Cafeteria & Parking Lot Expansion

### AFFIDAVIT OF NOTICE

I, Robyn Lampa, being first duly sworn/affirmed, say that I gave notice of a proposed zone change affecting land described as Lot(s) 07100, 02500, Map 15116AD, 15116AC; and that, pursuant to the City of Beaverton Ordinance 2050, Sections 50.30.1, and 50.30.2, I did on the 11<sup>th</sup> day of march, 2002, give public notice to those listed on the attached Exhibit A. The notices were mailed on 3/11/02, which was on or before the deadline date determined by City Staff for this application(s).

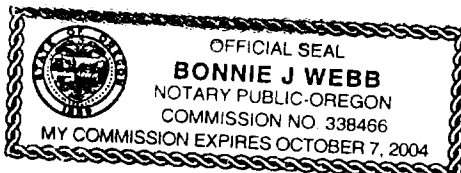
Dated this 11 day of march, 2002.

Robyn Lampa  
Signature

State of OREGON )  
County of Washington )

Signed and sworn/affirmed before me this 11 day of march, 2002 by Robyn Lampa.

Bonnie J Webb  
Notary Public for the State of Oregon  
My Commission Expires: Oct 7 2004







# CITY of BEAVERTON

4755 S.W. Griffith Drive, P.O. Box 4755, Beaverton, OR 97076 General Information (503) 526-2222 V/TDD

Project Number: BDR 2001-0213  
Project Name: Beaverton High School Cafeteria & Parking Lots Expansion

## AFFIDAVIT OF NOTICE

I, Carolyn W. Hubbard, being first duly sworn/affirmed, say that I gave notice of a proposed cafeteria/parking lot expansion affecting land described as Lot(s) 1100; 1000; 2900; 700; 10900; 2100; 2500, Map PSI-16AD; 151-16AC; and that, pursuant to the City of Beaverton Ordinance 2050, Sections 50.30.1, and 50.30.2, I did on the 5 day of February, 2002, give public notice to those listed on the attached Exhibit A. The notices were mailed on February 5, 2002, which was on or before the deadline date determined by City Staff for this application(s).

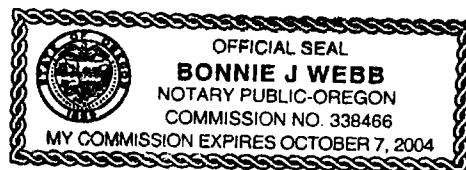
Dated this 5 day of February, 2002.

Carolyn W. Hubbard  
Signature

State of OREGON )  
County of Washington )

Signed and sworn/affirmed before me this 5<sup>th</sup> day of Feb, 2002 by Carolyn Hubbard.

Bonnie J Webb  
Notary Public for the State of Oregon  
My Commission Expires: Oct 7 2004



**BDR 2001-0213  
BEAVERTON HIGH SCHOOL  
CAFETERIA & PARKING LOTS**

**RUSS PITKIN/ERIC NIELSEN  
ARCHITECT  
LSW ARCHITECTURE  
1953 NW KEARNY  
PORTLAND OR 97209**

**CAROL FRANKLIN  
CENTRAL BEAVERTON NAC  
5025 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005**

**FRANK ANGELO  
APPLICANT  
ANGELO EATON & ASSOCIATES  
620 SW MAIN SUITE 201  
PORTLAND OR 97205**

**RON MATELA  
LANDSCAPE ARCHITECT  
MATELA ASSOCIATES  
601 MAIN STREET SUITE 201  
VANCOUVER WA 98660**

**MARV DOTY  
CENTRAL BEAVERTON CCI  
7350 SW WILSON AVENUE  
BEAVERTON OR 97008**

**MIKE MALONEY  
OWNER  
BEAVERTON SCHOOL DISTRICT  
16550 SW MERLO ROAD  
BEAVERTON OR 97006-5152**

**GARY ALFSON  
ENGINEER  
HARPER HOUF RIGHELLIS  
5200 SW MACADAM SUITE 580  
PORTLAND OR 97201**

BEVERLY FROUDE  
CPO 4 BULL MOUNTAIN  
12200 SW BULL MTN ROAD  
TIGARD OR 97224

DAVID WILSON  
CPO 3  
4170 SW PARKVIEW AVENUE  
PORTLAND OR 97225

[REDACTED]

JOHN BREILING  
CPO 7 SUNSET WEST  
4690 NW COLUMBIA  
PORTLAND OR 97229

[REDACTED]

BILL AVERY  
WASHINGTON COUNTY - DLUT  
PLANNING DIVISION # 350-13  
155 NORTH FIRST AVENUE  
HILLSBORO OR 97124-3072

PLANNING DIRECTOR  
CITY OF TIGARD  
13125 SW HALL BOULEVARD  
TIGARD OR 97223

[REDACTED]

BEAVERTON POST OFFICE  
4550 SW BETTS  
BEAVERTON OR 97005

PHIL HEALY  
WASHINGTON COUNTY - DLUT  
LAND DEVELOPMENT # 350-13  
155 NORTH FIRST  
HILLSBORO OR 97124

[REDACTED]

MIKE BURTON  
METRO GROWTH MANAGEMENT  
MANAGER COMMUNITY DEV  
600 NE GRAND AVENUE  
PORTLAND OR 97232

JAN YOUNGQUIST  
BEAVERTON SCHOOL DIST  
16550 SW MERLO ROAD  
BEAVERTON OR 97006

[REDACTED]

SONYA KAZEN  
DEVELOPMENT REVIEW  
ODOT REGION 1  
123 NW FLANDERS  
PORTLAND OR 97209-4037

[REDACTED]

SAM HUNAIDI  
ODOT DISTRICT 2A  
5440 SW WESTGATE DR STE 350  
PORTLAND OR 97221-2414

[REDACTED]

[REDACTED]

[REDACTED]

1S116AA04700  
ADAMS-HAGLUND JENNIFER  
2605 NE ROBERTS PL  
GRESHAM OR 97030

1S116AD02500  
ADOLPHSON LOVING TRUST  
BY RICHARD & SHIRLEY ADOLPHSON  
25505 SW LONE FIR LANE  
WEST LINN OR 97068

1S116AD02501  
ADOLPHSON LOVING TRUST  
25505 SW LONE FIR LN  
WEST LINN OR 97068

1S116AD02300  
ADOLPHSON R G  
ADOLPHSON R G JR  
BY A-2 ENTERPRISES  
4555 SW MAIN ST  
BEAVERTON OR 97005

1S116DB00400  
AFOURKEEFF ANJA T  
5075 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD07000  
AHN DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

1S116AD07001  
~~AHN DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005~~

1S116AD06100  
ALPROP CO  
6149 SW SHATTUCK RD  
PORTLAND OR 97221

1S116AC04700  
ANDERSON ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC05500  
~~ANDERSON ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005~~

1S116DB03400  
ARLT LINDA L  
13275 SW PEAR ST  
BEAVERTON OR 97005

1S116AC01800  
ARMSTRONG JAMES R  
LINDA L  
13325 SW 2ND  
BEAVERTON OR 97005

1S116DA90027  
BABB DORIS LEE  
5136 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90023  
BAKER VIRGIL A & MARIE  
5166 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AC04500  
BARTLETT CLIFFORD L  
13500 SW 2ND  
BEAVERTON OR 97005

1S116AA05200  
BASEEL PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

1S116AA05301  
~~BASEEL PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007~~

1S116AB03100  
BEAVERTON FOODS INC  
PO BOX 687  
BEAVERTON OR 97005

1S116AC04400  
BEAVERTON PROPERTIES  
13470 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD11100  
BEAVERTON SCHOOL DISTRICT 48  
MERLE DAVIS ELEMENTARY  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AC02100  
BEAVERTON SCHOOL DISTRICT 48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD11000  
BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AD02900  
~~BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006~~

1S116AD07100  
~~BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006~~

1S116AC02100  
~~BEAVERTON SCHOOL DISTRICT 48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006~~

1S116AC02500  
~~BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006~~

1S116AD10900  
BEAVERTON SCHOOL DISTRICT 48J  
ATTN FACILITIES MNGMT  
16550 SW MERLO RD  
BEAVERTON OR 97006

1S116AA02800  
~~BEAVERTON CITY OF  
PO BOX 475  
BEAVERTON OR 97076~~

1S116AD11200  
~~BEAVERTON CITY OF  
00000~~

1S116AC04600  
~~BEAVERTON CITY OF  
4950 SW HALL BLVD  
BEAVERTON OR 97005~~

1S116AD01300  
~~BEAVERTON CITY OF~~  
~~PO BOX 4755~~  
BEAVERTON OR 97076

1S116DB11100  
~~BEAVERTON CITY OF~~  
~~PO BOX 4755~~  
BEAVERTON OR 97076

1S116AA06000  
BENNETT CLEVE E & LOIS W TRUSTE  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

1S116AA05900  
~~BENNETT CLEVE E AND~~  
~~LOIS W TRUSTEES~~  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

1S116AD10400  
BENNETT TERRI LOUISE  
12820 SW 4TH ST  
BEAVERTON OR 97005

1S116AA02900  
BIGGI STEVE AND LAURA  
3843 SW HALL BLVD  
BEAVERTON OR 97005

1S116DB00100  
BLOOMER DANIEL D MARY L  
4925 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA90055  
BYRNES DONALD B & B JERI  
12900 SW 5TH ST  
BEAVERTON OR 97005

1S116AA05400  
CAFFEE CENTER JOINT VENTURE  
BY ANDERSON & ASSOCIATES  
PO BOX 3821  
BELLEVUE WA 98009

1S116DB03000  
CARSON JERRY/PAULA  
13270 SW 6TH  
BEAVERTON OR 97005

1S116AC05700  
CASSIDY MARI  
4820 SW MENLO  
BEAVERTON OR 97005

1S116DA02500  
CASSON KATHLEEN ANN  
3818 SE 9TH AVE  
PORTLAND OR 97202

1S116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

1S116DA90000  
CENTRAL PARK CONDO  
UNIT OWNERS  
~~BY LINCOLN COMPANY~~  
~~PO BOX 586~~  
BEAVERTON OR 97005

1S116AC01300  
CHIA CHIH CHIANG & YEA HUEY  
13200 SW FARMINGTON RD  
BEAVERTON OR 97006

1S116AC01400  
CHIA FUO FUH &  
WU CHUN ER  
13150 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD10601  
COLEMAN ANNE S  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC03000  
COLEMAN ANNE SCOTT  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC01900  
COOLEY CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

1S116DA07003  
COOLEY CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

1S116AD08000  
CORP OF PRESIDING BISHOP OF  
CHURCH OF JESUS CHRIST LDS  
TAX ADMIN FILE 536-2490  
50E NORTH TEMPLE  
SALT LAKE CITY UT 84150-2201

1S116AC05801  
CORSON JOHN D  
4850 SW MENLO DR  
BEAVERTON OR 97005

1S116AD07400  
CRITELLI FAMILY TRUST  
c/o CRITELLI TINA  
BY GARY A & SHARON L CRITELLI  
PO BOX 87058  
VANCOUVER WA 98687

1S116AD10300  
CRITELLI GARY A & SHARON L TRS  
PO BOX 87058  
VANCOUVER WA 98687

1S116DA01600  
~~CRITELLI GARY A & SHARON L TRS~~  
~~PO BOX 87058~~  
VANCOUVER WA 98687

1S116DB00200  
D'ALFONSO PAULA L  
4975 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AD10500  
DAMIANA RONALD G  
GEORGIA M  
12870 SW 4TH  
BEAVERTON OR 97005

1S116DB00800  
DANEIL HOLLIS MARION  
13425 SW 6TH  
BEAVERTON OR 97005

1S116DB00900  
DANIEL AVERILL H/HOLLIS M  
13425 SW 6TH  
BEAVERTON OR 97005

1S116AC01500  
DANISH SEYMOUR R REV TRUST &  
BERRY HARRY L ET AL  
BY DANISH CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005

1S116AD03100  
D'SILVA FELIX & LOURDES  
8765 SW JAMIESON RD  
PORTLAND OR 97225

1S116DA01700  
EIMERS SANDRA P &  
EIMERS ANGELA  
6045 SW 167TH AVE  
ALOHA OR 97007

1S116DB03101  
ENGEL MICHAEL J &  
REBECCA C  
13210 SW 6TH ST  
BEAVERTON OR 97005

1S116AA05700  
ETON LANE LTD PARTNERSHIP  
WALKER HUBERT E TRUSTEE &  
ETON LANE LLC  
PO BOX 14746  
PORTLAND OR 97293

1S116AA05800  
ETON LANE LTD PARTNERSHIP  
WALKER HUBERT E TRUSTEE  
PO BOX 14746  
PORTLAND OR 97214

1S116AC04200  
EVANS CATHERINE B  
11775 SW 11TH STREET  
BEAVERTON OR 97005

1S116AC01200  
FISHBACK MERL E &  
GLORIA M TRUST  
FISHBACK MERL E/GLORIA M TR  
9700 NW GROVELAND RD  
HILLSBORO OR 97124

1S116DA02200  
FLOREN JANET M AND GREG E  
12870 SW 6TH AVE  
BEAVERTON OR 97005

1S116DB00300  
FRANKLIN JOHN A  
5025 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116AC04900  
FRIEZE REX D SHIRLEY L  
4720 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116DB01100  
GABRIEL KATHLEEN  
13470 SW 3RD ST  
BEAVERTON OR 97005

1S116AC05600  
GARCIA WAGNER  
4800 SW MENLO DR  
BEAVERTON OR 97005

1S116DA90025  
GARTLAND SHARIN  
5162 SW ERICKSON  
BEAVERTON OR 97005

1S116AD03000  
GARVER JON & REBECCA  
12855 SW 2ND ST  
BEAVERTON OR 97005

1S116AD02200  
GENEST VIRGINIA E  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD02100  
GENEST VIRGINIA E  
BY VALLEY DAIRY QUEEN  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116AD02700  
GHIM MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

1S116AD02600  
GHIM MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

1S116DB05000  
GLASNER GENE R  
c/o DREY BRIAN S & PAMELA R  
5225 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AC06000  
GLICKMAN TERRY L  
13555 SW THIRD ST  
BEAVERTON OR 97005

1S116DA01300  
GORMAN JOHN & PATRICIA A  
16555 SW HIGH HILL LN  
BEAVERTON OR 97007

1S116AC01000  
GRABHORN CHAD  
350 NW FREEMAN  
HILLSBORO OR 97124

1S116AC03700  
GREEN ERNESTINE RUTH  
4820 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

1S116AA02100  
GREER DAVID J & LISA D  
19370 SW SOUTHVIEW ST  
ALOHA OR 97007

1S116AC05300  
GREMAUX DEANE B AND  
PATRICIA A  
4755 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

1S116AB02100  
GRL INC  
13095 SW CANYON RD  
BEAVERTON OR 97005

1S116DB10800  
GUSTAFSON CONRAD W & ANNA M  
13245 SW PEARL ST  
BEAVERTON OR 97007

1S116AA02500  
GUTZLER CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116AA02600  
GUTZLER CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116AA02300  
GUTZLER THOMAS M & CAROLYN E  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116AA02400  
GUTZLER THOMAS M AND  
CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

1S116DA90058  
HALL LAURA I  
12888 SW 5TH  
BEAVERTON OR 97005

1S116DA01900  
HALSTEN ELSIE M TRUSTEE  
12855 SW 6TH  
BEAVERTON OR 97005

1S116DA02000  
HAMREUS CALVIN E & JOYCE K  
5075 SW MAIN AVE  
BEAVERTON OR 97005

1S116DA07004  
HATTENHAUER CHRISTINE  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

1S116DA07002  
HATTENHAUER CHRISTINE  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

1S116DA01800  
HEDGES SALLY C  
12850 SW 5TH AVE  
BEAVERTON OR 97005

1S116DB04600  
HEISLER MARYANNE &  
NEUFELL PATRICIA J  
13250 SW PEARL  
BEAVERTON OR 97005

1S116DA01100  
HETU TED G  
5075 SW ANGEL AVE  
BEAVERTON OR 97005

1S116AD06000  
HILL RICHARD E AND FLORENCE E  
8405 SW MAVERICK PL  
BEAVERTON OR 97008

1S116AD10100  
HOUDROJ ALI  
4970 SW MAIN ST  
BEAVERTON OR 97005

1S116DA90028  
HUESMAN NICOLE K  
5134 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116AA03000  
HUNKAPILLAR PATRICIA W  
BY THE TAGGART TRUST  
21185 NW EVERGREEN PKWY  
SUITE 101  
HILLSBORO OR 97124

1S116AA03100  
HUNKAPILLAR PATRICIA W  
SMITH HARRIET L AND GEORGE  
BY TAGGART TRUST THE  
21185 NW EVERGREEN PKW STE 101  
HILLSBORO OR 97124

1S116AC00800  
HUNT SHERYLL R/WILLIAM J &  
HUNT JONATHAN J  
13380 SW FARMINGTON RD  
BEAVERTON OR 97005

1S116DA02100  
HUTCHENS DANA C & LORI E  
12820 SW SIXTH ST  
BEAVERTON OR 97005

1S116DA06700  
HUYGENS JOSEPH D &  
JULIE ANN HUTCHINSON  
7830 SW LAUREL ST  
PORTLAND OR 97225

1S116AD09901  
JOHNSON CAROL L R  
PO BOX 524  
SCAPPOOSE OR 97056

1S116AC04000  
JONES E CHANNING & PAULA KAY  
13420 SW 2ND AVE  
BEAVERTON OR 97005

1S116DA90052  
JONES JERRY C  
12912 SW 5TH 52  
BEAVERTON OR 97005

1S116AC03600  
JONES KRISTIN L  
4870 SW FAIRMOUNT DR  
BEAVERTON OR 97005

1S116DA07000  
JUNELL BONNIE M &  
BABCOCK DOUGLAS C  
4820 SW 18TH PL  
PORTLAND OR 97201

1S116AD01600  
KABDEBO JOSEPH & CAMILLE  
725 SW VIEWMONT DR  
PORTLAND OR 97225

1S116AD01700  
KABDEBO JOSEPH & CAMILLE  
725 SW VIEWMONT DR  
PORTLAND OR 97225

1S116AD01800  
KABDEBO JOSEPH AND CAMILLA  
725 SW VIEWMONT DR  
PORTLAND OR 97225

1S116DB03202  
KAUTH JANE F  
5115 SW ERICKSON AVE  
BEAVERTON OR 97007

1S116DA07300  
KIKO JOHN R &  
ORTIZ INGRID  
5155 SW MAIN AVE  
BEAVERTON OR 97005

1S116AD04300  
KIM UN & SUE J  
14756 SW KILCHIS  
BEAVERTON OR 97007

1S116AA02700  
KIM YOUNG M & LINDA  
12855 SW CANYON RD  
BEAVERTON OR 97005

1S116DA06800  
KING CHARLES C & ROM N  
5150 SW ERICKSON AVE  
BEAVERTON OR 97005

1S116DA90024  
KLANECKY ARDITH ANN  
5164 SW ERICKSON  
BEAVERTON OR 97005

1S116DB10900  
KO JOON B  
13215 SW PEARL ST  
BEAVERTON OR 97005

1S116AD06800  
KRAEMER JOHN L & CLAUDIA R  
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SCAPPOOSE OR 97056

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KRIEG MARGIE J  
12924 SW 5TH ST  
BEAVERTON OR 97005

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LAMB KEVIN M &  
YANT SUSAN A  
12892 SW 5TH ST  
BEAVERTON OR 97005

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LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

1S116AA04690  
~~LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005~~

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LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

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ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005~~

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~~LANPHERE ROBERT  
ATTN DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005~~

1S116DB03100  
LEE SANDY  
344 NE 156TH AVE  
PORTLAND OR 97230

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LEIVA LUIS ALONSO & SANDRA J  
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BEAVERTON OR 97005

1S116AC04300  
LEONARD CLIFFORD A & SHARON B  
7345 SW 84TH AVE  
PORTLAND OR 97223

1S116AD06200  
LESHER SAM E/HELENA R &  
LESHER CARL H  
7725 SE 32ND AVE  
PORTLAND OR 97202

1S116AC06200  
LEWIS SUNNY M  
13425 SW 3RD  
BEAVERTON OR 97005

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LIDBERG MARY ALICE C  
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BEAVERTON OR 97005

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LIEDTKE CATHERINE A  
17561 SW ALBERT CT  
BEAVERTON OR 97007

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LISICKI THOMAS D & RUTH S  
13332 SW BENCHVIEW TER  
TIGARD OR 97223

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LOCKARD DOROTHY J  
LIVING TRUST THE  
5125 SW SHERWOOD PL  
BEAVERTON OR 97005

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LONG PATRICIA C & JOE PAT  
5070 SW FAIRMONT DR  
BEAVERTON OR 97005

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LOPEZ JORGE & CARMEN  
4700 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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LOY JOHN G  
5105 SW ERICKSON AVE  
BEAVERTON OR 97005

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LUCAS LAURIE G  
12880 SW 5TH AVE  
BEAVERTON OR 97005

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LUCKE WILLIAM T TRUSTEE  
5205 SW MAIN AVE  
BEAVERTON OR 97005

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LUND BRADLEY W  
4855 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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LUNDGREN TIMOTHY  
18213 PORTLAND AVE  
GLADSTONE OR 97027

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MACK TIMOTHY F  
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BEAVERTON OR 97005

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MATELICH RAE MARIE  
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MAYES PATSY L  
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MERCADO NEVILLE & RENEE  
PO BOX 487  
BEAVERTON OR 97075



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JOHN M  
5450 SW 150TH CT  
BEAVERTON OR 97007

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MILLARD KATHLEEN  
4790 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

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MOHR PATRICIA ALICE  
WILLIAM RICHARD  
7495 SW 136TH AVE  
BEAVERTON OR 97008

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MORGAN WILLIAM L & EVE E  
20855 NW CHILOQUIN  
PORTLAND OR 97229

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MORRIS GLENN R AND ANITA S  
12755 SW 6TH  
BEAVERTON OR 97005

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NELSEN MARY G  
9965 SW 170TH  
BEAVERTON OR 97007

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NEUSCHWANDER FRED ANDREW &  
INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064

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NEUSCHWANDER FRED ANDREW &  
INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064~~

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5020 SW FAIRMOUNT  
BEAVERTON OR 97005

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NORMAN CONSTANCE R  
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BEAVERTON OR 97005

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OKAMOTO BRETT K  
12916 SW 5TH  
BEAVERTON OR 97005

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OVERBY ARDON L  
PO BOX 704  
BEAVERTON OR 97075

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OVERBY ARDON L  
PO BOX 704  
BEAVERTON OR 97075~~

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PARKS LOREN E  
PO BOX 5669  
ALOHA OR 97006

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PEARCE JOSEPH J  
4920 SW MAIN  
BEAVERTON OR 97005

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PETERSEN DONALD E  
10240 SW HAWTHORNE LN  
PORTLAND OR 97225

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PHAM HIEN &  
NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006

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PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006

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PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

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PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

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PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

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PIES SANDRA J & EVERETT M TRS  
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HILLSBORO OR 97123

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PODEBSKI ADAM P & GUTA H  
PODEBSKI BENNY  
12936 SW 5TH  
BEAVERTON OR 97005

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QUINN CAROLYN F  
13395 SW 2ND ST  
BEAVERTON OR 97005

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QUTUB WAHBI M &  
MATAR RIAD A  
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BEAVERTON OR 97007

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RANDOLPH GLORIA  
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ALOHA OR 97006

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RANDOLPH GLORIA C  
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ALOHA OR 97005~~

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CARRIE  
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BEAVERTON OR 97005

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3830 SE BENTLEY AVE  
HILLSBORO OR 97123

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ROSENHOLM DAVID  
ROSENHOLM ANDREA  
13220 SW PEARL ST  
BEAVERTON OR 97005

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SANABRIA-BARILLAS JOSE ALFREDO  
ALVARADO JUANA  
13170 SW PEARL ST  
BEAVERTON OR 97005

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SCALONE DAVID  
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BEAVERTON OR 97005

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SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

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3815 WAUNA VISTA DR  
VANCOUVER WA 98661~~

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SESSIONS KEITH W & LAVERNA M  
TRUSTEES  
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BEAVERTON OR 97005

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SKORO MARION & MIRELA  
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PORTLAND OR 97231

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SMITH IAN C & NADINE T  
12750 SW 4TH ST  
BEAVERTON OR 97005

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SMITH RICHARD BRENDAN &  
HILARY ANTOINETTE MARY  
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BEAVERTON OR 97005

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SMOLENSKY G MARIKA  
PO BOX 1114  
BEAVERTON OR 97075

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SNODGRASS PHILIP R & PATRICIA  
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BEAVERTON OR 97005

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SOTH COLLEEN V TRUSTEE  
SOTH FORREST C TRUSTEE  
4890 SW MENLO DR  
BEAVERTON OR 97005

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SPRINGER JAMES F & SUE  
c/o BEAVERTON FUNERAL HOME INC  
4126 NORLAND AVE  
BURNABY BC CANADA  
V5G 3S8

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c/o BEAVERTON FUNERAL HOME INC  
4126 NORLAND AVE  
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V5G 358~~

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SHIRLEY ANN  
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BEAVERTON OR 97005

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STARKE DONALD B EDNA A  
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BEAVERTON OR 97005

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STEICHEN NANCY & MCCOURT JOHN  
c/o HOVERSLAND GORDON & MYRNA L  
4350 SW CEDAR HILLS BLVD  
BEAVERTON OR 97005

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STEIGER JOHN M & LOU-WAYNE  
301 NW MURRAY BLVD  
PORTLAND OR 97229

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BROWN GARY E  
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BEAVERTON OR 97005

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STOLTENBURG BARBARA  
c/o RAINEY WALLACE L & LESLIE C  
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BEAVERTON OR 97005

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SURBY PAUL W L FRANCES  
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BEAVERTON OR 97005

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THAO PANCHONG & MAO HER  
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BEAVERTON OR 97005

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THOMAS BARBARA A  
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BEAVERTON OR 97005

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THOMAS KAREN JO  
12650 SW 1ST  
BEAVERTON OR 97005

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THOMAS KELLY R &  
BOTHNE MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005

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BOTHNE MELISSA  
12720 SW 2ND ST  
BEAVERTON OR 97005~~

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BEAVERTON OR 97005~~

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THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

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THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

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THOMPSON AND ADAMS  
4500 SW HALL BLVD  
BEAVERTON OR 97005

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THOMPSON TOM G &  
THOMPSON MARGIE J  
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BEAVERTON OR 97005

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BEAVERTON OR 97005

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THOMPSON TOMMY G  
JEANNETTE L  
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TISCHLER STEPHEN & KATHLEEN  
5195 SW ERICKSON AVE  
BEAVERTON OR 97005

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TOSCO CORP  
PROPERTY TAX DEPT-DC17  
PO BOX 52085  
PHOENIX AZ 85072

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TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

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TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

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UNDERHILL MICHAEL  
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BEAVERTON OR 97005

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VADEN MICHAEL & JANE  
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BEAVERTON OR 97005

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VADEN MICHAEL & JANE  
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BEAVERTON OR 97005

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BY LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

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VEAZEY THOMAS C AND  
ROXANNE T  
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BEAVERTON OR 97005

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VINK NANCY E  
12940 SW 5TH ST  
BEAVERTON OR 97005

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VOGEL MICHAEL J & D'ANNE T  
4770 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116AC05200  
VON W METHENEY FAMILY TRUST  
4775 SW FAIRMONT DR  
BEAVERTON OR 97005

1S116DA90026  
VOSE BENJAMIN A  
PO BOX 73  
TOLOVANA PARK OR 97145

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WABNITZ STEVEN G  
2648 SW 153RD AVE  
BEAVERTON OR 97006

1S116AC00400  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

1S116AC00200  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

1S116AC00100  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

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WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

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WELDON GEORGE E AND  
FLORENCE GRACE  
8783 SW 175TH AVE  
BEAVERTON OR 97007

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BEAVERTON OR 97005

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BEAVERTON OR 97005

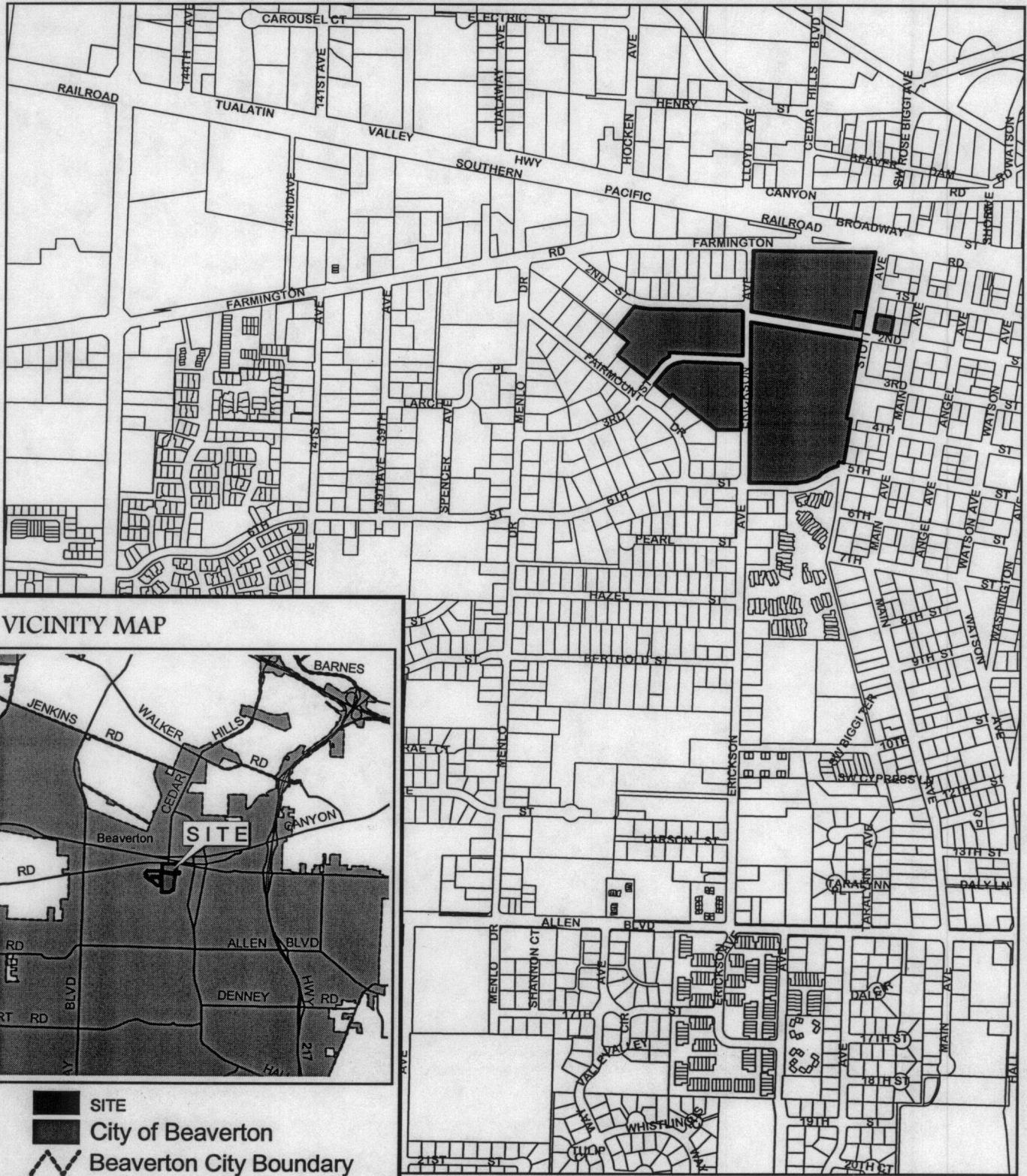
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BEAVERTON OR 97005

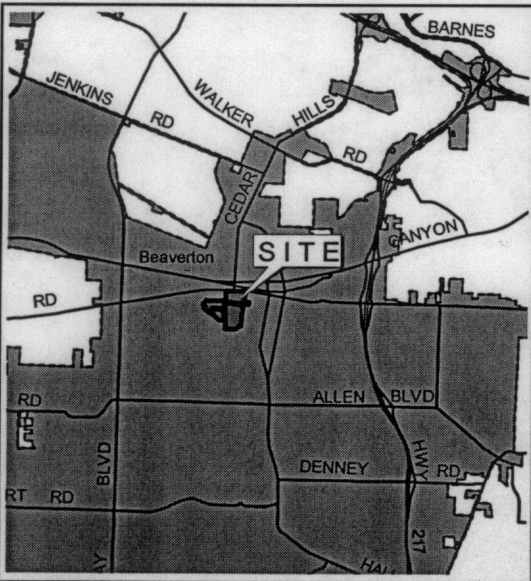
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YI HA YON &  
YI CHO SIM  
4975 SW ANGEL AVE  
BEAVERTON OR 97005




1S116AD03600  
YOSHIDA JUNKI  
8440 NE ALDERWOOD RD STE A  
PORTLAND OR 97220

# DETAIL MAP



## VICINITY MAP



-  SITE
-  City of Beaverton
-  Beaverton City Boundary



City of Beaverton

BEAVERTON HIGH SCHOOL CAFETERIA & PARKING LOTS

COMMUNITY DEVELOPMENT DEPARTMENT

Development Services Division

01/02/02

Map Number

1S116AD

Application #

BDR 2001-0213/CUP 2001-0031  
VAR 2002-0002/TPP 2001-0008

N







IS116AA04700  
ADAMS-HAGLUND JENNIFER  
2605 NE ROBERTS PL  
GRESHAM OR 97030

IS116AD02500  
ADOLPHSON LOVING TRUST  
BY RICHARD & SHIRLEY ADOLPHSON  
25505 SW LONE FIR LANE  
WEST LINN OR 97068

IS116AD02501  
ADOLPHSON LOVING TRUST  
25505 SW LONE FIR LN  
WEST LINN OR 97068

IS116AD02300  
ADOLPHSON R G  
ADOLPHSON R G JR  
BY A-2 ENTERPRISES  
4555 SW MAIN ST  
BEAVERTON OR 97005

IS116DB00400  
AFOURKEEFF ANJA T  
5075 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116AD07000  
AHN DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

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AHN DANIEL H  
12820 SW 2ND ST  
BEAVERTON OR 97005

IS116AD06100  
ALPROP CO  
6149 SW SHATTUCK RD  
PORTLAND OR 97221

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ANDERSON ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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ANDERSON ANSEL E & MARIAN L  
4715 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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ARLT LINDA L  
13275 SW PEAR ST  
BEAVERTON OR 97005

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ARMSTRONG JAMES R  
LINDA L  
13325 SW 2ND  
BEAVERTON OR 97005

IS116DA90027  
BABB DORIS LEE  
5136 SW ERICKSON AVE  
BEAVERTON OR 97005

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BAKER VIRGIL A & MARIE  
5166 SW ERICKSON AVE  
BEAVERTON OR 97005

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BARTLETT CLIFFORD L  
13500 SW 2ND  
BEAVERTON OR 97005

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BASEEL PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

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BASEEL PRESTON  
6977 SW TIERRA DEL MAR  
BEAVERTON OR 97007

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BEAVERTON FOODS INC  
PO BOX 687  
BEAVERTON OR 97005

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BEAVERTON PROPERTIES  
13470 SW FARMINGTON RD  
BEAVERTON OR 97005

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BEAVERTON SCHOOL DISTRICT 48  
MERLE DAVIS ELEMENTARY  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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BEAVERTON SCHOOL DISTRICT 48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

IS116AD11000  
BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

IS116AD02900  
BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

IS116AD07100  
BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

IS116AC02100  
BEAVERTON SCHOOL DISTRICT 48  
ADMINISTRATION COMPLEX  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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BEAVERTON SCHOOL DISTRICT 48  
BEAVERTON HIGH SCHOOL  
ATTN FACILITIES DEPT  
16550 SW MERLO RD  
BEAVERTON OR 97006

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BEAVERTON SCHOOL DISTRICT 48J  
ATTN FACILITIES MNGMT  
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PO BOX 4753  
BEAVERTON OR 97076

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BEAVERTON OR 97076

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~~PO BOX 4755~~  
BEAVERTON OR 97076

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7265 SW WILLOWMERE DR  
PORTLAND OR 97225

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~~BENNETT CLEVE E AND~~  
~~LOIS W TRUSTEES~~  
7265 SW WILLOWMERE DR  
PORTLAND OR 97225

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12820 SW 4TH ST  
BEAVERTON OR 97005

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BIGGI STEVE AND LAURA  
3843 SW HALL BLVD  
BEAVERTON OR 97005

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BLOOMER DANIEL D MARY L  
4925 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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BYRNES DONALD B & B JERI  
12900 SW 5TH ST  
BEAVERTON OR 97005

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CAFFEE CENTER JOINT VENTURE  
BY ANDERSON & ASSOCIATES  
PO BOX 3821  
BELLEVUE WA 98009

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CARSON JERRY/PAULA  
13270 SW 6TH  
BEAVERTON OR 97005

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CASSIDY MARI  
4820 SW MENLO  
BEAVERTON OR 97005

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CASSON KATHLEEN ANN  
3818 SE 9TH AVE  
PORTLAND OR 97202

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CENTRAL PARK CONDO  
UNIT OWNERS  
BY LINCOLN COMPANY  
PO BOX 586  
BEAVERTON OR 97005

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~~UNIT OWNERS~~  
~~BY LINCOLN COMPANY~~  
~~PO BOX 586~~  
BEAVERTON OR 97005

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BEAVERTON OR 97006

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CHIA FUO FUH &  
WU CHUN ER  
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BEAVERTON OR 97005

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COLEMAN ANNE S  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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COLEMAN ANNE SCOTT  
4990 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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COOLEY CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

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COOLEY CRAIG L & JUNE E  
21797 SW OAK HILL LN  
TUALATIN OR 97062

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CORP OF PRESIDING BISHOP OF  
CHURCH OF JESUS CHRIST LDS  
TAX ADMIN FILE 536-2490  
50E NORTH TEMPLE  
SALT LAKE CITY UT 84150-2201

IS116AC05801  
CORSON JOHN D  
4850 SW MENLO DR  
BEAVERTON OR 97005

IS116AD07400  
CRITELLI FAMILY TRUST  
c/o CRITELLI TINA  
BY GARY A & SHARON L CRITELLI  
PO BOX 87058  
VANCOUVER WA 98687

IS116AD10300  
CRITELLI GARY A & SHARON L TRS  
PO BOX 87058  
VANCOUVER WA 98687

~~IS116DA01600~~  
~~CRITELLI GARY A & SHARON L TRS~~  
~~PO BOX 87058~~  
VANCOUVER WA 98687

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D'ALFONSO PAULA L  
4975 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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DAMIANA RONALD G  
GEORGIA M  
12870 SW 4TH  
BEAVERTON OR 97005

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DANEIL HOLLIS MARION  
13425 SW 6TH  
BEAVERTON OR 97005

IS116DB00900  
DANIEL AVERILL H/HOLLIS M  
13425 SW 6TH  
BEAVERTON OR 97005

IS116AC01500  
DANISH SEYMOUR R REV TRUST &  
BERRY HARRY L ET AL  
BY DANISH CAROL J/SEYMOUR R TRS  
PO BOX 55  
BEAVERTON OR 97005



IS116AD03100  
D'SILVA FELIX & LOURDES  
8765 SW JAMIESON RD  
PORTLAND OR 97225

IS116DA01700  
EIMERS SANDRA P &  
EIMERS ANGELA  
6045 SW 167TH AVE  
ALOHA OR 97007

IS116DB03101  
ENGEL MICHAEL J &  
REBECCA C  
13210 SW 6TH ST  
BEAVERTON OR 97005

IS116AA05700  
ETON LANE LTD PARTNERSHIP  
WALKER HUBERT E TRUSTEE &  
ETON LANE LLC  
PO BOX 14746  
PORTLAND OR 97293

~~IS116AA05800  
ETON LANE LTD PARTNERSHIP  
WALKER HUBERT E TRUSTEE  
PO BOX 14746  
PORTLAND OR 97214~~

IS116AC04200  
EVANS CATHERINE B  
11775 SW 11TH STREET  
BEAVERTON OR 97005

IS116AC01200  
FISHBACK MERL E &  
GLORIA M TRUST  
FISHBACK MERL E/GLORIA M TR  
9700 NW GROVELAND RD  
HILLSBORO OR 97124

IS116DA02200  
FLOREN JANET M AND GREG E  
12870 SW 6TH AVE  
BEAVERTON OR 97005

IS116DB00300  
FRANKLIN JOHN A  
5025 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116AC04900  
FRIEZE REX D SHIRLEY L  
4720 SW FAIRMONT DR  
BEAVERTON OR 97005

IS116DB01100  
GABRIEL KATHLEEN  
13470 SW 3RD ST  
BEAVERTON OR 97005

IS116AC05600  
GARCIA WAGNER  
4800 SW MENLO DR  
BEAVERTON OR 97005

IS116DA90025  
GARTLAND SHARIN  
5162 SW ERICKSON  
BEAVERTON OR 97005

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GARVER JON & REBECCA  
12855 SW 2ND ST  
BEAVERTON OR 97005

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GENEST VIRGINIA E  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

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GENEST VIRGINIA E  
BY VALLEY DAIRY QUEEN  
12870 SW FARMINGTON RD  
BEAVERTON OR 97005

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GHIM MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

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GHIM MINJE P  
495 SW 169TH PL  
BEAVERTON OR 97006

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GLASNER GENE R  
c/o DREY BRIAN S & PAMELA R  
5225 SW ERICKSON AVE  
BEAVERTON OR 97005

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GLICKMAN TERRY L  
13555 SW THIRD ST  
BEAVERTON OR 97005

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GORMAN JOHN & PATRICIA A  
16555 SW HIGH HILL LN  
BEAVERTON OR 97007

IS116AC01000  
GRABHORN CHAD  
350 NW FREEMAN  
HILLSBORO OR 97124

IS116AC03700  
GREEN ERNESTINE RUTH  
4820 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

IS116AA02100  
GREER DAVID J & LISA D  
19370 SW SOUTHVIEW ST  
ALOHA OR 97007

IS116AC05300  
GREMAUX DEANE B AND  
PATRICIA A  
4755 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

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GRL INC  
13095 SW CANYON RD  
BEAVERTON OR 97005

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GUSTAFSON CONRAD W & ANNA M  
13245 SW PEARL ST  
BEAVERTON OR 97007

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BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005

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GUTZLER CAROLYN E  
BY TOM'S PANCAKE HOUSE  
12925 SW CANYON RD  
BEAVERTON OR 97005~~

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BEAVERTON OR 97005~~

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GUTZLER THOMAS M AND  
CAROLYN E  
BY TOM'S PANCAKE HOUSE  
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BEAVERTON OR 97005

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HALL LAURA I  
12888 SW 5TH  
BEAVERTON OR 97005

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HALSTEN ELSIE M TRUSTEE  
12855 SW 6TH  
BEAVERTON OR 97005

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HAMREUS CALVIN E & JOYCE K  
5075 SW MAIN AVE  
BEAVERTON OR 97005

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HATTENHAUER CHRISTINE  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

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HATTENHAUER CHRISTINE  
261 SW ASHDOWN CIR  
WEST LINN OR 97068

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HEDGES SALLY C  
12850 SW 5TH AVE  
BEAVERTON OR 97005

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HEISLER MARYANNE &  
NEUFELL PATRICIA J  
13250 SW PEARL  
BEAVERTON OR 97005

IS116DA01100  
HETU TED G  
5075 SW ANGEL AVE  
BEAVERTON OR 97005

IS116AD06000  
HILL RICHARD E AND FLORENCE E  
8405 SW MAVERICK PL  
BEAVERTON OR 97008

IS116AD10100  
HOUDROJ ALI  
4970 SW MAIN ST  
BEAVERTON OR 97005

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HUESMAN NICOLE K  
5134 SW ERICKSON AVE  
BEAVERTON OR 97005

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HUNKAPILLAR PATRICIA W  
BY THE TAGGART TRUST  
21185 NW EVERGREEN PKWY  
SUITE 101  
HILLSBORO OR 97124

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SMITH HARRIET L AND GEORGE  
BY TAGGART TRUST THE  
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HILLSBORO OR 97124

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HUNT SHERYLL R/WILLIAM J &  
HUNT JONATHAN J  
13380 SW FARMINGTON RD  
BEAVERTON OR 97005

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HUTCHENS DANA C & LORI E  
12820 SW SIXTH ST  
BEAVERTON OR 97005

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HUYGENS JOSEPH D &  
JULIE ANN HUTCHINSON  
7830 SW LAUREL ST  
PORTLAND OR 97225

IS116AD09901  
JOHNSON CAROL L R  
PO BOX 524  
SCAPPOOSE OR 97056

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JONES E CHANNING & PAULA KAY  
13420 SW 2ND AVE  
BEAVERTON OR 97005

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JONES JERRY C  
12912 SW 5TH 52  
BEAVERTON OR 97005

IS116AC03600  
JONES KRISTIN L  
4870 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DA07000  
JUNELL BONNIE M &  
BABCOCK DOUGLAS C  
4820 SW 18TH PL  
PORTLAND OR 97201

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KABDEBO JOSEPH & CAMILLE  
725 SW VIEWMONT DR  
PORTLAND OR 97225

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KABDEBO JOSEPH & CAMILLE  
725 SW VIEWMONT DR  
PORTLAND OR 97225

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KABDEBO JOSEPH AND CAMILLA  
725 SW VIEWMONT DR  
PORTLAND OR 97225

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KAUTH JANE F  
5115 SW ERICKSON AVE  
BEAVERTON OR 97007

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KIKO JOHN R &  
ORTIZ INGRID  
5155 SW MAIN AVE  
BEAVERTON OR 97005

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KIM UN & SUE J  
14756 SW KILCHIS  
BEAVERTON OR 97007

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KIM YOUNG M & LINDA  
12855 SW CANYON RD  
BEAVERTON OR 97005

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KING CHARLES C & ROM N  
5150 SW ERICKSON AVE  
BEAVERTON OR 97005

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KLANECKY ARDITH ANN  
5164 SW ERICKSON  
BEAVERTON OR 97005

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KO JOON B  
13215 SW PEARL ST  
BEAVERTON OR 97005

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KRAEMER JOHN L & CLAUDIA R  
33713 NE WILLIAMS APT F-6  
SCAPPOOSE OR 97056

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KRIEG MARGIE J  
12924 SW 5TH ST  
BEAVERTON OR 97005

IS116DA90057  
LAMB KEVIN M &  
YANT SUSAN A  
12892 SW 5TH ST  
BEAVERTON OR 97005

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LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

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LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

IS116AC00500  
LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

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LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

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LANPHERE ROBERT  
ATTN DOUG MEATH  
12505 SW BROADWAY ST  
BEAVERTON OR 97005

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LEE SANDY  
344 NE 156TH AVE  
PORTLAND OR 97230

IS116DB02900  
LEIVA LUIS ALONSO & SANDRA J  
13320 SW 6TH ST  
BEAVERTON OR 97005

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LEONARD CLIFFORD A & SHARON B  
7345 SW 84TH AVE  
PORTLAND OR 97223

IS116AD06200  
LESHER SAM E/HELENA R &  
LESHER CARL H  
7725 SE 32ND AVE  
PORTLAND OR 97202

IS116AC06200  
LEWIS SUNNY M  
13425 SW 3RD  
BEAVERTON OR 97005

IS116DA90047  
LIDBERG MARY ALICE C  
12932 SW 5TH AVE  
BEAVERTON OR 97005

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LIEDTKE CATHERINE A  
17561 SW ALBERT CT  
BEAVERTON OR 97007

IS116DA01400  
LISICKI THOMAS D & RUTH S  
13332 SW BENCHVIEW TER  
TIGARD OR 97223

IS116DA07400  
LOCKARD DOROTHY J  
LIVING TRUST THE  
5125 SW SHERWOOD PL  
BEAVERTON OR 97005

IS116AC02800  
LONG PATRICIA C & JOE PAT  
5070 SW FAIRMONT DR  
BEAVERTON OR 97005

IS116AC04800  
LOPEZ JORGE & CARMEN  
4700 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DB03201  
LOY JOHN G  
5105 SW ERICKSON AVE  
BEAVERTON OR 97005

IS116DA90060  
LUCAS LAURIE G  
12880 SW 5TH AVE  
BEAVERTON OR 97005

IS116DA06200  
LUCKE WILLIAM T TRUSTEE  
5205 SW MAIN AVE  
BEAVERTON OR 97005

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LUND BRADLEY W  
4855 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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LUNDGREN TIMOTHY  
18213 PORTLAND AVE  
GLADSTONE OR 97027

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MACK TIMOTHY F  
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BEAVERTON OR 97005

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MATELICH RAE MARIE  
12928 SW 5TH AVE  
BEAVERTON OR 97005

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MAYES PATSY L  
12904 SW 5TH ST  
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MERCADO NEVILLE & RENEE  
PO BOX 487  
BEAVERTON OR 97075

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MEYER JANE E & RICHARD J &  
JOHN M  
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BEAVERTON OR 97007

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MILLARD KATHLEEN  
4790 SW FAIRMOUNT DRIVE  
BEAVERTON OR 97005

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MOHR PATRICIA ALICE  
WILLIAM RICHARD  
7495 SW 136TH AVE  
BEAVERTON OR 97008

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MORGAN WILLIAM L & EVE E  
20855 NW CHILOQUIN  
PORTLAND OR 97229

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MORRIS GLENN R AND ANITA S  
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BEAVERTON OR 97005

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NELSEN MARY G  
9965 SW 170TH  
BEAVERTON OR 97007

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INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064

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INGRID  
9700 INDUSTRIAL PKWY  
PLAIN CITY OH 43064

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BEAVERTON OR 97005

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OKAMOTO BRETT K  
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PO BOX 704  
BEAVERTON OR 97075

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OVERBY ARDON L  
PO BOX 704  
BEAVERTON OR 97075~~

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PARKS LOREN E  
PO BOX 5669  
ALOHA OR 97006

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PEARCE JOSEPH J  
4920 SW MAIN  
BEAVERTON OR 97005

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PETERSEN DONALD E  
10240 SW HAWTHORNE LN  
PORTLAND OR 97225

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PHAM HIEN &  
NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006

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PHAM HIEN & NGUYEN THAO &  
NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

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NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

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NGUYEN GIA & QUYNH  
16620 NW PADDINGTON DR  
BEAVERTON OR 97006~~

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NGUYEN GIA & QUYNH  
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BEAVERTON OR 97006~~

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PIES SANDRA J & EVERETT M TRS  
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HILLSBORO OR 97123

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PODEBSKI ADAM P & GUTA H  
PODEBSKI BENNY  
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BEAVERTON OR 97005

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QUINN CAROLYN F  
13395 SW 2ND ST  
BEAVERTON OR 97005

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QUTUB WAHBI M &  
MATAR RIAD A  
7883 SW BARNARD DR  
BEAVERTON OR 97007

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RANDOLPH GLORIA  
2725 SW 185TH  
ALOHA OR 97006

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RANDOLPH GLORIA C  
2725 SW 185TH AVE  
ALOHA OR 97005~~

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REMCHEK GEORGE & FLORENCE  
4240 SW 102ND AVE  
BEAVERTON OR 97005

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ROBERTS LEON &  
CARRIE  
4725 SW FAIRMOUNT DR  
BEAVERTON OR 97005

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ROBINSON JACK A & PATRICIA A  
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HILLSBORO OR 97123

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ROSENHOLM DAVID  
ROSENHOLM ANDREA  
13220 SW PEARL ST  
BEAVERTON OR 97005

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SANABRIA-BARILLAS JOSE ALFREDO  
ALVARADO JUANA  
13170 SW PEARL ST  
BEAVERTON OR 97005

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SCALONE DAVID  
13255 SW 6TH ST  
BEAVERTON OR 97005

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SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661

1S116AD01400  
~~SCHOEN ENTERPRISES LLC  
3815 WAUNA VISTA DR  
VANCOUVER WA 98661~~

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SESSIONS KEITH W & LAVERNA M  
TRUSTEES  
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SKORO MARION & MIRELA  
19300 NW GILLIHAN LOOP RD  
PORTLAND OR 97231

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SMITH DONALD J PHYLLIS J  
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SMITH IAN C & NADINE T  
12750 SW 4TH ST  
BEAVERTON OR 97005

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SMITH RICHARD BRENDAN &  
HILARY ANTOINETTE MARY  
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SMOLENSKY G MARIKA  
PO BOX 1114  
BEAVERTON OR 97075

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SNODGRASS PHILIP R & PATRICIA  
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BEAVERTON OR 97005

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SOTH COLLEEN V TRUSTEE  
SOTH FORREST C TRUSTEE  
4890 SW MENLO DR  
BEAVERTON OR 97005

1S116AD04400  
SPRINGER JAMES F & SUE  
c/o BEAVERTON FUNERAL HOME INC  
4126 NORLAND AVE  
BURNABY BC CANADA  
V5G 3S8

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~~SPRINGER JAMES F & SUE  
c/o BEAVERTON FUNERAL HOME INC  
4126 NORLAND AVE  
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V5G 3S8~~

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ST CLAIR JERALD LEE  
SHIRLEY ANN  
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BEAVERTON OR 97005

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STARKE DONALD B EDNA A  
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BEAVERTON OR 97005

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STEICHEN NANCY & MCCOURT JOHN  
c/o HOVERSLAND GORDON & MYRNA L  
4350 SW CEDAR HILLS BLVD  
BEAVERTON OR 97005

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STEIGER JOHN M & LOU-WAYNE  
301 NW MURRAY BLVD  
PORTLAND OR 97229

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STOCKWELL KATHERINE R &  
BROWN GARY E  
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BEAVERTON OR 97005

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STOLTENBURG BARBARA  
c/o RAINEY WALLACE L & LESLIE C  
5025 SW ANGEL AVE  
BEAVERTON OR 97005

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SURBY PAUL W L FRANCES  
13375 SW 2ND  
BEAVERTON OR 97005

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THAO PANCHONG & MAO HER  
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BEAVERTON OR 97005

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THOMAS BARBARA A  
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BEAVERTON OR 97005

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THOMAS KAREN JO  
12650 SW 1ST  
BEAVERTON OR 97005

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THOMAS KELLY R &  
BOTHNE MELISSA  
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BEAVERTON OR 97005

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BOTHNE MELISSA  
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BEAVERTON OR 97005~~

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BEAVERTON OR 97005~~

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BEAVERTON OR 97005

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~~THOMPSON AND ADAMS~~  
4500 SW WALKER BLVD  
BEAVERTON OR 97005

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THOMPSON TOM G &  
THOMPSON MARGIE J  
12896 SW 5TH AVE  
BEAVERTON OR 97005

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THOMPSON MAURICE J & KIMBERLY  
13475 SW 3RD ST  
BEAVERTON OR 97005

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THOMPSON TOMMY G  
JEANNETTE L  
13305 SW 2ND ST  
BEAVERTON OR 97005

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TISCHLER STEPHEN & KATHLEEN  
5195 SW ERICKSON AVE  
BEAVERTON OR 97005

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TOSCO CORP  
PROPERTY TAX DEPT-DC17  
PO BOX 52085  
PHOENIX AZ 85072

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TUALATIN HILLS PARK  
RECREATION DISTRICT  
15707 SW WALKER RD  
BEAVERTON OR 97005

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~~RECREATION DISTRICT~~  
~~15707 SW WALKER RD~~  
BEAVERTON OR 97005

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BEAVERTON OR 97005

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BEAVERTON OR 97005

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~~VADEN MICHAEL & JANE~~  
~~12740 SW CANYON RD~~  
BEAVERTON OR 97005

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BY LANPHERE ENTERPRISES INC  
ATTN DOUG MEATH  
12520 SW CANYON RD  
BEAVERTON OR 97005

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VEAZEY THOMAS C AND  
ROXANNE T  
12770 SW 6TH ST  
BEAVERTON OR 97005

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VINK NANCY E  
12940 SW 5TH ST  
BEAVERTON OR 97005

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VOGEL MICHAEL J & D'ANNE T  
4770 SW FAIRMONT DR  
BEAVERTON OR 97005

IS116AC05200  
VON W METHENEY FAMILY TRUST  
4775 SW FAIRMOUNT DR  
BEAVERTON OR 97005

IS116DA90026  
VOSE BENJAMIN A  
PO BOX 73  
TOLOVANA PARK OR 97145

IS116AC03100  
WABNITZ STEVEN G  
2648 SW 153RD AVE  
BEAVERTON OR 97006

IS116AC00400  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

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~~WASHINGTON COUNTY INVESTMENTS~~  
~~LTD PTNSHP &~~  
~~FRANCIS JOHN L~~  
PO BOX 744  
BEAVERTON OR 97075

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~~WASHINGTON COUNTY INVESTMENTS~~  
~~LTD PTNSHP &~~  
~~FRANCIS JOHN L~~  
PO BOX 744  
BEAVERTON OR 97075

IS116AC00190  
WASHINGTON COUNTY INVESTMENTS  
LTD PTNSHP &  
FRANCIS JOHN L  
PO BOX 744  
BEAVERTON OR 97075

IS116AD10800  
WELDON GEORGE E AND  
FLORENCE GRACE  
8783 SW 175TH AVE  
BEAVERTON OR 97007

IS116DA06801  
WENDT WILLIAM J CAROL  
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BEAVERTON OR 97005

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BEAVERTON OR 97005

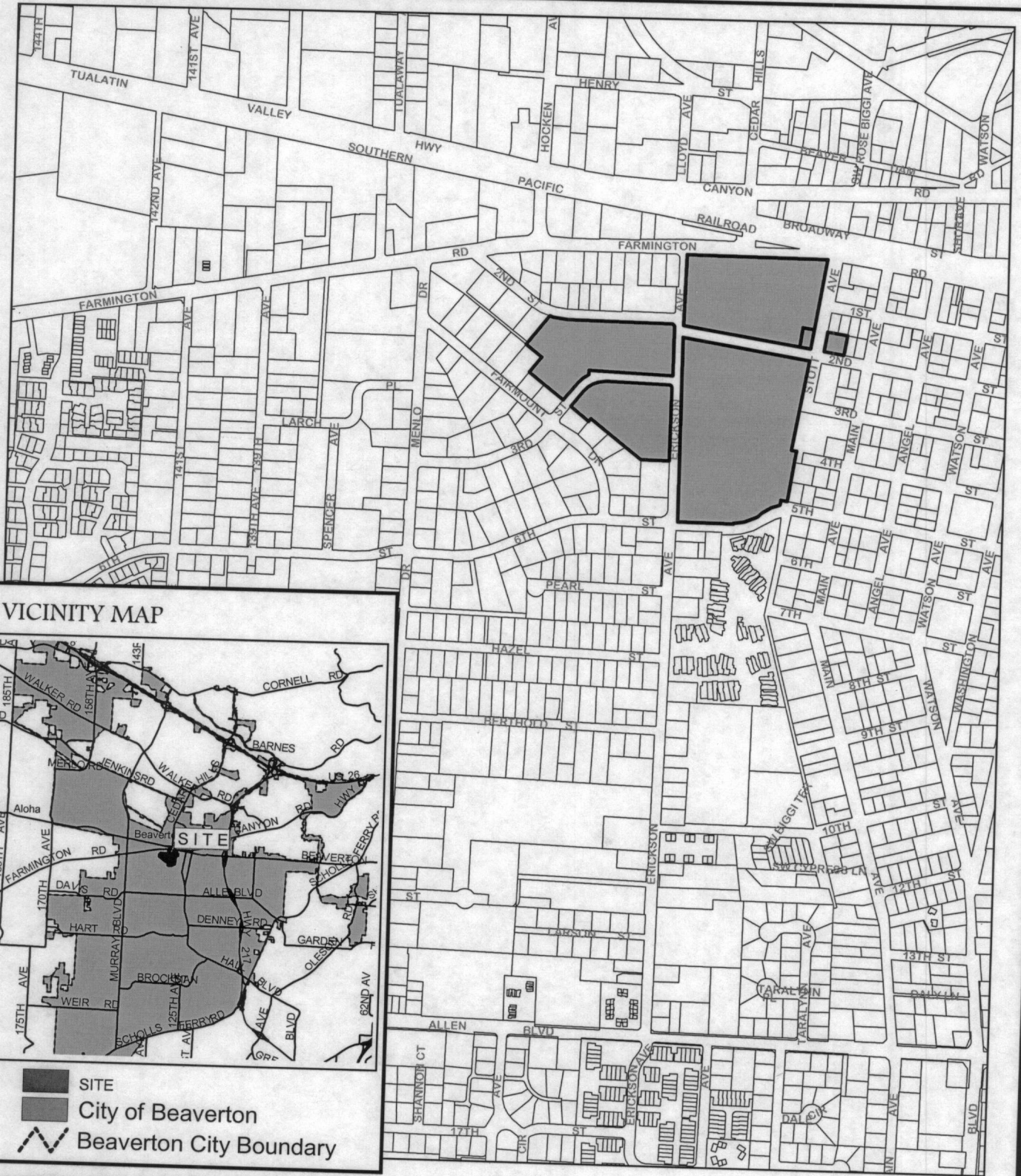
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WILD GENE D AND BETTY LOU  
4975 SW MAIN  
BEAVERTON OR 97005

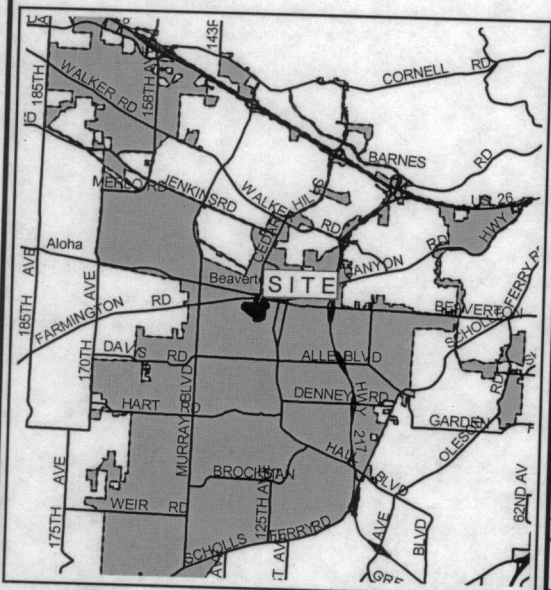
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YI CHO SIM  
4975 SW ANGEL AVE  
BEAVERTON OR 97005

IS116AD03600  
YOSHIDA JUNKI  
8440 NE ALDERWOOD RD STE A  
PORTLAND OR 97220

# DETAIL MAP



## VICINITY MAP



- SITE
- City of Beaverton
- Beaverton City Boundary



City of Beaverton

BEAVERTON HIGH SCHOOL CAFETERIA & PARKING LOTS

COMMUNITY DEVELOPMENT DEPARTMENT

Development Services Division

11/27/01

Map Number  
1S116AC  
1S116AD

Application #

CUP 2001-0031







City of Beaverton  
Community Development Department  
4755 S.W. Griffith Drive  
P.O. Box 4755  
Beaverton, OR 97076

**BDR 2001-0213/CUP 2001-0031/VAR 2002-0002/TPP 2001-0008 BEAVERTON HIGH SCHOOL  
CAFETERIA AND PARKING LOT EXPANSION** **03/28/02**

**NOTICE TO MORTGAGEE, LIENHOLDER  
VENDOR OR SELLER: IF YOU RECEIVE THIS  
NOTICE, IT MUST BE PROPERLY  
FORWARDED TO PURCHASER**

**City of Beaverton  
DEVELOPMENT APPLICATION  
NOTICE OF REVIEW**

**Notice Date: February 5, 2002**

The Historic Resource Review Committee will conduct a public hearing on **March 21, 2002** at 6:30 pm. The Planning Commission will conduct a public hearing on **March 20, 2002** at 7 pm. The Board of Design Review will conduct a public hearing on **March 28, 2002**, at 6:30 p.m. The public hearings will be held to consider the following applications:

**Beaverton High School Cafeteria and Parking Lot Expansion**

The following land use applications have been submitted to construct a new two-level, 30,000-square foot cafeteria building located directly north of and connecting to the existing high school building. The development proposal is located at 13000 SW 2<sup>nd</sup> Street; Washington County Assessor's Map 1S1-16AD, on Tax Lots 11100, 11000, 02900, 07100 and 10900 and 1S116AC, on Tax Lots 02100 and 02500. The site is zoned Urban Low Density (R-10) and is approximately 27 acres in size.

**BDR2001-0213: Type III Design Review**

The applicant is requesting the Board of Design Review to approve construction of a new cafeteria building and parking area at the existing Beaverton High School location. The proposal includes construction of a new two-level, 30,000-square foot cafeteria building located directly north of and connecting to the existing high school building, new parking area, and associated landscaping. In taking action on the proposed development, the Board of Design Review shall base its decision on the approval criteria listed in Section 40.10.15.3.C.

**CUP2001-0031: Conditional Use Permit**

The applicant requests approval of a Conditional Use Permit (CUP) to build new cafeteria building and parking area at the existing Beaverton High School location. Educational institutions, including public, private or parochial academic schools are identified specifically as a conditional use within the R-10 zone. The proposed project is an expansion of an existing conditional use. The Planning Commission, during a public hearing, will review this request. In taking action on the proposed development, the Planning Commission shall base its decision on the approval criteria listed in Section 40.05.15.2.C.

**VAR2002-0002: Variance (Regular)**

The applicant requests a variance to reduce the amount of bicycle parking required at Beaverton High School. Beaverton Development Code requires the minimum parking ratio of one long-term bicycle space for every 18 students. Based on the potential enrollment of the school of 2,200 students, 122 long-term bicycle parking spaces would be required. The applicant requests a variance to reduce the total number to 56 spaces, 66 fewer than the required amount. The Planning Commission, during a public hearing, will review this request. In taking action on the proposed development, the Planning Commission shall base its decision on the approval criteria listed in Section 40.80.15.2.C.

**TPP2001-0008: Tree Preservation Plan (Historic Trees)**

Request for Historic Resource Review Committee approval for the removal of 20 trees the City of Beaverton has designated as Historical Trees. In taking action on the proposed development, the Historic Resource Review Committee shall base its decision on the approval criteria listed in Section 40.75.15.1.C.2.

The Public Hearings will be conducted at Beaverton City Hall in the Council Chambers located at 4755 S.W. Griffith Drive, Beaverton, Oregon. These hearings will be conducted for the purpose of receiving testimony from interested persons on the proposed development described above. Further information may be obtained by contacting the Development Services Division at 503-526-2348.

Any person may appear before the Planning Commission, Board of Design Review, and/or the Historic Resource Review Committee public hearings and be heard in support of or in opposition to the granting

of the request. Written testimony is acceptable if received by either of the two Commissions and/or Board prior to the meeting. However, the public is encouraged to submit their written comments by **February 27, 2002**. On that date, the Planning Director will convene the Facilities Review Committee to consider the proposed development's compliance with the technical criteria of Section 40.10.15.3.C.1. The Committee will then forward recommended findings and conditions on the technical approval criteria to the Board of Design Review. The Committee's recommendation to the Board of Design Review shall be whether to approve, approve with conditions, or deny the proposed development.

The Beaverton Development Code requires that a notice be sent to property owners within 500 feet of the property proposed for development and to the chairperson of the affected Neighborhood Association Committee (NAC). Affected parties may review plans that have been submitted for the project. Copies of the staff report are available for inspection at no cost seven (7) calendar days prior to the public hearing, and will be duplicated upon request for a reasonable cost.

Please note: The Planning Commission reviews and takes action on zone changes, conditional uses, variances, and other proposals which result from the application of the requirements of the City Development Code. The Board of Design Review hears requests for development on properties where zoning, land use, traffic impacts, drainage and other functional issues have already been evaluated and approved. The Board's jurisdiction is over design issues such as landscaping, materials, building designs, etc. The Historic Resource Review Committee hears requests for the alteration or removal of designated historic resources within the City.

Following the hearings, the Planning Commission, Board of Design Review, and Historic Resource Review Committee can each do one of the following:

- 1) Deny the application.
- 2) Approve the application.
- 3) Approve the application with conditions.
- 4) Continue the application.

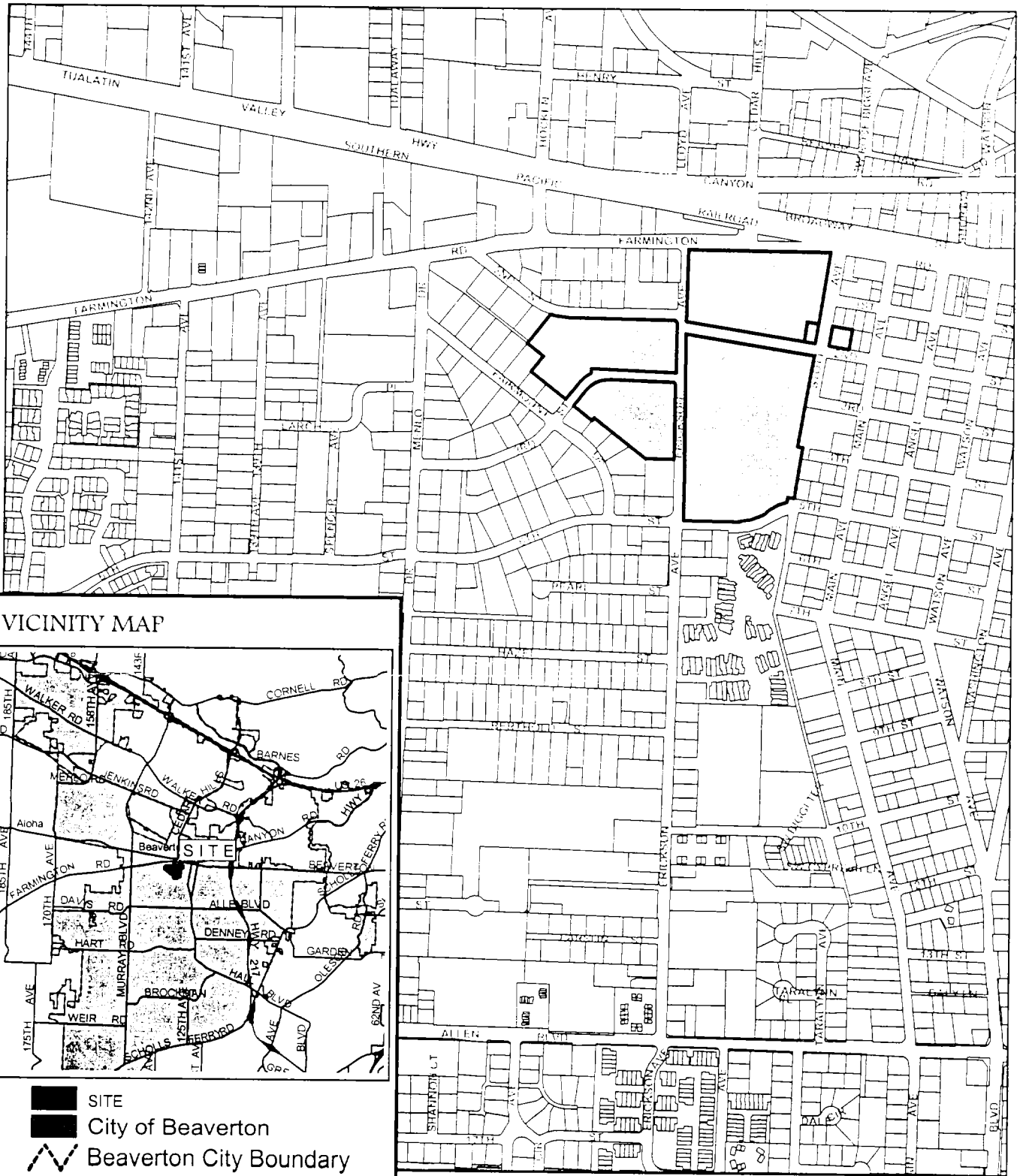
A participant in the hearing may request, before the close of the hearing, that the record remain open for at least seven days after the hearing.

Failure of an issue to be raised in person or by letter with sufficient specificity to afford decision makers an opportunity to respond to the issue precludes appeal to the Land Use Board of Appeals on that issue.

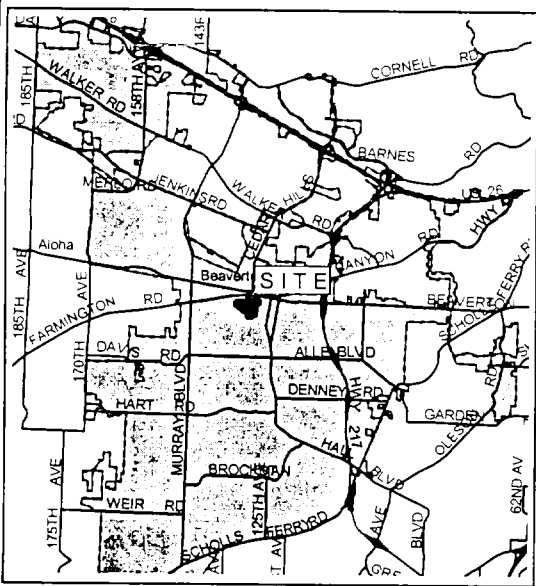
An applicant, aggrieved person, or the Mayor may appeal the action of the Planning Commission, Board of Design Review, or Historic Resource Review Committee to the City Council. The appeal must be filed within ten days of the Commission's or Board's action and be in writing. Appeal of any of the above specified applications must meet the requirements of Section 50.40.2 of the Development Code.

THIS INFORMATION IS AVAILABLE IN LARGE PRINT OR AUDIO TAPE UPON REQUEST. IN ADDITION, ASSISTED LISTENING DEVICES, SIGN LANGUAGE INTERPRETERS, OR QUALIFIED BILINGUAL INTERPRETERS WILL BE MADE AVAILABLE AT ANY PUBLIC MEETING OR PROGRAM WITHIN 72 HOURS ADVANCE NOTICE. TO REQUEST THESE SERVICES, PLEASE CALL 503-526-2222/VOICE/TDD.

# DETAIL MAP



## VICINITY MAP



- SITE
- City of Beaverton
- Beaverton City Boundary



City of Beaverton

BEAVERTON HIGH SCHOOL CAFETERIA & PARKING LOTS

COMMUNITY DEVELOPMENT DEPARTMENT

Development Services Division

11/27/01

Map Number

1S116AC

1S116AD

Application #

BDR 2001-0213



CHECKLIST

Project Number: BDR 2001-0213 / CUP 2001-0031 / VAR 2002-0002  
Re-Notice. MAP Revision. TPP 2001-0008

Project Name: Beaverton High School Cafeteria + Parking  
lot expansion

Map Number: 1S116AC 1S116AD

Tax Lot(s): 02500 07100

Mailing Date: 3/11/02

Meeting Date(s): 3/20/02 / 3/28/02

RECEIVED: Request PC BDR (X)  
Map (X)  
Copy of Application (X)  
Vicinity Map, Detail Map, Reference Map (X)  
ArcView Labels (X)

COMPLETED: Notice to Newspaper (X)  
Date Submitted 3/8/02 (X)  
To Print 3/14/02 (X)  
Newspaper Transmittal (X)  
Labels for Owner, Applicant, Etc. (X)  
500' Labels (Remove Duplicates) (X)  
Request typed on PHN form (X)  
Map and Criteria (X)  
Reprographics form filled out (X)  
20 Day Notice ( )  
30 Day Notice ( )  
Posting - Front Rack (X)  
Affidavit (X)  
Email (to PHN) (X)

SK

PC

BDR

FAC REV

CITY COUNCIL

HRRC

### CHECKLIST

Project Number: BDR 2001-0213/CUP 2001-0031/VAC 2002.0002/TA 2001-008

Project Name: Beaverton High School, Cafeteria & Parking Lots

Map Number: 151-16AD; 151-16AC

Tax Lot(s): 11100; 11000; 2900; 7100; 10900; 2100; 2500

Mailing Date: 151-16AD; 151-16AC  
FAC 2-27-02

Meeting Date(s): PC 3-20-02; HRRC 3-21-02; BDR 3-28-02

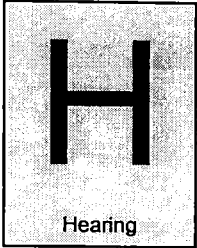
RECEIVED:

Request	(X)
Map	(X)
Copy of Application	(X)
Vicinity Map, Detail Map, Reference Map	(X)
ArcView Labels	(X)

COMPLETED:

Notice to Newspaper	(X)
Date Submitted <u>2-1-02</u>	(X)
To Print <u>2-7-02</u>	(X)
Newspaper Transmittal	(X)
Labels for Owner, Applicant, Etc.	(X)
500' Labels (Remove Duplicates)	(X)
Request typed on PHN form	(X)
Map and Criteria	(X)
Reprographics form filled out	(X)
20 Day Notice	(X)
30 Day Notice	( )
Posting - Front Rack	(X)
Affidavit	(X)
Email (to PHN)	(X)

<input type="checkbox"/> BDR-3	<input type="checkbox"/> VAR-D	<input type="checkbox"/> CPA	<input type="checkbox"/> APP
<input checked="" type="checkbox"/> CUP-R	<input type="checkbox"/> VAR-R	<input type="checkbox"/> RZ	<input type="checkbox"/> HRRC
<input type="checkbox"/> TPP	<input type="checkbox"/> VAR-S	<input type="checkbox"/> TA	<input type="checkbox"/> _____



Community Input Meeting: _____	<input type="checkbox"/> BDR _____	<input type="checkbox"/> HRRC _____
	<input type="checkbox"/> PC _____	<input type="checkbox"/> CC _____

Project Number: CUP2001-0031

Project Name: Beaverton High School Cafeteria & Pkg lots

See also: BDR 2001-0215; TPP 2001-0008

**Request for:**

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

The site is within the R-10 zone.

The site is located 13000 SW 2nd

and is approximately 17.69 acres in size.

Tax lot (s): 11000, 11100, 10900 Map IS116AD

Owner/Applicant: Beaverton School District

**Routing**

From: Phelan COUNTER PLANNER

To: \_\_\_\_\_ COMPLETENESS PLANNER

Assigned Planner: \_\_\_\_\_

**Approvals**

Draft Notice: \_\_\_\_\_ ASSIGNED PLANNERS INITIALS

Final Notice: \_\_\_\_\_ ASSIGNED PLANNERS INITIALS

**Items Received**

<input type="checkbox"/> Newspaper Notice	<input type="checkbox"/> Application copy
<input type="checkbox"/> Property list	<input checked="" type="checkbox"/> N.A.C. <u>Central Beaverton</u>
<input type="checkbox"/> Location map	<input type="checkbox"/> Other _____

# APPLICATION FOR COMPLETENESS

Completeness comments are due at Fac Rev on: Jan 9 2002

<input checked="" type="checkbox"/> Development Services Planner (file) <input checked="" type="checkbox"/> Jim Duggan <input checked="" type="checkbox"/> Don Gustafson	Assigned Planner:  _____ Phone: _____
--	--

## Project Information:

Project Number: CUP2001-0031

Project Type: CUP-R

Project Name: Beaverton High School Cafeteria & Pkg Lots

Location: 13000 SW 2nd

Map & Tax Lot #s 15116AD 10900, 11000, 11100

Zone: R-10

Owner/Applicant: Beaverton School Dist.

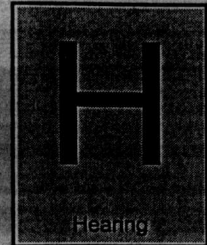
Comments prepared by: \_\_\_\_\_ Phone: \_\_\_\_\_

Complete  Incomplete

## Comments:

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

<input checked="" type="checkbox"/> BDR-3	<input type="checkbox"/> VAR-D	<input type="checkbox"/> CPA	<input type="checkbox"/> APP
<input type="checkbox"/> CUP-R	<input type="checkbox"/> VAR-R	<input type="checkbox"/> RZ	<input type="checkbox"/> HRRRC
<input type="checkbox"/> TPP	<input type="checkbox"/> VAR-S	<input type="checkbox"/> TA	<input type="checkbox"/> _____



Community Input Meeting: _____	<input type="checkbox"/> BDR _____	<input type="checkbox"/> HRRRC _____
	<input type="checkbox"/> PC _____	<input type="checkbox"/> CC _____

Project Number: BDR2001-0213

Project Name: Beaverton High School Cafeteria & Pkg Lots

See also: TPP 2001-0008; CUP 2001-0031

**Request for:**

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

The site is within the R-10 zone.

The site is located 13000 SW 2nd

and is approximately 17.69 acres in size.

Tax lot (s): 11000, 11100, 10900 Map 1S116AD

Owner/Applicant: Beaverton School Dist.

**Routing**

From: Shell  
COUNTER PLANNER

To: \_\_\_\_\_  
COMPLETENESS PLANNER

Assigned Planner: \_\_\_\_\_

Approvals

Draft Notice: \_\_\_\_\_  
ASSIGNED PLANNERS INITIALS

Final Notice: \_\_\_\_\_  
ASSIGNED PLANNERS INITIALS

**Items Received**

<input type="checkbox"/> Newspaper Notice	<input type="checkbox"/> Application copy
<input type="checkbox"/> Property list	<input checked="" type="checkbox"/> N.A.C. <u>Central Beaverton</u>
<input type="checkbox"/> Location map	<input type="checkbox"/> Other _____



A G E N D A   B I L L

Beaverton City Council  
Beaverton, Oregon

91-118

SUBJECT:   INVENTORY OF  
          HISTORIC TREES

FOR AGENDA OF: 5/20/91 BILL NO. **91-118**

APPROVED FOR AGENDA BY: *AA*

DEPT. OF ORIGIN: PLANNING DIVISION *JD*

DATE SUBMITTED:   APRIL 30, 1991

PROCEEDING:   CONSENT AGENDA

CLEARANCES:

EXHIBITS: REPORT DATED MARCH 7, 1991  
          HRRC MINUTES DATED APRIL 18, 1991

---

EXPENDITURE	AMOUNT	APPROPRIATION
REQUIRED:   \$ 0	BUDGETED   \$ 0	REQUIRED   \$ 0

---

BACKGROUND

On April 18, 1991, the Historic Resource Review Committee approved the attached list of sites as containing historic trees.

RECOMMENDED ACTION

Approve on Consent Agenda.

TO: Historic Resource Review Committee      DATE: March 7, 1991  
FROM: Nadine M. Smith  
Senior Planner  
RE: Historic Tree Inventory

A. Background

The City Council recently adopted an amendment to Ordinance 2050 addressing tree and vegetation preservation. One of the requirements of that ordinance amendment was as follows:

"An inventory shall be conducted of trees or groves which may qualify as historic. After drawing up a proposed list of historic trees and groves of trees, the Historic Resource Review Committee (HRRRC) shall hold at least one public hearing of which the affected property owners have been notified, pursuant to the procedures of Section 130. Each property owner of the tree or trees under consideration shall be notified by certified mail 30 days prior to any public hearing.

"Following public hearing, the HRRRC shall develop a recommended list of historic trees and groves. The Council may adopt the recommended list on its consent agenda or may hold a public hearing in the same manner and form as the hearing held by the HRRRC. Any affected party may request a hearing before the City Council prior to adoption."  
(Section 265(A and B), Ordinance 2050)

Using the adopted list of historic resources in Beaverton and following research to determine the existence of historic trees in Beaverton, approximately 50 sites were evaluated for potential historic designation.

Historic trees are defined in the ordinance as "those designated by the City to be of historic significance based upon their association with historic figures, properties or general growth and development of the City.

Prior to completion of the inventory, Neighborhood Association were contacted for their input and staff appeared before most associations to provide information on the ordinance and inventory process.

B. Historic Tree Inventory

The final proposed historic tree inventory consists of a total of six sites. Attached to this report is a summary sheet on each of the sites. Historic trees will be evaluated by the City's Historic Resource Review Committee as a separate public hearing process.

C. Ordinance Regulation of Trees on the Inventory

Those trees finally adopted as historic will require Historic Resource Review Committee approval for removal. If development is proposed that will impact an individual historic tree, a public hearing will be required before the Historic Resource Review Committee to evaluate the impact on the tree or trees. Trees may be removed if the HRRRC finds that the circumstances listed under Section 263 of the Ordinance have been met or the tree has lost its historic significance for some reason.

The ordinance will also regulate pruning of historic trees. A permit from the Planning Department is required for pruning. The permit will require that pruning take place in such a way that will not damage or kill the protected tree.

D. HRRRC Action

Staff held a public information meeting on February 7th to meet with individuals who are proposed for the inventory and address individual concerns. As of the preparation of this report, the Planning Department has received one letter of opposition to listing on the inventory which is attached.

It is our intent to give a presentation at the public hearing on March 7th on the individual sites that are proposed to be listed as containing historic trees. Unless further information is required, the Historic Resource Review Committee can adopt the historic inventory at that hearing.

The inventory that is adopted by the Committee will appear on the City Council's consent agenda for final adoption. An optional public hearing date has also been provided for a hearing before Council.

Historic Tree Inventory  
March 7, 1991  
Page 3 of 3

E. Recommendation

Staff recommends that the Historic Resource Review Committee adopt following trees as historic trees:

1. Ponderosa pine located at 4875 SW Angel
2. Sequoias located at 4440 SW 148th (St. Mary's Academy)
3. Birches located at 16270 SW Hart Road
4. Oak located at 10010 SW 125th
5. Landscaping at Merle Davies Elementary school on SW Farmington
6. "Century trees" at Fanno Farm house in the Creekside industrial park.

Attachments

1. Section 260 - 272 of Ordinance 2050 (Preservation of Trees and Vegetation)
2. Inventory Forms and public responses

Historic Trees # 1

BEAVERTON TREE INVENTORY

ADDRESS: 4875 SW Angel

CROSS STREETS: 4th ST, NW Corner

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
(DBH: Diameter at breast height)  
Formula for DBH

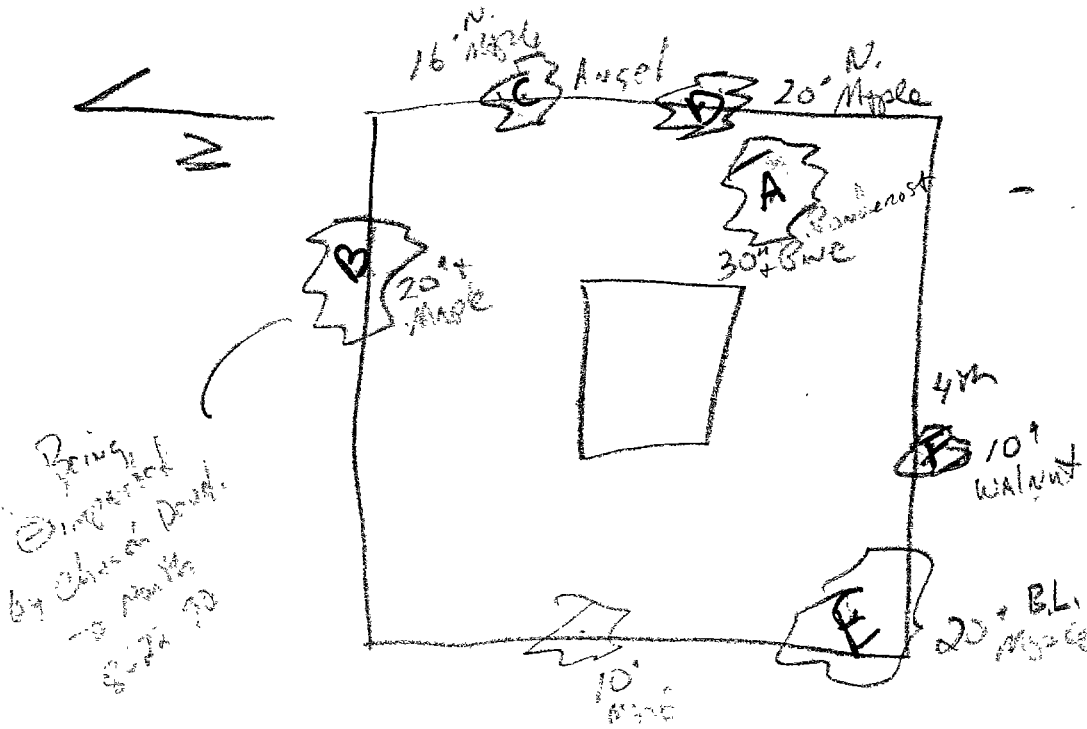
Diameter = Circumference divided by pi (3.1416)

Historic Potential  
Species:

- 1. Big leaf Maple (4)
- 2. Ponderosa Pine
- 3.
- 4.
- 5.
- 6.
- 7.
- 8.
- 9.
- 10.
- 11.
- 12.
- 13.
- 14.

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_  
Number of Street Trees  
Define street trees

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

- 1. - No historical value.  
Age 20 years or under.
- 2. - Little historical value.  
Age 20 years or over.
- 3. - Some historical value.  
Age 50 years or over.
- 4. - Has historical value  
Age 80 years or over.
- 5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

- 1. - Poor condition  
Unhealthy with parts dead or dying.
- 2. - Fair condition - OK health but needing care.
- 3. - Good condition - Healthy.  
(May still need care)

LOCATION 4875 SW Angel

	<u>AGE</u>	<u>CONDITION</u>	
A	<u>5</u>	<u>3</u>	} stressed from recent const.
B	<u>4</u>	<u>1-2</u>	
C	<u>3</u>	<u>1-2</u>	
D	<u>3</u>	<u>2</u>	
E	<u>4</u>	<u>3</u>	
F	<u>1</u>	<u>3</u>	

TREE/GROVE HISTORICAL ANALYSIS:

	<u>RATING</u>	<u>RATING</u>	<u>RATING</u>
1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.	<u>A 5</u>	_____	_____

1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

Comments: Ponderosa pine of approximately age. The historic Cady structure was ~~built~~ <sup>constructed</sup> after the tree indicating that the tree was ~~dead~~ <sup>preserved</sup> during construction. Cady was an important figure in Beaverton's growth and development.

2) Tree/Grove association with general growth and development of the City.

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

TOTAL

<u>5</u>	_____	_____
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# Historic Trees # 2

## BEAVERTON TREE INVENTORY

ADDRESS: ST MARKS (EAST side)  
\_\_\_\_\_  
\_\_\_\_\_

CROSS STREETS: 148<sup>th</sup> T.U.  
\_\_\_\_\_

LOT SIZE: \_\_\_\_\_

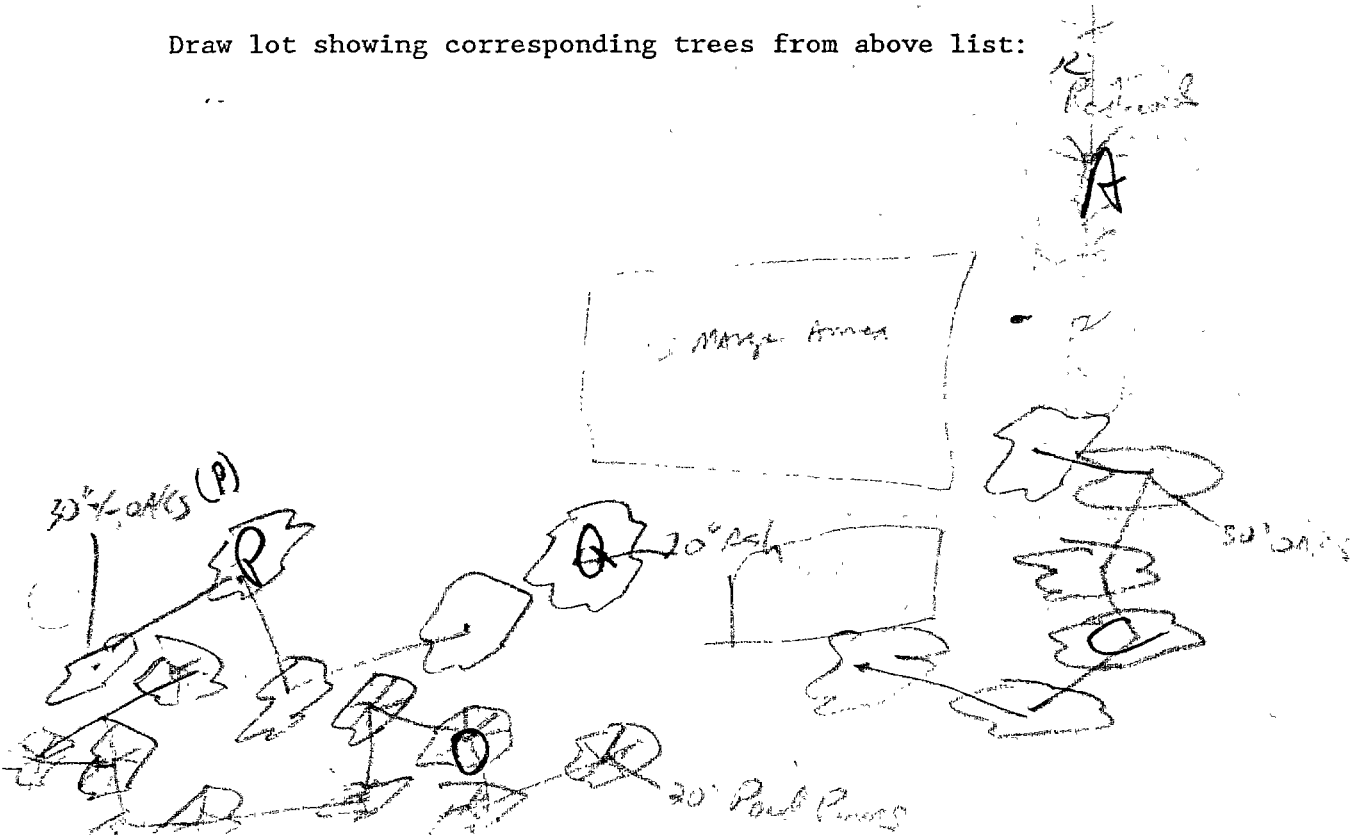
Number of trees over 3" DBH  
(DBH: Diameter at breast height)  
Formula for DBH

Diameter = Circumference divided by pi (3.1416)

- Species:
- |                     |     |
|---------------------|-----|
| 1. OAKS 30" C+P     |     |
| 2. Pond Pines 30" O | 8.  |
| 3. Redwoods 50" A   | 9.  |
| 4. Oregon Ash 20" Q | 10. |
| 5.                  | 11. |
| 6.                  | 12. |
| 7.                  | 13. |
|                     | 14. |

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_  
Number of Street Trees \_\_\_\_\_  
Define street trees \_\_\_\_\_

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
 TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

1. - No historical value.  
Age 20 years or under.
2. - Little historical value.  
Age 20 years or over.
3. - Some historical value.  
Age 50 years or over.
4. - Has historical value  
Age 80 years or over.
5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

1. - Poor condition  
Unhealthy with parts dead or dying.
2. - Fair condition - OK health but needing care.
3. - Good condition - Healthy.  
(May still need care)

AGE                      CONDITION

H	<u>3</u> (70yrs)	<u>3</u>
I	<u>3</u>	<u>2</u>
J	<u>2</u> (40yrs)	<u>2</u>
K	<u>3</u>	<u>2-3</u>
L	<u>3</u>	<u>2-3</u>
M	<u>2</u> (40yrs)	<u>2</u>

LOCATION 4440 SW 148<sup>th</sup>

TREE/GROVE HISTORICAL ANALYSIS:

- 1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

RATING      RATING                      RATING

5                      \_\_\_\_\_                      \_\_\_\_\_

Comments: The landscaping at St. Mary's Academy is intrical to the grandeur of the building setting.

- 2) Tree/Grove association with general growth and development of the City.

5                      \_\_\_\_\_                      \_\_\_\_\_

Comments: The mature trees are associated with the structure that is a landmark in the City

TOTAL

10                      \_\_\_\_\_                      \_\_\_\_\_



BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

- 1. - No historical value.  
Age 20 years or under.
- 2. - Little historical value.  
Age 20 years or over.
- 3. - Some historical value.  
Age 50 years or over.
- 4. - Has historical value  
Age 80 years or over.
- 5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

- 1. - Poor condition  
Unhealthy with parts dead  
or dying.
- 2. - Fair condition - OK health  
but needing care.
- 3. - Good condition - Healthy.  
(May still need care)

LOCATION 4440 SW 148<sup>th</sup>

	<u>AGE</u>	<u>CONDITION</u>
A	<u>5</u>	<u>3</u>
B	<u>5</u>	<u>3</u>
C	<u>5</u>	<u>3</u>
D	<u>3 (70yrs)</u>	<u>3</u>
e	<u>5</u>	<u>3</u>
F	<u>5</u>	<u>3</u>
G	<u>3 (70yrs)</u>	<u>3</u>

TREE/GROVE HISTORICAL ANALYSIS:

- 1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

<u>RATING</u>	<u>RATING</u>	<u>RATING</u>
<u>3</u>	_____	_____

Comments: Redwoods associated  
with historic location

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- 2) Tree/Grove association with general growth and development of the City.

<u>2</u>	_____	_____
----------	-------	-------

Comments: \_\_\_\_\_

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TOTAL

_____	_____	_____
-------	-------	-------

*The Sisters of St.* **MARY** *of Oregon*

RECEIVED

FEB 11 1991

PLAINVILLE, OREGON

February 7, 1991

Nadine Smith  
City of Beaverton  
4755 SW Griffith Drive  
P. O. Box 4755  
Beaverton, OR 97076

Dear Ms. Smith,

On behalf of the Sisters of St. Mary of Oregon, I would like to express concern about inclusion of St. Mary of the Valley in the City's significant tree inventory.

In recent telephone conversations, you have indicated to Sister Delores Adelman and Kevin Van Hoomissen that our convent building is designated as a historic site and that trees and landscaping around it are your prime interest.

Please be advised that we share the intent of such interest. The Sisters of St. Mary of Oregon have occupied this site for more than 100 years--(we received the J. Arthur Young Award from the Beaverton Chamber in 1985 on the occasion of our Centennial)--and have maintained our campus with pride during that time. Most of the trees and shrubs were planted by the Sisters themselves with the necessary attention to cultivation and pruning. The area surrounding the Convent is a visual blessing in our area, and it is our intention to keep it that way.

We feel that inclusion in the City's significant tree inventory could result in regulation of tree pruning and/or removal of what your inventory would identify as "mature trees."

In your telephone conversations with us, you have cited certain circumstances which would soften the impact of restrictive regulatory requirements; these include safety hazards, damaged trees, access to buildings or equipment, and coping with water drainage, etc.

We find all of these problems in our current site management scenario, as we search for ways to safely improve access/egress from our site, improve on-site traffic and parking, provide for new campus programs, and accommodate new surface water requirements, while maintaining our existing site functions of education and health care.

We respectfully ask that no unnecessary administrative hurdles be placed in the way of the achievement of our plan and programs. Therefore, we request that our trees not be placed on the City's significant tree inventory at this time.

*4440 Southwest 148th · Beaverton, Oregon 97007*

We pledge our continued cooperation in helping to keep Beaverton beautiful.  
Our century in Beaverton should make that pledge credible.

Sincerely,

*Sister Anna Hertel*

Sister Anna Hertel  
Superior General

BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

- 1. - No historical value.  
Age 20 years or under.
- 2. - Little historical value.  
Age 20 years or over.
- 3. - Some historical value.  
Age 50 years or over.
- 4. - Has historical value  
Age 80 years or over.
- 5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

- 1. - Poor condition  
Unhealthy with parts dead  
or dying.
- 2. - Fair condition - OK health  
but needing care.
- 3. - Good condition - Healthy.  
(May still need care)

LOCATION 4440 SW 148<sup>th</sup>

	<u>AGE</u>	<u>CONDITION</u>
N	5	2 + 1 (20% dead)
O	5	3
P	5	3
Q	3	2
R	3	3
S	3	3

TREE/GROVE HISTORICAL ANALYSIS:

1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

RATING      RATING      RATING

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

2) Tree/Grove association with general growth and development of the City.

\_\_\_\_\_

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

TOTAL

\_\_\_\_\_

Historic Trees # 3

MAP #

BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

- 1. - No historical value.  
Age 20 years or under.
- 2. - Little historical value.  
Age 20 years or over.
- 3. - Some historical value.  
Age 50 years or over.,,
- 4. - Has historical value  
Age 80 years or over.
- 5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

- 1. - Poor condition  
Unhealthy with parts dead  
or dying.
- 2. - Fair condition - OK health  
but needing care.
- 3. - Good condition - Healthy.  
(May still need care)

AGE                      CONDITION

A

3  
\_\_\_\_\_  
\_\_\_\_\_

3  
\_\_\_\_\_  
\_\_\_\_\_

All 6 same  
Age +/-  
+ Condition

LOCATION 16270 SW Hart

TREE/GROVE HISTORICAL ANALYSIS:

- 1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

RATING

RATING

RATING

4

Comments: Trees associated with  
historic figure and the  
general growth of the City

- 2) Tree/Grove association with general growth and development of the City.

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

TOTAL

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

LINDA DAVIS  
BEAVERTON PLANNING DIRECTOR  
CITY OF BEAVERTON  
4755 SW GRIFFITH DR  
BEAVERTON, OR 97005

H4

DEAR LINDA;

THIS IS A REQUEST FOR PLACEMENT OF THE 6 (SIX) WEEPING BIRCH TREES LOCATED ON MY PROPERTY UPON THE INVENTORY LIST OF HISTORICAL AND SIGNIFICANT TREES TO BE PROTECTED UNDER THE PROPOSED TREE ORDINANCE.

MY PROPERTY ADDRESS IS 16270 SW HART ROAD, MORE SPECIFICALLY DESCRIBED AS LOT 1 OF WEEPING BIRCH ESTATES. SINCE THESE ARE THE ONLY BIRCH TREES ON THESE DEVELOPED LOTS, THEY ARE OBVIOUSLY THE NAMESAKES FOR THIS AREA.

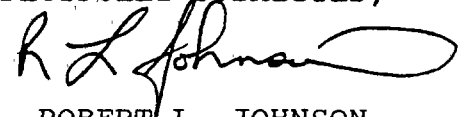
I RESIDE IN THE OLD HART HOMESTEAD FOR WHICH HART ROAD IS NAMED. THESE TREES WERE PLANTED IN THE EARLY 1940'S BY MR ROSS HART AS A PROPERTY BARRIER FROM THE ROAD, WHICH WAS UNPAVED AT THAT TIME. MR HART WAS A LEADING CITIZEN OF THE CITY OF BEAVERTON AND INVOLVED IN MANY CIVIC PURSUITS. HE WAS A TRUE ENTREPRENEUR; WITH A HATCHERY U-PICK STRAWBERRY FIELDS (APPROX 18 ACRES) AND TALENT AGENCY ALL RUN FROM THIS LOCATION.

THIS PROPERTY, ALONG WITH THE AFORE-MENTIONED TREES, ARE A SIGNIFICANT PART OF THE HISTORY OF THIS CITY. MY WIFE AND I HAVE TAKEN GREAT PRIDE IN THE RESTORATION OF THIS HOME AND WOULD LIKE TO HAVE THE PROPERTY UPON WHICH IT SITS PROTECTED AND PRESERVED AS WELL.

A FEW YEARS AGO, WE LED THE FIGHT AGAINST A LOCAL IMPROVEMENT DISTRICT PROPOSED BY WASHINGTON COUNTY TO WIDEN HART ROAD. WE AND OUR SUPPORTIVE NEIGHBORS SUCCESSFULLY DEFEATED THIS INITIATIVE, NOT BECAUSE OF THE PLAN TO WIDEN THIS MAJOR COLLECTOR STREET (WHICH WE MAY SUPPORT), BUT PRIMARILY DUE TO THE PLAN CALLING FOR THE REMOVAL OF THESE TREES. RATHER THAN WIDENING THE STREET TO THE MOSTLY UNDEVELOPED NORTH SIDE, THE PLAN CALLED FOR AN ABANDONMENT OF PROPERTY MOSTLY TO THE SOUTH. THIS WAS UNACCEPTABLE TO EACH OF US AND WAS SUBSEQUENTLY DEFEATED.

BASED UPON THE ABOVE-SUBMITTED INFORMATION, I RESPECTFULLY REQUEST PROTECTION FOR THESE MAJESTIC TREES, IN ORDER THAT THEY REMAIN A PART OF THE HERITAGE OF BEAVERTON.

RESPECTFULLY SUBMITTED,

A handwritten signature in cursive script, appearing to read "R L Johnson", written in dark ink.

ROBERT L. JOHNSON

PS: IF YOU WOULD BE SO KIND AS TO DISTRIBUTE THE ENCLOSED COPIES,  
I WOULD APPRECIATE IT VERY MUCH.

CC: CITY COUNCIL MEMBERS  
KAREN JAMES--CHAIRWOMAN OF THE COMMITTEE  
TO SAVE BEAVERTON TREES

BEAVERTON TREE INVENTORY

ADDRESS: 16270 SW Hart Rd  
\_\_\_\_\_  
\_\_\_\_\_

CROSS STREETS: West of 162 South Side of Hart  
\_\_\_\_\_

LOT SIZE: \_\_\_\_\_

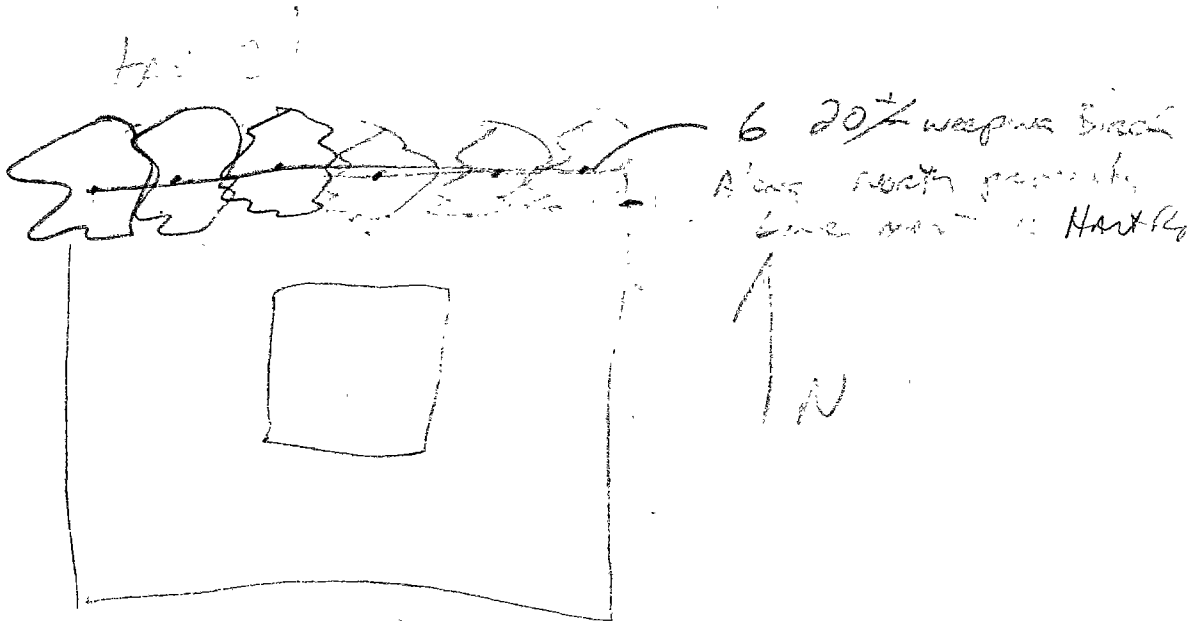
Number of trees over 3" DBH  
(DBH: Diameter at breast height)  
Formula for DBH

Diameter = Circumference divided by pi (3.1416)

- Species:
- |                        |     |
|------------------------|-----|
| 1. 6-20+ white Birches |     |
| 2.                     | 8.  |
| 3.                     | 9.  |
| 4.                     | 10. |
| 5.                     | 11. |
| 6.                     | 12. |
| 7.                     | 13. |
|                        | 14. |

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_  
Number of Street Trees \_\_\_\_\_  
Define street trees \_\_\_\_\_

Draw lot showing corresponding trees from above list:





# Historical # 4

## BEAVERTON TREE INVENTORY

ADDRESS: 10010 SW 125<sup>th</sup> - (Sorrento)  
\_\_\_\_\_  
\_\_\_\_\_

CROSS STREETS: SE corner with Conestoga  
\_\_\_\_\_

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
(DBH: Diameter at breast height)  
Formula for DBH

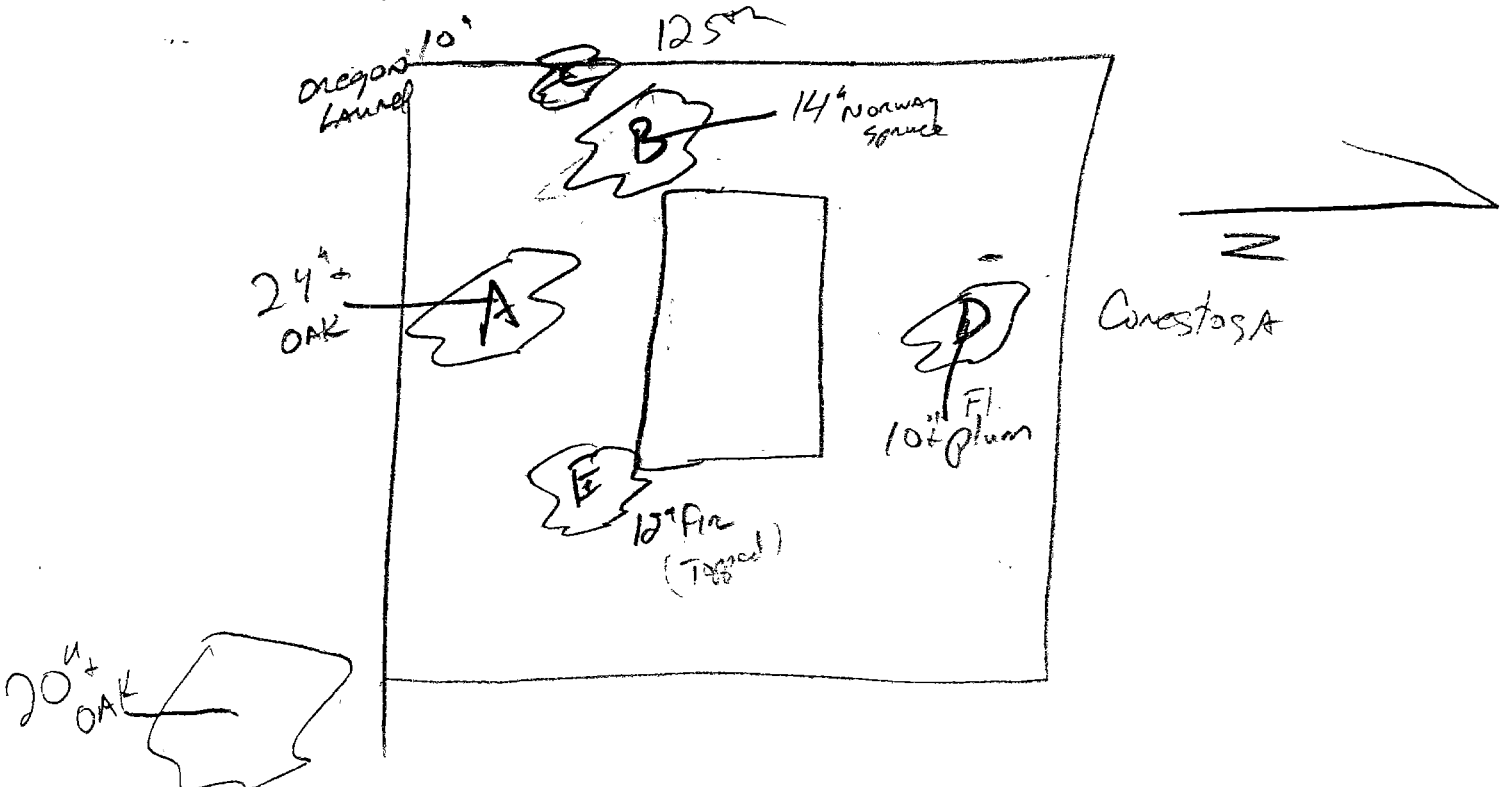
Diameter = Circumference divided by pi (3.1416)

Potential Historic  
Species:

- 1. 2 OAKS
- 2. 10" Oregon (Calf.) Laurel
- 3. Norway Spruce 14"
- 4.
- 5.
- 6.
- 7.
- 8.
- 9.
- 10.
- 11.
- 12.
- 13.
- 14.

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_  
Number of Street Trees \_\_\_\_\_  
Define street trees \_\_\_\_\_

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

1. - No historical value.  
Age 20 years or under.
2. - Little historical value.  
Age 20 years or over.
3. - Some historical value.  
Age 50 years or over.
4. - Has historical value  
Age 80 years or over.
5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

1. - Poor condition  
Unhealthy with parts dead or dying.
2. - Fair condition - OK health but needing care.
3. - Good condition - Healthy.  
(May still need care)

AGE                      CONDITION

A	<u>4</u>	<u>2</u>	
B	<u>2</u>	<u>3</u>	
C	<u>2 (40yrs)</u>	<u>3</u>	
D	<u>2 (40yrs)</u>	<u>3</u>	
E	<u>2 (40yrs)</u>	<u>2/3</u>	Topped c/40'

LOCATION 10010 SW Sorrento (125<sup>th</sup>)

TREE/GROVE HISTORICAL ANALYSIS:

RATING      RATING                      RATING

- 1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

3                      \_\_\_\_\_                      \_\_\_\_\_

Comments: The 20" + caliper oak  
is of an age that it is  
associated with the site of  
The Hutton School

- 2) Tree/Grove association with general growth and development of the City.

\_\_\_\_\_                      \_\_\_\_\_                      \_\_\_\_\_

Comments: The oak as part of an  
early school site is associated  
with Beaverton

TOTAL

\_\_\_\_\_                      \_\_\_\_\_                      \_\_\_\_\_

Historic tree # 5

BEAVERTON TREE INVENTORY

ADDRESS: 13150 Farmington  
\_\_\_\_\_  
\_\_\_\_\_

CROSS STREETS: \_\_\_\_\_  
\_\_\_\_\_

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
(DBH: Diameter at breast height)  
Formula for DBH

Diameter = Circumference divided by pi (3.1416)

Potential Historic

Species:

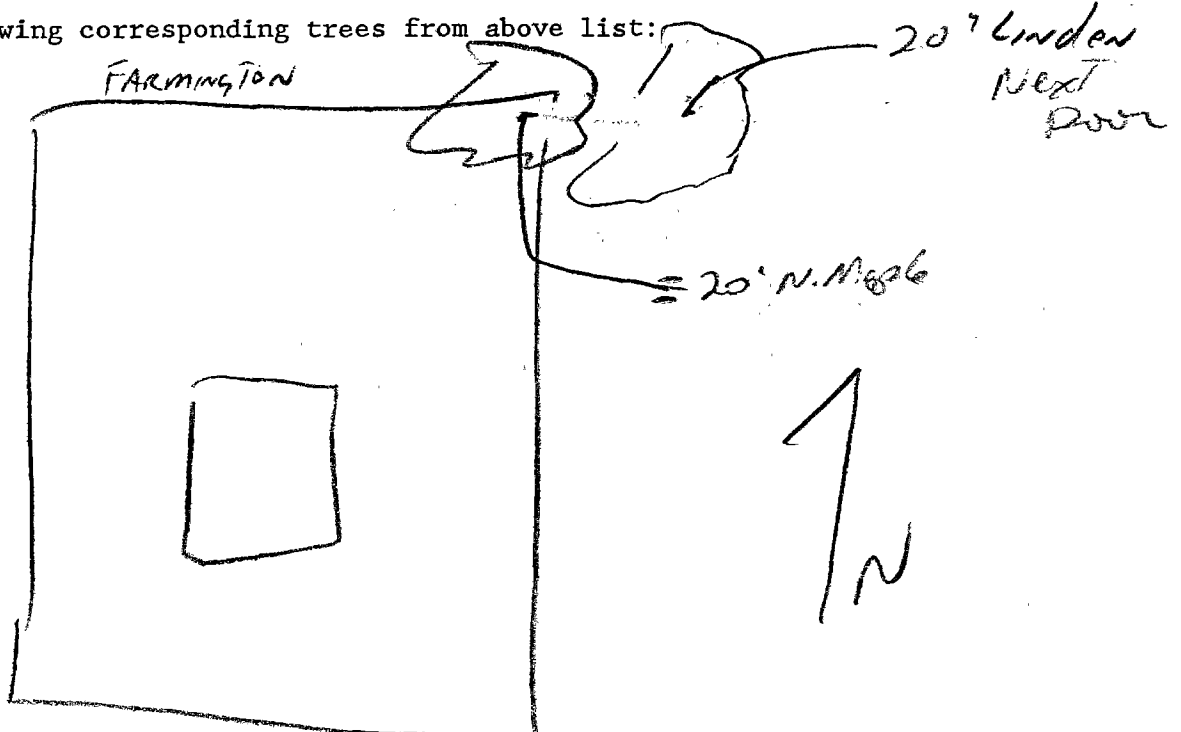
- 1. N Maple
- 2.
- 3.
- 4.
- 5.
- 6.
- 7.
- 8.
- 9.
- 10.
- 11.
- 12.
- 13.
- 14.

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_

Number of Street Trees

Define street trees

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY

H3

ADDRESS: 13000 Farmington  
 \_\_\_\_\_  
 \_\_\_\_\_

CROSS STREETS: West Side of Stott  
 \_\_\_\_\_

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
 (DBH: Diameter at breast height)  
 Formula for DBH

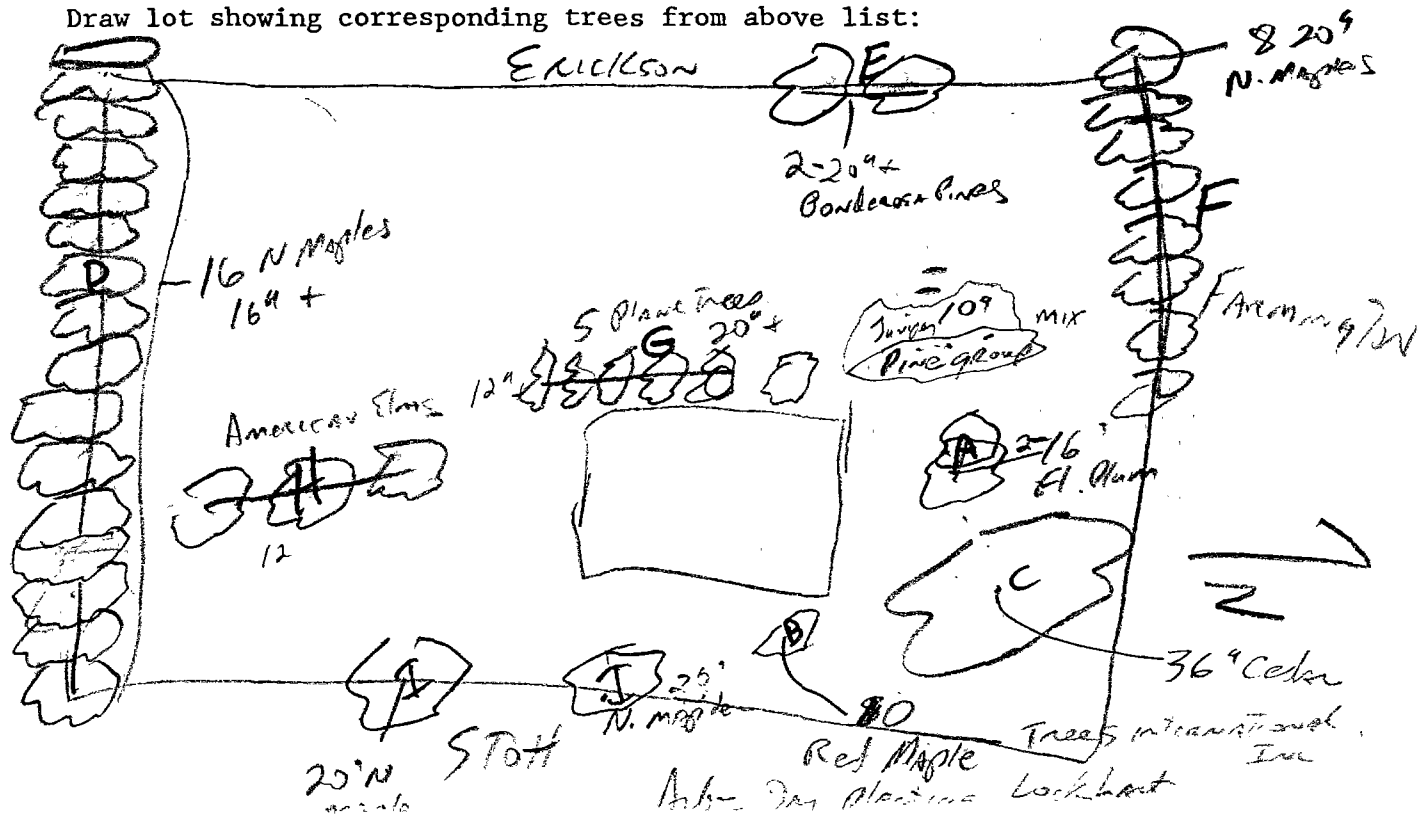
Diameter = Circumference divided by pi (3.1416)

Species:

1. All Potential Histories
2. 8.
3. 9.
4. 10.
5. 11.
6. 12.
7. 13.
- 14.

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_  
 Number of Street Trees \_\_\_\_\_  
 Define street trees \_\_\_\_\_

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

1. - No historical value.  
Age 20 years or under.
2. - Little historical value.  
Age 20 years or over.
3. - Some historical value.  
Age 50 years or over.
4. - Has historical value  
Age 80 years or over.
5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

1. - Poor condition  
Unhealthy with parts dead  
or dying.
2. - Fair condition - OK health  
but needing care.
3. - Good condition - Healthy.  
(May still need care)

	AGE	CONDITION
I	3	2
H	3	2
A	2	3
B	1	3
C	3	3
D	3	2
E	4	2
F	3	2
G	2 (30yrs+)	3
	<u>RATING</u>	<u>RATING</u>

LOCATION 13000 Farmington

TREE/GROVE HISTORICAL ANALYSIS:

- 1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

Comments: Arbor day / Earth day ceremony tree circa 1986

Merle Davis early respected teacher  
in Beaverton. Landscaping of an  
age that it was probably planted  
with original structure

- 2) Tree/Grove association with general growth and development of the City.

Comments: Trees are associated with  
structure of historic importance  
in Beaverton

TOTAL

<u>3</u>	_____	_____
<u>3</u>	_____	_____
<u>6</u>	_____	_____

# Historic trees # 6

## BEAVERTON TREE INVENTORY

ADDRESS: 8405 Creekside Place  
FANNO HOUSE

CROSS STREETS: \_\_\_\_\_

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
 (DBH: Diameter at breast height)  
 Formula for DBH

Diameter = Circumference divided by pi (3.1416)

Potential Historic

Species:

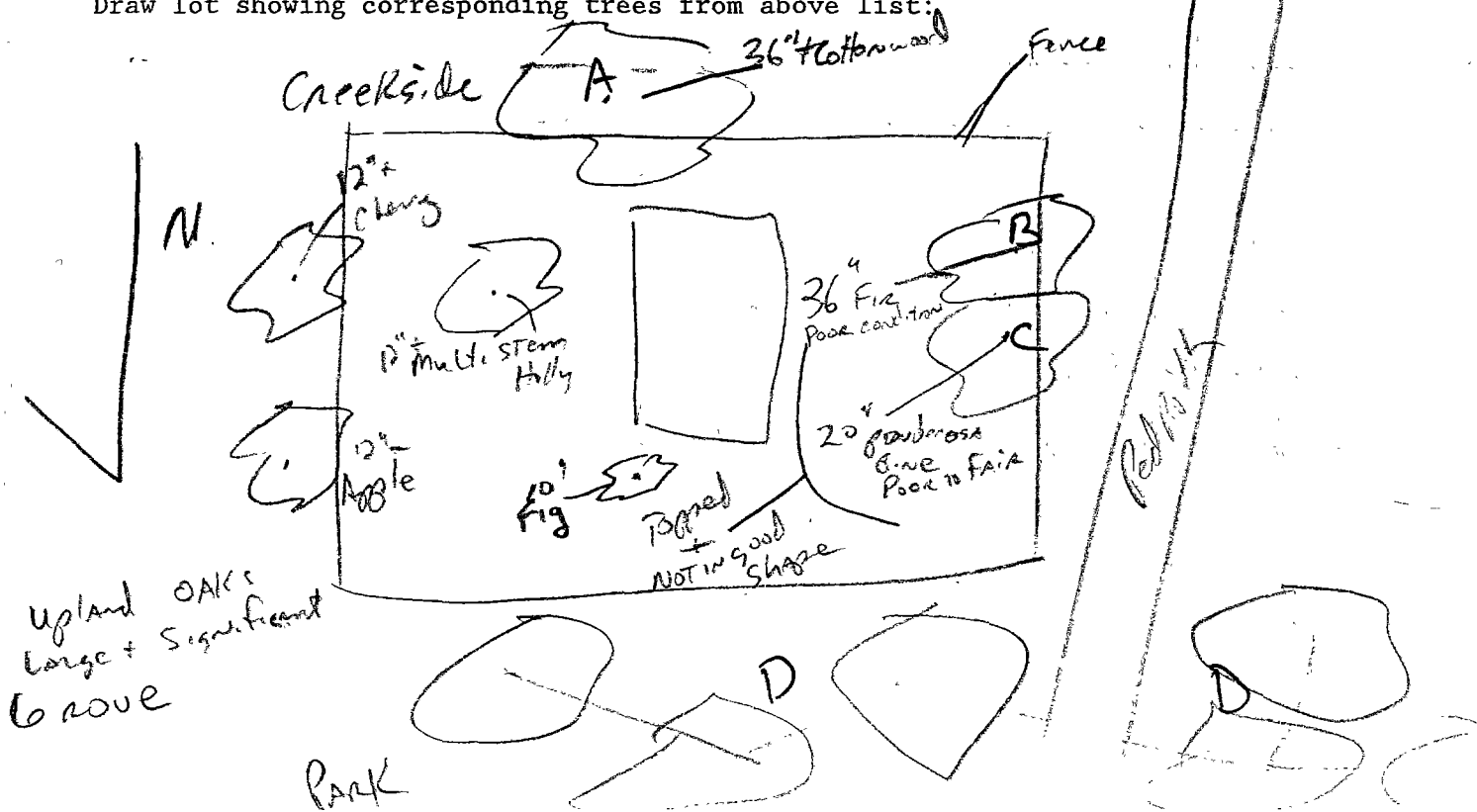
- |                      |                            |     |
|----------------------|----------------------------|-----|
| 1. Cottonwood 36"    | } NOT IN<br>Good Condition | 8.  |
| 2. Fir 36" (Doug)    |                            | 9.  |
| 3.ponderosa pine 20" |                            | 10. |
| 4.                   |                            | 11. |
| 5.                   |                            | 12. |
| 6.                   |                            | 13. |
| 7.                   |                            | 14. |

Groves - number of trees \_\_\_\_\_ Average DBH \_\_\_\_\_

Number of Street Trees

Define street trees

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
TREE/GROVE HISTORICAL RATING SHEET

RATING SCALE:

- 1. - No historical value.  
Age 20 years or under.
- 2. - Little historical value.  
Age 20 years or over.
- 3. - Some historical value.  
Age 50 years or over.
- 4. - Has historical value  
Age 80 years or over.
- 5. - Has significant historical value.  
Age 100 years or over.

CONDITION ASSESSMENT:

- 1. - Poor condition  
Unhealthy with parts dead  
or dying.
- 2. - Fair condition - OK health  
but needing care.
- 3. - Good condition - Healthy.  
(May still need care)

	<u>AGE</u>	<u>CONDITION</u>
A	<u>5</u>	<u>3</u>
B	<u>5</u>	<u>1</u>
C	<u>5</u>	<u>1</u>
D	<u>5</u>	<u>3</u>

LOCATION 8405 Creekside

TREE/GROVE HISTORICAL ANALYSIS:

RATING      RATING      RATING

- 1) Tree/Grove association with historic figures, events or significantly associated with a location, place or structure.

5      \_\_\_\_\_      \_\_\_\_\_

Comments: Douglas Fir and Pine associated with Fanno. Planted in association with 1876 centennial

- 2) Tree/Grove association with general growth and development of the City.

5      \_\_\_\_\_      \_\_\_\_\_

Comments: Fanno farm on registers of historic places - associated with pioneer days of Beaverton

TOTAL

10      \_\_\_\_\_      \_\_\_\_\_

The Committee agreed that the appearance of the house will be improved with the removal of the garage and were in favor of the proposal.

Ms. Maldonado MOVED for approval of HRC 2-91 (Methodist/Episcopal Church).

Mr. Grant SECONDED the motion.

The question was called and the motion CARRIED unanimously.

2. HISTORIC TREE INVENTORY

Ms. Smith explained that the Board of Design Review has adopted a list of significant trees in the City. She and Mr. Plamondon have reviewed the historical structures to see if there are trees on these sites that should be preserved along with the structure. Once the list is adopted, the owner will be required to obtain approval from the City to cut the trees down or to do major pruning on the trees. There are a total of six sites that ranked high enough to be put on the list. The only location which has received any objection from the property owners has been from the Sisters of St. Mary's. Their objection stems from the fact that they will be required to go through an added process. However, staff feels that the impact on the site will not be as significant as the owners anticipate.

page  
inadvertently  
omitted  
from  
AB 91-118  
attachments

Ms. Smith presented slides showing some of the trees proposed for the inventory. After approved by the Committee, the inventory will go to City Council as part of their consent agenda.

Mr. Grant MOVED for approval of the Historic Tree Inventory.

Ms. Maldonado SECONDED the motion.

The question was called and the motion CARRIED unanimously.

Further discussion with regard to the trees ensued pertaining to the notification given to the property owners, what it means to the property owner to be on the list, and possible financial aid with the maintenance of the trees on the list.

MISCELLANEOUS

Ms. Smith indicated that she has arranged for a presentation to be given to the Committee on the architectural styles and character of the historic buildings in the City. This is scheduled for the next meeting.

The Committee's role in the Beaverton Centennial to be held in 1993 will be defined at a future meeting.

ADJOURNMENT: 7:20 p.m.



BEAVERTON INVENTORY OF HISTORIC RESOURCES

HIST. NAME: Merle Davies School  
COMMON NAME: \_\_\_\_\_  
ADDRESS: 13000 SW Farmington Rd.  
OWNER: Beaverton School District  
No. 48  
MAP NO.: 1S1 W 16AD TAX LOT: 11100  
ADDITION: Town of Beaverton  
BLOCK: 12 LOT: \_\_\_\_\_

DATE OF CONSTR: 1937  
ORIGINAL USE: School  
PRESENT USE: School  
ARCH./BLDR.: Unknown  
STYLE: Half Modern  
RESOURCE TYPE: Building  
ZONE: R-10  
QUAD: Beaverton  
THEME: Education. Architecture -  
20th Century

PLAN TYPE/SHAPE: Asymmetrical NO OF STORIES: 1  
FOUNDATION MATERIAL: Concrete BASEMENT (Y/N): Yes  
ROOF FORM & MATERIALS: Hipped and clipped gable. Composition shingles.  
WALL CONSTRUCTION: Masonry STRUCTURAL FRAME: Unknown  
PRIMARY WINDOW TYPE: 8/8 double hung sash with 4 light transoms

EXTERIOR SURFACING MATERIALS: Brick  
DECORATIVE FEATURES: Bull's eye windows. Decorative brickwork.

OTHER: \_\_\_\_\_

CONDITION: Good  
EXTERIOR ALTERATION/ADDITIONS (DATED): Numerous additions, n.d.

NOTEWORTHY LANDSCAPE FEATURES: Well landscaped plantings and well tended lawn.

ASSOCIATED STRUCTURES: 1 1/2 story gymnasium attachment.

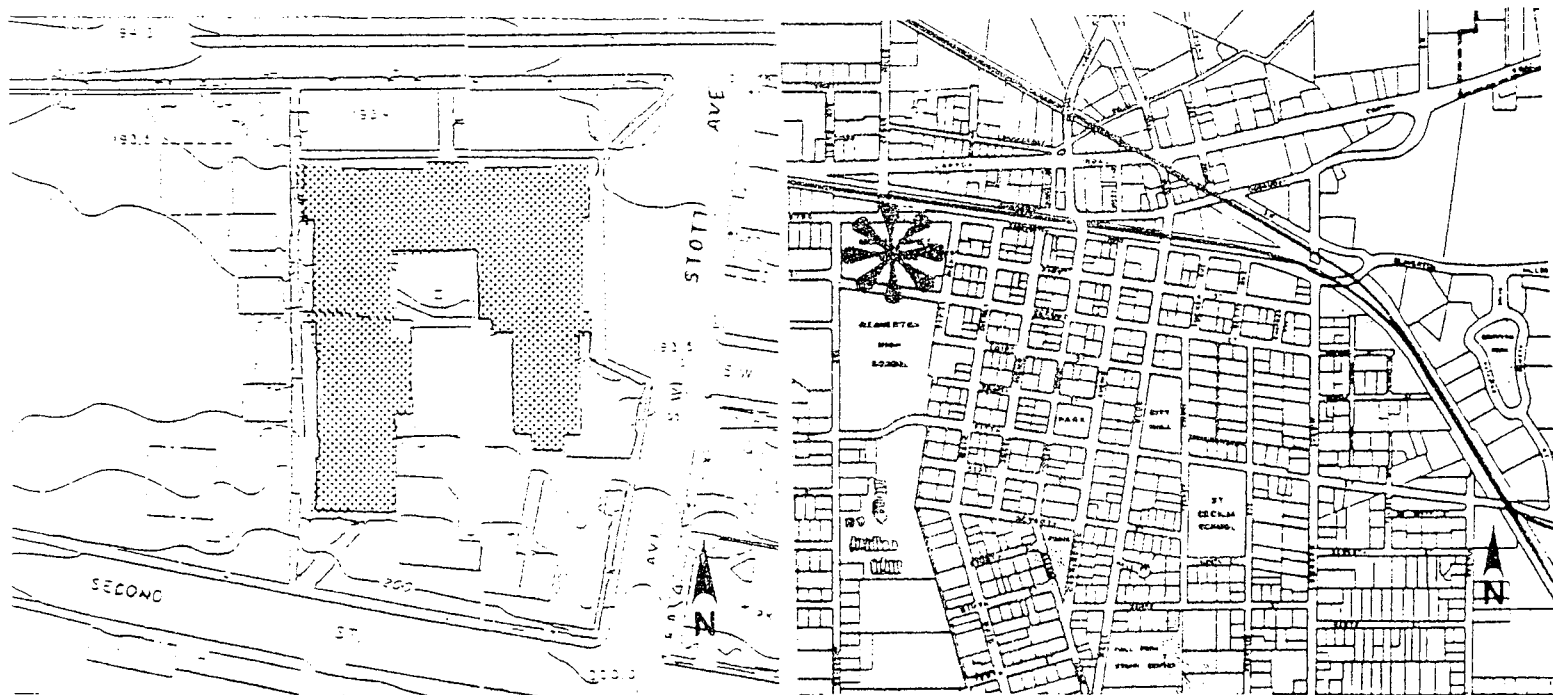
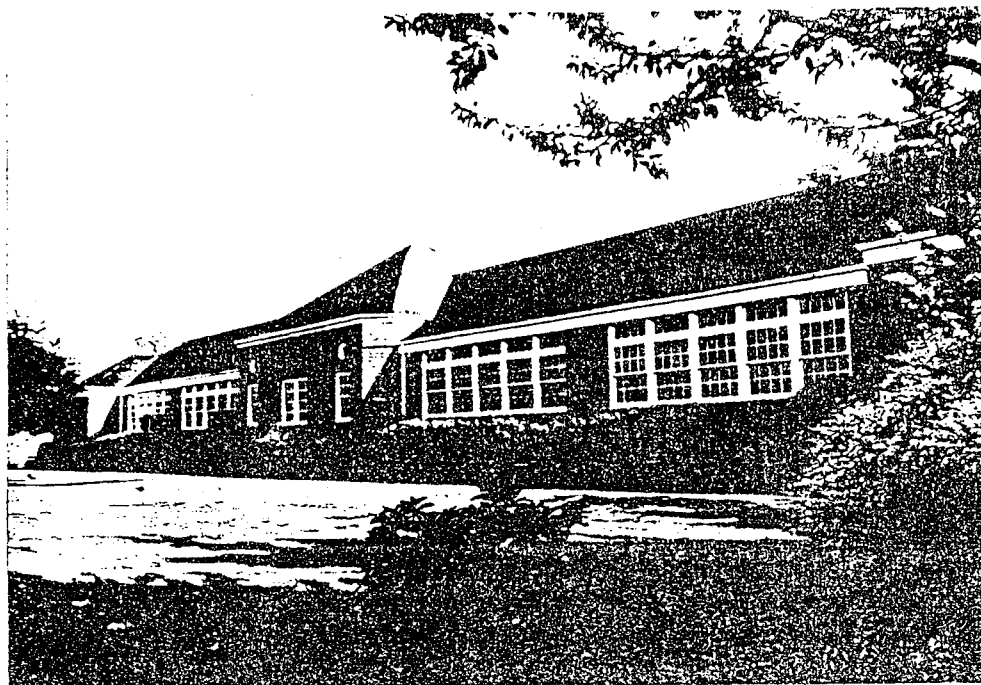
KNOWN ARCHEOLOGICAL FEATURES: None

SETTING: Merle Davies School is situated on the south side of Farmington Road. To the south is Beaverton High School and a commercial area lies to the west. The school complex envelopes several downtown Beaverton blocks.

STATEMENT OF SIGNIFICANCE (Historical and/or architectural importance, dates, events, persons, contexts). Constructed in 1937, this school was named after Merle Davies, who lived in Beaverton her entire life. Merle's family came from Indiana to Oregon in 1853. The family first settled in the Scholls area of Washington County. Merle was born in 1891 on a 240 acre farm at the corner of Hall and Allen Blvds. She was 6 years old when her family moved into a house on Farmington Road where she lived for many years. Her father and uncle owned a sawmill south of Beaverton and were also farmers.

BEAVERTON INVENTORY OF HISTORIC RESOURCES

ADDRESS: 13000 SW Farmington Road MAP NO.: 1S1 W 16ABTAX LOT: 11100



BEAVERTON TREE INVENTORY

ADDRESS: 5707 C 2nd  
\_\_\_\_\_  
\_\_\_\_\_

T/6-19

CROSS STREETS: SW 5707 C 2nd NE Corner  
\_\_\_\_\_

LOT SIZE: \_\_\_\_\_

Number of trees over 3" DBH  
(DBH: Diameter at breast height)  
Formula for DBH

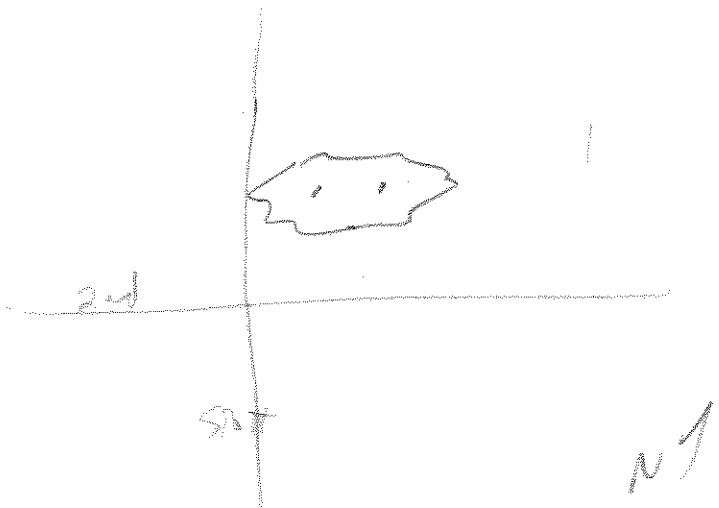
Diameter = Circumference divided by pi (3.1416)

Species:

- 1. 2 OAKS 1
- 2.
- 3.
- 4.
- 5.
- 6.
- 7.
- 8.
- 9.
- 10.
- 11.
- 12.
- 13.
- 14.

Groves - number of trees 2 Average DBH 1-25" 1-36"  
Number of Street Trees  
Define street trees

Draw lot showing corresponding trees from above list:



BEAVERTON TREE INVENTORY  
TREE RATING SHEET

RATING SCALE:

- 1. - No significant value
- 2. - Little significant value
- 3. - Some significant value
- 4. - Has significant value
- 5. - Has a very significant value

CONDITION ASSESSMENT:

- 1. - Poor condition  
Unhealthy with parts dead or dying
- 2. - Fair condition - OK health- but needing care
- 3. - Good condition - Healthy - (May still need care)

LOCATION N 2nd West St

<u>TREE ANALYSIS</u>		<u>RATING</u>	<u>RATING</u>	<u>RATING</u>
1) Tree has a distinctive	Size	_____	_____	_____
Tree Species <sup>2</sup> <u>O. oak</u>	Shape	_____	_____	_____
<u>1 Smaller Both form</u>	Location	<u>3</u>	_____	_____
<u>1 canopy</u>				
Comments: _____				
_____				
2) Tree has exceptional beauty		<u>3</u>	_____	_____
Comments: <u>Nice canopy</u>				
_____				
_____				
3) Tree has a functional or aesthetic relationship to a natural resource		<u>1</u>	_____	_____
Comments: _____				
_____				
4) Tree's Condition	Poor	_____	_____	_____
	Fair	<u>2/3</u>	_____	_____
	Good	_____	_____	_____
Comments: <u>Some negative impact by construction</u>				
_____				
_____				
TOTAL		<u>9/10</u>	_____	_____

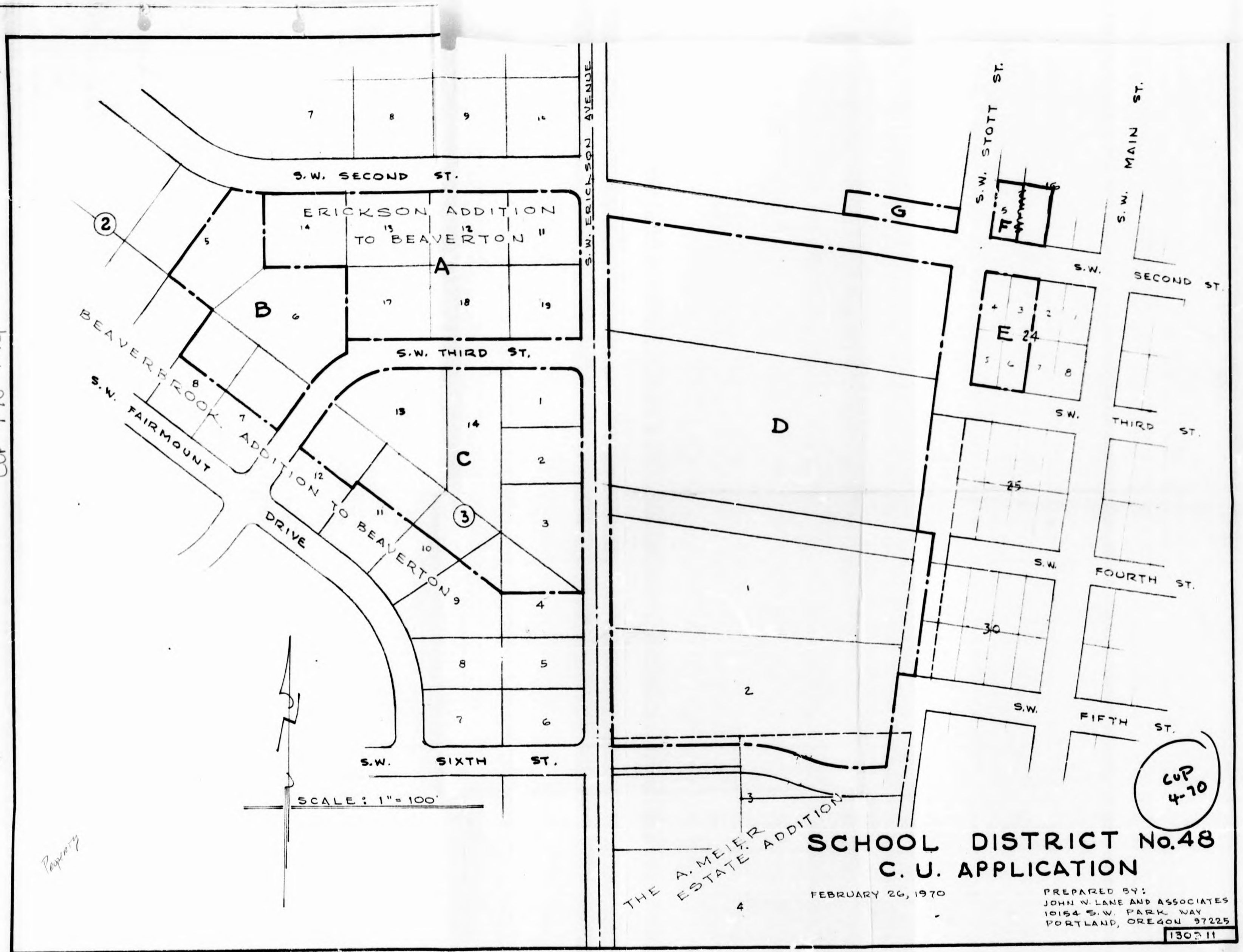


CUP 04-70

Expand Beaverton High School

1 OF 1

CUP 4-70 map



Property

SCALE: 1" = 100'

THE A. MEIER ESTATE ADDITION

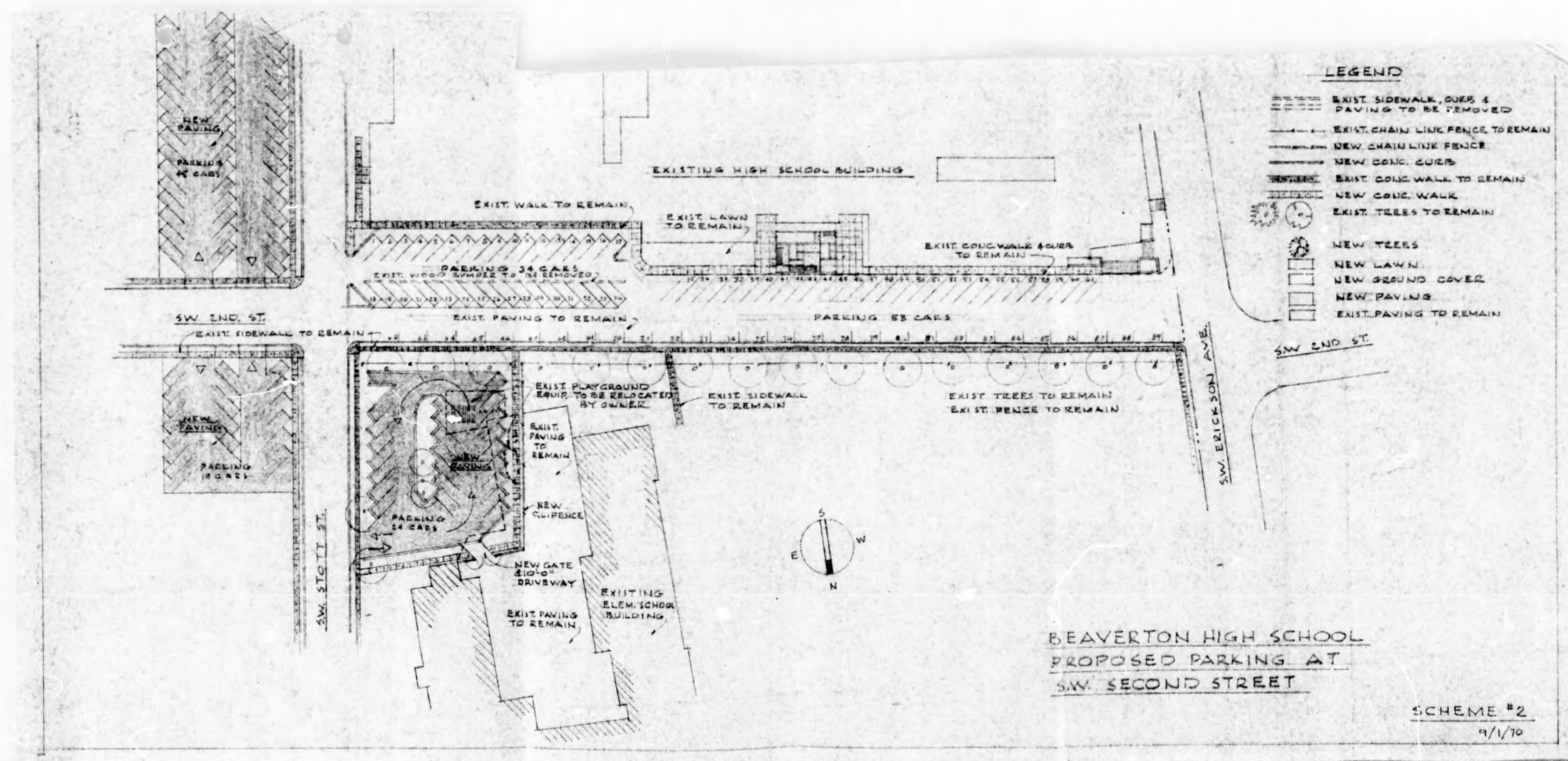
**SCHOOL DISTRICT No. 48  
C. U. APPLICATION**

FEBRUARY 26, 1970

PREPARED BY:  
JOHN W. LANE AND ASSOCIATES  
10154 S.W. PARK WAY  
PORTLAND, OREGON 97225

1305 11

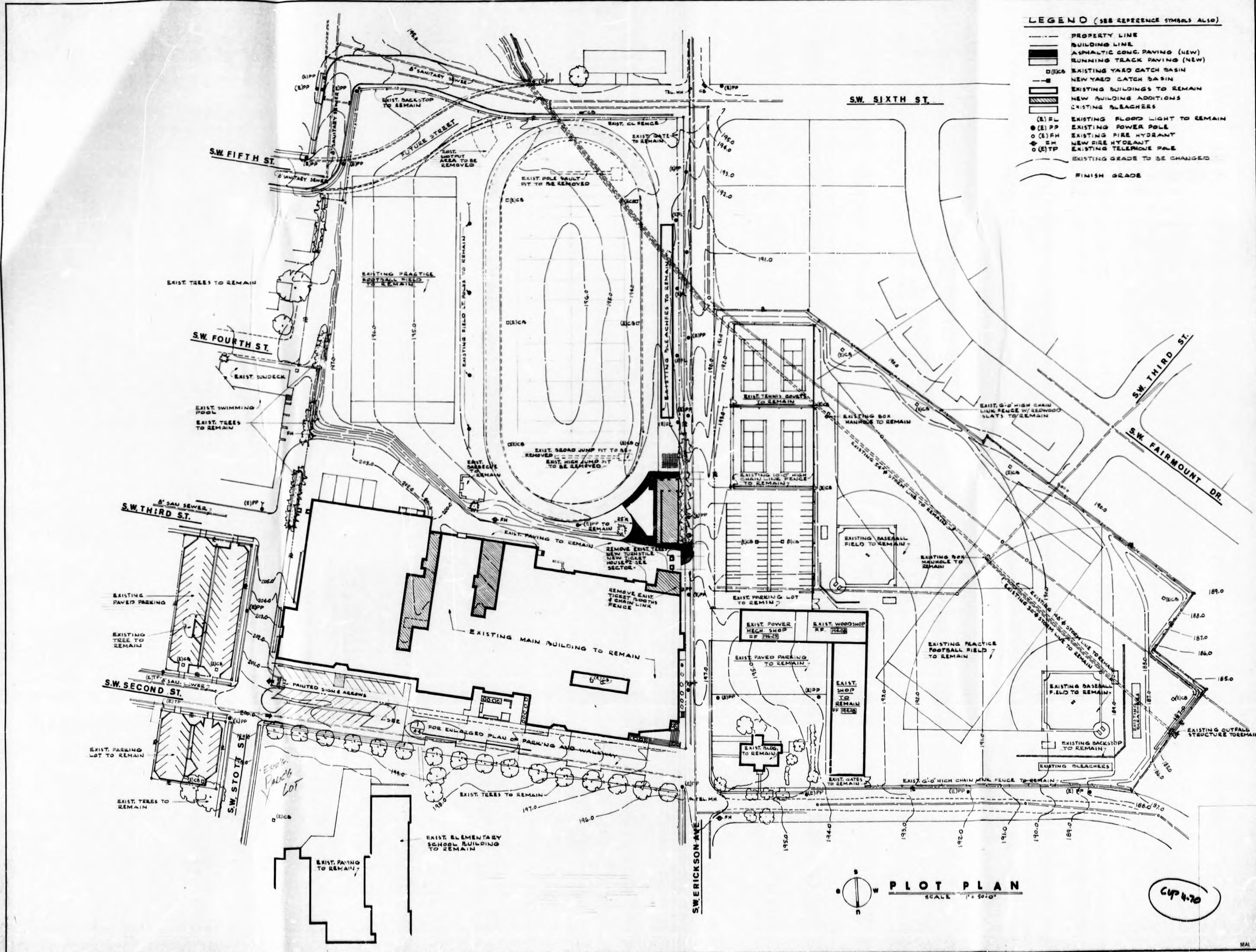
CUP  
4-70







Date	Initial	Notes
7/27/71	R.C.	8
7/27/71	R.C.	8
7/27/71	R.C.	2
7/27/71	R.C.	2



HEWLETT, JAMISON & ATKINSON - AIA - ARCHITECTS  
 PORTLAND, OREGON 97201  
 DRAWING NO. 474-70  
 SHEET NO. 2  
 DATE 7/27/71  
 SCALE 1" = 50'-0"